

ACON 2022

June 10, 2022

**TO THE MAYOR AND MEMBERS OF COUNCIL:**

A special meeting of Council will be held on **Monday, June 13, 2022, at 3:30 o'clock p.m., via hybrid in Room 139, 350 City Hall Square. All members will have the option of participating in person in Room 139 or electronically and will be counted towards quorum in accordance with Procedure By-law 98-2011 as amended. The minutes will reflect this accordingly.** Council will at the special meeting adopt a resolution to authorize Council to meet in closed session, and the resolution shall contain the general nature of the matters to be considered in the closed session. The resolution must be adopted by a majority of Council present during the open special meeting before the meeting may be closed.

The **regular meeting** of Council will be held on **Monday, June 13, 2022 at 4:00 o'clock p.m., via hybrid in the Council Chambers, 350 City Hall Square.** All members will have the option of participating in person in the Council Chambers or electronically and will be counted towards quorum in accordance with Procedure By-law 98-2011 as amended. The minutes will reflect this accordingly. **Delegations will be participating electronically.**

BY ORDER OF THE MAYOR.

Yours very truly,



Steve Vlachodimos  
City Clerk

/bm

c.c. Chief Administrative Officer

**Consolidated City Council Meeting Agenda**

**Date:** June 13, 2022  
**Time:** 4:00 o'clock p.m.

**Location:** Council Chambers, 1<sup>st</sup> Floor, Windsor City Hall

All members will have the option of participating in-person in Council Chambers or electronically and will be counted towards quorum in accordance with Procedure By-law 98-2011 as amended, which allows for electronic meetings. The minutes will reflect this accordingly. Any delegations will be participating electronically.

**MEMBERS:**

Mayor Drew Dilkens

Ward 1 – Councillor Fred Francis

Ward 2 – Councillor Fabio Costante

Ward 3 – Councillor Rino Bortolin

Ward 4 – Councillor Chris Holt

Ward 5 – Councillor Ed Sleiman

Ward 6 – Councillor Jo-Anne Gignac

Ward 7 – Councillor Jeewen Gill

Ward 8 – Councillor Gary Kaschak

Ward 9 – Councillor Kieran McKenzie

Ward 10 - Councillor Jim Morrison



## ORDER OF BUSINESS

Item #	Item Description
1.	<b>ORDER OF BUSINESS</b>

- |      |   |
|------|---|
| 1.1. | In the event of the absence of the Mayor, Councillor Bortolin has been Appointed Acting Mayor for the month of June, 2022 in accordance with By-law 176-2018, as amended. |
|------|---|

2.	<b>CALL TO ORDER</b> - Playing of the National Anthem
----	---

**READING OF LAND ACKNOWLEDGEMENT**

We [I] would like to begin by acknowledging that the land on which we gather is the traditional territory of the Three Fires Confederacy of First Nations, which includes the Ojibwa, the Odawa, and the Potawatomie. The City of Windsor honours all First Nations, Inuit and Métis peoples and their valuable past and present contributions to this land.

3.	<b>DISCLOSURE OF PECUNIARY INTEREST AND THE GENERAL NATURE THEREOF</b>
----	--

4.	<b>ADOPTION OF THE MINUTES</b>
----	--------------------------------

- |      |  |
|------|--|
| 4.1. | Minutes of the May 30, 2022 Regular City Council Meeting ( <b>SCM 167/2022</b> ) ( <i>enclosed</i> ) |
|------|--|

5.	<b>NOTICE OF PROCLAMATIONS</b>
----	--------------------------------

**Proclamations**

"Recreation & Parks Month" – June 2022

"Garden Day" – Saturday, June 18, 2022

"Longest Day of SMILES®" – Sunday, June 19, 2022

"World Sickle Cell Day" – Sunday, June 19, 2022

"Jennifer Jones Day" – Monday, June 27, 2022

"Father Paul Charbonneau Day" – Sunday, July 3, 2022

"Parachute National Injury Prevention Day" – Tuesday, July 5, 2022

**Illumination**

"World Blood Donor Day" – Sunday, June 12 to Saturday, June 18, 2022

"World Sickle Cell Day" – Sunday, June 19, 2022

"Parachute National Injury Prevention Day" – Tuesday, July 5, 2022

6.	<b>COMMITTEE OF THE WHOLE</b>
----	-------------------------------

7. **COMMUNICATIONS INFORMATION PACKAGE** (This includes both Correspondence and Communication Reports)

7.1. Correspondence 7.1.1. through 7.1.9. (**CMC 9/2022**) (*enclosed*)

8. **CONSENT AGENDA**

8.2. Capital Variance Report - March 31, 2022 - City Wide (**C 91/2022**)

8.3. Little River Pollution Control Plant -Sludge Pump House No 01 Upgrades- City Wide (**C 99/2022**)

**CONSENT COMMITTEE REPORTS**

8.5. Official Plan and Rezoning Amendments – Tunio Development – 3885 & 0 Sandwich Street – OPA 152 OPA [6504] Z-028-21 ZNG [6503] – Ward 2 (**SCM 156/2022**) (**S 65/2022**) (*attached*)

8.6. Draft Plan of Condominium with Exemption under Section 9(3) of the Condominium Act – St. Clair Rhodes Development – 233 Watson Avenue – Ward 6 (**SCM 152/2022**) (**S 55/2022**) (*attached*)

8.8. Request for Heritage Permit – 3036 Sandwich Street, McKee Park (Ward 2) (**SCM 153/2022**) (**S 61/2022**) (*attached*)

8.9. Request for Partial Demolition of a Heritage Listed Property – 2038 Willistead Crescent, C.E. Platt House (Ward 4) (**SCM 154/2022**) (**S 62/2022**) (*attached*)

8.10. Bill 109, More Homes for Everyone Act, 2022 – Changes to the Planning Act Affecting Site Plan Control Approval, City Wide (**SCM 155/2022**) (**S 57/2022**) (*attached*)

9. **REQUEST FOR DEFERRALS, REFERRALS AND/OR WITHDRAWALS**

10. **PRESENTATIONS AND DELEGATIONS**

**PRESENTATIONS:** 10-minute maximum

10.1. The Essex and Kent Scottish (**SCM 150/2022**)  
a) LCol (Ret'd) Michael Robinson; Mr. David Woodall; and Chief of Police (Ret'd) Glenn Stannard

**DELEGATIONS:** (5-minute maximum)

- 11.2. Ward 9 Ditch Survey and Inspection of Private Culverts and Private Catch Basins - Ward 9 (**SCM 120/2022**) (**S 14/2022**)  
**Clerk's Note:** Jayme Lauzon, area resident submitting the **attached** chart as a written submission.  
a) Jayme Lauzon, area resident
- 8.7. Approval of a Draft Plan of Subdivision for lands located on the south side of North Talbot Road, between Southwood Lakes Blvd. and HWY 401; File No. SDN-001/21 [SDN/6575]; Applicant – Bellocorp Inc.; Ward 1 (**SCM 157/2022**) (**S 59/2022**) (**attached**)  
a) Tosin Bello, applicant; and Chintan Virani, architect
- 8.4. Multi-Residential Interim Control By-law Study – Proposed Official Plan Amendment and Zoning By-law Amendment (**SCM 149/2022**) (**S 64/2022**) (**attached**)  
a) Raymond Hoang, resident of Ward 7  
b) Jim Dymont, Municipal Planning Consultants; Ron Palmer, The Planning Partnership and Daryl Keleher and Peter Norman, Altus Group Consulting
- 8.1. Request for Provincial Assistance with Zoning for Automotive Battery Manufacturing Facility located at Banwell Road and EC ROW Avenue East (Ward 9) (**C 86/2022**)  
**Clerk's Note:** CP Proximity Ontario submitting the **attached** email dated June 6, 2022 as a written submission. ERCA submitting the **attached** letter dated June 9, 2022 as a written submission. Walpole Island First Nation submitting the **attached** email dated June 9, 2022 as a written submission. Caldwell First Nation and Chippewas of Kettle and Stony Point First Nation submitting the **attached** letter dated June 10, 2022 as a written submission.  
a) Chippewas of Kettle and Stony Point representatives: Chief Jason Henry, Vince George, Lincoln Jackson, Jane Manning, Marshall George, Claire Sault and Jodi George  
b) Southwind representatives: Philip Lee, Harvey Manning, Sharman Bressette, Gale George, Tom Milliken, Todd Jardine, Alaina Monkhouse and Alex Li  
c) Association of Iroquois and Allied Indians: Grand Chief Joel Abrams  
d) Caldwell First Nation representatives: Chief Mary Duckworth, Councillor Ian Duckworth, Councillor Doug Heil, Councillor Larry Johnson, Councillor Nikki vanOrichot and Zack Hamm, Consultation Coordinator CFN  
e) Intergovernmental Affairs, Caldwell First Nation: Larry Sault  
f) Olthuis, Kleer, Townshend LLP representative: Oliver MacLaren  
g) Chippewas of the Thames First Nation: Chief Jackie French, and Jennifer Mills

11. **REGULAR BUSINESS ITEMS** (Non-Consent Items)

- 11.1. Charles Clark Square Repairs Versus Replacement at City Hall (Esplanade Project) - Ward 3 (**C 98/2022**)

12. **CONSIDERATION OF COMMITTEE REPORTS**

- 12.1. (i) Report of the Special In-Camera meeting or other Committee as may be held prior to Council (if scheduled)

13. **BY-LAWS** (First and Second Reading) (*attached*)

- 13.1. **By-law 86-2022** A BY-LAW TO FURTHER AMEND BY-LAW NUMBER 8600 CITED AS THE "CITY OF WINDSOR ZONING BY-LAW" authorized by CR444/2019 dated August 26, 2019
- 13.2. **By-law 87-2022** A BY-LAW TO FURTHER AMEND BY-LAW NUMBER 8600 CITED AS THE "CITY OF WINDSOR ZONING BY-LAW" authorized by CR173/2022 dated April 25, 2022 and CR216/2022 dated May 9, 2022
- 13.3. **By-law 88-2022** A BY-LAW TO FURTHER AMEND BY-LAW NUMBER 8600 CITED AS THE "CITY OF WINDSOR ZONING BY-LAW" authorized by By-law 221-1998 dated July 13, 1998 and By-law 199-2001 dated June 4, 2001
- 13.4. **By-law 89-2022** A BY-LAW TO FURTHER AMEND BY-LAW NUMBER 85-18 CITED AS THE "TOWNSHIP OF SANDWICH SOUTH COMPREHENSIVE ZONING BY-LAW" authorized CR254/2018 dated May 7, 2018 and By-law 59-2018 dated May 7, 2018
- 13.5. **By-law 90-2022** A BY-LAW TO FURTHER AMEND BY-LAW NUMBER 8600 CITED AS THE "CITY OF WINDSOR ZONING BY-LAW" authorized by CR175/2022 dated April 25, 2022
- 13.6. **By-law 91-2022** A BY-LAW TO FURTHER AMEND BY-LAW NUMBER 8600 CITED AS THE "CITY OF WINDSOR ZONING BY-LAW" authorized by CR487/2021 dated November 1, 2021
- 13.7. **By-law 92-2022** A BY-LAW TO ADOPT AMENDMENT NO. 143 TO THE OFFICIAL PLAN OF THE CITY OF WINDSOR authorized by CR115/2022 dated March 21, 2022
- 13.8. **By-law 93-2022** A BY-LAW TO FURTHER AMEND BY-LAW NUMBER 8600 CITED AS THE "CITY OF WINDSOR ZONING BY-LAW" authorized by CR115/2022 dated March 21, 2022
- 13.9. **By-law 94-2022** A BY-LAW TO FURTHER AMEND BY-LAW NUMBER 8600 CITED AS THE "CITY OF WINDSOR ZONING BY-LAW" authorized by CR239/2022 dated May 30, 2022
- 13.10. **By-law 95-2022** A BY-LAW TO PROVIDE THAT PART-LOT CONTROL SHALL NOT APPLY TO CERTAIN LAND THAT IS WITHIN REGISTERED PLAN 1196 IN THE CITY OF WINDSOR authorized by By-law 139-2013 dated August 26, 2013
- 13.11. **By-law 96-2022** A BY-LAW TO FURTHER AMEND BY-LAW NUMBER 8600 CITED AS THE "CITY OF WINDSOR ZONING BY-LAW" authorized by By-law 8600 s. 5.4.20

- 13.12. **By-law 97-2022** A BY-LAW TO CONFIRM PROCEEDINGS OF THE COUNCIL OF THE CORPORATION OF THE CITY OF WINDSOR AT ITS MEETING HELD ON THE 13th DAY OF JUNE, 2022

14. **MOVE BACK INTO FORMAL SESSION**

15. **NOTICES OF MOTION**

- 15.1. Moved by Councillor Gill, seconded by Councillor \_\_\_\_\_

**Whereas** there has been an increase in residential development in the east end of the City of Windsor, particularly in Ward 7, and;

**Whereas** the grade crossings on Banwell, north of Tecumseh Road East; on Clover, north of Tecumseh Road East; and on Lauzon Road, between Spitfire Way and Tecumseh Road East are located in heavily populated residential areas, and;

**Whereas** residents regularly call with complaints regarding trains blowing whistles late at night, affecting those who live near these train crossings and ask that train whistling be stopped at night unless absolutely necessary, and;

**Whereas** the process per Transport Canada to apply to stop train whistling begins with a Citizen or group expressing interest followed by the municipality consulting with the railway company;

**THEREFORE BE IT RESOLVED** that Council **DIRECT** Administration to follow the procedures of Transport Canada and engage with the railway company to initiate the process to request train whistling stop at the following public grade crossings: Banwell, north of Tecumseh Road East; on Clover, north of Tecumseh Road East; and on Lauzon Road, between Spitfire Way and Tecumseh Road East.

MT2022

16. **THIRD AND FINAL READING OF THE BY-LAWS**

By-laws 71-2022 and 86-2022 through 97-2022 (inclusive)

17. **PETITIONS**

18. **QUESTION PERIOD (*attached*)**

18.1. Summary of Outstanding Council Questions as at June 9, 2022 (**SCM 162/2022**)

18.2. Summary of Outstanding Council Directives as of June 13, 2022 (**SCM 161/2022**)

19. **STATEMENTS BY MEMBERS**

20. **UPCOMING MEETINGS**

Property Standards Committee  
Tuesday, June 14, 2022  
4:00 p.m., Zoom video conference

Housing & Homelessness Advisory Committee  
Tuesday, June 21, 2022  
10:00 a.m., Zoom video conference

ENWIN Utilities Ltd. Board  
Wednesday, June 22, 2022  
9:00 a.m.

Windsor Utilities Commission Board  
Wednesday, June 22, 2022  
11:00 a.m.

ENWIN Energy Ltd. Board / Windsor Canada Utilities Ltd. Board; Annual General  
Meetings for EWU, WEW and EFS  
Wednesday, June 22, 2022  
1:00 p.m.

Environment, Transportation & Public Safety Standing Committee  
Wednesday, June 22, 2022  
4:30 p.m., Zoom/Hybrid Platform

International Relations Committee  
Thursday, June 23, 2022  
3:30 p.m., Zoom video conference

Regular City Council Meeting - **CANCELLED**  
Monday, June 27, 2022  
4:00 p.m.

Vision Zero Stakeholder Group  
Wednesday, June 29, 2022  
10:00 a.m., Zoom video conference

Committee of Management for Huron Lodge  
Thursday, June 30, 2022  
9:00 a.m., Zoom video conference

Development and Heritage Standing Committee  
Monday, July 4, 2022  
4:00 p.m., Zoom/Hybrid Platform

Community Services Standing Committee  
Wednesday, July 6, 2022  
9:00 a.m., Zoom/Hybrid Platform

21. **ADJOURNMENT**



**Committee Matters: SCM 167/2022**

**Subject: Adoption of the Windsor City Council meeting minutes held May 30, 2022**





## CITY OF WINDSOR MINUTES 05/30/2022

### City Council Meeting

Date: Monday, May 30, 2022

Time: 4:00 o'clock p.m.

#### **Members Present:**

##### **Mayor**

Mayor Dilkens

##### **Councillors**

Ward 1 - Councillor Francis

Ward 2 - Councillor Costante

Ward 3 - Councillor Bortolin

Ward 4 - Councillor Holt

Ward 5 - Councillor Sleiman

Ward 6 - Councillor Gignac

Ward 7 - Councillor Gill

Ward 8 - Councillor Kaschak

Ward 9 - Councillor McKenzie

Ward 10 - Councillor Morrison

**Clerk's Note:** Several members of Administration and members of the public participated via video conference (Zoom), in accordance with Procedure By-law 98-2011 as amended, which allows for electronic participation.

# Minutes

## City Council

Monday, May 30, 2022

Page 2 of 40

### 1. ORDER OF BUSINESS

### 2. CALL TO ORDER

Following the playing of the Canadian National Anthem and reading of the Land Acknowledgement, the Mayor calls the meeting to order at 4:00 o'clock p.m.

### 3. DISCLOSURE OF PECUNIARY INTEREST AND THE GENERAL NATURE THEREOF

None disclosed.

### 4. ADOPTION OF THE MINUTES

#### 4.1. Adoption of the Windsor City Council meeting minutes held May 9, 2022

Moved by: Councillor Sleiman

Seconded by: Councillor Bortolin

That the minutes of the meeting of Council held May 9, 2022 **BE ADOPTED** as amended to indicate Councillor Bortolin voting nay on Item 11.1 "2021 Year-End Operating Budget Variance Report – City Wide."

Carried.

Report Number: SCM 147/2022

### 5. NOTICE OF PROCLAMATIONS

#### Proclamations

"VON Week" – Sunday, May 22 to Saturday, May 28, 2022

"Bike Month" – June 2022

"Brain Injury Awareness Month" – June 2022

"Dads Matter Month" – June 2022

"Windsor Seniors' Month" – June 2022

#### Flag Raising Ceremony

"Windsor Seniors' Month" – Wednesday, June 1 to Wednesday, June 8, 2022

#### Illumination

"VON Week" – Sunday, May 22 to Saturday, May 28, 2022

"Windsor Seniors' Month" – Wednesday, June 1 to Wednesday, June 8, 2022

# Minutes

## City Council

Monday, May 30, 2022

Page 3 of 40

### 6. COMMITTEE OF THE WHOLE

Moved by: Councillor Costante

Seconded by: Councillor Francis

That Council do now rise and move into Committee of the Whole with the Mayor presiding for the purpose of dealing with:

- (a) communication items;
  - (b) consent agenda;
  - (c) hearing requests for deferrals, referrals and/or withdrawals of any items of business;
  - (d) hearing presentations and delegations;
  - (e) consideration of business items;
  - (f) consideration of Committee reports;
  - (g) Report of Special In-Camera Meeting or other Committee as may be held prior to Council (if scheduled); and
  - (h) consideration of by-laws 77-2022 through 85-2022 (inclusive).
- Carried.

### 7. COMMUNICATIONS INFORMATION PACKAGE

#### 7.1. Correspondence for May 30, 2022

Moved by: Councillor Gignac

Seconded by: Councillor Gill

Decision Number: CR224/2022

That the following Communication Items 7.1.1 through 7.1.6 (inclusive) as set forth in the Council Agenda **BE REFERRED** as noted:

No.	Sender	Subject
7.1.1.	Ontario Office of the Solicitor General	Letter regarding the completion of the community safety and well-being (CSWB) plan for the City of Windsor and the County of Essex  Project Manager, CDHS Commissioner, Human & Health Services Executive Director, Housing & Children's Services Chief of Police Deputy Chief of Operational Support GP2022 Note & File

**Minutes**  
**City Council**  
**Monday, May 30, 2022**

Page 4 of 40

No.	Sender	Subject
7.1.2.	Ontario Land Tribunal	<p>Ahmed v. Windsor (City) case heard by video hearing. Decision delivered by T.F. Ng and S. Mann on April 20 and 21, 2022, Case No. OLT-21-001184</p> <p>Commissioner, Legal &amp; Legislative Services  City Planner  Chief Building Official  Deputy City Solicitor  Senior Legal Counsel  Development Applications Clerk  ZB/13839  Note &amp; File</p>
7.1.3.	Landmark Engineers Inc.	<p>Sandpoint Beach Park Master Plan and Environmental Assessment Notice of Intent &amp; Invitation to Comment</p> <p>Commissioner, Community Services  Commissioner, Infrastructure Services  Commissioner, Legal &amp; Legislative Services  City Planner  Deputy City Solicitor  SR/14130  Note &amp; File</p>
7.1.4.	<p>Patrick Winters,  Development Project Manager, City of Windsor</p> <p>AND</p> <p>Andrea Winter,  Consultant, Dillon Consulting Limited</p>	<p>Notice of Study Area Expansion and Public Consultation regarding the Stormwater Management Lauzon Parkway/CR42 Improvements</p> <p>Commissioner, Infrastructure Services  Executive Director, Engineering/  Deputy City Engineer  Executive Director, Pollution Control/  Deputy City Engineer  Development Project Manager  EI2022  Note &amp; File</p>
7.1.5.	City Planner/ Executive Director	<p>Application for Draft Plan of Subdivision/ Condominium, St Clair Rhodes Development Corp, 233 Watson avenue, Application to approve a Plan of Condominium for a 24-unit residential development consisting of multiple dwellings</p> <p>Z/14381  Note &amp; File</p>

**Minutes**  
**City Council**  
**Monday, May 30, 2022**

Page 5 of 40

No.	Sender	Subject
7.1.6.	Manager of Urban Design	<p>Recent Site Plan Control (SPC) applications:</p> <ul style="list-style-type: none"> <li>• Southwestern Sales Corporation, 10150 Riverside E., removal and Replacement of Storage Building</li> <li>• 1486187 Ont. Ltd (Rosati), 3030 Deziel, construct new building</li> <li>• Our Daily Bread Ministries, 11325 Firgrove, addition to existing Office Building</li> <li>• Francesco Vella, 1246,1256-1270 Erie E, parking lot &amp; bocce courts</li> <li>• 12843951 CDA Inc (Levon Khajadur), 3461 Walker, auto repair garage addition</li> </ul> <p style="text-align: right;">Z2022 Note &amp; File</p>

Carried.

Report Number: CMC 8/2022

**7.2. Pay As You Go (PAYG) Transfer for Eligible 2021 Computer Equipment Related Expenses - Windsor Police Service (WPS) - City Wide**

Moved by: Councillor Gignac  
 Seconded by: Councillor Gill

Decision Number: CR225/2022

That in compliance with CR139/2015, City Council **RECEIVE** for information this report confirming that an amount of \$640,958.63 was transferred from the Pay As You Go (PAYG) Leasing Reserve Fund 170 to the Windsor Police Service operating account Dept. ID 0192616, Class Code 18084, for eligible 2021 computer, printer, phone system upgrades, server and network related expenses.  
 Carried.

Report Number: CM 6/2022  
 Clerk's File: SP2022

**7.3. Status Report on Implementation of Management Action Plans - 2021 Q4 - City Wide**

Moved by: Councillor Gignac  
 Seconded by: Councillor Gill

Decision Number: CR226/2022

That the report of the Executive Initiatives Coordinator dated May 5, 2022 entitled "Status Report on Implementation of Management Action Plans - 2021 Q4 - City Wide" **BE RECEIVED** for information.  
 Carried.

#### **7.4. Disconnecting from Work - City Wide**

Moved by: Councillor Gignac  
Seconded by: Councillor Gill

Decision Number: CR227/2022

That the report of the Executive Director of Human Resources dated May 12, 2022 entitled "Disconnecting from Work - City Wide" **BE RECEIVED** for information.  
Carried.

Report Number: C 89/2022  
Clerk's File: AS2022

#### **7.5. CQ 7-2022 - Response to CQ-7-2022 - Expansion of Dog Parks within the City - City Wide**

Moved by: Councillor Gill  
Seconded by: Councillor Francis

Decision Number: CR228/2022

That the report of the Executive Initiatives Coordinator dated May 4, 2022 entitled "CQ 7-2022 - Response to CQ 7-2022 - Expansion of Dog Parks within the City - City Wide" **BE RECEIVED** for information; and,

That Administration **BE DIRECTED** to begin the process of adding two additional dog parks at Oakwood Park and Elizabeth Kishkon Park after the community has **BEEN CONSULTED** as outlined in the Dog Park Policy; and,

That these additional dog parks **BE FUNDED** as follows: reallocation of \$750,000 in 2022 PAYG funds and the reallocation and pre-commitment of \$130,000 in 2023 PAYG funds from capital project PFO-009-12 – Parks Bridges/Shelters/Buildings/Capital Rehabilitation Program to capital project PFO-010-17 – Dog Park Development, to be replaced with the transfer of \$880,000 from Fund 151 – Parkland Acquisition Reserve back to capital project PFO-009-12; and further,

That Administration **REPORT BACK** to Council with potential edits to the Dog Park Policy to allow smaller, urban parks to be used as dog parks as well.  
Carried.

Report Number: C 82/2022  
Clerk's File: SR2022

# Minutes

## City Council

Monday, May 30, 2022

Page 7 of 40

### 8. CONSENT AGENDA

#### 8.1. Animal Control Contract - City Wide

Moved by: Councillor Holt

Seconded by: Councillor Kaschak

Decision Number: CR229/2022

That City Council **APPROVE** the proposal to provide contractual animal control services from the Windsor-Essex Humane Society (WECHS) in the annual amount of \$1,081,601 (exclusive of any applicable tax) with an annual inflation adjustment of 2% for a five year period commencing July 1, 2022 as detailed in the Financial Assessment section of this report; and,

That the contract **INCLUDE** a termination clause upon the provision of six (6) months written notice; and,

That the City Solicitor **BE AUTHORIZED** to prepare a contract for those services; and further,

That the Chief Administrative Officer and City Clerk **BE AUTHORIZED** to sign the contract, satisfactory in form to the City Solicitor, in financial content to the City Treasurer, and in technical content to the Acting Licence Commissioner.

Carried.

Report Number: C 94/2022

Clerk's File: MHS/9496

#### 8.2. Cancellation of June 27, 2022 City Council Meeting due to 2022 Ford Fireworks

Moved by: Councillor Holt

Seconded by: Councillor Kaschak

Decision Number: CR230/2022

That the regular meeting of Council scheduled for Monday, June 27, 2022 **BE CANCELLED** as the 2022 Ford Fireworks are scheduled for that date.

Carried.

Report Number: C 78/2022

Clerk's File: ACO2022

#### 8.3. Your Quick Gateway (Windsor) Inc. - Annual General Meeting of Shareholder - City Wide

Moved by: Councillor Holt

Seconded by: Councillor Kaschak

# Minutes

## City Council

### Monday, May 30, 2022

Page 8 of 40

Decision Number: CR231/2022

- I. That Council, acting as the shareholder for Your Quick Gateway (Windsor) Inc. ("YQG"), **APPROVE** the resolutions presented by YQG; and further,
- II. That the Chief Administrative Officer (CAO) and the City Clerk and Senior Manager of Council Services **BE AUTHORIZED** to execute the shareholder's resolutions on behalf of The Corporation of the City of Windsor, in accordance with Council direction, with respect to all matters presented herein by YQG for the year 2021.

Carried.

Report Number: C 93/2022

Clerk's File: AL/14399

#### 8.4. Award of Tender: 6-22 - Banwell Road, Phase 2 from Palmetto Street to Mulberry Drive / Wildwood Drive Roundabout - Ward 7

Moved by: Councillor Holt

Seconded by: Councillor Kaschak

Decision Number: CR232/2022

- I. That the following low tender **BE ACCEPTED**:  
**TENDERER:** SheaRock Construction Group Inc.  
**TENDER NO:** 6-22  
**TOTAL TENDER PRICE:** \$5,455,346.00 (excluding HST)  
**ACCOUNT CHARGED:** 007-5410-7P17-02942-7171077

and,

That the CAO and City Clerk **EXECUTE** an agreement with the low bidder, SheaRock Construction Group Inc., in the amount of \$5,455,346.00 plus tax, with said contract being satisfactory in form to the Commissioner of Legal & Legislative Services, in technical content to the Commissioner of Infrastructure Services, and in financial content to the Commissioner of Corporate Services CFO/City Treasurer.

- II. That \$375,000 in 2025 F169 - Pay As You Go funding, \$125,000 in 2025 F221 Service Sustainability funding, \$70,640 in 2026 F169 - Pay As You Go funding and \$131,192 in 2026 F221 Service Sustainability funding, previously approved in principle as part of the 2022 Capital Budget in the Tecumseh Rd E. Infrastructure Improvements project (ECP-005-07) **BE TRANSFERRED** to the Banwell Rd. Infrastructure Improvements project (ECP-002-10) and **BE APPROVED** as a pre-commitment in project 7171077 – Banwell Road Improvement and available for immediate use.
- III. That \$445,640 in 2028 F169 - Pay As You Go funding and \$256,192 in 2028 F221 Service Sustainability funding, approved in principle as part of the 2022 Capital Budget in the Banwell Rd. Infrastructure Improvements project (ECP-002-10) **BE TRANSFERRED** to the Tecumseh Rd E. Infrastructure Improvements (ECP-005-07) to replenish the project.

Carried.



## **8.5. Request for Proposal (RFP) 55-22 - Cabana/Division Corridor Improvements Phase 5 Consulting Services - Ward 9**

Moved by: Councillor Holt  
Seconded by: Councillor Kaschak

Decision Number: CR233/2022

- I. That the firm of Dillon Consulting Limited **BE RETAINED** for the engineering consulting services for Cabana / Division Corridor Improvements Phase 5 – East of Provincial Road to West of Walker Road, at a maximum fee of \$991,000.00 (plus HST), in accordance with their proposal for consulting services, dated April 13, 2022 as a charge to Project ID 7152001.
- II. That the Chief Administrative Officer and City Clerk **BE AUTHORIZED** to sign an agreement with Dillon Consulting Limited, satisfactory in form to the Commissioner of Legal & Legislative Services, in financial content to the Commissioner of Corporate Services CFO/City Treasurer, and in technical content to the Commissioner of Infrastructure Services.

Carried.

Report Number: C 81/2022  
Clerk's File: SW/14397

## **8.6. Tender 59-22 – 2022 Construction of Concrete Sidewalks, Curb and Gutter ---City Wide**

Moved by: Councillor Holt  
Seconded by: Councillor Kaschak

Decision Number: CR234/2022

- I. That the following low tender **BE ACCEPTED**:  
**TENDERER:** Piera Con Enterprises Inc.  
**TENDER NO:** 59-22  
**TOTAL TENDER PRICE:** \$1,998,695.25 (excluding HST)  
**ACCOUNTS CHARGED:** Project 7221011 – 2022 Sidewalk Rehab  
Project 7204003 – ICIP Citywide Sidewalks

and,

That the CAO and City Clerk **EXECUTE** the contract with the low tenderer, with said contract being satisfactory in form to the City Solicitor, satisfactory in technical content to the City Engineer, and in financial content to the City Treasurer.

- II. That City Council **APPROVE** a pre-commitment of \$305,000 in Service Sustainability funding from the 2023 Sidewalk Rehabilitation Project (OPS-004-07) for immediate use to complete the construction of Sidewalk, Curb and Gutter project identified in this report.

Carried.

Report Number: C 90/2022

Clerk's File: SW/14398

## **8.7. City of Windsor Heritage Recognition 2022**

Moved by: Councillor Holt

Seconded by: Councillor Kaschak

Decision Number: CR235/2022 DHSC 390

- I. That Council **BE INFORMED** of the joint partnership between the City of Windsor Heritage Recognition 2022 with Doors Open Windsor 2022 and that Administration **ASSIST** in cross promotional efforts.
- II. That the property owners and project team for the recent heritage conservation work at:
- 1008 Drouillard Road
  - Hiram Walker Bridge (Pêche Island) Restoration
  - 1785 Walker Road- Teron Building
  - 225 Giles Boulevard W.- William T. Wesgate House
  - 3277 Sandwich Street- Mackenzie Hall Masonry Restoration Project
- BE RECOGNIZED** with the 2022 Built Heritage Awards.
- III. That the property owners of Neils C. Ortvad House, 766 Devonshire Road, and 436 Askin Avenue **BE RECOGNIZED** with a heritage designation plaque.

Carried.

Report Number: SCM 126/2022 & S 43/2022

Clerk's File: MBA/2274

## **8.8. Draft Plan of Subdivision Application - east of 3550 Howard Avenue SDN-002/21 [SDN/6593]- Wonsch Construction Company Limited - Ward 9**

Moved by: Councillor Holt

Seconded by: Councillor Kaschak

Decision Number: CR236/2022 DHSC 386

- I. That the application of Wonsch Construction Company Limited for Draft Plan of Subdivision approval of Part of Block A, Plan 1259, more particularly described as Part 2, 12R-28366, City of Windsor; **BE APPROVED** on the following basis:

- A. That this approval applies to the draft plan of subdivision, as shown on the attached Drawing SDN002/21-1, which will facilitate the creation of 4 residential lots.

- 
- B. That the Draft Plan Approval shall lapse on May 30, 2025.
- C. That the owner(s) enter into a subdivision agreement with the Corporation of the City of Windsor for the proposed development on the subject lands:

That prior to the execution and registration of the subdivision agreement between the Owner(s) and the Corporation of the City of Windsor, the Owner(s) shall submit for approval of the City Planner/Executive Director of Planning & Building a final draft M-Plan, which shall include the names of all road allowances within the plan, as approved by the Corporation.

That the subdivision agreement between the Owner(s) and the Corporation of the City of Windsor be registered on title prior to the registration of the final plan of subdivision and shall contain, among other matters, the following provisions:

1. The Owners will include all items as set out in the results of circularization and other relevant matters set out in CR233/98 (Standard Subdivision Agreement).
2. The Owners create, the following rights-of-way, in accordance with the approved Plan of Subdivision:
  - a) 20m right of way for the for the extension of Oakridge Avenue and Farrow Avenue to the northerly limit of the subject lands;
3. The Owners convey 0.3m reserve blocks along the north limit of Oakridge Avenue and Farrow Avenue to the City of Windsor, to the satisfaction of the City Planner.
4. The Owners agrees to complete a geotechnical report to determine the capacity of the soil below the road base and building envelopes to the satisfaction of the City Engineer;
5. The Owner agrees to provide a Noise Study for review prior to registration of the Final Plan of Subdivision and agrees to implement any mitigation measures recommended, to the satisfaction of the City Planner;
6. The Owners agrees to complete an MECP species at risk screening and comply with all requirements, including any required remediation measures, resulting from any study or report submitted to the MECP/MNRF regarding SAR assessment, all at its entire expense, to the satisfaction of the City Planner.
7. The Owners will comply with all the following requirements relating to sidewalks:

Sidewalks will be constructed:

On the East Side of Oakridge Avenue and Farrow Avenue, to the satisfaction of the City Engineer and the City Planner;

8. The Owners shall provide a detailed servicing study report on the impact of the increased flow to the existing municipal sewer systems to the satisfaction of the City Engineer, prior to the issuance of a construction permit.
  1. The study shall review the proposed impact and recommend solutions to addressing the problems and ultimate implementation of solutions should there be a negative impact to the system.
  2. The study shall be finalized to the satisfaction of the City Engineer.
9. The Owners(s) will:
  - a) Undertake an engineering analysis to identify stormwater quality and quantity measures as necessary to control any increases in flows in downstream watercourses, up to and including the 1:100 year design storm, to the satisfaction of the Municipality and the Essex Region Conservation Authority.
  - b) Install stormwater management measures identified above, as part of the development of the site, to the satisfaction of the City Engineer and the Essex Region Conservation Authority.
  - c) Obtain the necessary permit or clearance from the Essex Region Conservation Authority prior to undertaking site alterations and/or construction activities.
10. The Owners provide cash-in-lieu of parkland as permitted in Section 51.1 of the *Planning Act* and in accordance with By-law 12780, as amended, or any successor by-law to the satisfaction of the Executive Director of Parks and the City Planner prior to the issuance of construction permits.
11. The owner shall agree to provide to Union Gas the necessary easements and/or agreements required by Union Gas for the provision of gas services for this project, in a form satisfactory to Enbridge.
12. The Owner(s) shall agree to place the following warnings in all Offers to purchase, Agreements of Purchase and Sale or lease between the Developer and all prospective home buyers, and in the title:

“Students from this area may not be able to attend the closest neighbourhood school due to insufficient capacity and may have to be bussed to a distant school with available capacity or could be accommodate in temporary portable space.”

NOTES TO DRAFT APPROVAL (File: SDN-002/21)

1. The applicant is directed to Section 51(39) of *The Planning Act 1990* regarding appeal of any imposed conditions to the Ontario Land Tribunal. Appeals are to be directed to the City Clerk of the City of Windsor.

2. It is the applicant's responsibility to fulfil the conditions of draft approval and to ensure that the required clearance letters are forwarded by the appropriate agencies to the City of Windsor, to the attention of the Executive Director/City Planner, quoting the above-noted file number.
  3. The applicant should consult with an Ontario Land Surveyor for this proposed plan concerning registration requirements relative to the *Certification of Titles Act*.
  4. The final plan approved by the Corporation of the City of Windsor must be registered within thirty (30) days or the Corporation may withdraw its approval under Section 51(59) of *The Planning Act 1990*.
  5. All plans of subdivision/condominium are to be prepared and presented in metric units and certified by the Ontario Land Surveyor that the final plan is in conformity to the approved zoning requirements.
- II. That the City Clerk and Licence Commissioner **BE AUTHORIZED** to issue the required notice respecting approval of the draft plan of subdivision under Section 51(37) of *The Planning Act*; and,
- III. That prior to the final approval of the plan of subdivision by the Corporation of the City of Windsor, the Executive Director/City Planner shall **BE ADVISED**, in writing, by the appropriate agencies that conditions have been satisfied; and,
- IV. That the Chief Administrative Officer and City Clerk **BE AUTHORIZED** to sign all necessary agreements and documents approved as to form and content satisfactory to the City Solicitor.

Carried.

Report Number: SCM 127/2022 & S 45/2022  
Clerk's File: Z/14266

**8.9. Draft Plan of Subdivision Application 0 Liberty Street n/s Liberty Street, between Dougall Avenue and Gundy Park Lane SDN-003/21 [SDN/6630]-2342046 Ontario Inc. - Ward 9**

Moved by: Councillor Holt  
Seconded by: Councillor Kaschak

Decision Number: CR237/2022 DHSC 387

- I. That the application of 2342046 Ontario Inc. for Draft Plan of Subdivision approval of Part of Lots 42 & 43, Plan 713, and Part of Lot 80, Concession 3, City of Windsor, more particularly described as Parts 1, 2 and 3, 12R-13390; **BE APPROVED** on the following basis:
  - A. That this approval applies to the draft plan of subdivision, as shown on the enclosed Drawing SDN-003/21-1, which will facilitate the creation of 4 residential lots.

- 
- B. That the Draft Plan Approval shall lapse on May 30, 2025.
- C. That the owner(s) enter into a subdivision agreement with the Corporation of the City of Windsor for the proposed development on the subject lands:

That prior to the execution and registration of the subdivision agreement between the Owner(s) and the Corporation of the City of Windsor, the Owner(s) shall submit for approval of the City Planner/Executive Director of Planning & Building a final draft M-Plan, which shall include the names of all road allowances within the plan, as approved by the Corporation.

That the subdivision agreement between the Owner(s) and the Corporation of the City of Windsor be registered on title prior to the registration of the final plan of subdivision and shall contain, among other matters, the following provisions:

1. The Owners will include all items as set out in the results of circularization and other relevant matters set out in CR233/98 (Standard Subdivision Agreement).
2. The Owners create, prior to the issuance of a building permit, the following rights-of-way, in accordance with the approved Plan of Subdivision:
  - a) 20m right of way for Street A;
3. The Owner agrees, prior to the issuance of a building permit, to remove the existing barrier on Liberty Avenue and erect a new barrier on the Liberty Avenue Right of way at the west Limit of the Plan of Subdivision to the satisfaction of the City Engineer.
4. The Owners convey 0.3m reserve block along the west limit of Street A to the City of Windsor, to the satisfaction of the City Planner.
5. The Owner agrees to complete a geotechnical report to determine the capacity of the soil below the road base to the satisfaction of the City Engineer;
6. The Owner agrees to complete an MECP species at risk screening and comply with all requirements, including any required remediation measures, resulting from any study or report submitted to the MECP/MNRF regarding SAR assessment, all at its entire expense.
7. The Owners will comply with all the following requirements relating to sidewalks:

Sidewalks will be constructed:

On the East Side of Street A, to the satisfaction of the City Engineer and the City Planner;

8. The Owners shall provide a detailed servicing study report on the impact of the increased flow to the existing municipal sewer systems to the satisfaction of the City Engineer, prior to the issuance of a construction permit.
  1. The study shall review the proposed impact and recommend solutions to addressing the problems and ultimate implementation of solutions should there be a negative impact to the system.
  2. The study shall be finalized to the satisfaction of the City Engineer.
9. The Owners(s) will:
  - a) Undertake an engineering analysis to identify stormwater quality and quantity measures as necessary to control any increases in flows in downstream watercourses, up to and including the 1:100 year design storm, to the satisfaction of the Municipality and the Essex Region Conservation Authority.
  - b) Install stormwater management measures identified above, as part of the development of the site, to the satisfaction of the City Engineer and the Essex Region Conservation Authority.
  - c) Obtain the necessary permit or clearance from the Essex Region Conservation Authority prior to undertaking site alterations and/or construction activities.
10. The Owners provide cash-in-lieu of parkland as permitted in Section 51.1 of the *Planning Act* and in accordance with By-law 12780, as amended, or any successor by-law to the satisfaction of the Executive Director of Parks and the City Planner prior to the issuance of construction permits.
11. The owner shall agree to provide to Union Gas the necessary easements and/or agreements required by Union Gas for the provision of gas services for this project, in a form satisfactory to Enbridge.
12. The Owner(s) shall agree to place the following warnings in all Offers to purchase, Agreements of Purchase and Sale or lease between the Developer and all prospective home buyers, and in the title:

“Students from this area may not be able to attend the closest neighbourhood school due to insufficient capacity and may have to be bussed to a distant school with available capacity or could be accommodate in temporary portable space.”

NOTES TO DRAFT APPROVAL (File: SDN-003/21)

1. The applicant is directed to Section 51(39) of *The Planning Act 1990* regarding appeal of any imposed conditions to the Ontario Land Tribunal. Appeals are to be directed to the City Clerk of the City of Windsor.

# Minutes

## City Council

Monday, May 30, 2022

Page 16 of 40

- 
2. It is the applicant's responsibility to fulfil the conditions of draft approval and to ensure that the required clearance letters are forwarded by the appropriate agencies to the City of Windsor, to the attention of the Executive Director/City Planner, quoting the above-noted file number.
  3. The applicant should consult with an Ontario Land Surveyor for this proposed plan concerning registration requirements relative to the *Certification of Titles Act*.
  4. The final plan approved by the Corporation of the City of Windsor must be registered within thirty (30) days or the Corporation may withdraw its approval under Section 51(59) of *The Planning Act 1990*.
  5. All plans of subdivision/condominium are to be prepared and presented in metric units and certified by the Ontario Land Surveyor that the final plan is in conformity to the approved zoning requirements.
- II. That the City Clerk **BE AUTHORIZED** to issue the required notice respecting approval of the draft plan of subdivision under Section 51(37) of *The Planning Act*; and,
  - III. That prior to the final approval of the plan of subdivision by the Corporation of the City of Windsor, the Executive Director/City Planner shall **BE ADVISED**, in writing, by the appropriate agencies that conditions have been satisfied; and,
  - IV. That the Chief Administrative Officer and City Clerk **BE AUTHORIZED** to sign all necessary agreements and documents approved as to form and content satisfactory to the City Solicitor; and,
  - V. That a one-foot wide portion of the Liberty Street right-of-way as shown on Drawing Number. SDN-003/21-1, contained in this report **BE CLOSED AND RETAINED** for municipal purposes subject to the following:
    - a. Easements, subject to their being accepted in the City's standard form and in accordance with the City's standard practice, be granted to Enbridge Ltd. and EnWin Utilities Ltd.
  - VI. That the City Planner **BE REQUESTED** to supply the appropriate legal description for the area to be closed, in accordance with Drawing Number. SDN 003-21-1, contained in this report; and,
  - VII. That the City Planner, or designate, **BE AUTHORIZED** to publish the required legal notice regarding the portion of the Liberty St. right-of-way to be closed; and,
  - VIII. That the City Solicitor **BE REQUESTED** to prepare the necessary by-law(s) to facilitate the right-of-way closure; and,



# Minutes

## City Council

Monday, May 30, 2022

Page 17 of 40

- IX. That the Chief Administrative Officer and City Clerk **BE AUTHORIZED** to sign all necessary documents approved as to form and content satisfactory to the City Solicitor; and,
- X. That the matter **BE COMPLETED** electronically pursuant to By-law Number 366-2003; and,
- XI. That the portion of Liberty Street closed by By-law 5588 **BE OPENED** for vehicular traffic.
- Carried.

Report Number: SCM 128/2022 & S 47/2022

Clerk's File: Z/14316

### 8.10. Rezoning - 1933923 Ontario Ltd – 0 and 817 Elinor Street - Z-002/22 ZNG/6657 - Ward 7

Moved by: Councillor Holt

Seconded by: Councillor Kaschak

Decision Number: CR238/2022 DHSC 388

- I. That Zoning By-law 8600 **BE AMENDED** by changing the zoning of Part Alley, Registered Plan 1142, further described as Parts 3 & 4, Plan 12R-25749, and Lots 26 to 31, Registered Plan 1142, (Roll No: 060-450-13120 & 060-450-13130), situated at the southwest corner of Wyandotte Street East and Elinor Street, and known municipally as 817 Elinor Street and 0 Elinor Street from Development Reserve District 1.1 (DRD1.1) and Residential District 1.2 (RD1.2) to Residential District 2.5 (RD2.5) and by adding a site specific exception to Section 20 (1) as follows:

#### 442. SOUTHWEST CORNER OF WYANDOTTE STREET EAST AND ELINOR STREET

For the lands comprising of Part Alley, Registered Plan 1142, further described as Parts 3 & 4, Plan 12R-25749, and Lots 26 to 31, Registered Plan 1142, a *multiple dwelling* with five or more *dwelling units* shall be subject to the following additional provisions:

- a) Lot Area – per *dwelling unit* - minimum 130.0 m<sup>2</sup>
- b) That the required *front yard depth*, required *rear yard depth*, and required *side yard* width shall not apply.
- c) Building Setback – minimum
  - from the lot line adjacent to Wyandotte Street East (including the corner cut-off) 1.20 m
  - from the lot line adjacent to Elinor Street 2.50 m
  - from an *interior lot line* 2.50 m
- d) Notwithstanding Section 25.5.20.1.6, the minimum separation of a *parking area* from a building wall containing a *habitable room window* or containing both a main pedestrian entrance and a *habitable room window* facing the *parking area* where the *building* is located on the same *lot* as the *parking area* shall be 3.50 m
- e) Notwithstanding Section 24.40, a *loading space* is not required.
- f) An *access area* or direct vehicular access to Wyandotte Street East is prohibited.

[ZDM 14; ZNG/6588]

# Minutes

## City Council

Monday, May 30, 2022

Page 18 of 40

- II. That the Site Plan Approval Officer **BE DIRECTED** to:
- a) Circulate any application to the Essex Region Conservation Authority for their review and comment;
  - b) Consider the comments from municipal departments and external agencies in Appendix D attached to Report S 41/2022.

Carried.

Report Number: SCM 129/2022 & S 41/2022

Clerk's File: Z/14296

### 8.12. Brownfield Redevelopment Community Improvement Plan (CIP) application submitted by Albert and Maria Folino for 660 University Avenue East (Ward 3)

Moved by: Councillor Holt

Seconded by: Councillor Kaschak

Decision Number: CR240/2022 DHSC 391

- I. That the request made by Albert and Maria Folino to participate in the Environmental Site Assessment Grant Program **BE APPROVED** for the completion of a proposed Phase II Environmental Site Assessment Study for property located at 660 University Avenue East pursuant to the City of Windsor Brownfield Redevelopment Community Improvement Plan; and,
- II. That the grant funds in the amount of \$8,550 **BE TRANSFERRED** from the CIP Reserve Fund 226 to Brownfield Strategy Remediation (project 7069003) when the eligible work is completed to the satisfaction of the City Planner; and,
- III. That the City Treasurer **BE AUTHORIZED** to issue payment up to a maximum of \$8,550 based upon the completion and submission an eligible study Phase II Environmental Site Assessment Study completed in a form acceptable to the City Planner and City Solicitor; and,
- IV. That should the proposed Phase II Environmental Site Assessment Study and Remedial Work Plan not be completed within two (2) years of Council approval, the approval **BE RESCINDED** and the funds be uncommitted and made available for other applications; and,
- V. That the request made by Albert and Maria Folino to participate in the Brownfield Rehabilitation Grant Program **BE APPROVED** for 70% (or 100% if LEED certified) of the municipal portion of the tax increment resulting from the proposed redevelopment at 660 University Avenue East for up to 10 years or until 100% of the eligible costs are repaid pursuant to the City of Windsor Brownfield Redevelopment Community Improvement Plan; and,
- VI. That Administration **BE DIRECTED** to prepare an agreement between Albert and Maria Folino, the City, and any persons legally assigned the right to receive grant payments to

implement the Brownfield Tax Assistance and Rehabilitation Grant Programs in accordance with all applicable policies, requirements, and provisions contained within the Brownfield Redevelopment Community Improvement Plan to the satisfaction of the City Planner as to content, the City Solicitor as to legal form, and the CFO/City Treasurer as to financial implications; and,

- VII. That the CAO and City Clerk **BE AUTHORIZED** to sign the Rehabilitation Grant Agreement; and,
- VIII. That the City Planner **BE AUTHORIZED** to sign an Assignment Agreements, if required, satisfactory in form and content to the City Solicitor, in technical content to the City Planner and in financial content to the City Treasurer; and further,
- IX. That the approval to participate in the Brownfield Rehabilitation Grant Program **EXPIRE** if the grant agreement is not signed by applicant within one year following Council approval. The City Planner may extend the deadline for up to one year upon request from the applicant.

Carried.

Report Number: SCM 131/2022 & S 40/2022  
Clerk's File: Z/14362

### **8.13. Economic Revitalization Community Improvement Plan (CIP) application submitted by Bijoy Foods Inc. for 3190 Devon Drive - Ward 9**

Moved by: Councillor Holt  
Seconded by: Councillor Kaschak

Decision Number: CR241/2022 DHSC 392

- I. That the request made by Bijoy Foods Inc. to participate in the Business Retention and Expansion Grant Program **BE APPROVED** for the property located at 3190 Devon Drive for up to 10 years or until 100% of the eligible costs are repaid pursuant to the City of Windsor Economic Revitalization Community Improvement Plan; and,
- II. That Administration **BE DIRECTED** to prepare an agreement between the City, Bijoy Foods Inc., and/or persons or companies that have legally been assigned the right to receive grant payments, to implement the Business Retention and Expansion Grant Program in accordance with all applicable policies, requirements, and provisions contained within the Economic Revitalization Community Improvement Plan to the satisfaction of the City Planner for content, the Commissioner of Legal Services as to legal form, and the CFO/City Treasurer as to financial implications; and,
- III. That the CAO and City Clerk **BE AUTHORIZED** to sign the Business Retention and Expansion Grant Agreement; and further,
- IV. That the approval to participate in the Business Retention and Expansion Grant Program

**EXPIRE** if the grant agreement is not signed by applicant and owner within one year following Council approval. The City Planner may extend the deadline for up to one year upon request from the applicant.

Carried.

Report Number: SCM 132/2022 & S 48/2022

Clerk's File: Z/14364

#### **8.14. Northway Avenue Development from Malden Road to Manitoba Street | Cost Sharing | Carthage Development Inc.**

Moved by: Councillor Holt

Seconded by: Councillor Kaschak

Decision Number: CR242/2022 DHSC 393

- I. That Council **APPROVE** a Cost Sharing payment to Carthage Developments Incorporated estimated at \$313,069, excluding HST (final payment to be based on actual construction costs), as the City's share of infrastructure costs associated with the Northway Avenue Development, to be funded from the New Infrastructure Development Project (Project ID# 7035119).
- II. That the CAO and City Clerk **BE AUTHORIZED** to execute a Servicing Agreement with Carthage Developments Inc. for the installation of full municipal services on Northway Avenue from Malden Road to Manitoba Street, satisfactory in form to the Commissioner of Legal & Legislative Services and in content to the Commissioner of Infrastructure Services in accordance with the following terms:
  - a. The general servicing requirements as detailed by CR233/98.
  - b. Cost Sharing - The Corporation agrees to pay to the Owner **THREE HUNDRED THIRTEEN THOUSAND SIXTY NINE DOLLARS (\$313,069)** excluding HST, based on estimated construction costs, final payment to be based on actual progress certificate payments, representing the Corporation's share of costs associated with the following:
    - i. The extension of Malden Road from Daytona Avenue east to Northway Avenue [The City is responsible for 70% of the construction costs];
    - ii. Curb and base asphalt repairs, as well as the installation of surface asphalt on Northway Avenue, north of Malden Road [The City is responsible for 100% of the construction costs]; and,
    - iii. The installation of storm and sanitary private drain connections, as well as water and hydro connections needed to service two new building lots fronting Daytona Avenue and Northway Avenue.

Carried.

Report Number: SCM 133/2022 & S 27/2022

Clerk's File: SW/14365

# Minutes

## City Council

Monday, May 30, 2022

Page 21 of 40

### 8.15. Minutes of the International Relations Committee of its meeting held February 2, 2022

Moved by: Councillor Holt

Seconded by: Councillor Kaschak

Decision Number: CR243/2022 DHSC 394

That the minutes of the International Relations Committee of its meeting held February 2, 2022 **BE RECEIVED**.

Carried.

Report Number: SCM 134/2022 & SCM 82/2022

Clerk's File: MB2022

### 8.16. Minutes of the International Relations Committee of its meeting held March 31, 2022

Moved by: Councillor Holt

Seconded by: Councillor Kaschak

Decision Number: CR244/2022 DHSC 395

That the minutes of the International Relations Committee of its meeting held March 31, 2022 **BE RECEIVED**.

Carried.

Report Number: SCM 135/2022 & SCM 95/2022

Clerk's File: MB2022

### 11.1. Windsor-Canada Utilities Ltd. – Annual General Meeting – City Wide

Moved by: Councillor Holt

Seconded by: Councillor Kaschak

Decision Number: CR247/2022

I. That Council, acting as the shareholder for Windsor Canada Utilities Ltd. ("WCUL") **APPROVE** the resolutions presented by WCUL; and further,

II. That the Mayor **BE AUTHORIZED** to execute the shareholder's resolutions on behalf of The Corporation of the City of Windsor, in accordance with Council direction, with respect to all matters presented herein by WCUL for the year 2021.

Carried.

Report Number: C 80/2022

Clerk's File: MU2022

# Minutes

## City Council

Monday, May 30, 2022

Page 22 of 40

### 11.2. Declaration of a Vacant Parcel of Land Municipally Known as 0 Tuscarora Street Surplus and Authority to Offer for Sale - Ward 4

Moved by: Councillor Holt

Seconded by: Councillor Kaschak

Decision Number: CR248/2022

I. That the following City of Windsor (the “**City**”) vacant parcel of land **BE DECLARED** surplus:

- Municipal address: **0 Tuscarora Street** – vacant land situate on the north side of Tuscarora Street, east of Marion Avenue
- Legal Description: Part of Lot 4 on Registered Plan 231 as in WE38392 save and except WE41042
- Approximate Lot size: 6.8 feet (2 m) x 164 feet (50 m)
- Approximate Lot area: 1,115.2 sq ft (100 m<sup>2</sup>)  
(herein the “**Subject Parcel**”); and,

II. That the Manager of Real Estate Services **BE AUTHORIZED** to offer the vacant parcel of land identified in Recommendation I for sale to the abutting property owner at 1106 Tuscarora Street at a price to be determined by the Manager of Real Estate Services, commensurate with an independent appraisal, as appropriate.

Carried.

Report Number: C 83/2022

Clerk's File: APM2022

### 11.3. Declaration of Vacant Parcel Municipally Known as 0 Provincial Road Surplus and Authority to Offer for Sale - Ward 9

Moved by: Councillor Holt

Seconded by: Councillor Kaschak

Decision Number: CR249/2022

I. That the following City of Windsor (the “**City**”) vacant parcel **BE DECLARED** surplus:

- Municipal address: **0 Provincial Road** – vacant land situate on the south side of Provincial Road, west of Legacy Park Drive
- Legal Description: Part of Lot 14, Concession 6, Sandwich East further described as Parts 4 to 7 on Plan 12R-27714
- Approximate Lot size: 47.2 m (154.8 feet) x 70 m (229.6 feet) x 8.2 m (26.9 feet) x 58.37 m (191.5 feet)
- Approximate Lot area: 17,507.5 sq ft (1,626.5 m<sup>2</sup>)  
(herein the “**Subject Parcel**”); and,

II. That the Manager of Real Estate Services **BE AUTHORIZED** to offer the vacant parcel identified in Recommendation I for sale to the abutting property owners for lot consolidation

# Minutes

## City Council

Monday, May 30, 2022

Page 23 of 40

purposes at a price to be determined by the Manager of Real Estate Service, subject to the previous owner of the Subject Parcel having the option to match the best offer received by the City; and,

- III. That the City Solicitor or designate **BE DIRECTED** to prepare a by-law to dedicate Part 1 to 3 on Plan 12R-27714 as part of the public highway known as Provincial Road.

Carried.

Report Number: C 92/2022

Clerk's File: APM2022

### 11.5. Award of Tender: 13-22 - Aubin Road Rehabilitation - Sewer, Pavement, Watermain, and Street Lighting Rehabilitation - Seminole Street to Alice Street - Ward 5

Moved by: Councillor Holt

Seconded by: Councillor Kaschak

Decision Number: CR251/2022

- I. That the following low tender **BE ACCEPTED**:

<b>TENDERER:</b>	<b>Sterling Ridge Infrastructure Inc.</b>
<b>TENDER NO:</b>	<b>13-22</b>
<b>TOTAL TENDER PRICE:</b>	<b>\$3,694,672.00 (excluding HST)</b>
<b>ACCOUNT CHARGED:</b>	<b>007-5410-9998-02942-7201026</b>

and,

That the CAO and City Clerk **EXECUTE** an agreement with the low bidder, Sterling Ridge Infrastructure Inc., in the amount of \$3,694,672.00 plus tax, with said contract being satisfactory in form to the Commissioner of Legal & Legislative Services, satisfactory in technical content to the Commissioner of Infrastructure Services, and in financial content to the Commissioner of Corporate Services CFO/City Treasurer.

- II. That City Council **APPROVE** a transfer of \$125,674.00 in surplus funding from the Francois – Seminole to Alice project (ID 7181006) to Aubin Road – Seminole to Alice project (ID 7201026) as detailed in the financial matters section.

Carried.

Report Number: C 96/2022

Clerk's File: SW/14405

### 11.6. Award of Tender: 04-22 - St. Luke Road Rehabilitation - Alice Street to Seminole Street (Ward 5)

Moved by: Councillor Holt

Seconded by: Councillor Kaschak

Decision Number: CR252/2022

- I. That the following low tender **BE ACCEPTED**:

<b>TENDERER:</b>	<b>D'Amore Construction (2000) Ltd.</b>
<b>TENDER NO:</b>	<b>04-22</b>
<b>TOTAL TENDER PRICE:</b>	<b>\$4,500,000.00 (excluding HST)</b>
<b>ACCOUNT CHARGED:</b>	<b>007-5410-9998-02942-7201028</b>

and,

That the CAO and City Clerk **EXECUTE** an agreement with the low bidder, D'Amore Construction (2000) Ltd., in the amount of \$4,500,000.00 plus tax, with said contract being satisfactory in form to the Commissioner of Legal & Legislative Services, satisfactory in technical content to the Commissioner of Infrastructure Services, and in financial content to the Commissioner of Corporate Services CFO/City Treasurer.

- II. That City Council **PRE-COMMIT** for immediate use funding from City Wide Sewer Rehabilitation Program (ECP-035-07) in the amount of \$780,000 from Fund 028 (Sewer Surcharge) in 2026 that was part of the 2022 Capital Budget approved in principle to allow for 2022 construction of Project 7201028 – St. Luke Rd – Seminole to Alice to proceed as planned.

Carried.

Report Number: C 97/2022  
Clerk's File: SW/14306

## **9. REQUEST FOR DEFERRALS, REFERRALS AND/OR WITHDRAWALS**

None requested.

## **10. PRESENTATIONS AND DELEGATIONS**

### **10.1. Auditor General Annual Internal Audit Plan**

**AND**

### **10.2. Internal Audit Summary Report - December 1, 2021 to April 30, 2022**

**Christopher O'Connor, Auditor General**

Christopher O'Connor, Auditor General, appears before Council to make a presentation entitled "Auditor General Annual Internal Audit Plan" and "Internal Audit Summary Report – December 1, 2021 to April 30, 2022" and provides a brief summary of the current activities being undertaken by the Auditor General including the following: Summary of use of unallocated effort; Complaints and investigations; Road Infrastructure Maintenance Processes VFM Internal Audit Report—



# Minutes

## City Council

### Monday, May 30, 2022

Page 25 of 40

performance audit value for money, modified performance measures were provided and management to set own performance measures which would streamline the process; Smart City Cyber Risk Mitigation Internal Audit Report, focusing on privacy and cyber risks and uses the established framework; Management Action Plan Validation Report including details related to the pandemic; Annual Performance Report; An amendment to the current plan, modified under the Auditor General Charter cancelling the Ransomware review and how the City determines priorities and projects; Stress of the pandemic and work that has been undertaken to mitigate risks; and a Compliance review of the procurement process including training and ensuring people are receiving the training.

Mayor Dilkens leaves the meeting at 4:54 o'clock p.m. and Councillor Francis assumes the Chair.

Moved by: Councillor McKenzie  
Seconded by: Councillor Bortolin

Decision Number: CR245/2022

That the Auditor General Annual Internal Audit Plan 2022-2023 (*attached*) **BE RECEIVED** for information and **BE APPROVED**.  
Carried.

Report Number: SCM 144/2022  
Clerk's File: AF/14041

Moved by: Councillor McKenzie  
Seconded by: Councillor Bortolin

Decision Number: CR246/2022

That the attached Internal Audit Summary report provided by PricewaterhouseCoopers LLP (PwC) for the period December 1, 2021 to April 30, 2022 comprised of the following:

- a) Summary of use of unallocated effort
- b) Complaints and Investigations
- c) Road Infrastructure Maintenance Processes VFM Internal Audit Report
- d) Smart City Cyber Risk Mitigation Internal Audit Report
- e) Management Action Plan Validation Report
- f) Annual Performance Report

**BE RECEIVED** for information; and,

That City Council **AUTHORIZE** administration to proceed to implement the Management Action Plans as prescribed in the internal audit reports; and further,

That City Council **DIRECT** administration to report on the progress of the implementation of the Management Action Plans and that such reports should coincide with the Auditor General's internal audit updates to City Council.

Carried.

Report Number: SCM 145/2022  
Clerk's File: AF/14041

Mayor Dilkens returns to the meeting at 4:58 o'clock p.m. and Councillor Francis returns to his seat at the Council table.

#### **11.4. Peace Fountain Replacement – Ward 6**

##### **Pooya Baktash and Nathan Bishop, Partisans**

Pooya Baktash and Nathan Bishop, Partisans, appear before Council regarding the report "Peace Fountain Replacement – Ward 6" and are available for questions.

##### **George Brooks (Son of Charlie Brooks) and David Brooks (Grandson of Charlie Brooks)**

George Brooks (son of Charlie Brooks) and David Brooks (grandson of Charlie Brooks) appear before Council in support of the Administrative recommendations in the report "Peace Fountain Replacement – Ward 6" and indicate that they were both involved with the consultants related to the three options that were presented in the report and conclude by suggesting that the Peace Fountain should be renamed to the Charlie Brooks International Peace Fountain and encourages administration to seek funding from the Federal and Provincial Governments; and suggest an annual celebration gathering in peace at the fountain.

##### **Rick Labonte, Unifor Local 444 Community Environment Representative**

Rick Labonte, Unifor Local 444 Community Environment Representative, appears before Council regarding the report "Peace Fountain Replacement – Ward 6" and provides a brief history of Charlie Brooks as well as his legacy and encouraging peace; and concludes by suggesting a more modern and sustainable long term solution is required which should include renewable energy which is environmentally friendly; encourages modern technology to be considered when deciding; and encourages more accessible parking be installed closer to the peace fountain which would be more beneficial to the community.

##### **Kristina Bradt**

Kristina Bradt appears before Council regarding the report "Peace Fountain Replacement – Ward 6" and provides a brief history of her arts, culture and heritage work and how this fountain can be an opportunity to move forward with Public Art in this region; after consultations with the consultant suggests arts being commissioned; festivals; different types of arts and culture to provide artists a

# Minutes

## City Council

### Monday, May 30, 2022

Page 27 of 40

platform; encouraging tourism bringing people to the fountain; and concludes by suggesting that Council consider her suggestions as integrating the work of local artists with this project would be beneficial to the community.

Moved by: Councillor Gignac  
Seconded by: Councillor Francis

Decision Number: CR250/2022

- I. That the report of the Project Administrator, Corporate Projects, dated May 18, 2022 entitled "Peace Fountain Replacement – Ward 6" and its appendices (*attached*) **BE RECEIVED** for information; and,
- II. That Council **DIRECT** Administration to **MOVE FORWARD** with Option 1: Floating Fountain design concept, and that Administration **PERFORM** public consultation and **BRING FORWARD** final design, costing and funding options for presentation to Council for the 2023 Capital Budget; and,
- III. That City Council **PRE-APPROVE** and **AWARD** any procurement(s) necessary related to the approved option, provided that the procurement(s) are within approved budget amounts, pursuant to the Purchasing By-Law 93-2012 and amendments thereto; satisfactory in legal content to the Commissioner of Legal & Legislative Services, in financial content to the Chief Financial Officer/City Treasurer, and in technical content to the Commissioner of Infrastructure Services; and,
- IV. That Administration **BE AUTHORIZED** to take any other steps as may be required to bring effect to these resolutions, and that the Chief Administrative Officer and City Clerk **BE AUTHORIZED** to execute any required documentation/agreement(s) for that purpose, subject to legal approval by the Commissioner of Legal & Legislative Services, financial approval by the Chief Financial Officer/City Treasurer, and technical approval by the Commissioner of Infrastructure Services; and,
- V. That a committee **BE STRUCK** to begin fundraising to assist in covering costs associated with the Charles Brooks Peace Fountain replacement; and further,

Carried.

Councillors Costante and Sleiman were absent from the meeting when the vote was taken on this matter.

Report Number: C 95/2022  
Clerk's File: SR/14084

**8.11. Zoning By-law Amendment Application to add a site specific zoning provision to allow a permanent patio in the rear yard at 642 Windermere Road, which would be exclusive to the restaurant, Vito's Pizzeria, located on the property to the north, known municipally as 1731-1737 Wyandotte Street East Z-008/22 [ZNG/6670]**

---

**Jeffrey Nanson, Solicitor representing the Applicant**

Jeffrey Nanson, Solicitor representing the applicant, appears before Council regarding the report "Zoning By-law Amendment Application to add a site specific zoning provision to allow a permanent patio in the rear yard at 642 Windermere Road, which would be exclusive to the restaurant, Vito's Pizzeria, located on the property to the north, known municipally as 1731-1737 Wyandotte Street East Z-008/22 [ZNG/6670]" and provides a brief history of the patio which has been met with positive feedback during the pandemic; and concludes by suggesting that they are in support of the License Commission's option and encourages Council to approve the recommendation of the Standing Committee.

**Lynne Pearlman, Area Resident**

Lynne Pearlman, area resident, appears before Council and expresses concern regarding the recommendation in the administrative report "Zoning By-law Amendment Application to add a site specific zoning provision to allow a permanent patio in the rear yard at 642 Windermere Road, which would be exclusive to the restaurant, Vito's Pizzeria, located on the property to the north, known municipally as 1731-1737 Wyandotte Street East Z-008/22 [ZNG/6670]" and encourages Council to support the administrative recommendation to deny the request; provides details related to the residential creep and setting a negative precedent in the area; brief history of the office building; restaurant, the temporary patio during COVID restrictions and now this request to make it permanent; and concludes by providing specific concerns related to parking; noise, alley traffic; more rodents including rats, rodents and skunks in the area and requests that a parking study be undertaken to determine the impacts to the area.

Moved by: Councillor Holt

Seconded by: Councillor Sleiman

Decision Number: CR239/2022 DHSC 389

That Zoning By-law 8600 **BE AMENDED** by changing the zoning of Lot 3, Plan 502 (642 Windermere Road; Roll No. 020-070-06600; PIN No. 01136-0246), located on the east side of Windermere Road, south of Wyandotte Street East, by adding a site specific provision to Section 20(1) as follows:

**447. EAST SIDE OF WINDERMERE ROAD, SOUTH OF WYANDOTTE STREET EAST**

For the lands comprising of Lot 3, Registered Plan 502 (Roll No. 020-070-06600; PIN 01136-0246), a permanent patio (deck) exclusive to the *Restaurant* located on the property to the north, known municipally as 1731-1737 Wyandotte Street East and described as Lot 1, Registered Plan 502 (Roll No. 020-070-06900; PIN 01136-0386), shall be an additional permitted use and the following additional provisions shall apply:

- a) Fence with a height of 1.0 m shall be installed along the east *lot line* and the segment of the north *lot line* which bounds the *rear yard*, save and except a 1.5 m wide opening to provide pedestrian access to the permanent patio.

- b) *Landscaped open space yard* with a minimum depth of 1.2 m shall be installed along the east *lot line* and the segment of the north *lot line* which bounds the *rear yard*, save and except a 1.5 m wide opening to provide pedestrian access to the permanent patio.
- c) Screening fence with a minimum height of 1.8 m shall be maintained along the segment of the south *lot line* which bounds the *rear yard*.
- d) Notwithstanding Table 24.20.5.1 and the registered Site Plan Control Agreement, dated May 16, 1996, for file number SPC-015/96, no *parking spaces* shall be required for the existing legal non-conforming *Business Office* use.  
[ZDM 6; ZNG/6670]

That City Council **DIRECT** the Licence Commissioner to recommend conditions as part of the review and approval of the license as follows:

That the License Commission **GIVE CONSIDERATION** to the following conditions regarding the Hospitality Food Service/Liquor Service Ancillary business license for Vito's Pizzeria restaurant items as related to:

- The patio shall comply with City of Windsor Lighting Intensity Standards Study (Council Resolution 228-2005) (See Appendix A1);
- The hours of operation for the patio shall be limited to between 7:00 am -11:00 pm daily; and
- Notwithstanding Section 2 of Table 3-1 to Emission of Sounds By-law No. 6716, the sound from or created by any radio, tape player, television, public address system, sound equipment, loud speaker, or any musical or sound producing instrument of whatever kind shall be prohibited on the patio; and further,

That the Commissioner, upon making a recommendation to the Commission, **BE DIRECTED** to give the applicant (Vito's Pizzeria) notice of the hearing at least seven days prior to its scheduled date (Part 3, Section 3.30 of Business Licensing By-law No. 395-2004). Vito's Pizzeria and municipal or provincial departments or agencies identified herein may make submissions to the Commission in respect to the said recommendation. The Commission shall give due consideration to the submissions made to it when rendering a decision on the recommendation. The decision rendered by the Commission is final and cannot be appealed by any parties.  
Carried.

At the request of Councillor Gignac, a recorded vote is taken on this matter.

Aye votes: Councillors Francis, Costante, Bortolin, Holt, Sleiman, Gill, Kaschak, McKenzie, Morrison, and Mayor Dilkens.

Nay votes: Councillor Gignac.

Abstain: None.

Absent: None.

Report Number: SCM 130/2022, S 49/2022, AI 8/2022  
Clerk's File: Z/14315

# Minutes

## City Council

Monday, May 30, 2022

Page 30 of 40

### 11. REGULAR BUSINESS ITEMS (Non-Consent Items)

None presented.

### 12. CONSIDERATION OF COMMITTEE REPORTS

#### 12.1. (i) Report of the Special In-Camera meeting or other Committee as may be held prior to Council

Moved by: Councillor Sleiman

Seconded by: Councillor Bortolin

Decision Number: CR253/2022

That the report of the In Camera meeting of Council held May 30, 2022 **BE ADOPTED** as presented.

Carried.

Clerk's File: ACO2022

#### 12.2. Minutes of the Windsor Licensing Commission of its meeting held April 27, 2022

Moved by: Councillor Sleiman

Seconded by: Councillor Bortolin

Decision Number: CR254/2022

That the minutes of the Windsor Licensing Commission of its meeting held April 27, 2022 **BE RECEIVED**.

Carried.

Report Number: SCM 141/2022

Clerk's File: MB2022

#### 12.3. Minutes of the Special Meeting of the Council Compensation Review Committee of its meeting held April 12, 2022

Moved by: Councillor Sleiman

Seconded by: Councillor Bortolin

Decision Number: CR255/2022

That the minutes of the Council Compensation Review Committee of its Special Meeting held April 12, 2022 **BE RECEIVED**.

Carried.

Report Number: SCM 124/2022

Clerk's File: MB2022

# Minutes

## City Council

Monday, May 30, 2022

Page 31 of 40

### 12.4. Special Meeting of Council - In-Camera, May 18, 2022

Moved by: Councillor Sleiman  
Seconded by: Councillor Bortolin

Decision Number: CR256/2022

That the report of the Special In Camera meeting of Council held May 18, 2022 **BE ADOPTED** as presented.

Carried.

Report Number: SCM 146/2022  
Clerk's File: ACO2022

### 13. BY-LAWS (First and Second Reading)

Moved by: Councillor Costante  
Seconded by: Councillor Francis

That the following By-laws No. 77-2022 through 85-2022 (inclusive) be introduced and read a first and second time:

**77-2022** A BY-LAW TO ESTABLISH LANDS AS A PUBLIC HIGHWAY KNOWN AS HOWARD AVENUE, IN THE CITY OF WINDSOR authorized by CR76/2011 dated February 28, 2011.

**78-2022** A BY-LAW TO FURTHER AMEND BY-LAW NUMBER 8600 CITED AS THE "CITY OF WINDSOR ZONING BY-LAW" authorized by CR174/2022 dated April 25, 2022.

**79-2022** A BY-LAW TO FURTHER AMEND BY-LAW 9023 BEING A BY-LAW TO REGULATE VEHICULAR PARKING WITHIN THE LIMITS OF THE CITY OF WINDSOR ON MUNICIPAL STREETS, MUNICIPAL PARKING LOTS AND PRIVATE PROPERTIES approved by CAO 72/2022 dated March 30, 2022.

**80-2022** A BY-LAW TO FIX THE TAX RATES AND TO PROVIDE FOR THE LEVY AND COLLECTION OF PROPERTY TAXES FOR THIS YEAR authorized by CR202/2022 dated May 9, 2022.

**81-2022** A BY-LAW TO PROVIDE FOR THE IMPOSITION OF SPECIAL CHARGES UPON PRESCRIBED BUSINESS PROPERTY CLASSES OF RATEABLE PROPERTY WITHIN IMPROVEMENT AREAS FOR THE YEAR 2022 authorized by CR201/2022 dated May 9, 2022.

**82-2022** A BY-LAW TO RESCIND THE APPOINTMENT OF JASON WARD REYNAR AS THE CHIEF ADMINISTRATIVE OFFICER FOR THE CORPORATION OF THE CITY OF WINDSOR, AND TO APPOINT ONORIO COLUCCI AS THE ACTING CHIEF ADMINISTRATIVE OFFICER OF THE CORPORATION OF THE CITY OF WINDSOR authorized by CR157/2022 dated April 11, 2022.

# Minutes

## City Council

Monday, May 30, 2022

Page 32 of 40

**83-2022** A BY-LAW TO RESCIND THE APPOINTMENT OF GARY CIAN AS LICENCE COMMISSIONER FOR THE CORPORATION OF THE CITY OF WINDSOR, AND TO APPOINT STEVEN VLACHODIMOS AS LICENCE COMMISSIONER FOR THE CORPORATION OF THE CITY OF WINDSOR authorized by By-law 420-2001 dated November 19, 2001.

**84-2022** A BY-LAW TO RESCIND THE APPOINTMENT OF JASON WARD REYNAR AND GARY CIAN AS DEPUTY CLERKS FOR THE CORPORATION OF THE CITY OF WINDSOR authorized by By-law 420-2001 dated November 19, 2001.

**85-2022** A BY-LAW TO CONFIRM PROCEEDINGS OF THE COUNCIL OF THE CORPORATION OF THE CITY OF WINDSOR AT ITS MEETING HELD ON THE 30th DAY OF MAY, 2022.  
Carried.

## 14. MOVE BACK INTO FORMAL SESSION

Moved by: Councillor Gignac

Seconded by: Councillor Gill

That the Committee of the Whole does now rise and report to Council respecting the business items considered by the Committee:

- 1) Communication Items (as amended)
- 2) Consent Agenda (as amended)
- 3) Items Deferred  
Items Referred
- 4) Consideration of the Balance of Business Items (as amended)
- 5) Committee Reports as presented
- 6) By-laws given first and second readings as presented

Carried.

## 15. NOTICES OF MOTION

### 15.1. Councillor Bortolin's Notice of Motion with Conditional Delegations

**Eric Nadalin, Director of Public Health Programs, and Nicole Dupuis, CEO, Windsor Essex County Health Unit**

Eric Nadalin, Director of Public Health Programs, and Nicole Dupuis, CEO, Windsor Essex County Health Unit, appear before Council regarding Councillor Bortolin's motion regarding the Windsor-Essex County Health Unit's application for a Consumption and Treatment Site (CTS) at 101 Wyandotte St. E. and are available for questions.



# Minutes

## City Council

### Monday, May 30, 2022

Page 33 of 40

Moved by: Councillor Bortolin

Seconded by: Councillor Morrison

Decision Number: CR257/2022

**Whereas**, opioid-related morbidity and mortality have been increasing steadily in Windsor-Essex County over the past several years and are currently at the highest levels ever recorded in the region, and

**Whereas**, the onset of the COVID-19 pandemic in Ontario in March 2020 have contributed to substantial increases in opioid-related ED visits, opioid and drug overdose-related Emergency Medical Services (EMS) calls, hospitalizations, and deaths, and

**Whereas**, the majority of opioid and drug overdose cases that present in the emergency department resided in downtown Windsor, and

**Whereas**, Consumption and Treatment Services (CTS) sites have been established across the province to address opioid overdose and opioid related mortality, and

**Whereas**, the Windsor-Essex County Health Unit has completed a feasibility study indicating community support for a CTS, and

**Whereas**, the Windsor-Essex County Health Unit has established a Stakeholder Advisory Committee for the creation of a CTS in the downtown core of the City of Windsor, and

**Whereas**, the WECHU, in collaboration with partners involved in the WECOSS and the CTS Stakeholder Advisory Committee, has completed a series of comprehensive community consultations (2018-2021) that supported the local feasibility, need, and site-selection for establishing a local CTS site at 101 Wyandotte St E in the City of Windsor, and

**Whereas**, the WECHU has obtained a local Board of Health resolution in support of proceeding with the federal and provincial application processes for establishing a CTS site at the location of 101 Wyandotte St E, and

**Whereas**, the application for a CTS to the provincial government requires the WECHU obtain and submit local municipal council support (i.e. council resolution) endorsing the CTS site at 101 Wyandotte St E in the City of Windsor, and

**Now Therefore be it resolved** that the City of Windsor Council **SUPPORT** the WECHU's applications to the Ontario Ministry of Health and Health Canada to operate a Consumption and Treatment Services site at the location of 101 Wyandotte St E in the City of Windsor through a council resolution, and

**Be it further resolved** that the City of Windsor Council **SUPPORT** the WECHU in establishing a CTS site at 101 Wyandotte St E in the City of Windsor post-approval from the provincial and federal governments.

Carried.

# Minutes

## City Council

### Monday, May 30, 2022

Page 34 of 40

At the request of Mayor Dilkens, a recorded vote is taken on this matter.

Aye votes: Councillors Costante, Bortolin, Holt, Kaschak, McKenzie, and Morrison.

Nay votes: Councillors Francis, Sleiman, Gignac, Gill, and Mayor Dilkens.

Abstain: None.

Absent: None.

Clerks File: MH/14274

Councillor Gill gives notice that he intends to introduce a motion for consideration at the June 13, 2022 meeting of Council related to train whistles.

## 16. THIRD AND FINAL READING OF THE BY-LAWS

Moved by: Councillor Holt

Seconded by: Councillor Kaschak

That the By-laws No. 77-2022 through 85-2022 having been read a first and second time be now read a third time and finally passed and that the Mayor and Clerk **BE AUTHORIZED** to sign and seal the same notwithstanding any contrary provision of the Council.

Carried.

## 17. PETITIONS

Moved by: Councillor McKenzie

Seconded by: Councillor Morrison

Decision Number: CR258/2022

That the petition presented by Councillor Morrison on behalf of the residents of the 2200 block of Roxborough Boulevard opposing the installation of sidewalks on the west side of Roxborough Boulevard **BE RECEIVED** by the Clerk and the Clerk **BE DIRECTED** to forward the petition to the Commissioner, Infrastructure Services for the purpose of an examination of the requested works or undertakings.

Carried.

Clerk's File: SW2022

## 18. QUESTION PERIOD

None registered.

# Minutes

## City Council

Monday, May 30, 2022

Page 35 of 40

### 21. ADJOURNMENT

Moved by: Councillor Costante  
Seconded by: Councillor Francis

That this Council meeting stand adjourned until the next regular meeting of Council or at the call of the Mayor.

Carried.

Accordingly, the meeting is adjourned at 5:56 o'clock p.m.

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
City Clerk

**SPECIAL MEETING OF COUNCIL – IN CAMERA**  
**May 30, 2022**

**Members have the option of participating in person or electronically and will be counted towards quorum in accordance with Procedure By-law 98-2011, as amended.**

**Meeting called to order at: 2:00 p.m.**

**Members in Attendance (in person):**

Councillor F. Francis (Acting Mayor)  
Councillor R. Bortolin  
Councillor G. Kaschak  
Councillor J. Gignac  
Councillor K. McKenzie  
Councillor J. Morrison  
Councillor E. Sleiman  
Councillor J. Gill  
Councillor F. Costante  
Councillor C. Holt

**Members Absent:**

Mayor D. Dilkens

**Also in attendance:**

S. Askin Hager, Acting Chief Administrative Officer  
D. Cercone, Acting Commissioner, Human and Health Services  
C. Nepsy, Commissioner, Infrastructure Services  
J. Mancina, Commissioner, Corporate Services CFO/City Treasurer  
D. Paladino, Acting Commissioner, Legal and Legislative Services  
R. Mensour, Commissioner, Community Services  
J. Payne, Commissioner Economic Development and Innovation  
S. Vlachodimos, City Clerk  
A. Teliszewsky, Mayor's Chief of Staff  
J. Guthrie, Deputy Treasurer  
P. Brode, Senior Legal Counsel (Items 1 and 2)  
F. Scarfone, Manager of Real Estate Services (Items 3, 4, 5, 6, 7, 8, 9, 10)  
M. Vujanovic, Senior Economic Development Officer (Item 13)

# Minutes

## City Council

Monday, May 30, 2022

Page 37 of 40

V. Mihalo, Executive Director of Human Resources (Item 16)  
N. Wolf, Manager of Employee Relations (Item 16)  
M. Nazarewich, Senior Legal Counsel (Item 11)  
J. Knights, Executive Director of Recreation and Culture (Item 9)  
K. Whittal, Executive Director of Housing and Children Services (Item 8)

**Verbal Motion is presented by Councillor Bortolin, seconded by Councillor Gill, to move in Camera for discussion of the following item(s), withdrawing Item 15 at the request of Administration:**

<b><u>Item No.</u></b>	<b><u>Subject</u></b>	<b>Section – Pursuant to Municipal Act, 2001, as amended</b>
1.	Legal/property – expropriation settlement	2392(e)(f)
2.	Legal/property – expropriation settlement	239(2)(e)(f)
3.	Property matter – sale of land	239(2)(c)
4.	Property matter – acquisition of land/expropriation	239(2)(c)
5.	Property matter – acquisition of land/expropriation – Plan B	239(2)(c)
6.	Property matter – acquisition of land/expropriation – Plan C	239(2)(c)
7.	Property matter – sale of land	239(2)(c)
8.	Property matter – lease	239(2)(c)
9.	Property matter – lease for parking	239(2)(c)
10.	Property matter - lease	239(2)(c)
11.	Legal/property matter – litigation settlement	239(2)(e)
12.	Legal matter – advice subject to solicitor-client privilege/plan	239(2)(f)(k)
13.	Property matter – economic development opportunity	239(2)(c)

# Minutes

## City Council

Monday, May 30, 2022

Page 38 of 40

14.	Legal matter – advice subject to solicitor-client privilege – <u>verbal report</u>	239(2)(f)
15.	WITHDRAWN	
16.	Personal matter – labour negotiations update – <u>verbal report</u>	239(2)(d)

**Motion Carried.**

### Declarations of Pecuniary Interest:

Councillor Bortolin declares an interest and abstains from voting on Item 8 as it pertains to the employer of a family member.

Discussion on the items of business. (Items 1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12, 13, 14 and 16)

Verbal Motion is presented by Councillor Gill, seconded by Councillor Sleiman, to move back into public session.

**Motion Carried.**

### **\*\*SEE NOTE BELOW**

Moved by Councillor Costante, seconded by Councillor Gignac,  
**THAT the Clerk BE DIRECTED to transmit the recommendation(s) contained in the report(s) discussed at the In-Camera Council Meeting held May 30, 2022 directly to Council for consideration at the next Regular Meeting.**

1. That the recommendation contained in the in-camera report from Senior Legal Counsel, Commissioner, Legal and Legislative Services and Acting Commissioner, Corporate Services CFO/City Treasurer respecting a legal/property matter – expropriation settlement **BE APPROVED.**

2. That the recommendation contained in the in-camera report from Senior Legal Counsel, Commissioner, Legal and Legislative Services and Acting Commissioner, Corporate Services CFO/City Treasurer respecting a legal/property matter – expropriation settlement **BE APPROVED.**

3. That the recommendation contained in the in-camera report from the Coordinator of Real Estate Services, Manager of Real Estate Services, Acting Commissioner, Legal and Legislative Services and Acting Commissioner, Corporate Services CFO/City Treasurer respecting a property matter – sale of land **BE APPROVED.**

# Minutes

## City Council

### Monday, May 30, 2022

Page 39 of 40

4. That the recommendation contained in the in-camera report from the Coordinator of Real Estate Services, Manager of Real Estate Services, Acting Commissioner, Legal and Legislative Services, Commissioner, Infrastructure Services and Acting Commissioner, Corporate Services CFO/City Treasurer respecting a property matter – acquisition of land/expropriation **BE APPROVED.**

5. That the recommendation contained in the in-camera report from the Coordinator of Real Estate Services, Manager of Real Estate Services, Acting Commissioner, Legal and Legislative Services, Commissioner, Infrastructure Services and Acting Commissioner, Corporate Services CFO/City Treasurer respecting a property matter – acquisition of land/expropriation – Plan B **BE APPROVED.**

6. That the recommendation contained in the in-camera report from the Coordinator of Real Estate Services, Manager of Real Estate Services, Acting Commissioner, Legal and Legislative Services, Commissioner, Infrastructure Services and Acting Commissioner, Corporate Services CFO/City Treasurer respecting a property matter – acquisition of land/expropriation – Plan C **BE APPROVED.**

7. That the recommendation contained in the in-camera report from the Coordinator of Real Estate Services, Manager of Real Estate Services, Acting Commissioner, Legal and Legislative Services, Commissioner, Infrastructure Services, Commissioner, Community Services and Acting Commissioner, Corporate Services CFO/City Treasurer respecting a property matter – sale of land **BE APPROVED.**

8. That the recommendation contained in the in-camera report from the Lease Administrator, Manager of Real Estate Services, Acting Commissioner, Legal and Legislative Services, Executive Director of Parks and Facilities, Commissioner, Community Services, Executive Director of Housing and Children Services, Acting Commissioner, Human and Health Services and Acting Commissioner, Corporate Services CFO/City Treasurer respecting a property matter – lease **BE APPROVED.**

**Councillor Bortolin discloses an interest and abstains from voting on this item**

9. That the recommendation contained in the in-camera report from the Lease Administrator, Manager of Real Estate Services, Acting Commissioner, Legal and Legislative Services, Executive Director of Recreation and Culture, Executive Director of Parks and Facilities, Commissioner, Community Services, Commissioner, Infrastructure Services and Acting Commissioner, Corporate Services CFO/City Treasurer respecting a property matter – lease for parking **BE APPROVED.**

**Councillor Kaschak voting nay**

10. That the recommendation contained in the in-camera report from the Lease Administrator, Manager of Real Estate Services, Acting Commissioner, Legal and Legislative Services, Executive Director of Parks and Facilities, Commissioner, Community Services, Commissioner, Infrastructure Services and Acting Commissioner, Corporate Services CFO/City Treasurer respecting a property matter – lease **BE APPROVED.**

# Minutes

## City Council

Monday, May 30, 2022

Page 40 of 40

11. That the recommendation contained in the in-camera report from Senior Legal Counsel, Commissioner, Legal and Legislative Services and Acting Commissioner, Corporate Services CFO/City Treasurer respecting a legal/property matter –litigation settlement **BE APPROVED**.

12. That the recommendation contained in the in-camera report from Deputy City Solicitor/Purchasing/Risk Management and Provincial Offences, Commissioner, Legal and Legislative Services and Acting Commissioner, Corporate Services CFO/City Treasurer respecting a legal matter – advice subject to solicitor-client privilege/plan **BE APPROVED**.

13. That the recommendation contained in the in-camera report from Commissioner of Economic Development and Innovation, Acting Commissioner, Legal and Legislative Services, Commissioner of Infrastructure Services and Acting Commissioner, Corporate Services CFO/City Treasurer respecting a property matter – economic development opportunity **BE APPROVED**.

**Councillors Bortolin and Holt voting nay**

14. That the confidential verbal report from Acting Chief Administrative Officer respecting a legal matter – advice subject to solicitor-client privilege **BE RECEIVED**, and further that Administration **BE AUTHORIZED TO PROCEED** on the verbal direction of Council.

15. THIS ITEM WAS WITHDRAWN AT THE REQUEST OF ADMINISTRATION.

16. That the confidential verbal report from Acting Chief Administrative Officer, Executive Director of Human Resources and Manager of Employee Relations respecting a personal matter – labour negotiations **BE RECEIVED**, and further that Administration **BE AUTHORIZED TO PROCEED** on the verbal direction of Council.

**Motion Carried.**

**Moved by Councillor Holt, seconded by Councillor Costante,  
That the special meeting of council held May 30, 2022 BE ADJOURNED.  
(Time: 3:13 p.m.)**

**Motion Carried.**

**\*\*CLERK'S NOTE: The transmittal motion and final votes were held in the open session of Council on May 30, 2022**





## Correspondence Report: CMC 9/2022

### ATTACHMENTS

**Subject: Correspondence for June 13, 2022**

No.	Sender	Subject
7.1.1.	Minister of Veterans Affairs	Letter in response to Windsor City Council's resolution to endorse the nomination of Private Jess Randall Larochelle for the award of the Canadian Victoria Cross  APR2022 Note & File
7.1.2.	Member of Parliament – Windsor West	Request for a communication from the City of Windsor to the Chair of the House of Commons Environment Committee to request scheduling the hearings on C-248 as soon as possible (An Act to amend the <i>Canada National Parks Act</i> ) regarding the Ojibway National Urban Park (NUP)  Commissioner, Community Services Commissioner, Infrastructure Services SR/14270 COUNCIL DIRECTION REQUESTED, otherwise Note & File

No.	Sender	Subject
7.1.3.	Ontario Land Tribunal	<p>Symonds v. Windsor (City) case heard February 2-3, 2022 by video hearing. Decision delivered by D. S. Colbourne on February 2, 2022, Case No. OLT-22-002012 (Formerly PL210246)</p> <p>Commissioner, Legal &amp; Legislative Services  Commissioner, Economic Development &amp; Innovation  City Planner  Chief Building Official  Deputy City Solicitor  Senior Legal Counsel  Development Applications Clerk  ZB/13918  Note &amp; File</p>
7.1.4.	Ministry of Transportation, Carrier Safety and Enforcement Branch	<p>Letter regarding the Safety Rating Upgrade  062-452-600</p> <p>Commissioner, Infrastructure Services  Commissioner, Legal &amp; Legislative Services  Executive Director, Operations/Deputy City Engineer  GP2022  Note &amp; File</p>
7.1.5.	Vice-President, Public, Government and Regulatory Affairs, Canadian National Railway Co. (CN)	<p>Letter to the Minister of Agriculture, Food and Rural Affairs confirming receipt of the March 14 letter addressed to CN CEO Tracy Robinson regarding municipal drainage matters</p> <p>Commissioner, Infrastructure Services  MT2022  Note &amp; File</p>
7.1.6.	City of Niagara Falls AND Town of Fort Erie AND Niagara Falls Bridge Commission & Buffalo and Fort Erie Public Bridge Authority	<p>Letter requesting the Government of Canada continue to market the ArriveCAN application as an “optional” tool when crossing the border</p> <p>GM2022  Note &amp; File</p>

No.	Sender	Subject
7.1.7.	Town of Tecumseh	<p>The Town of Tecumseh will hold a virtual/electronic public meeting on Tuesday, June 28, 2022 at 6:00 p.m. to consider a proposed Zoning By-law Amendment pursuant to the provisions of the <i>Planning Act, R.S.O. 1990</i> for a parcel of land situated at the southwest corner of the Lesperance Road/Dillon Drive intersection</p> <p style="text-align: right;">City Planner Commissioner, Legal &amp; Legislative Services Deputy City Solicitor Development Applications Clerk Z2022 Note &amp; File</p>
7.1.8.	Windsor Police Service	<p>Windsor Police Service Annual Report 2021</p> <p style="text-align: right;">SP2022 Note &amp; File</p>
7.1.9.	Committee of Adjustment	<p>Applications to be heard by the Committee of Adjustment/Consent Authority, Thursday, June 16, 2022 at, 3:30 p.m., through Electronic Meeting Participation</p> <p style="text-align: right;">Z2022 Note &amp; File</p>

---

**From:** Minister-Ministre (VAC/ACC) <[minister-ministre@veterans.gc.ca](mailto:minister-ministre@veterans.gc.ca)>

**Sent:** June 6, 2022 9:58 AM

**To:** clerks <[clerks@citywindsor.ca](mailto:clerks@citywindsor.ca)>

**Cc:** [DND\\_MDN@forces.gc.ca](mailto:DND_MDN@forces.gc.ca)

**Subject:** Veterans Affairs Canada / Anciens Combattants Canada

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.



Ms. Anna Ciacelli  
Deputy City Clerk/Supervisor of Council Services  
City of Windsor

Dear Ms. Ciacelli:

Thank you for your correspondence regarding Windsor City Council's resolution to endorse the nomination of Private Jess Randall Larochelle for the award of the Canadian Victoria Cross.

Windsor's interest in honouring the bravery and sacrifice of those who served our country is deeply appreciated. Generations of Canadians have served in the quest for peace, freedom and the preservation of human values worldwide, and we remain truly grateful to them.

As Mr. Shawn MacNeil references in the Royal Canadian Legion Leslie Sutherland Branch 447's request to council, Private Larochelle's courage on October 14, 2006, which earned him a Star of Military Valour, is a testament to the tremendous commitment shown by him and other members of our military who contributed to Canada's mission in Afghanistan.

The awarding of the Canadian Victoria Cross is based on the recommendation from the Canadian Forces Decorations Advisory Committee, which falls under the purview of the Minister of National Defence. Therefore, I have forwarded a copy of your correspondence to the Honourable Anita Anand, Minister of National Defence, for her awareness and consideration.

Please be assured that Veterans Affairs Canada appreciates and recognizes the contributions of all those who have served Canada and will continue to do its best to meet its numerous and varied responsibilities to these individuals.

Again, thank you for writing.

Sincerely,

Lawrence MacAulay, PC, MP

c.c.: The Honourable Anita Anand, PC, MP, Minister of National Defence

The word "Canada" in a serif font, with a small Canadian flag (red maple leaf) positioned above the letter 'a'.

*Brian Masse*

Member of Parliament  
(Windsor West)



*Windsor*

1398 Ouellette Ave. Suite 2  
Windsor ON, N8X 1J8  
Tel: (519) 255-1631  
Fax: (519) 255-7913  
www.brianmasse.ca

*Ottawa*

Room 1000 The Valour Bldg.  
House of Commons  
Ottawa ON, K1A 0A6  
Tel: (613) 996-1541  
Fax: (613) 992-5397  
brian.masse@parl.gc.ca

June 9, 2022

Honourable Drew Dilkens, Mayor of the City of Windsor  
350 City Hall Square West  
Windsor, Ontario  
Canada  
N9A 6S1

Dear Mayor Dilkens,

As you are aware, a milestone has been reached in the effort to create Ojibway National Urban Park (NUP).

Yesterday, my private members bill C-248, *An Act to amend the Canada National Parks Act (Ojibway National Urban Park of Canada)*, passed second reading in the House of Commons in a vote of 169 for the bill and 147 against. The bill had the support of the Bloc, Green Party, Conservative party, the NDP, and two liberal MPs. It will go to the House of Commons (HOC) Environment Committee for review.

In Canada, establishing a national park requires passing legislation to add the name and description of the park to a schedule in the Canada National Parks Act which is done by amending the act which C-248 does. As part of this process, the hearings at the HOC Environment Committee are an opportunity to showcase the rare environmental gem that is the Ojibway Prairie Complex for the whole country to begin to appreciate.

With the support members of parliament from across all parties to create Ojibway NUP as demonstrated in yesterday's vote, the momentum to keep the bill moving forward and pass the future stages must be seized upon.

Accordingly, as with other supporters of Ojibway NUP from Caldwell First Nation to the Wildlands League, many local, national, and international environmental organizations, scientists, educational research institutes, and relevant members of the tourism sector will be invited to appear at the HOC Environment Committee, the City of Windsor's participation in the hearings is essential, necessary, and definitive.

While the City has been direct, vigilant, and persistent on the urgency to establish Ojibway NUP, I am requesting a specific communication from the City to the Chair of the HOC Environment Committee to schedule the hearings on C-248 as soon as possible.

I thank you in advance for your time and attention to this matter.

Sincerely,

A handwritten signature in blue ink that reads "Brian Masse". The signature is written in a cursive style with a horizontal line underneath.

Brian Masse MP

Windsor West

NDP Innovation, International Trade, Economic Development, and Great Lakes Critic

**Ontario Land Tribunal**  
Tribunal ontarien de l'aménagement  
du territoire



**ISSUE DATE:** May 30, 2022

**CASE NO(S):** OLT-22-002012  
(Formerly PL210246)

**PROCEEDING COMMENCED UNDER** subsection 34(19) of the *Planning Act*, R.S.O. 1990, c. P.13, as amended

Appellant:	Veronica Symonds
Subject:	By-law No. BL 33-2021
Municipality:	City of Windsor
OLT Case No.:	OLT-22-002012
Legacy Case No.:	PL210246
OLT File No.:	OLT-22-002012
Legacy File No.:	PL210246
OLT Case Name:	Symonds v. Windsor (City)

**Heard:** February 2-3, 2022 by video hearing

**APPEARANCES:**

**Parties**

**Counsel\*/Representative**

Veronica Symonds

B. Gagnier

City of Windsor

W. Vendrasco\*

2516053 Ontario Inc.

E. Hooker\*

**MEMORANDUM OF ORAL DECISION DELIVERED D.S. COLBOURNE  
ON FEBRUARY 2, 2022 AND ORDER OF THE TRIBUNAL**

---

**INTRODUCTION**

[1] The City of Windsor ("City") passed a Zoning By-law Amendment (the "ZBA") to allow the development of four, three storey condominium buildings with 96 units at 3129



Lauzon Road. Veronica Symonds, an immediate neighbour appealed the City's passing of the ZBA to the Tribunal.

[2] The neighbourhood map attached to the witness statement of James Abbs clearly sets the context. The lands are 1.52 hectares ("ha"), in the area which is currently vacant, previously occupied by a single family dwelling. This is the largest of the eight long narrow lots on the west side of Lauzon Road, this being at least three times the width of the others, as well as those of similar size on the east side of Lauzon Road, facing the subject site. There are only these eight properties on the west side of Lauzon Road, north of which a golf course fronts the west side of the road.

[3] The properties on the west side, the subject and the six to the north and the one to the south, back onto the Little River Municipal Golf Course. Central to the Course is the Little River. To the west of the golf course is multiple residential development fronting on both sides of Meadowbrook Lane.

[4] To the south is a single family dwelling and south of that, situate at the south west corner of Lauzon Road, and Forest Glade Drive, is a Salvation Army Church. On the opposite corner is the Eastwood Public School and north of that on the east side of Lauzon Road, are the aforementioned 10 single family lots. The lots north and south of the subject lands and the 10 on the east side north from the school north are all narrow lots, especially those on the east side although and they are fairly long.

[5] Development east and surrounding those 10 lots on the east side of Lauzon Road is a large area of single family dwellings in subdivision formed of later eras, with all of those lots certainly smaller in size than this enclave of long narrow lots.

[6] Mr. Abbs, Planner for the municipality, recommended the zoning change from RD1.1 to Residential (RD) 2.5 to allow 96 units with site specific regulations. Those regulations are; - Lot area 15,000 square metres ("m<sup>2</sup>"), and for Interior Side Yard- where a habitable room window of any dwelling unit faces a side lot line – minimum side yard of 20 metres ("m").

[7] At the Development and Heritage Standing Committee meeting (“DHSC”), the residents expressed concerns over the development generally as well the height proposed. That committee recommended that the height of the development should be capped at 12 m. (2 m. higher than the surrounding single family development) as opposed to the 18 m. permitted in the zoning category proposed. This proposal was approved by Council on March 8, 2021.

[8] Ms. Gagnier as representative of the Appellant called and examined the Appellant and some of her neighbours. In addition to the concerns of her neighbours, the Appellant is concerned about the decreasing wildlife she observes which she says is due to the development of the multiple dwellings west of the golf course. She expects it will be exacerbated by the proposed development. She is also concerned about the density, the overlook and impact on her very natural space.

[9] Her neighbours, Nancy Tomika, Proulx, including those residing on the east side of Lauzon Road, were called and expressed their concerns on the impact on “flooding” and traffic.

[10] Ms. Gagnier, as Representative of the Appellant argued her views of the provisions of the Official Plan, verbally and practically in her argument by underlining the sections in issue. She viewed these as being important and concluded they were not appropriately considered by the planners, the committee, and especially not council.

[11] Quoting in part from her argument on her views of the Official Plan:

The first thing I would like to do is give you a visual view of the area in question. I would ask permission to show Google Earth to show the entire area. This, I believe, will give you a clear look showing this area as residential. I will also do some measurements so you can clearly see how far away “multiple dwelling buildings are located” and I think you will appreciate they are nowhere near this area as the city and developer argue. When it is referred to as “surrounding area”, you will see this is not so. “This proposed development is definitely inconsistent with the surrounding homes and area. today.

It is very clear that the City, when developing their Official Plan, were very concerned about massing, complementary surroundings, gradual

transition in heights of buildings and setbacks, to state a few but in this instance, and I will point out that in this case, and in many other instances, the City disregarded their own plan.

I would just like to reiterate, we are opposed to the changing of the zoning and the exemption from bylaw 103-2020 on the following grounds:

The by-law (interim) was set in place so the City could consider their future plans and ensure developments would not be in conflict with that eventual plan. Since the plan is not completed, it is impossible to know whether this will be in conflict or not. Therefore, this should not be built. In addition, the mere scale, massing, height, siting, orientation, setback, and parking is not compatible for the area. These buildings would not provide a gradual transition from low to medium profile buildings. Traffic is a concern. The need to protect trees and wildlife and ability to provide a suitable answer for the current and future flooding has not been finalized. These buildings are not complementary in any way to the surrounding area and the development of this area is not consistent with the City's own Official Plan as shown on Schedule H.

[12] Although she ventured into the area of future applications on the site presuming refusal of this proposal which are beyond my consideration. "Our request of this Board is to have the decision of the City council overturned both with respect to the rezoning and the exemption under Bylaw 103-2020. Both the rezoning and exemption should be denied.

[13] Mr. Abbs, Planner for the municipality, who originally reviewed the application, and who was qualified by the Tribunal to provide expert land use planning evidence had recommended changes and prepared his report for both the DHSC and Council supporting the development with the special controls.

[14] In his witness statement Mr. Abbs, deals with the issue of compatibility as described in the O.P. 6.3.2.5. (one of the OP sections on the Issues List).

At the time of submission, the proponent shall demonstrate to the satisfaction of the Municipality that a proposed residential development within an area having a Neighbourhood development pattern is:

(c) compatible with the surrounding area in terms of scale, massing, height, siting, orientation, setbacks, parking and amenity areas;

[15] As opposed to the neighbourhood selected by Ms. Gagnier, the 8 plus 10 lots on

Lauzon Road and all of the extensive area of single family to the east and surrounding those lots, the planner for the City selects as follows:

Mr. Abbs concludes: "Using the definition in Section 6.1.2.3 OP, the Neighbourhood area of the Subject Lands is illustrated by a broken black line on Exhibit "G" attached to his affidavit. **The boundary is defined by Lauzon Road to the east, Forest Glade Drive to the South, Lauzon Parkway to the west and Hawthorne Drive to the North**".

"Although the development pattern for the **immediately surrounding** development is characterized by large, long parcels fronting on the east and west sides of Lauzon Road

To the west of the golf course which is west of the lots on the west side of Lauzon Rd. there are large square or rectangular parcels containing high rise development fronting both sides of Meadowbrook Ln., some of which is of recent development.

The remainder of the existing Neighbourhood development is composed of a mix of Low, Medium and High

[16] Profile Multiple Unit Dwellings are located, west of the Little River on Meadowbrook Lane and along Hawthorne Drive. Two large institutional uses are south of the subject occupying both corners of Forest Glade and Lauzon Road. Most prominent is the large municipally owned recreational area (golf course) occupying the area immediately to the west of the Subject Lands.

Therefore, the Development pattern has a predominant characteristic lotting pattern of large parcels and a mix of development profiles.

**By contrast to the east of the Subject Lands on the east side of Lauzon Road, the predominant development pattern is low profile single unit buildings and large institutional buildings.**

In my opinion a mix of development profiles and land uses already co-exist within the Neighbourhood. ....

Compatible does not mean the proposed development needs to be identical to or even similar to the existing development. A development should be able to coexist with existing land uses. To assist in review, the OP sets out criteria to be considered while evaluating a new development application. Each of these criteria is to be considered within the context of the existing development. That context is identified in the OP as the Neighbourhood.

Large buildings with parking areas already co-exist and are compatible with single unit dwellings in this Neighbourhood. There is a single unit dwelling (3151 Lauzon Road) immediately south of the Subject lands.

Immediately south and abutting 3151 Lauzon Road is a substantial existing institutional church use. This church is located 20m south of the interior lot line of 3151 Lauzon Road.

In my opinion a mix of development profiles and land uses already co-exist within the Neighbourhood. This illustrates that a development containing the proposed use with the regulations contained in By-law 33-2021 can co-exist with other development in the Neighbourhood and is therefore compatible.

[17] Mr. Abbas also deals with the issue of height in the OP as guided by Section 6.2.1.2, which defines the concept of development profile as follows:

- For the purpose of this Plan, Development Profile refers to the height of a building or structure. Accordingly, the following Development Profiles apply to all land use designations on Schedule D: Land Use unless specifically provided elsewhere in this Plan:
- Low Profile developments are buildings or structures generally no greater than three (3) storeys in height;
- Section 6.2.1.2 OP defines both the existing single unit dwellings at a zoning bylaw permitted height of 10m or two stories, and the proposed Multiple Unit building at 12m height, or three stories, as specified in By-law 33-2021 as Low Profile Development. In my opinion the height of the proposed development and existing development are compatible with respect to height and conform to the OP.

[18] He also reviews scale in the OP as defined by Section 6.3.2.3. For the purposes of this Plan, Low Profile housing development is further classified as follows: small scale forms: single detached, semi-detached, duplex and row and multiplexes with up to 8 units; and large scale forms: buildings with more than 8 units..”

[19] In the opinion of Mr. Abbs, the OP contemplates that low profile development may have more than 8 units. That is what this Application contemplates. In this respect the Application conforms to the OP with respect to Scale. The ultimate size of this

development is limited by the regulations contained in By-law No. 33-2021 to guide the Applicant in their creation of a development for this site. The massing and scale of the proposed development are generally dealt with through the Site Plan Control Process.

I reviewed and considered each of these parameters when I prepared the Report and my Recommendations for that Report. In my opinion each of these parameters conform to the policy direction in the OP. The final layout of the development in compliance with the provisions of the Zoning By-law will be determined through the Site Plan Control Process.

[20] On parking any development on the Subject Lands will have to comply with the city wide standard parking provisions.

It is my professional opinion that 6.3.2.5(f), 6.3.2.6(a)(iv), 8.7.2.1 (a), 8.7.2.2(f), and 8.7.2.4 of the OP as identified in the Applicants Issues list of the Procedural Order are not applicable to the Subject Lands.

[21] Mr. Abbs exhaustively dealt with every remaining OP provision applicable to these circumstances, not previously dealt above and either concluded the provision(s) do not apply or indeed support the enactment of the by-law.

By-law No. 33-2021 in my professional opinion conforms to Sections 6.3.2.5(c), 6.3.2.5(a)(iv), 7.3.4.7(c), 8.5.2.7, and 8.7.2.3, of the OP as identified in the Applicants Issues list of the Procedural Order, that By-law 33-2021 represents good land use planning, and that the Appeal by Appellant should be dismissed.

[22] The challenge to Council's ability to amend interim control by-law No. 8600 in my view was satisfactorily dealt with by Mr. Abbs reference to the exemption for by-laws passed after January 1, 2017. Melanie Muir, Planner for the Applicant concurs that the exemption is appropriate and that it will not prejudice the outcome of the study contemplated by Interim Control By-law No. 103-2020.

[23] Ms. Muir, an Associate with Dillon Consulting Limited ("Dillon"), is a Registered Professional Planner and a Member of the Canadian Institute of Planners. She has over twenty-one (21) years of experience in land use planning. She was qualified by the Tribunal as a land use planner and gave evidence on behalf of the Applicant.

[24] Her witness statement and testimony covered the OP issues of concern to the Appellant and although not an issue dealt by any of the Appellant's witnesses she dealt exhaustively with the PPS.

[25] Based on her evidence and opinion, without contrary opinion evidence, I accept her conclusion on the PPS.

[26] With respect to the OP, she points out that much of the compatibility will be considered as part of the Site Plan Control, having regard to such matters as materials, landscaping, **scaled amenities**, and the appropriate orientation of the site layout. As well, Stage 1 and 2 Archeological Assessments have been carried out and a clearance letter is required and is expected from Essex Region Conservation Authority ("ERCA") before development. She describes the development as complementary to the neighbourhood, and considering the adjoining development (both north and south), accommodates it with reference to the reduced height, the matter of habitable windows, side yards and the use of materials.

It is my professional planning opinion is that the Zoning By-law Amendment before the OLT to amend the Zoning By-law from RDI.1 to a site specific RD2.5 constitutes "good land use planning" and is in the public interest for the reason identified above and for the following reasons:

It provides an opportunity to create a range of housing types on an underutilized property;

The proposed development conforms to policies found in the City of Windsor OP including, without limitation, all of those sections raised in the Issues List as they may relate to: Massing; Building Height ("...facilitating a gradual transition from low profile residential development to Medium and/or high profile development."); Volumes of Defined Space — as it relates to storm water management and risk of flooding; Position Traffic Intensity; Identified Issues with Current Flooding.

Ms. Muir's final conclusion is that:

"The site is suitable to allow for the type of development contemplated by the zoning by-law such as the construction of four (4) apartment buildings (96 units in total) with associated parking areas in the concept plan provided during the application process; allows for the development of a range of tenure and affordable housing units; The property is generally flat and fronts a municipal roadway;

The lot is generally rectangular in shape and has appropriate dimensions for the proposed use.

## **Traffic**

[27] As Ms. Muir describes, Lauzon Road is designated as a Class 1 Collector Road intended to carry high volumes of traffic. In terms of its function, beyond the opposition opinion that it was busy, the only other evidence tendered was that the present flow is comfortable. The fact is that the Transportation Department of the City was not concerned with the impacts this development would have on this Road and required no study as to impact. As of now there is a single point of access proposed for the development with one lane each way. Obviously, it will be busier than a single family driveway.

## **“Flooding” and the Little River:**

[28] The Appellant and her witnesses, even those residing on the east side of Lauzon Road, raised the issue of backyard “flooding” and suggested that the development would suffer the same results and could exacerbate the situation for the other residents and would obviously be in the flood plain.

[29] Ms. Gagnier the Appellant’s representative said that ERCA in the Municipal circulation of this application had asked that consideration of the application be delayed until the Floodplain mapping for the Little River, currently underway, came into force. This was not acted upon by the municipality. Nor did ERCA raise any subsequent concerns, when the municipality proceeded.

[30] Actually, ERCA’s final recommendation contained within the Planning Report documentation dated Oct 2, 2020 is as follows:

ERCA has no concerns with the proposed rezoning application as it relates the specific amendment for a modification to the maximum height of dwellings in this zone, ERCA is supportive of the approach to recognize the ongoing nature of the study to assess the extent of the floodplain and associated floodway setback at this location and proceed



with the rezoning but wait for the result of the Little River Floodplain Study ( to be completed by Dillon) prior to proceeding with detailed site plan control. ERCA expresses caution and recommends that additional confirmation about whether natural heritage policies of the PPS 2018 Scoped Environmental Impact Assessment which ERCA has not had a chance to review.

[31] Ryan Langlois is a Senior Water Resources Engineer with Dillon Consulting and was retained by the Applicant to provide opinion evidence with respect to the issue of “flooding” raised by the Appellant and her other witnesses and his opinion of the development in relation to the river.

[32] Ms. Gagnier also raised the fact that Dillon Consulting was retained by the Windsor /Essex Region Conservation Authority to chart the Floodplain mapping for the Little River. She suggested this as a conflict of interest with his appearing on behalf of the Applicant in this matter. In my opinion, I fail to appreciate that the firm’s professional work, or of his in respect of this waterway, would be altered in any way for one property owner in the whole of the system, especially since, according to Mr. Langlois, the Floodplain Mapping for ERCA has been completed and has received ERCA’s approval, but not yet in force.

[33] Mr. Langlois concludes:

In my opinion, the development property is able to provide a sufficient stormwater management strategy to meet the current regional and municipal standards set forth within the Windsor/Essex Region Stormwater Management Standards Manual (ERCA, December 2018) and provincial Stormwater Management Planning and Design Manual (MOE, 2003) requirements for regional and provincial approvals;

In my opinion, based on the ERCA approved technical results from the Regulated Little River Floodplain Mapping update results shown, the development property is considered to be outside of the Primary Floodway and within the Secondary Flood Fringe area where development is permitted;

In my opinion, the development property can meet the floodproofing measures outlined within "4.2" that is required for future regional and provincial approvals. The approval of the requested zoning (and the subsequent development accommodated thereby) in advance of formal completion and adoption of the updated floodplain mapping study by ERCA and the City for the Little River area, and is not premature at this time, as the technical component of the study has been completed and

approved by ERCA;

In my opinion, the development property is not expected to adversely impact adjacent properties from the flood line perspective under a future development condition.

Based on review of the City of Windsor Sewer and Coastal Flood Protection Master Plan (Dillon, 2020) and a review of the currently proposed stormwater management strategy for the development property, it is my opinion that the existing storm sewers along Lauzon Road at this location are adequately sized to accommodate the allowable release rate established.

In my opinion, the existing municipal storm conveyance system will continue, under a re-developed property condition, provide a standard level of conveyance during frequent storm events with an appropriate overland flow route along Lauzon Road for major storm events.

In my opinion, the development property under a future condition will not cause any adverse impacts on the surface ponding experienced along Lauzon Road and along existing residences property.

In my opinion, the approval of the requested zoning will not create adverse impacts to the area with respect to flooding, notwithstanding the concerns of local residents. The development can and will provide appropriate stormwater management controls and floodproofing measures, as approved through the normal site plan approval and permitting processes.

[34] He further states that with respect to Sec. 7.3.4.7 of the OP:

Council shall require proponents of development that require stormwater management systems to:

(c) Consider, where appropriate, enhancing the vegetation, wildlife habitats and corridors in and along the stormwater management system and the receiving watercourse."

In my opinion, the development property can satisfy this Official Plan requirement by abiding by the required 15 m drain setback (which will remain vegetated) to be established by ERCA from the Little River as part of any approval obtained through the normal site plan approval and permitting processes. Stormwater Management for the site will be primarily underground with no stormwater pond features on the surface to vegetate.

[35] While I understand that the appearance of wildlife is appreciated by the residents, I do not accept that the river, its banks and the golf course and the adjacent multiple development can possibly be a significant wildlife preserve given the fact of the

continuing urban development on the other side the river. Nor do I accept that the subject development will in anyway significantly affect the intermittent and diminishing appearances of wildlife already experienced by these few residents.”

[36] The issue of “flooding” as described by the neighbours, is, given the evidence of Mr. Langlois, not an issue and in terms of any impact, this development can be managed on site with appropriate engineering as already proposed and to be further developed at the Site Plan stage. ERCA’s natural heritage concerns will also be settled at that time and they would have to issue the appropriate certificate.

[37] There is no question that the cluster of large size lots of the immediate development and that to the east is all single family. The lot sizes in this cluster on both sides of Lauzon Road are large and lengthy and provide a very open setting for these few single family residences. No evidence was provided to give the background of their creation, or how the subject property was, or became, three times the width of these adjoining lots, given the surrounding development.

[38] The issue here is “what is the neighbourhood”. The views are divergent, poles opposite in character.

[39] Ms. Garnier gathers the cluster of these few large lots on both sides of Lauzon Road, around the subject, and continues east and northeast into the normal sized residential lot areas all east of Lauzon Road, to create the single family residential neighbourhood.

[40] The Planning witnesses both for the City and the Applicant take the normal block approach to include Lauzon Road, Lauzon Parkway, Forest Glade Drive and Hawthorne Drive, which contains this enclave of a few single lots on the west side of Lauzon Road, the church to the south, the river and the golf club in the middle, and multi density blocks to the west.

[41] The nature of Lauzon Road is of assistance in that it not a minor residential

street. The residents described it as busy, but according to Ms. Muir, for its classification she is comfortable in concluding that Lauzon Road, would not be impacted by this proposal, and is expected to carry greater traffic, for its classification and capacity.

[42] Lauzon Road to me, becomes the dividing line between neighbourhoods, which is clear with reference to Exhibit G to Mr. Abbs' witness statement. The neighbourhood is that as described by Mr. Abbs and Ms. Muir, and it is mixed development over it's breadth and it remains for the Site Plan Control Process, to assist in moderating the changes between the singles and the development.

[43] For all of the reasons indicated, and upon the whole of the uncontroverted planning evidence before the Tribunal, I dismiss the appeal and confirm the decision of Council.

*"D.S. Colbourne"*

D.S. COLBOURNE  
MEMBER

**Ontario Land Tribunal**

Website: [olt.gov.on.ca](http://olt.gov.on.ca) Telephone: 416-212-6349 Toll Free: 1-866-448-2248

The Conservation Review Board, the Environmental Review Tribunal, the Local Planning Appeal Tribunal and the Mining and Lands Tribunal are amalgamated and continued as the Ontario Land Tribunal ("Tribunal"). Any reference to the preceding tribunals or the former Ontario Municipal Board is deemed to be a reference to the Tribunal.

**Item No. 7.1.4.**

2022/03/25

THE CORP OF THE CITY OF WINDSOR  
1266 MCDUGALL STREET  
WINDSOR ON N8X3M7

ATTENTION: CHRISTOPHER NEPSZY  
CVOR #: 062-452-600

**Safety Rating**

Section 17.1(1) of the *Highway Traffic Act* requires the Registrar of Motor Vehicles to assign a safety rating to every commercial motor vehicle operator using Ontario's roadways. In accordance with this Section and based on your safety record, the following safety rating has been proposed for your operation:

Satisfactory Un-audited

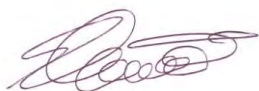
If you wish to dispute the proposed rating noted above, you may do so by making a written submission, including the reasons for your dispute, to the Registrar of Motor Vehicles within 30 days. Your written submission should be directed to:

Carrier Sanctions and  
Investigation Office  
301 St. Paul Street, 3rd Floor  
St. Catharines ON L2R 7R4

If you do not dispute your proposed rating, it will be assigned and will be available to the public as part of your commercial vehicle operator record.

Your CVOR safety record provides a clear picture of your overall safety performance. As a CVOR holder, you are required to monitor your CVOR safety record regularly. Please visit [www.ontario.ca/carrierrecords](http://www.ontario.ca/carrierrecords) to view and monitor your CVOR safety record information.

Please contact our office if you have any questions.



**Alfred Sean Doussept**  
Deputy Registrar of Motor Vehicles



[www.cn.ca](http://www.cn.ca)

**Corporate Services****Cyrus Reporter**

Vice-President, Public, Government  
and Regulatory Affairs

935 de La Gauchetière Street West  
Montreal, Quebec H3B 2M9  
Canada  
T 514-399-6588

**Services corporatifs**

Vice-président, Affaires publiques,  
gouvernementales et réglementaires

935, rue de la Gauchetière ouest  
Montréal (Québec) H3B 2M9  
Canada  
T 514-399-6588

May 26, 2022

Hon. Lisa M. Thompson  
Minister of Agriculture, Food and Rural Affairs  
Office of the Minister  
77 Grenville Street, 11th Floor  
Toronto, ON M7A 1B3

Dear Minister Thompson:

By way of this letter, I would like to confirm receipt of your March 14 letter addressed to CN CEO Tracy Robinson regarding municipal drainage matters. CN takes the matter of drainage seriously across our entire network. Managing waterflow in proximity to the right of way is an important priority for any railway to ensure the safe movement of the goods they transport.

Utility crossings, over or under CN's right of way, are governed by the Canadian Transportation Act which has established processes to determine cost sharing pertaining to the construction or maintenance of those utility crossings. We recognize this may conflict with the Ontario Drainage Act resulting in an impasse between railways in the province and some of the municipalities through which they operate.

Discussions regarding drainage infrastructure between municipalities and the railways have successfully taken place in Ontario for a long period of time under the scope of the Canada Transportation Act and its predecessor statutes.

CN has been in dialogue with the Association of Municipalities of Ontario (AMO), the Ontario Good Roads Association (OGRA) and with various municipal officials and staff as well as with the Province of Ontario. We remain confident that this matter can be resolved to the satisfaction of all.

The Railway Association of Canada (RAC), representing all Class 1 and Short Line railroads in Canada, has also reached out to AMO to begin a dialogue on municipal drains. We feel that conducting this dialogue through RAC and AMO, with the opportunity for the province of Ontario to be involved, would be prudent provided it establishes a forum involving all affected parties.

Hon. Lisa M. Thompson

May 26, 2022

Page 2

CN remains committed to dialogue with our neighbours on this important safety issue and will be engaged in dialogue involving RAC, the Province and AMO. Consistent with our existing practice, we also want to confirm that CN will fully collaborate with municipalities for any emergency work required while the working group completes its discussions.

It is our hope that through dialogue and mutual collaboration that this matter will be resolved.

Sincerely,

A handwritten signature in black ink, appearing to be 'C. Reporter', written in a cursive style.

Cyrus Reporter



## Community Services

### Legislative Services

May 31, 2022  
File #120203

Public Health Agency of Canada  
130 Colonnade Road  
A.L. 6501H  
Ottawa, ON K1A 0K9

To Whom it May Concern:

**Re: City of Niagara Falls Resolution – Cross Border Travel and ArriveCAN**

Please be advised the Municipal Council of the Town of Fort Erie at its meeting of May 30, 2022 received and supported correspondence from the City of Niagara Falls dated May 16, 2022 requesting the Government of Canada to continue to market the ArriveCAN application as an “optional” tool when crossing the border and it no longer be a mandatory requirement.

Attached please find a copy of the City of Niagara Falls’ correspondence dated May 16, 2022.

Thank you for your attention to this matter.

Yours very truly,

Carol Schofield, Dipl.M.A.  
Manager, Legislative Services/Clerk

[cschofield@forterie.ca](mailto:cschofield@forterie.ca)

CS:dlk

Attach

c.c. Tony Baldinelli, MP, Niagara Falls [tony.baldinelli@parl.gc.ca](mailto:tony.baldinelli@parl.gc.ca)  
Wayne Gates, MPP, Niagara Falls [wgates-co@ndp.on.ca](mailto:wgates-co@ndp.on.ca)  
Mayor Mike Bradley, City of Sarnia [mike.bradley@sarnia.ca](mailto:mike.bradley@sarnia.ca)  
Drew Dikens, Mayor, City of Windsor [mayoro@citywindsor.ca](mailto:mayoro@citywindsor.ca)  
Local Area Municipalities

---

Mailing Address:

The Corporation of the Town of Fort Erie  
1 Municipal Centre Drive, Fort Erie ON L2A 2S6

Office Hours 8:30 a.m. to 4:30 p.m.

Phone: (905) 871-1600 FAX: (905) 871-4022

Web-site: [www.forterie.ca](http://www.forterie.ca)





May 16, 2022

Public Health Agency of Canada  
130 Colonnade Road  
A.L. 650 1H  
Ottawa, ON K1A 0K9

To Whom It May Concern:

**Re: Cross Border Travel and ArriveCAN**

Please be advised that at the May 10, 2022 meeting of Niagara Falls City Council, the following motion was passed:

**Ordered on the motion of Councillor Vince Kerrio, Seconded by Councillor Chris Dabrowski that Council endorse the letter received from the Niagara Falls Bridge Commission and the Buffalo and Fort Erie Bridge Authority to ask the Government of Canada to continue to market the ArriveCAN application as an "optional" tool when crossing the border and that it no longer be a mandatory requirement; And that a copy of the letter and Council's motion be forwarded to the Public Health Agency of Canada, bordering municipalities across Ontario, Members of Parliament for Niagara and Local Area Municipalities in Niagara for support.**

The background to the above motion is relating to the current mandatory ArriveCAN application at border crossings to enter Canada. A copy of the letter from the Niagara Falls Bridge Commission and the Buffalo and Fort Erie Bridge Authority is attached.

If you have any questions, please contact me directly.

Sincerely,

William G. Matson  
City Clerk

cc.

Tony Baldinelli, MP, Niagara Falls [tony.baldinelli@parl.gc.ca](mailto:tony.baldinelli@parl.gc.ca)  
Wayne Gates, MPP, [wgates-co@ndp.on.ca](mailto:wgates-co@ndp.on.ca)  
City of Windsor, [mayoro@citywindsor.ca](mailto:mayoro@citywindsor.ca)  
City of Sarnia, [mayor@sarnia.ca](mailto:mayor@sarnia.ca)  
Local Area Municipalities

**RECEIVED**

**MAY 30 2022**

**BY COUNCIL**

***Working Together to Serve Our Community***

Clerks  
Ext 4342 Fax 905-356-9083  
[billmatson@niagarafalls.ca](mailto:billmatson@niagarafalls.ca)



May 10, 2022

**VIA EMAIL**

Mayor Jim Diodati and Councillors  
City of Niagara Falls  
4310 Queen Street  
Niagara Falls Ontario  
L2E 6X5

Dear Mayor Diodati and Councillors

**Re: Cross Border Travel and ArriveCan**

The Covid-19 pandemic has decimated cross border traffic. Even after the Covid testing requirements to enter Canada were lifted on April 1, 2022, auto traffic for the month of April was down 52% at the bridges of the Niagara Falls Bridge Commission and 43% at the Peace Bridge, compared to pre-pandemic April 2019. This has had a devastating impact on our toll revenues, the tourism industry and other border dependent businesses.

The Government of Canada's **mandatory** ArriveCan app to enter Canada is a major contributing factor to depressing discretionary travel. This is due to a number of factors:

- For travelers by air, airlines require and assist passengers in completing ArriveCan and in airports people are much easier to move than cars. ArriveCan simply works better and is more applicable at an airport than at a land port of entry.
- ArriveCan is not known in the U.S. The result is people arriving at the border without ArriveCan and then completing it at the inspection booth or having technical difficulties (especially prevalent at borders with competing U.S. and Canadian cell towers and roaming) resulting in inordinately long processing times, sometime exceeding ten minutes. Meanwhile, compliant drivers behind such a vehicle have no place to go and are also delayed leading to lengthy border wait times. CBSA processing times have increased dramatically compared to pre-pandemic processing.
- The technology discriminates against a large population of senior travelers. A January 2022 PEW Research survey found *"that 96% of those ages 18 to 29 own a smartphone compared with 61% of those 65 and older, a 35 percentage point difference."* 50% of Niagara tourism revenue comes from Americans and with 39% of Americans over 65 not having a smartphone it will undoubtedly have a negative impact on their ability to enter Canada and therefore Niagara tourism revenue. 40,000 people in the Niagara Region rely on tourism to provide for their families.

- Even for people aware of ArriveCan, the requirement to use the app for every border crossing is a disincentive to discretionary travel. Unlike air travel, the nature of this bi-national community is being able to cross frequently to visit friends or family, for dinner, a show, a winery tour, a baseball game, shopping, beach, etc without going through the process and inconvenience of having to file with ArriveCan each and every time. People just won't bother to cross the border for discretionary travel as they have enjoyed for decades.
- CBSA/Government of Canada will at some point declare success and say that a high percentage of travelers are using ArriveCan. That will **not** account for the half of the population that has decided it is no longer worth the inconvenience and delays to cross the border. That will be devastating to the Niagara tourism industry and other border dependent businesses.
- While CBSA has not provided us with the American/Canadian split of entrants to Canada, we believe, based on our license plate observations, that it is overwhelmingly Canadians returning to Canada after spending time and money in the U.S., as the U.S. is much simpler and easier to enter with no digital app requirement.

ArriveCan was developed in April 2020, to provide mandatory travel information before and after entry to Canada including the filing of quarantine plans. As of April 25, 2022 it is no longer necessary to file a quarantine plan to visit Canada or for Canadians returning to Canada. Both Canada and the U.S. only allow vaccinated individuals to cross the border. The exception is Canadians who are able to return to Canada unvaccinated but at this stage that number would be very small. Accordingly, ArriveCan now provides very little public health benefit while depressing border traffic and the tourism economy. ArriveCan does have potential traffic facilitation benefits for CBSA as it relates to pre-arrival information and for this reason it should be marketed as such and made optional at the land border.

The Victoria Day and Memorial Day holidays are only couple of weeks away. This is also the traditional start of the summer tourist season. Continuing to mandate ArriveCan will insure another disappointing season for traffic and tourism, not much better than the pandemic summers of 2020 and 2021.

Yours truly,



Kenneth Bieger  
CEO  
Niagara Falls Bridge Commission



Ron Rienas  
General Manager  
Buffalo and Fort Erie Public Bridge Authority

# TOWN OF TECUMSEH NOTICE OF VIRTUAL/ELECTRONIC PUBLIC MEETING PROPOSED ZONING BY-LAW AMENDMENT

**TAKE NOTICE** that the Council of the Corporation of the Town of Tecumseh will hold a virtual/electronic public meeting **Tuesday, June 28, 2022 at 6:00 p.m.** to consider a proposed Zoning By-law amendment pursuant to the provisions of the *Planning Act, R.S.O. 1990*. Please see below for information on how to provide comments or participate in the virtual/electronic public meeting.

A Zoning By-law amendment application (*Application/File: D19 205LES*) has been submitted for a 0.38 hectare (0.94 acre) parcel of land situated at the southwest corner of the Lesperance Road/Dillon Drive intersection (205-227 Lesperance Road) (see Key Map below for location). The purpose of the application is to amend Zoning By-law 1746 by rezoning the subject property from "Residential Zone 3 (R3)" to a site-specific "Residential Zone 3" in order to facilitate the construction of a residential development consisting of four, 2.5-storey, six-unit dwellings and establish site-specific lot, building and yard provisions. The subject property is designated "Residential" in the Tecumseh Official Plan.

**ANY PERSON** may attend the virtual public meeting and/or make written or verbal representation either in support of or in opposition to the proposed Zoning By-law amendment.

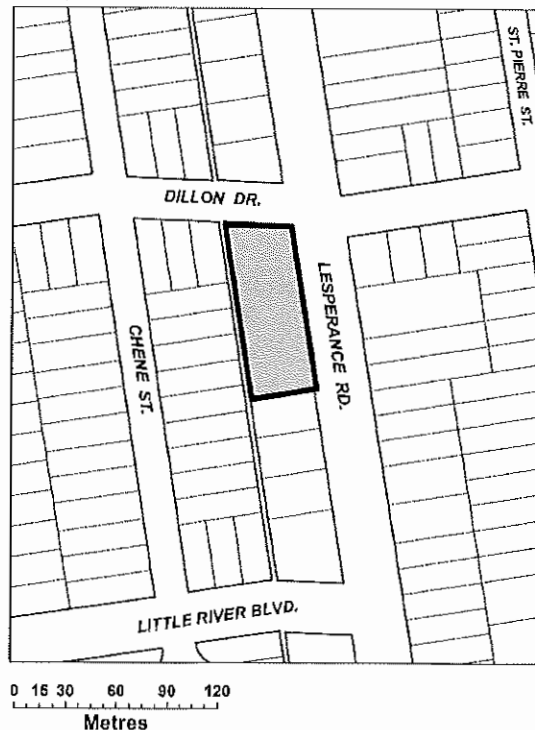
If a person or public body does not make oral submissions at a public meeting or make written submissions to the Corporation of the Town of Tecumseh before the Zoning By-law amendment is passed, the person or public body is not entitled to appeal the decision of the Council of the Corporation of the Town of Tecumseh to the Ontario Land Tribunal.


If a person or public body does not make oral submissions at a public meeting, or make written submissions to the Corporation of the Town of Tecumseh before the Zoning By-law amendment is passed, the person or public body may not be added as a party to the hearing of an appeal before the Ontario Land Tribunal unless, in the opinion of the Ontario Land Tribunal, there are reasonable grounds to do so.

## How to Provide Comments or Participate in an Electronic Public Meeting

Due to the COVID-19 declared emergency, Public Meetings are being held virtually through electronic means and live-streamed on the Town's website. Any person who wishes to make oral submissions during the electronic Public Meeting **must register as a delegate**. To register as a delegate, persons may use the online registration form on the Town's website at [www.tecumseh.ca/delegations](http://www.tecumseh.ca/delegations) or by providing their contact information (name, address, email and phone number) to the Town Clerk by **12:00 p.m. (noon) on Tuesday, June 28<sup>th</sup>, 2022**. Registered delegates will receive the necessary log-in details to the meeting. By registering as a delegate persons are consenting to their image, voice and comments being recorded and available for public viewing on the Town's website. **Written submissions** may also be provided to the Clerk by **12:00 p.m. (noon) on Thursday, June 23<sup>rd</sup>, 2022**.

KEY MAP



 Lands Subject to Proposed Amendment

If you wish to be notified of the decision of the Council of the Corporation of Town of Tecumseh on the proposed Zoning By-law amendment, you **must** make a written request to the Corporation of the Town of Tecumseh, c/o Laura Moy, Clerk, at the mailing address noted below.

**ADDITIONAL INFORMATION** and any associated reports/documents relating to this matter are available for review during regular office hours at the Town Municipal Office on Lesperance Road.

The Public Meeting Agenda regarding this application will be available on the Town's website (<https://calendar.tecumseh.ca/meetings>) on Friday, June 24, 2022.

DATED AT THE TOWN OF TECUMSEH  
THIS 25<sup>TH</sup> DAY OF MAY, 2022.

LAURA MOY, CLERK  
TOWN OF TECUMSEH  
917 LESPERANCE ROAD  
TECUMSEH, ONTARIO  
N8N 1W9





Windsor Police Service

# ANNUAL REPORT

2021



# EXECUTIVE SUMMARY

Similar to 2020, 2021 was heavily defined and influenced by the COVID-19 global pandemic. The Windsor Police Service continued to adhere to safe practices established through 2020 in order to mitigate internal risk and to ensure continuity of operations throughout Windsor and Amherstburg.

Overall, the Service saw a slight increase in calls for service and overall crime stats from those reported in 2020, but generally the numbers show a slight decrease over figures from 2019. It could be expected that these trends continue to reflect the uncertainty brought with COVID-19 and the legislated public restrictions that lead to decreased community activity. Online reporting of crime nearly doubled from that reported in 2020, likely attributable to the COVID restricted access policies of Windsor Police Headquarters.

Throughout 2021 the WPS continued to provide effective and efficient policing services to the Town of Amherstburg. This partnership continues to deliver community safety benefits and other synergies to our communities.

The Service continued to make progress on the initiatives outlined in the 2020-2022 Strategic Plan. Ninety-nine distinct projects/initiatives were identified and tracked via the Strategic Plan Work Plan. Progress on each goal is outlined in this Annual Report and overall highlights include:

- Development and implementation of the Community Consultative Committee and the Youth Community Consultative Committee;
- Virtual Community Services engagement sessions (Virtual Cop, Virtual VIP, Anti-bullying, UNITE, engagement with new Canadians, recruitment outreach sessions);
- Procurement and engagement with Equity Diversity Inclusion, Communications, and Human Resources Consultants;
- Implementation of shared Multi-jurisdictional Records Management System including Computer Aided Dispatch with LaSalle Police Service ;
- Procured funding for a wide variety of initiatives, including enhanced installation of camera systems throughout the city;
- Continued development of employee wellness strategies (Wellness plan, PeerConnect App Pilot);
- Implementation and expansion of Digital Evidence Management Systems.

In August of 2020, the Ontario Civilian Police Commission (OCPC) provided 37 recommendations to the Windsor Police Service and Windsor Police Services Board following complaints from members of the WPS. The Commission directed the Service to report back within one year. The Commission's recommendations related to a broad array of issues, including organizational policies and procedures as well as how WPS leadership can improve communication both inside and outside the organization. The final report from the WPS and WPSB was submitted to OCPC in August (2021) and detailed the completion of 26 of the 37 recommendations. The remaining 11 recommendations are on track for completion in early 2022. Many of the recommendations formed the goals and objectives identified in the WPS Strategic Plan. The Windsor Police Service remains committed to furthering the progress identified in the report into the future.



# COVID-19 RESPONSE

As the COVID-19 pandemic endured through 2021, Windsor Police Service continued to mitigate health and safety risks to our employees and the communities we serve. Masks, Zoom meetings, virtual presentations and temporary work-from-home opportunities were all considered.

With changing provincial and federal legislation in response to COVID-19 infection rates, our members adapted and maintained our continuity of operations, experiencing minimal disruption to public service delivery. The Windsor Police Service continued to work with Public Health and City of Windsor By-Law Enforcement Officers to educate the public and enforce the Emergency Management Civil Protection Act and the Reopening Ontario Act.

Early in the year, the widespread distribution of approved COVID-19 vaccines began. Frontline members of police services were prioritized to receive the vaccine. With the rollout of the

vaccine, safeguarding the limited supply became a concern, and our officers were tasked with providing security as it was transported throughout the City and to the mass vaccination centres. WPS officers were contracted to provide 24/7 day-to-day security at the various mass vaccination sites.

Over the course of the year, public anti-mandate protests and demonstrations occurred, impacting police resources at times. WPS publicly acknowledged the right to peacefully protest and balanced that right with the enforcement of relevant legislation at the time. The Problem Oriented Policing (POP) Unit and City Centre Patrol (CCP) Unit were both tasked with managing the fluid nature of the protests, employing a variety of tactics to allow the lawful demonstrations without negatively impacting public safety.

Members of the Windsor Police Service were almost entirely compliant with a newly implemented mandatory vaccine policy. Any staffing impacts were offset by reassignment of members from elsewhere in the organization and aimed to bolster frontline response and continuity of essential operations. Ultimately, there was effectively no adverse impact on emergency response by the Windsor Police Service.

Overall, spread of COVID-19 or outbreaks of COVID-19 remained manageable throughout the year; however, the potential of large outbreaks remained an organizational threat. Thanks to the diligence of our members adhering to the precautions and protocols that were put in place beginning in March of 2020, there was no substantial impact on service delivery. Our members and our community resolutely navigated another challenging year of the pandemic.



ENFORCEMENT ACTION	TOTAL (2020)	TOTAL (2021)
PON - Windsor Police	11	79
EMCPA/ROA Warning Tickets - Windsor Police	13	30
EMCPA/ROA Verbal Warnings - Windsor Police	18	18
PON - Bylaw	55	15
EMCPA/ROA Warnings - Bylaw	269	95
EMCPA/ROA Warnings - Auxiliary	717	0
311/WPS Calls to Bylaws	1098	1028
911 Calls for Service	1341	1873

\*Auxiliary Park patrol finished Sept 6, 2020

\*Reopening Ontario Act effective July 24, 2020

# MISSION, VISION & CORE VALUES

## Our Vision

We are dedicated to excellence in service through the commitment of our people to teamwork, integrity and our core values. Together we work to be the safest community in Canada.

## Our Mission

The Mission of the Windsor Police Service is to serve our community. Working together we prevent and investigate crime and provide support to those in need. We perform our duties with professionalism, accountability and integrity to ensure the safety of all the members of our diverse community.

## Our Core Values

### Integrity

We are committed to fair, ethical and respectful treatment of all persons within our organization and in our community.

### Respect

We show respect and compassion, through deed and word, to every person.

### Professionalism

We are dedicated to professionalism in appearance and conduct, and the continuing pursuit of excellence in all we do.

### Accountability

We openly identify and address problems, and willingly accept responsibility for our actions.

### Innovation

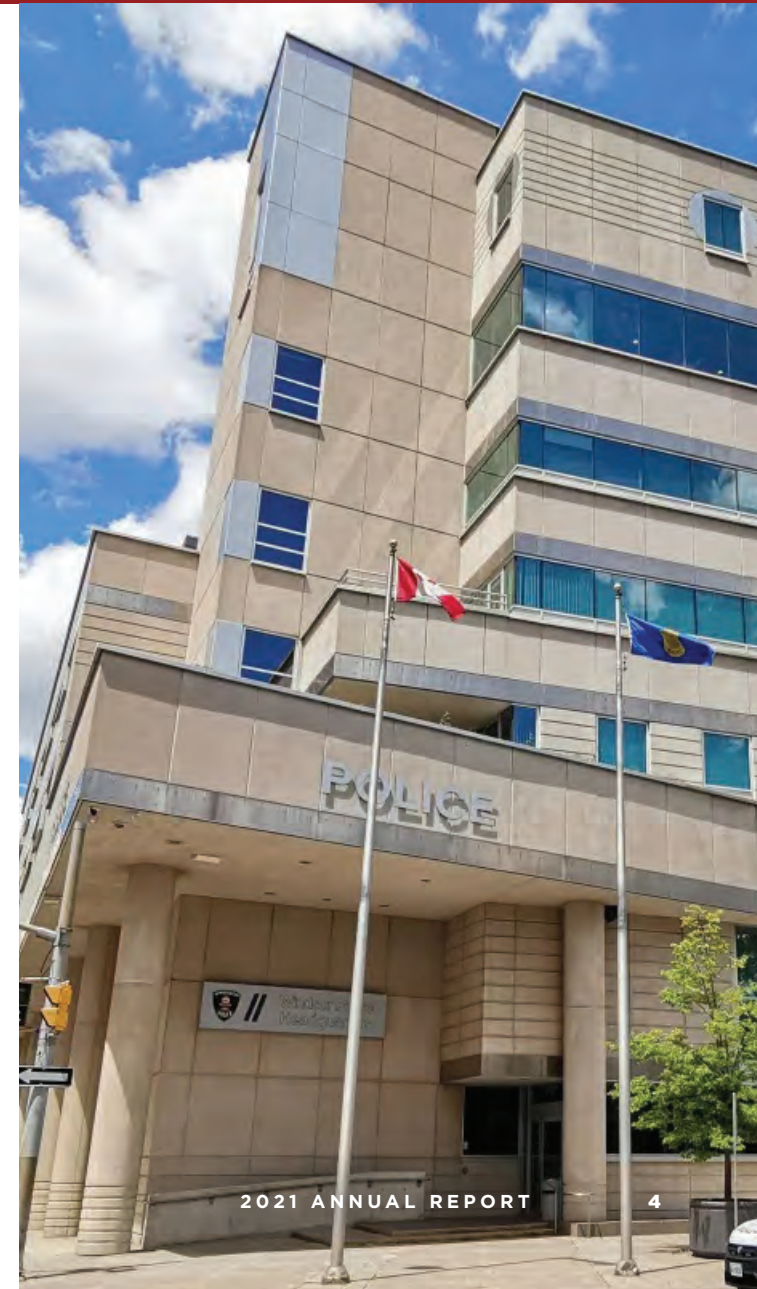
We are creative in developing effective policy and inventive practices, investing in our people and technologies to provide industry-leading service.

### Teamwork

We all succeed by working together and in partnership with our diverse community.

### Service

We are devoted to serving our community with honour, placing the needs of the community above those of our own.





# WINDSOR POLICE SERVICES BOARD

## Message from the Chair



**Drew Dilkens**

Windsor Police Services Board, Chair

As Chair of the Windsor Police Services Board, I am pleased to introduce the 2021 Annual Report of the Windsor Police Service.

This past year was a year like no other. It has challenged us, made us adapt to new realities and inspired us to never give up. The pandemic has been global in its scale, but personal in its impact. The past year has shown us that even in a time of great challenge that we can stand united and more committed than ever to our vision of providing adequate and effective policing to our community.

The Board will continue to push for progress alongside the Windsor Police Service as we move forward into 2022. We are very pleased with the many accomplishments made by the Service over the year, including the day-to-day work performed by the sworn and civilian members to keep our community safe. We truly appreciate the dedication and sacrifice of our police service. In spite of these uncertain times, the members of the WPS have answered the call to protect and serve our community.

As I reflect on the achievements of the past year, I know that the WPS Board is looking forward to the continued delivery of the 2020-2022 Strategic Plan with an eye to the future and work underway for the development of our next Strategic Plan. As the Board Chair, I know that the Board is committed to providing excellence in police oversight and governance and we look forward to continuing our work in this vital area.

The WPS Board is extremely proud of all members of the WPS and we will continue to work together to provide guidance for how the Service can best meet the complex needs of all those they encounter and interact with in our community.

## Meet the Board



1



2



3



4

**1. Aldo Dicarolo, Vice-Chair WPSB**

**2. Rino Bortolin**

**3. Robert de Verteuil**

**4. Denise Ghanam**

# MESSAGE FROM THE CHIEF OF POLICE

I am pleased to present the Windsor Police Service 2021 Annual Report. This report provides to our community an overview of our activities as well as our progress on the goals and objectives set out in our 2020-2022 Strategic Plan. I also wish to acknowledge the recent retirement of Chief Pamela Mizuno who served our community for over 30 years. We wish her all the best in this next chapter of her life.

The Windsor Police Service garnered some significant achievements and continued to mark organizational progress on a number of the ongoing priorities contained in our Strategic Plan. Some of these achievements included:

- The formation and launching of the Mobile Crisis Rapid Response Team, providing expertise to the frontline for incidents involving persons in crisis due to mental health challenges.
- The formation of the Community Consultative Committee, providing external community-based recommendations to the Windsor Police Service
- Several technology project launches that strengthened regional interoperability with external first responder partners
- Completed the implementation of a Digital Evidence Management System (DEMS) across the Windsor Police Service, providing a significant enhancement and pivotal change in how digital evidence is captured, processed, stored and shared

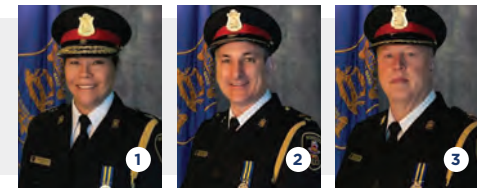
Moreover, working with the Windsor Police Services Board, the Windsor Police Service sought opportunities to improve the overall work environment for our members with the parallel intent on improving service delivery for our communities. Subsequently, new initiatives and policies emerged with the aim of positively impacting the culture at the Windsor Police Service. The Windsor Police Service remains committed to a workplace that is fair, inclusive and safe.

It should be noted that these achievements were made while navigating a global pandemic and were only possible thanks to the professionalism and commitment of our members as well as the support and collaboration with our community partners. We are committed to continuing our broad community engagement as we strive to be the safest community in Canada.

As always, we are grateful for the ongoing support of the Windsor Police Services Board, the Windsor and Amherstburg municipal councils and our communities who provide the resources and support needed to achieve our goals.

— Written by Interim Chief Jason Bellaire

## The Senior Leadership Team



1. **Pamela Mizuno**, Chief Of Police
2. **Jason Bellaire**, Interim Chief
3. **Frank Providenti**, Deputy Chief Operational Support

### Superintendents:

Tim Berthiaume – Patrol Service  
Jason Crowley – Investigations Services  
Brendan Dodd – Corporate Services  
Dan Potvin – Administrative Support

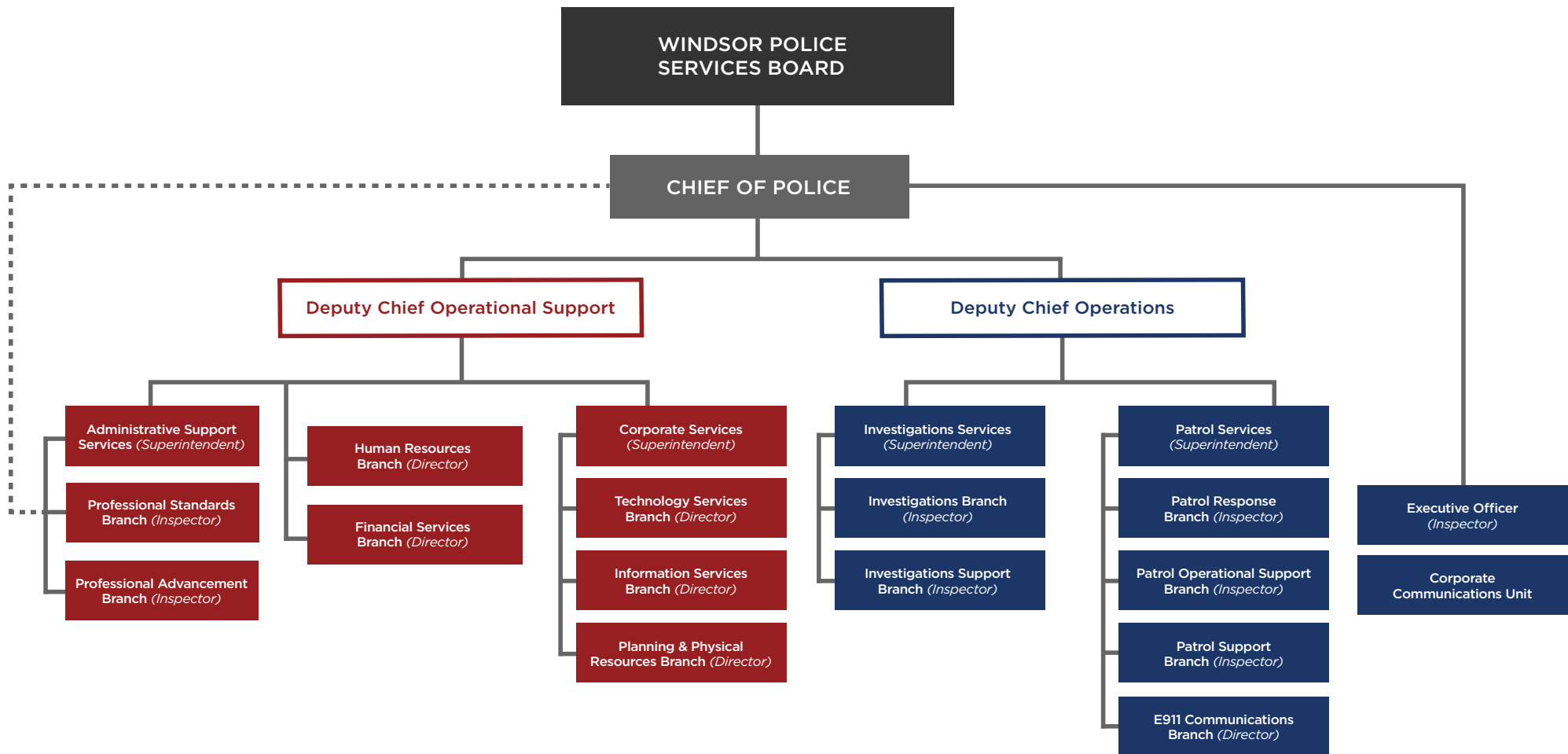
### Directors:

Melissa Brindley – Financial Services  
Matt Caplin – Technology Services  
Bryce Chandler – Human Resources and Legal Counsel  
Dawn Hill – Information Services  
Barry Horrobin – Planning and Physical Resources  
Laura Smith – E911 Centre

### Inspectors:

Jennifer Crosby – Patrol Support  
Ed Armstrong – Professional Advancement  
Jill Lawrence – Executive Officer  
Marc Murphy – Patrol Response  
Dave Deluca – Patrol Operational Support  
Andrew Randall – Investigations  
Karel DeGraaf – Investigative Support  
Ken Cribley – Professional Standards

# 2021 ORGANIZATIONAL STRUCTURE



# OVERVIEW

The Windsor Police Service provides frontline response and investigative services to the residents and visitors of the City of Windsor and the Town of Amherstburg. Geographically, Windsor is the southernmost city in Canada and lies directly across from Detroit, Michigan separated by the Detroit River.



The majority of our operational and administrative functions, including patrol deployment, investigative services, E911 Communications, fleet management, prisoner detention, records management and administration are facilitated from Police Headquarters at 150 Goyeau Street in the downtown core of Windsor, Ontario, Canada. Additionally, the Windsor Police Service operates from four other facilities:

- WPS Amherstburg Detachment at 532 Sandwich Street South in Amherstburg, Ontario; WPS officers who are dedicated to policing the Town of Amherstburg are deployed from this location
- Jefferson Operational Support Services facility located at 2696 Jefferson Boulevard housing the Traffic Enforcement Unit and the Collision Reporting Centre
- Sandwich Community Station at 3312 Sandwich Street which accommodates the Community Services Branch
- Major F.A. Tilson, VC Armoury and Police Training Centre at 4007 Sandwich Street accommodating all training functions including classroom learning, firearms, tactical, physical and canine training

# COMMUNICATIONS CENTRE

TOTAL CALLS RECEIVED:

**403,432**

**1105 PER DAY**

**↑ 5% INCREASE**  
(over 2020)

Non-emergency: 519-258-6111 | Emergency: 9-1-1

911 CALLS RECEIVED:

**105,363**

**288 PER DAY**

**↑ 5% INCREASE**  
(over 2020)

TOTAL CALLS FOR SERVICE:

**124,114**

**340 PER DAY**

**↑ 7% INCREASE**  
(over 2020)

**22,511**

**Emergency Response:**  
Life-threatening  
situation

**PRIORITY 1**

**7,422**

**Urgent Response:**  
Requires immediate  
intervention but no  
threat to life or  
bodily harm

**PRIORITY 2**

Dispatched  
**Police  
Response**

TOTAL Citizen Initiated  
**113,326**

**17,542**

**Regulatory Response**  
Events requiring police  
presence (follow-ups,  
traffic escort)

**PRIORITY 4**

**63,851**

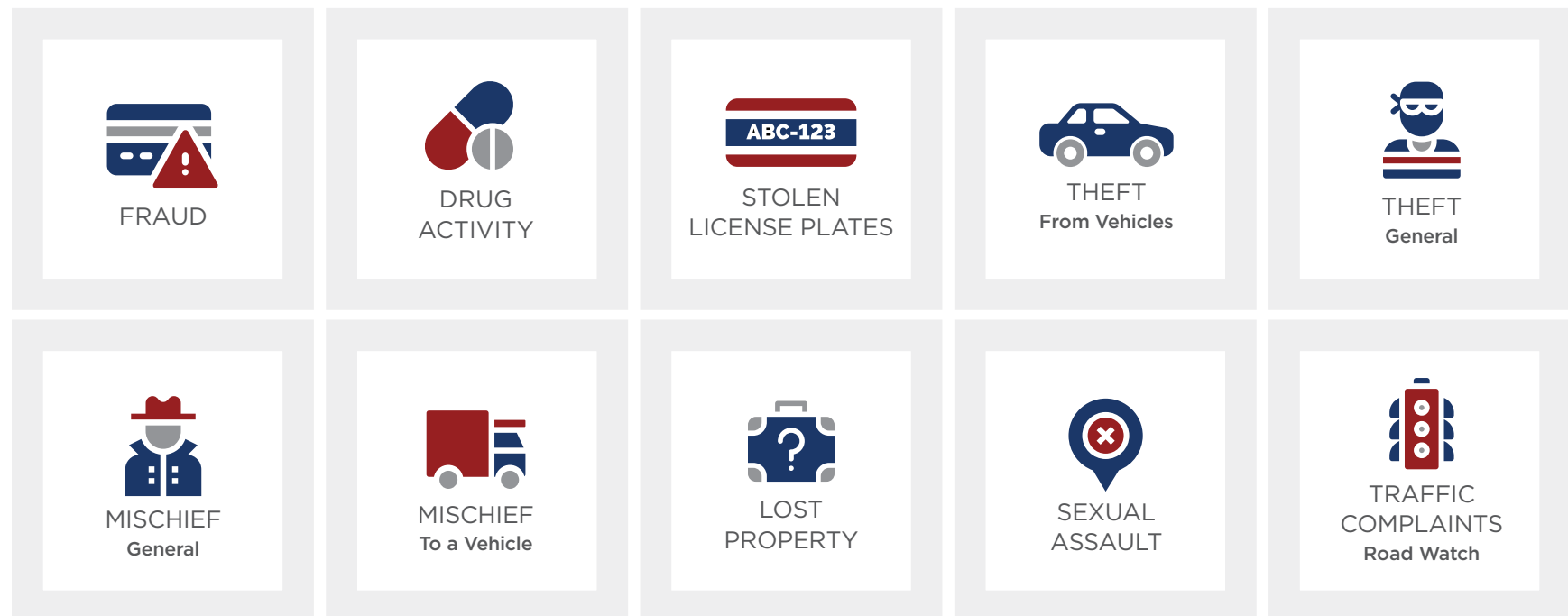
**Routine Response**  
Urgent or emergent response  
not required dispatched  
within 1 hour

**PRIORITY 3**

# ALTERNATIVE REPORTING OPTIONS

The Windsor Police Service offers various online reporting options from our website at [www.windsorpolice.ca](http://www.windsorpolice.ca) for a variety of offences that meet certain criteria. Our online reporting system and registries are a great tool for the public to report crimes and information in a timely and efficient manner. In addition, the data collected is also very useful for the Windsor Police Service to track and assess crime trends that can be useful in crime prevention.

## Online Reporting Options



# ALTERNATIVE REPORTING OPTIONS

## DID YOU KNOW?

Many calls are handled through an alternative method that does not require the dispatching of a Patrol Response unit. Calls may be handled by the call-taker who provides information/referrals for an alternative response such as telephone reporting and online reporting.



## NOW AVAILABLE

In response to the Pandemic several additions to our online reporting system were implemented. We believe these additions will better assist the communities we serve and enhance the reporting of designated offences that meet certain criteria.



## CONVENIENCE

Our online reporting system and registries are a great tool for the public to report crimes and information in a timely and efficient manner. In addition, the data collected is also very useful for the Windsor Police Service to track and assess crime trends that can be useful in crime prevention.



**6,905**

Online Coplogic  
Calls

**4,809**

Telephone Reporting  
Calls

Total Reports  
For Service  
**Alternative  
Methods**

**2,355**

Collision Reporting  
Calls

# PATROL SERVICES

Patrol Services provides front-line policing to the citizens of Windsor and Amherstburg. Their core functions include crime prevention, law enforcement, assisting victims of crime, public order maintenance and emergency response.

## TRAFFIC ENFORCEMENT UNIT

**Total Motor Vehicle Accidents**  
2021: **4,374** | 2020: **4,454**

**Total Provincial Offences Notices**  
2021: **13,371** | 2020: **11,226**

## DETENTION UNIT

Provides temporary confinement of individuals involved in the judicial process

**Total Prisoners Booked**  
2021: **4,982** | 2020: **4,589**

## CITY CENTRE PATROL UNIT (CCP)

CCP is dedicated to policing the downtown core of the City

**Total Actions**  
2021: **10,329** | 2020: **10,072**

## PROBLEM ORIENTED POLICING UNIT (POP)

Provides a rapid response to address issues based on crime trends

**Total Actions**  
2021: **4,543** | 2020: **4,350**

## MOBILE CRISIS RAPID RESPONSE TEAM (MCRRT)

Calls attended (Dispatched, Patrol Request, Assist Patrol)

2021: **1093** | 2020: **NA\***

Apprehensions  
2021: **42** | 2020: **NA\***

\*Program launched in April 2021

## MARINE UNIT

Vessel Stops

2021: **136** | 2020: **187**

Enforcement Actions  
(Provincial Offences and By-laws)

2021: **136** | 2020: **170**

## POLICE DOG UNIT (PDU)

**Total number of PDU calls**  
2021: **1,249** | 2020: **1,422**

## COMMUNITY OUTREACH & SUPPORT TEAM (COAST)

Officers and crisis workers combine services to follow-up and assist with referrals for individuals affected by mental illness

**Total Number of Follow-Ups**  
2021: **2,163** | 2020: **2,429**

## REMOTE PILOTED AIRCRAFT SYSTEM (RPAS)

**Total Actions**  
2021: **14** | 2020: **14**



# AMHERSTBURG DETACHMENT

On January 1, 2019, the Windsor Police Service began policing the Town of Amherstburg. Amherstburg operates as a Detachment and separate statistics are maintained. Thirty officers are dedicated to the Detachment and are responsible for policing the town, additionally the community benefits from enhancements available to them via specialty units of the Windsor Police Service.

## POLICING ACTIVITIES AMHERSTBURG DETACHMENT

CALLS FOR SERVICE			CRIME STATISTICS		
	2020	2021		2020	2021
Dispatch Generated Incidents (CAD Calls)	5,508	5,813	Homicide/Attempted Murder	0	0
Self-Generated Walk-in Incidents	216	N/A	Robbery (All)	4	2
Reports	1783	1769	Break and Enter	45	41
Arrests	125	149	Theft Over \$5000	3	5
PROVINCIAL OFFENCES			Theft Under \$5000	109	62
Traffic Offences	1,149	1,996	Possession of Stolen Property	4	6
Part III Summons	49	78	Fraud	64	70
Liquor Offences	8	4	Mischief	68	50
Other Provincial Offences	0	25	Assaults (All)	58	50
COMMUNITY OUTREACH ACTIVITIES			Drugs	3	8
Community Service Calls	80	N/A	Firearms	0	9
Persons in Crisis	236	218	Arson	3	2
COAST Follow Ups	628	534	Impaired Driving	11	21
			Federal Statutes	4	0
			Other Criminal Code	223	141
			Total Criminal Offences	599	467

Community service officers were re-assigned to patrol due to the pandemic.



# WPS AUXILIARY

The Windsor Police Auxiliary continues to be a well-regarded and supported program with a complement of officers providing service to both the City of Windsor and Town of Amherstburg. Officers are utilized to provide support at special events and park patrols. In 2021, COVID protocols halted most in-person special events decreasing the number of events attended and volunteer hours. In response to the pandemic, Auxiliary Officers were called upon to provide an enhanced role in COVID-19 regulation enforcement primarily in terms of a broader education strategy.



**90**  
MEMBERS



**8374**  
PARK PATROL HOURS

**1384**  
OCCURRENCES  
Legislation Enforcement  
(Liquor offences, disorderly behavior,  
graffiti, vandalism, smoking violations)

**15**   
OFFICERS  
Obtained full time employment  
in law enforcement



**568.50**  
VOLUNTEER HOURS



**11**  
SPECIAL EVENTS

# INVESTIGATIONS/ DRUGS AND GUNS UNIT

## CRIMES AGAINST PERSONS:

**2700** **7 PER DAY**  
**↓ 6% DECREASE**  
 (over 2020)

## CRIMES AGAINST PROPERTY:

**11,052** **30 PER DAY**  
**↑ 5% INCREASE**  
 (over 2020)

## YOUTH RELATED CRIMES:

**227** **0.6 PER DAY**  
**↓ 21% DECREASE**  
 (over 2020)

### 2021 Firearms Seizure by Type

**123**  
HANDGUNS

**176**  
RIFLES/SHOTGUNS

**39**  
AIR RIFLES

**99**  
AIR PISTOLS

**50**  
TOY GUNS

**13**  
OTHER WEAPONS TOTAL

**18**  
CEW

**518**  
TOTAL WEAPONS SEIZED

### DRUG AND GUN ENFORCEMENT UNIT

	2017	2018	2019	2020	2021
CDSA Charges	163	170	238	239	166
CC Charges	96	137	289	186	324
Total Charges	259	337	534	474	495
Persons Charged	74	81	84	118	91

**2020:** HANDGUNS 93 | RIFLES 160 | AIR RIFLES 37 | AIR PISTOLS 78 | TOY GUNS 41 | OTHER 20 | CEW (CONDUCTED ENERGY WEAPON) 17 — **TOTAL - 446**

### 2021 Firearm Calls for Service by Type

**157**  
PERSONS WITH A GUN

**176**  
SHOTS FIRED

**35**  
FIREARM  
INVESTIGATION

**141**  
FIREARMS REPORT/  
SURRENDER/SEIZURE

**2020:** PERSONS WITH GUN 134 | SHOTS FIRED 236 | FIREARM INVEST 31 | REPORT 111

# PROMOTIONS

## 2021 PROMOTIONS

### Sergeant

Lulic Izidor      Luke Harmon  
Robert Hallett      Jacqueline Khoury  
Paul Maziak      Adriano Cipolla  
Derek Nurmi

### Staff Sergeant

Chris Werstein  
Paolo DiCarlo  
Jeff Taylor  
Brian Caffarena  
Joe Faddoul

### Inspector

Marc Murphy  
Jennifer Crosby  
Norman Armstrong

*Ed Armstrong was promoted to the rank of Inspector on November 7, 2021 becoming the highest ranking black officer in the Service's history*



# RECOGNITION AND REMEMBRANCE

## 2021 Retirees

John Ajersch	Shelley Gray	Christine Lucier	Gar Renaud
Sean Bender	Tim Harrington	Jeff Mailloux	Sean Rogan
Caron Braunstein	Dale Harvie	Dave McCubbin	Christine Rybinsky
Anthony Coughlin	Richard Labutte	Neil McEachrane	Shelley Shadd
Chris Dupuis	Ronald Le Clair	Robert Moore	Joyce Webster
James Farrand	Vera Little	Rocco Pelaccia	Steve Wortley
Kristine Fielding	Pam Lombardo	Itza Pinell	

## Recognizing Excellence 20 Years of Service

Deanna Lamont	William (BJ) Wiley	Leslie Plant-Jimmerfield
Jeff Danby	Fred Adair	Rosanna Smith
Colin Wemyss	Karrie Koekstat	Linda Kerr
Joe Faddoul	Sandra Mackenzie	
Nathan Harris	Shelley Ross	

## Recognizing Excellence 30 Years of Service

Anna May Gillis	Richard Beauchamp	Janine Willett	Carol Forbes
Jason Belanger	James Lepine	Anne Campeau	Michelle Koutros
Matthew D'Asti	James Westenberg	June Windsor-McIntosh	Pam Fitzgerald
Mathew McGhee	Chris Dupuis	Robert Moore	Shelley Lenehan
Tammy Fryer	Michelle Herz	Jack Greenway	Kristine Fielding
Todd Cox	Mary-Ann Papineau	Deborah Hurst	

## Retired Service Members In Memoriam

*Retired Special Constable*  
Keith Mitchell

*Retired Staff Sergeant*  
Donald Sampson

*Retired Sergeant*  
Alvin Proctor

*Retired Staff Sergeant*  
Leo Berekoff

*Retired Sergeant*  
Len Skinner

*Retired Constable*  
David Doidge

*Retired Sergeant*  
Neil Lester

*Retired Sergeant*  
Paul Linton

*Retired Constable*  
Leo Lepage

*Retired Constable*  
Norm Lesperance



# COST OF POLICING

## 2021 BUDGET AT A GLANCE

<b>Approved Net Budget</b>	<b>\$93,932,409</b>
<b>Actual Revenues</b>	
Grants	\$7,719,064
Permits, Fees, Charges	\$606,598
Recovery of Expenses	\$9,868,488
Other Miscellaneous Revenue	\$664,114
<b>Total Actual Revenues</b>	<b>\$18,858,264</b>
<b>Actual Expenditures</b>	
Minor Capital	\$5,113,639
Operating & Maintenance	\$2,012,620
Purchased Services	\$4,009,281
Salaries & Benefits	\$95,709,343
Transfers to Reserve Funds	\$3,269,096
Utilities, Insurance, Taxes	\$1,145,982
<b>Total Actual Expenditures</b>	<b>\$111,259,961</b>
<b>Net Surplus</b>	<b>\$1,530,712</b>

## GRANTS RECEIVED IN 2021

Community Safety & Policing - Local	\$1,421,654
Community Safety & Policing - Provincial	\$80,275
Internet Child Exploitation	\$275,985
Civil Remedies	\$34,076
Proceeds of Crime - Guns and Gangs	\$100,000
Proceeds of Crime - Technical Cooperative	\$88,962
Reduce Impaired Driving Enforcement	\$42,977
Federal Internet Child Exploitation	\$105,821
Ontario's Strategy to End Human Trafficking	\$17,400
<b>Total Grants Received</b>	<b>\$2,167,150</b>
Court Security and Prisoner Transportation Program	\$5,551,914
<b>Total Grants and Uploads</b>	<b>\$7,719,064</b>

## 2021 CAPITAL PROJECTS (BUDGET)

Police Fleet Replacement/Refurbishment	\$1,268,000
Other Capital Projects	\$2,296,000
<b>Total Capital Budget</b>	<b>\$3,564,000</b>

TOTAL ACTUAL  
REVENUES  
**\$18,858,264**

TOTAL ACTUAL  
EXPENDITURES  
**\$111,259,961**

TOTAL GRANTS  
AND UPLOADS  
**\$7,719,064**

# PROFESSIONALISM IN POLICING

**106**

LETTERS OF  
APPRECIATION FOR  
OVER 191 OFFICERS  
AND 25 CIVILIANS



**4**

COMMENDATIONS/  
INTERNAL RECOGNITIONS  
FOR 36 OFFICERS AND 0  
CIVILIANS



**A Job  
Well Done**

## 2021 NEW COMPLAINTS RECEIVED

Public Complaints	110
-------------------	-----

## 2021 DISPOSITION OF PUBLIC COMPLAINTS

Customer Service Resolution	5
Screened Out	57
Screened In	48

Dispositions reflect the complaints dealt with over the course of the year. May be disseminated from previous years.



*I wanted to let you know that you have wonderful officers on your hands, who go above and beyond to get the job done."*

*"I would like to thank Windsor Police Service for their professionalism and understanding, we are very fortunate to be blessed with these officers and thanks again for your service."*

*- Citizens of Windsor*



# STRATEGIC PLAN

Ontario regulations require every Police Services Board to prepare and adopt a Strategic Plan for the provision of policing for the communities for which it is responsible. This plan must layout the objectives, priorities and core functions of the police service. The purpose of this Strategic Plan is to identify how the Windsor Police Service will continue to provide adequate and effective policing in accordance with the needs of the diverse communities we serve in the City of Windsor and the Town of Amherstburg. Further, the Plan outlines a path forward for the Service to ensure responsiveness and continuous improvement in service delivery in order to adapt to changes in the policing environment. The Plan also serves as a guide to the members of the Windsor Police Service who work diligently towards the achievement of the organizational goals identified herein.

## **How does the Windsor Police Service Strategic Plan relate to our Vision, Mission and Values?**

The Windsor Police Service is vision inspired, mission focused and values driven.

In addition to numerous internal and external consultations, the Vision, Mission and Values of the Service were foundational to the development of the Strategic Plan as well. The priorities and organizational goals detailed in the 2020-2022 Strategic Plan are intended to guide every member of the Windsor Police Service as we work in service of our community. Each of the nine goals contained in the Strategic Plan seeks to achieve a desired outcome directly related to the achievement of the overall mission of the Windsor Police Service. Resources and efforts will be focused on achieving the outcomes being sought from each goal. Performance indicators will gauge our progress as we work to achieve our vision for community safety. To summarize, the top priorities identified in our Strategic Plan directly align with and enable our Vision, Mission and Values: We are driven by our values and our principles. Our people are inspired by our vision for the future. We are focused on performance as we work to achieve our mission.

Work will commence in 2022 to update the WPS Strategic Plan for the next planning cycle from 2023 to 2025.





# STRATEGIC PLAN

## OUR STRATEGIC PRIORITIES



# STRATEGIC PLAN PROGRESS

Strategic Priority: Our Principles

GOAL 1.1	GOAL 1.2	GOAL 1.3
Promote Community Trust and Support	Strive for Equity, Diversity and Inclusion in Delivery of Policing Services	Pursue Community Partnerships and Regional Collaboration
<b>Desired Outcome:</b>		
To increase trust and satisfaction levels as experienced and reported by community stakeholders.	To ensure that the policing services we provide are delivered in a fair and inclusive manner and are, to the maximum extent possible, free of prejudice and bias.	To improve community safety and maximize the efficiency of service delivery for the WPS and the agencies and communities with whom we partner.
<b>Actions Taken &amp; Goal Progress:</b>		
<ul style="list-style-type: none"> <li>• Short list of committee members submitted for Youth Community Consultative Committee selection panel</li> <li>• Launched community engagement initiative “Project Deedket” during Crime Prevention Week 2021; this partnership with local fast-food chains saw Community Services officers distributing meal coupons for citizens engaging in acts of community kindness</li> <li>• Implemented the WPS “Virtual Cop” Community Outreach Program to mitigate cancellation of in-person activities due to COVID restrictions; efforts included virtual VIP and Anti-Bullying programs for schools, recruitment events, and engagement with the Multicultural Council and New Canadian Centre of Excellence</li> <li>• Community outreach and youth engagement via modified annual Camp Brombal week</li> <li>• Formed and commenced meetings of Community Consultative Group to provide recommendations to WPS leadership for evaluation/implementation</li> <li>• Continued implementation of enhanced information security policies and operational/business continuity best practices</li> </ul>	<ul style="list-style-type: none"> <li>• Implemented “Project Unite” in which Community Services officers delivered presentations in the community and to WPS officers on the impact of racism and unconscious bias</li> <li>• Procured and engaged external Equity, Diversity and Inclusion (EDI) Consultant</li> <li>• WPS EDI Coordinator worked with EDI Consultant to draft the WPS EDI Strategy for action in 2022</li> <li>• Continued collection and reporting of race-based data in Use of Force Reports</li> </ul>	<ul style="list-style-type: none"> <li>• Conducted numerous place-based crime and disorder prevention strategies; these partnerships included neighbourhood safety walks, physical safety design audits and community meetings to outline principles of Crime Prevention Through Environmental Design (CPTED)</li> <li>• Implemented agreement with the LaSalle Police Service for the provision of a Shared Multi-jurisdictional Versaterm Records Management System (RMS) including Computer Aided Dispatch (CAD) and mobile policing software and equipment</li> <li>• Shared Backup Data Centre fully operational for WPS in partnership with Windsor Fire &amp; Rescue Services, and Corporation of the City of Windsor</li> <li>• Process on-going for renewal of Windsor Radio System Sharing Agreement with Canada Border Services Agency (CBSA)</li> <li>• Implemented numerous ongoing Joint Forces Operations agreements, including BEST, ROPE, Cannabis Enforcement and Provincial Weapons Enforcement Unit</li> <li>• Achieved significant cost reductions by implementing a Shared Training Centre CCTV System with Department of National Defence and Corporation of the City of Windsor for the Major FA Tilston Armoury &amp; Police Training Centre</li> <li>• Partnered with City of Windsor to win grant funding to modernize the City's Traffic Camera Network to enhance community and traffic safety</li> <li>• Engaged in continued partnership with Windsor-Essex Community Opioid Substance Strategy (WECOSS)</li> </ul>

# STRATEGIC PLAN PROGRESS

Strategic Priority: Our People

GOAL 2.1	GOAL 2.2	GOAL 2.3
Recruit and Develop an Effective and Diverse Workforce	Provide a Responsive, Fair and Inclusive Workplace	Foster Wellness and Resilience
<b>Desired Outcome:</b>		
Our organization is comprised of employees who are reflective of the community we serve, and our members have the skills and training needed to be responsive to the diverse needs of all citizens.	Our organization has open lines of communication, is responsive to the diverse needs of our employees, and treats every member with fairness, integrity and respect; our members act and feel like they are part of a team, and are engaged and committed to shared organizational values and goals.	Our members trust that they will be treated with compassion and integrity, and that their employer values and supports them by working together to mitigate the physical and mental demands of their work.
<b>Actions Taken &amp; Goal Progress:</b>		
<ul style="list-style-type: none"> <li>Virtual Recruitment sessions were held at the University of Windsor and St. Clair College; Recruitment video, brochures and 519 CONNECT podcast was launched to connect with community members and assist with recruitment during COVID related restrictions</li> <li>Procured and engaged external Equity, Diversity and Inclusion (EDI) Consultant</li> <li>WPS EDI Coordinator worked with EDI Consultant to draft the WPS EDI Strategy for action in 2022 regarding external EDI strategies related to recruitment</li> <li>Continued ongoing initiative to expand issued technology and communications equipment to all WPS members as required</li> </ul>	<ul style="list-style-type: none"> <li>Delivered results of 2020 WPS Employee Consultation Survey (Internal Survey, Year One Baseline)</li> <li>Conducted first "Virtual Town Hall" for WPS employees including Q &amp; A with Chief and Senior Leadership Team</li> <li>Created Employee Engagement Committee and Patrol Working Group to enhance internal communication and recognition processes</li> <li>Procured and engaged Communications Consultant; communications work plan drafted and implementation of recommendations to follow in early 2022</li> <li>Procured and engaged Equity, Diversity and Inclusion (EDI) Consultant</li> <li>WPS EDI Coordinator worked with EDI Consultant to draft the WPS EDI Strategy for action in 2022 regarding internal EDI action plan</li> <li>Procured and engaged third-party Human Resources Consultant; the evaluation of current promotional process and competency assessments is underway</li> <li>Updated internal policies including: <ul style="list-style-type: none"> <li>Employee Accommodation Directive</li> <li>Workplace Violence, Harassment, and Sexual Harassment</li> </ul> </li> </ul>	<ul style="list-style-type: none"> <li>WPS Health and Wellness Coordinator developed a formalized WPS Wellness Strategy for final approval and implementation in early 2022</li> <li>Implemented PeerConnect Employee Wellness App</li> <li>Several in-person wellness programs and training deferred due to COVID restrictions</li> <li>Provided enhanced online training for officers and new employees that includes mental health self-awareness for first responders training, police ethics and accountability training, cybersecurity training, customer service in the policing environment, human rights training, respectful workplace training, and Violence/Harassment/Sexual Harassment training</li> </ul>

# STRATEGIC PLAN PROGRESS

Strategic Priority: Our Performance

## GOAL 3.1

Provide Responsive and Effective Service Delivery

The WPS effectively responds to the needs and expectations of the citizens we serve; as those needs and expectations change, our organization changes and adapts service delivery as required.

## GOAL 3.2

Enhance Safety, Reduce Victimization and Loss

The WPS effectively responds to the needs and expectations of the citizens we serve; as those needs and expectations change, our organization changes and adapts service delivery as required.

## GOAL 3.3

Pursue Continuous Improvement and Innovation

The WPS leverages technology and information systems to inform decision-making and optimize resource allocations and measure the impact of changes; new service delivery models are explored to adapt to changing environments.

### Desired Outcome:

### Actions Taken & Goal Progress:

- Implemented the Mobile Crisis Rapid Response Team (MCRRT) to supplement and enhance the existing COAST Program (an alternative mental health response strategy). These initiatives work toward the goal of reduced Person In Crisis (PIC) interactions and enhanced service provision that is safer and better optimizes resource allocation, as more officers will be able to resume general law enforcement duties and proactive patrol.
- Ensured ongoing assessment of required service-delivery and staffing models (including remote work, physical distancing, PPE stock) to ensure operational continuity of policing services as demands of pandemic response changed
- Procured required call handling infrastructure (for primary and backup E911 Centres) in advance of mandated transition to Next Generation 9-1-1 (NG911) Systems

- Active Transportation School Safety Initiative: A pilot study comprised of six elementary schools located throughout the Windsor-Essex region is ongoing, with results to be finalized in the Spring of 2022. WPS is an active partner in this study. Pending study results, the program is anticipated to be expanded to all schools, commencing in the Fall of 2022.
- In person attendance for the "Update the Safety Village" initiative was impacted by COVID restrictions - virtual presentations created in collaboration with the Safety Village.
- Community Mobilization & Engagement initiative was implemented via deployment of Community Services Officers through neighbourhood patrols including discussions of crime prevention strategies with community members.
- Partnered with City of Windsor to procure grant funding to modernize the City's Traffic Camera Network to enhance community and traffic safety
- Enhanced Human Trafficking & Firearms Enforcement: Grant funding secured and funds fully expended; enforcement activities have been actively on going throughout the grant period. Expanded grant funding requests made for 2022-2023.
- Secured provincial grant funding to implement the "WPS CARES" Program (Windsor Police Service, Community Accessible Resource Education Support): This program improves efficiency and improves the assistance provided to victims of crime through the use of technology; smart television, website and videos in multi-languages that provides information to and about community partners for victims. These tools, along with enhanced interview environments minimize trauma to victims by enhancing recovery and providing resources available to support them

- Implemented a Patrol Continuous Improvement Working Group (PCIWG) to pursue a mandate to assess and implement a program of projects and initiatives designed to improve the efficiency and effectiveness of the Patrol Response services provided by the WPS. The working group engages broad group of employees from across the organization to improve the work environment and the services provided by the WPS
- Continued to develop and enhance data analytics solutions to measure response times, referrals to social service agencies, opioid/overdose incident tracking, impact of mental health calls for service (including police/hospital transition times)
- Completed implementation of the service-wide Digital Evidence Management System (DEMS) across the WPS, including Axon Capture, Axon Citizen and Axon Evidence subsystems; work pending in early 2022 for Interview Room integration with the Axon DEMS system

# 2021 STRATEGIC PLAN HIGHLIGHTS

The following are some of the major organizational milestones and strategic level initiatives that were implemented in 2021 in pursuit of the Organizational Goals outlined in the 2020 – 2022 WPS Strategic Plan:



## Mobile Crisis Rapid Response Team (MCRRT)

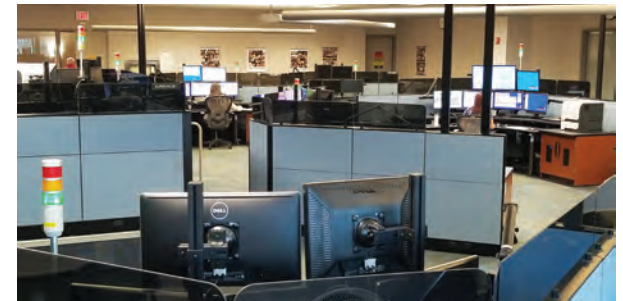
In accordance with the Service's goal to ensure responsive and effective service delivery, in April 2021, the WPS partnered with Hotel-Dieu Grace Healthcare (HDGH) to form a Mobile Crisis Rapid Response Team (MCRRT). The pairing of a mental health professional (social worker) with a WPS officer trained in Crisis Intervention Techniques (CIT), effectively allocates the available community resources to persons in crisis. The addition of a MCRRT shows the evolution of change and the adaptability of the WPS to meet the needs and expectations of our community, specifically as it relates to frontline police response to persons in crisis.



## WPS CARES Program

The WPS Cares (Community Accessible Resource Education Support) is a Provincial Grant funded initiative that aligns with the Service's goal of enhancing safety, and reducing victimization & loss. Specifically, this program is aimed at expanding existing community partnerships, with the assistance of technology for victims of crimes, particularly Intimate Partner Violence. Informative videos, offered in numerous languages, are being utilized to provide resource information to victims, specific to each community agency.

This initiative not only assists some of our communities' most vulnerable victims, it strengthens our partnerships with community agencies as well. Its unique approach is an out of the box idea developed by one of our Sergeants that is engaging and innovative. A new soft interview room at WPS HQ also accompanies the videos, to ensure a comfortable atmosphere for victims



## Regional Information Sharing Collaboration

In 2021, the Windsor Police Service and the LaSalle Police Service implemented several shared multi-jurisdictional information technology systems. This is a transformational regional collaboration and information sharing initiative, where police data for three municipalities - the City of Windsor, the Town of Amherstburg, and the Town of LaSalle - are now managed and shared on the same information technology systems.



# 2021 STRATEGIC PLAN HIGHLIGHTS



## Multi-jurisdictional Homicide Investigation

This matter commenced as a Missing Person investigation. Thorough attempts to locate the Missing Person were made, without success. Approximately two months after the initial report, the Missing Person's remains were located by members of the Walpole Island community.

The Windsor Police Service and Ontario Provincial Police conducted a multi-jurisdictional Homicide investigation, which resulted in the execution of Search warrants and Production Orders in several communities across the Province. Ultimately, several parties were identified, arrested and charged for their respective roles in the Homicide.



## Information Security, Technology & Business Continuity Investments

In 2021, the WPS's first geo-diverse, secondary data centre was completed and is operational. The WPS provides policing and 911 services that are vital for the community, which rely heavily on technology and electronic information. This implementation is a significant risk mitigation accomplishment that provides a level of information security and resiliency appropriate for WPS's critical operations.

Numerous other significant information and communications technology investments were made in 2021; these included radio communications hardware and infrastructure replacements as well as traffic camera and phone system upgrades. These strategic investments totaled over \$5 million dollars in procurements that were planned, budgeted and funded from a variety of sources, including provincial grant funding. These investments demonstrate the Service's commitment to information security as well as ensuring officers have access to the tools required for effective service-delivery into the future.



## Digital Evidence Management System (DEMS) Implementation

The WPS, like all police services, is dealing with exponential growth in the availability of digital evidence (digital pictures, videos, audio, and related digital files). While every police service is working toward digital evidence solutions, WPS became one of the first police services in Ontario to substantially complete an implementation of an enterprise digital evidence management system (DEMS) across the entire service, including full integration with the provincial and federal crown attorneys. In addition to the core system, which provides extensive functionality and efficiencies to centrally manage the constantly increasing volume of digital evidence, all WPS officers can now capture pictures, videos and audio, in the field, directly to their smartphones and securely and immediately upload that evidence to the WPS enterprise digital evidence management system. Also, from their smartphones, officers can send a secure upload link to a member of the public, via text or email, so the witness/victim/complainant can securely upload their digital evidence directly to the WPS enterprise digital evidence management system. This project not only improves operational speed and efficiencies, it allows WPS to interact with the public in a modern and efficient manner.

# FIVE YEAR SUMMARY

	2017	2018	2019	2020	2021		2017	2018	2019	2020	2021
<b>Motor Vehicle Accidents (MVA)</b>	<b>5503</b>	<b>5246</b>	<b>6106</b>	<b>4454</b>	<b>4374</b>	<b>Crimes Against Person Violations</b>					
MVA Non - Reportable	75	72	156	317	224	Total	2,166	2,454	2,468	2,858	2,700
MVA - Injury	1109	1264	1161	1025	1015	<b>Crimes Against Property Violations</b>					
MVA - Fatal	11	10	9	5	6	Total	10,433	12,930	12,641	10,480	11,052
<b>Provincial Offence Notices Issued</b>	<b>16441</b>	<b>17824</b>	<b>26748</b>	<b>19524</b>	<b>16502</b>	<b>Total Other Criminal Code Violations</b>					
Traffic Unit	11577	11172	15654	11226	7173	Total	2,675	3,029	3,240	2,513	2,628
Suspended Drivers	638	654	793	329	623	<b>Total Criminal Code</b>					
Prohibited Drivers	33	35	34	68	43	Total	15,274	18,413	18,349	15,851	16,380
Bus Watch Letters Sent	53	37	38	18	3	<b>Youth Crime Related Stats</b>					
Road Watch Letters Sent	140	133	26	8	23	Total Young Persons Committing Violent Crime	141	158	172	146	115
<b>RIDE Statistics</b>						Total Young Persons Committing Property Crime	115	131	114	76	43
Vehicles Stopped	3784	8600	7837	4847	8433	Total Young Persons Committing Other Crime	42	53	58	52	49
Suspensions Issued	14	14	44	13	12	Total Young Persons Committing Drug Crime	22	16	4	15	16
Impaired/80 mg or more	6	10	16	10	7	Total Youths in all Offence Category	320	358	348	289	223
Other Criminal Charges	7	2	0	10	8	<b>Complement</b>					
Roadside Tests	54	54	112	235	72	Police Officers (Sworn)	433	444	490	501	501
Liquor Licence and Control Act (LLCA)	0	0	3	1	8	Civilians	140	142	147	150	153
<b>Workload</b>						Cadets	21	18	21	14*	19*
Calls for Service	104,091	110,272	125,019	116,217	124,114	Total	594	604	658	665	673
Calls to 9-1-1	27,296	31,289	33,647	32,504	35,926						

\*Indicates the number of new cadets hired in the year.  
Authorized complement for 2020 is 5

## HONOUR IN SERVICE

### WINDSOR POLICE SERVICE

P.O. Box 60  
150 Goyeau St.  
Windsor, ON N9A 6J5  
(519) 255-6700  
info@windsorpolice.ca

**windsorpolice.ca**

**WPS Facebook:** @Windsor.Police.Service

**WPS Twitter:** @WindsorPolice

**WPS Instagram:** @WindsorPolice

**WPS Youtube:** WindsorPolice

**Amherstburg Facebook:** @WPS.Amherstburg

**Amherstburg Twitter:** @WPS\_Amherstburg





## **COMMITTEE OF ADJUSTMENT/CONSENT AUTHORITY AGENDA RECORD**

**PLEASE BE ADVISED THIS MEETING WILL BE CONDUCTED ELECTRONICALLY.**

**The following applications are scheduled to be heard by the Committee of Adjustment/Consent Authority on the afternoon of Thursday June 16, 2022 and in the order stated below. Persons wishing to provide comments to the Committee of Adjustment on the item contained herein are strongly encouraged to submit their comments in writing, by email to Jessica Watson, Secretary-Treasurer via email - [COAdjustment@citywindsor.ca](mailto:COAdjustment@citywindsor.ca)**

ITEM	TIME	ROLL #	FILE #	APPLICANT	LOCATION	REQUEST
1	3:30 PM	3739040140026000000	A-041/22	JINGJUE GU	444 CRAWFORD AVE	<b>RELIEF:</b> To Accommodate an existing single unit dwelling with reduced side yard width and minimum separation from a Lot line, and a proposed new duplex dwelling with reduced Lot width and parking spaces.
2	3:30 PM	739040140026000000	B-029/22	JINGJUE GU	444 CRAWFORD AVE	<b>CONSENT:</b> Create a new Lot
3	3:30 PM	3739060460045000000	A-042/22	WILLIAM MILETIC, URSULA MILETIC	11420 RIVERSIDE DR E	<b>RELIEF:</b> Creation of a new single unit dwelling with reduced side yard width, maximum garage width, and minimum separation from a Lot line.
4	3:30 PM	3739030070032000000	A-043/22	HIATUS HOUSE OF WINDSOR	0 LOUIS AVE ( VACANT LOT NEXT SOUTH OF 267 LOUIS AVE)	<b>RELIEF:</b> Creation of a new combined use building with reduced minimum parking separation from a habitable room window, parking, and front yard depth.
5	3:30 PM	3739060410041000000	A-044/22	9825924 CANADA INC.	9050 WYANDOTTE ST E	<b>RELIEF:</b> Creation of two new dwelling units in existing multiple dwelling exceeding the maximum number of units and minimum parking.
6	3:30 PM	3739070160005000000	B-030/22	DELICATA ESTATE	1275 PROVINCIAL RD	<b>CONSENT:</b> Create a new Lot
7	3:30 PM	3739070020091000000	B-031/22	GUY PINARD, CHERYL PINARD	772 CHARLOTTE ST	<b>CONSENT:</b> Create a new Lot
8	3:30 PM	373908061015000000	B-032/22	AHMAD ZRABAH	1550 RANDOLPH AVE	<b>CONSENT:</b> Create a new Lot
9	3:30 PM	739030430029000000	B-033/22	AHMED JUEAD	1780 PIERRE AVE	<b>CONSENT:</b> Create a new Lot
10	3:30 PM	3739080780074000000	B-034/22	CALEB WILLOCK, FRANCESCA DI LORENZO	3093 EMILIA RD	<b>CONSENT:</b> Lot Addition

11	3:30 PM	3739020090085000000	A-045/22	DUANE MATTHEWS, EVAN MATTHEWS	749 & 753 WALKER RD	<b>RELIEF:</b> The development of a rear detached garage with second floor Additional Dwelling Unit (ADU), with minimum separation from a rear lot line or side lot line.
12	3:30 PM	3739080590064050000	A-046/22	WINDSOR ISLAMIC ASSOCIATION	2348-2380 DOMINION BLVD	<b>RELIEF:</b> The creation of 5 new 6-unit multiple dwellings with minimum parking area separation from a habitable room window,

If a person or public body that files an appeal of a decision of the Committee of Adjustment in respect of the proposed consent does not make written submissions to the Committee of Adjustment before it gives or refuses to give a provisional consent, the Local Planning Appeal Tribunal may dismiss the appeal. In addition, if you wish to be notified of the decision of the Committee of Adjustment in respect of the proposed applications, you must make a written request to the Committee of Adjustment at the address shown below. **Note. To access the Agenda Record and Comments for the upcoming meeting, as well as the Minutes from the previous meeting, please visit our website at: [www.citywindsor.ca](http://www.citywindsor.ca) . Choose the Residents tab, then select Planning and click on Committee of Adjustment.**



**Committee Matters: SCM 149/2022**

**Subject: Multi-Residential Interim Control By-law Study - Proposed Official Plan Amendment and Zoning By-law Amendment**

Moved by: Councillor Holt

Seconded by: Councillor Sleiman

Decision Number: **DHSC 397**

1. THAT the reports titled "MRICBL Background Study" prepared by Municipal Planning Consultants, dated April 2022, attached as Appendix A to Report S 64/2022 **BE ACCEPTED**.
2. THAT Volume 1: The Primary Plan of the City of Windsor Official Plan ("Official Plan") **BE AMENDED** as follows:
  - 2A) Change the text of the Official Plan as follows:
    - a) "Commercial Corridor" changes to "Mixed Use Corridor"
    - b) "Commercial Centre" changes to "Mixed Use Centre"
    - c) "Mixed Use" changes to "Mixed Use Node"; and

THAT Schedule A-1: Special Policy Areas of the Official Plan **IS AMENDED** by adding the Mature Neighbourhoods designation as shown on Schedule A-1-1 attached to this report; and,

THAT Schedule D: Land Use of the Official Plan **IS AMENDED** by changing the names of the designations in the Legend as follows:

- i. "Commercial Corridor" changes to "Mixed Use Corridor"
- ii. "Commercial Centre" changes to "Mixed Use Centre"
- iii. "Mixed Use" changes to "Mixed Use Node"; and

THAT Schedule D: Land Use of the Official Plan **IS FURTHER AMENDED** by changing the existing land use designations to the Mixed Use Corridor, Mixed Use Centre, and Mixed Use Node designations as shown on Schedule D-1 attached to this report.

2B) Add the following to the Chapter 6 – Land Use:

<i>RESIDENTIAL AL INTENSIFICATION</i>	6.1.14 To direct residential intensification to those areas of the City where transportation, municipal services, community facilities and goods and services are readily available
---	---

2C) Delete 6.3.1.3 and replace it with the following:

*INTENSIFICATION, INFILL & REDEVELOPMENT* 6.3.1.3 To promote residential redevelopment, infill and intensification initiatives in appropriate locations in the City.

2D) Delete 6.3.2.1 and replace it with the following:

*PERMITTED USES* 6.3.2.1 Uses permitted in the Residential land use designation identified on Schedule D: Land Use include Low Profile and Medium Profile dwelling units.  
High Profile Residential Buildings shall be directed to locate in the City Centre, Mixed Use Centres and Mixed Use Corridors.

2E) Delete 6.3.2.3 and replace it with the following:

*TYPES OF LOW PROFILE HOUSING* 6.3.2.3 For the purposes of this Plan, Low Profile housing development is further classified as follows:

- (a) small scale forms: single detached, semi-detached, duplex and row and multiplexes with up to 8 units.

2F) Delete 6.3.2.4 and replace it with the following:

*LOCATIONAL CRITERIA* 6.3.2.4 Residential intensification shall be directed to the Mixed Use Nodes and areas generally within 200 metres of those Nodes. Within these areas mid-profile buildings, up to 4 storeys in height shall be permitted. These taller buildings shall be designed to provide a transition in height and massing from low-profile areas. Residential development shall be located where:

- (a) there is access to a collector or arterial road;
- (b) full municipal physical services can be provided;
- (c) adequate community services and open spaces are available or are planned; and
- (d) public transportation service can be provided.

2G) Delete 6.3.2.5 (c) and replace it with the following:

- (c) In existing neighbourhoods, compatible with the surrounding area in terms of scale, massing, height, siting, orientation, setbacks, parking and amenity areas.  
In Mature Neighbourhoods as shown on Schedule A-1, compatible with the surrounding area, as noted above, and consistent with the streetscape, architectural style and materials, landscape character and setback between the buildings and streets;

2H) Delete 6.3.2.5 (f) and replace it with the following:

- (f) facilitating a gradual transition from Low Profile residential development to Medium and/or High profile development and vice versa, where appropriate. In accordance with Design Guidelines approved by Council.

2I) Delete 6.3.2.9 and replace it with the following:

NEIGHBOURHOOD  
OD  
COMMERCIAL  
EVALUATION  
CRITERIA

Neighbourhood Commercial uses shall be encouraged to locate in Mixed Use Nodes as shown on Schedule J. Ideally these uses would form part of a multi-use building with residential uses located above or behind the non-residential uses on the street front.

At the time of submission, the proponent shall demonstrate to the satisfaction of the Municipality that a proposed Neighbourhood Commercial development within a designated Residential area is:

- (a) feasible having regard to the other provisions of this Plan, provincial legislation, policies and appropriate guidelines and support studies for uses:
  - (i) within or adjacent to any area identified on Schedule C: Development Constraint Areas and described in the Environment chapter of this Plan;
  - (ii) adjacent to sources of nuisance, such as noise, odour, vibration and dust;
  - (iii) within a site of potential or known contamination;
  - (iv) where traffic generation and distribution is a provincial or municipal concern;
  - (v) adjacent to heritage resources; and
  - (vi) where market impact is identified as a municipal concern;
- (b) in keeping with the goals, objectives and policies of any secondary plan or guideline plan affecting the surrounding area;
- (c) compatible with the surrounding area in terms of scale, massing, height, siting, orientation, setbacks, parking and landscaped areas;
- (d) capable of being provided with full municipal physical services and emergency services; and
- (e) provided with adequate off-street parking.

2J) Delete 6.3.2.17 and replace it with the following:

<i>HERITAGE CONSERVATION</i>	6.3.2.17	Council shall encourage the retention, restoration and sensitive renovation of historic and/or architecturally significant residential buildings in accordance with the Heritage Conservation chapter of this Plan. Infill and intensification within Mature Neighbourhoods, shown on Schedule A-1, shall be consistent with the built form, height, massing, architectural and landscape of the area. Council will adopt Design Guidelines to assist in the design and review of development in these areas.
----------------------------------	----------	--

2K) Delete 6.3.2.29 and replace it with the following:

<i>COMPATIBLE ADDITIONAL UNITS</i>	6.3.2.29	The creation of additional units through renovation or redevelopment in existing residential neighbourhoods shall be done in a manner that is compatible and complimentary to the character of the neighbourhood. The Zoning By-law will establish regulations for height, density, and massing that will preserve the character of stable neighbourhoods. Council will adopt Design Guidelines to assist in the design and review of development applications within existing stable neighbourhoods.
--	----------	---

2L) Delete 6.5 and replace it with the following:

## **6.5 Commercial**

Commercial lands provide the main locations for the purchase and sale of goods and services. In order to strengthen Windsor's economy, ensure convenient access and address compatibility concerns, Commercial land uses are provided under three designations on Schedule D: Mixed Use Centre, Mixed Use Corridor and Mixed Use Nodes.

Over the lifetime of this Plan, the Mixed Use Centres will evolve to become vibrant mixed-use commercial and residential areas. Ideally, the predominant form of new or redeveloped housing should be medium and high-density residential buildings with ground floor and possibly second floor commercial uses and upper floor residential dwellings.

The following objectives and policies establish the framework for development decisions in all Commercial areas.

2M) Add the 6.5.1.8 to the Chapter 6 – Land Use:

<i>RESIDENTIAL INTENSIFICATION</i>	6.5.1.8	To promote residential intensification with medium and high profile buildings to meet the housing needs of the City in appropriate areas where municipal services, transit and employment are in proximity.
--	---------	---

2N) Delete 6.5.2.2 and replace it with the following:

*RESIDENTIAL  
AND ANCILLARY  
USES* 6.5.2.2

Medium and high profile residential uses either as stand-alone buildings or part of a commercial-residential mixed use buildings shall be integrated within the Mixed Use Centres in a manner that creates a mixed-use community in a modern and attractive urban environment.

Institutional uses, community, cultural, recreational and entertainment facilities shall be permitted in stand-alone buildings, or in mixed-use buildings/developments. Hotels, institutional uses, community, cultural, recreational and entertainment facilities may be located on individual sites, or form part of a larger, comprehensively planned retail commercial centre.

In addition to the uses permitted above, Council may permit ancillary Open Space and Major Institutional uses in areas designated as Mixed Use Centre on Schedule D: Land Use without requiring an amendment to this Plan provided that:

- (a) the ancillary use is clearly incidental and secondary to, and complementary with, the main commercial use; and
- (b) the development satisfies the policies for the proposed land use.

2O) Add 6.5.2.6 (i) to 6.5.2.6:

- (i) Council will adopt Design Guidelines that will assist in the design and review of development applications in a manner that will ensure implementation of these policies.

2P) Delete 6.5.3 and replace it with the following:

The Mixed Use Corridor land use designation is intended for areas which are designed for vehicle oriented Mixed Use uses. Mixed Use Corridors take the form of Mixed Use strips along Arterial and Collector roads within Windsor. These Mixed Use Corridors are expected to provide people-oriented employment and to accommodate higher density/intensity development, while maintaining a broad mix of land uses that support investment in transit and the achievement of complete communities.

2Q) Delete 6.5.3.1 and replace it with the following:

*PERMITTED  
USES* 6.5.3.1

Uses permitted in the Mixed Use Corridor land use designation are primarily retail, wholesale store (added by OPA 58, 24 07 2006) and service oriented uses and, to a lesser extent, office uses.

Medium and High profile residential uses either as stand-alone buildings or part of a commercial-residential mixed use buildings shall be throughout the Corridors.

2R) Delete 6.5.3.3 and replace it with the following:

*STREET*            6.5.3.3       Council will encourage Mixed Use Corridor development to provide a continuous street frontage and presence. Accordingly, development along a Mixed-Use Corridor shall be:

*PRESENCE*

- (a) no more than four storeys in height, except on lands immediately adjacent to an intersection with a Class I or Class II Arterial Road or Class I or Class II Collector Road where the height of buildings shall generally not exceed the width of the road right-of-way abutting the development site; and
- (b) Notwithstanding the identified maximum building height, the City may consider additional height, where the City is satisfied that the proposed height achieves compatible development, and where appropriate transitions to abutting lower scale development are established. Appropriate transitions may be achieved through the implementation of regulatory techniques including, but not limited to new height limitations, enhanced building setbacks and step backs, enhanced landscape buffers and planting requirements and/or the implementation of an angular plane. Permissions for taller buildings may be established through a site specific zoning By-law Amendment;
- (c) encouraged to locate the buildings at the street frontage lot line with parking accommodated at the rear of the site.

2S) Add 6.5.3.8 (f) to 6.5.3.8:

- (i) Council will adopt Design Guidelines that will assist in the design and review of development applications in a manner that will ensure

2T) Delete 6.9 and replace it with the following:

The lands designated as “Mixed Use Nodes” on Schedule D: Land Use provide the main locations for compact clusters of commercial, office, institutional, open space and residential uses. These areas are intended to serve as the focal point for the surrounding neighbourhoods, community. As such, they will be designed with a pedestrian orientation and foster a distinctive and attractive area identity.

The following objectives and policies establish the framework for development decisions in Mixed Use Nodes areas.

2U) Delete 6.9.2.2



2V) Delete 6.9.2.3 and replace it with 6.9.2.2:

*LOCATIONAL CRITERIA* 6.9.2.2 Mixed Use Nodes development shall be located where:

- (a) there is access to Class I or Class II Arterial Roads or Class I Collector Road;
- (b) full municipal physical services can be provided;
- (c) public transportation service can be provided; and
- (d) the surrounding development pattern is compatible with Mixed Use Nodes development.

2W) Delete 6.9.2.5(b) and replace it with 6.9.2.4 (b):

- (b) the mass, scale, orientation, form, and siting of the development achieves a compact urban form and a pedestrian friendly environment. Building should not exceed 4 storeys in height;

2X) Add 8.7.2.3 (j) to the Chapter 8 – Urban Design:

- (j) Council may adopt Design Guidelines that will assist in the design and review of applications for development in accordance with the policies noted above.

2Y) Add 9.3.8 to Chapter 9 – Heritage Conservation:

*RECOGNIZE MATURE NEIGHBOURHOODS AS HERITAGE RESOURCE* 9.3.8 Schedule A-1 illustrates Mature Neighbourhoods in the City. These areas are not designated as Heritage Areas or Heritage Conservation Districts. However, the areas reflect the cultural heritage of the City and should be protected. When considering the development of these areas, the policies of Section 9.3.7(d) shall be applied.

3. THAT Zoning By-law 8600 **BE AMENDED** as follows:

3A) Delete Section 10.1.5.4 and substitute with a new Section 10.1.5.4 as follows:

		Duplex Dwelling	Semi-Detached Dwelling	Single Unit Dwelling
.4	Main Building Height - maximum	9.0 m	9.0 m	9.0 m

3B) Add Section 10.1.5.10 as follows:

	Duplex Dwelling	Semi-Detached Dwelling	Single Unit Dwelling
.10 Gross Floor Area - maximum	400 m <sup>2</sup>	400 m <sup>2</sup>	400 m <sup>2</sup>

3C) Delete Section 10.2.5.4 and substitute with a new Section 10.2.5.4 as follows:

				Duplex Dwelling	Semi-Detached Dwelling	Single Unit Dwelling
.4	Main Building Height	-	9.0 m	9.0 m	9.0 m	9.0 m
	maximum					

3D) Add Section 10.2.5.10 as follows:

			Duplex Dwelling	Semi-Detached Dwelling	Single Unit Dwelling
.10	Gross Floor Area - maximum		400 m <sup>2</sup>	400 m <sup>2</sup>	400 m <sup>2</sup>

3E) Delete Section 10.3.5.4 and substitute with a new Section 10.3.5.4 as follows:

			Duplex Dwelling	Semi-Detached Dwelling	Single Unit Dwelling
.4	Main Building Height	-	9.0 m	9.0 m	9.0 m
	maximum				

3F) Add Section 10.3.5.10 as follows:

			Duplex Dwelling	Semi-Detached Dwelling	Single Unit Dwelling
.10	Gross Floor Area - maximum		400 m <sup>2</sup>	400 m <sup>2</sup>	400 m <sup>2</sup>

3G) Delete Section 10.4.5.4 and substitute with a new Section 10.4.5.4 as follows:

			Duplex Dwelling	Semi-Detached Dwelling	Single Unit Dwelling
.4	Main Building Height	-	9.0 m	9.0 m	9.0 m
	maximum				

3H) Add Section 10.4.5.10 as follows:

			Duplex Dwelling	Semi-Detached Dwelling	Single Unit Dwelling
.10	Gross Floor Area - maximum		400 m <sup>2</sup>	400 m <sup>2</sup>	400 m <sup>2</sup>

3I) Delete Section 10.5.5.4 and substitute with a new Section 10.4.5.4 as follows:

.4	Main Building Height	-	9.0 m
	maximum		

3J) Add Section 10.5.5.10 as follows:

.10	Gross Floor Area - maximum		400 m <sup>2</sup>
-----	----------------------------	--	--------------------

3K) Delete Section 11.1.5.4 and substitute with a new Section 11.1.5.4 as follows:

		Duplex Dwelling	Semi-Detached Dwelling	Single Unit Dwelling
.4	Main Building Height - maximum	9.0 m	9.0 m	9.0 m

3L) Add Section 11.1.5.10 as follows:

		Duplex Dwelling	Semi-Detached Dwelling	Single Unit Dwelling
.10	Gross Floor Area - maximum	400 m <sup>2</sup>	400 m <sup>2</sup>	400 m <sup>2</sup>

3M) Delete Section 11.2.5 and substitute with a new Section 11.2.5 as follows:

#### **11.2.5 PROVISIONS**

.1 Duplex Dwelling				
.1	Lot Width – minimum		12.0 m	
.2	Lot Area – minimum		360.0 m <sup>2</sup>	
.3	Lot Coverage – maximum		45.0%	
.4	Main Building Height – maximum		9.0 m	
.5	Front Yard Depth – minimum		6.0 m	
.6	Rear Yard Depth – minimum		7.50 m	
.7	Side Yard Width – minimum		1.20 m	
.10	Gross Floor Area – maximum		400 m <sup>2</sup>	
.2 Semi-Detached Dwelling				
.1	Lot Width – minimum		15.0 m	
.2	Lot Area – minimum		450.0 m <sup>2</sup>	
.3	Lot Coverage – maximum		45.0%	
.4	Main Building Height – maximum		9.0 m	
.5	Front Yard Depth – minimum		6.0 m	
.6	Rear Yard Depth – minimum		7.50 m	
.7	Side Yard Width – minimum		1.20 m	
.10	Gross Floor Area – maximum		400 m <sup>2</sup>	
.3 Single Unit Dwelling				
.1	Lot Width – minimum		9.0 m	
.2	Lot Area – minimum		270.0 m <sup>2</sup>	
.3	Lot Coverage – maximum		45.0%	
.4	Main Building Height – maximum		9.0 m	
.5	Front Yard Depth – minimum		6.0 m	
.6	Rear Yard Depth – minimum		7.50 m	
.7	Side Yard Width – minimum		1.20 m	
.10	Gross Floor Area – maximum		400 m <sup>2</sup>	
.4 Double Duplex Dwelling or Multiple Dwelling				
.1	Lot Width – minimum		18.0 m	
.2	Lot Area – minimum		540.0 m <sup>2</sup>	
.3	Lot Coverage – maximum		45.0%	

.4	Main Building Height – maximum	9.0 m
.5	Front Yard Depth – minimum	6.0 m
.6	Rear Yard Depth – minimum	7.50 m
.7	Side Yard Width – minimum	1.80 m
.5	Townhome Dwelling	
.1	Lot Width – minimum	20.0 m
.2	Lot Area – per <i>dwelling unit</i> – minimum	200.0 m <sup>2</sup>
.3	Lot Coverage – maximum	45.0%
.4	Main Building Height – maximum	9.0 m
.5	Front Yard Depth – minimum	6.0 m
.6	Rear Yard Depth – minimum	7.50 m
.7	Side Yard Width – minimum	1.50 m

3N) Delete Section 11.3.5.4 and substitute with a new Section 11.3.5.4 as follows:

		Semi-Detached Dwelling	Single Unit Dwelling	Townhome Dwelling
.4	Main Building Height - maximum	9.0 m	9.0 m	9.0 m

3O) Add Section 11.3.5.10 as follows:

		Semi-Detached Dwelling	Single Unit Dwelling	Townhome Dwelling
.10	Gross Floor Area - maximum	400 m <sup>2</sup>	400 m <sup>2</sup>	400 m <sup>2</sup>

3P) Delete Section 11.4.5.4 and substitute with a new Section 11.4.5.4 as follows:

		Semi-Detached Dwelling	Single Unit Dwelling
.4	Main Building Height - maximum	9.0 m	9.0 m

3Q) Add Section 11.4.5.10 as follows:

		Semi-Detached Dwelling	Single Unit Dwelling
.10	Gross Floor Area - maximum	400 m <sup>2</sup>	400 m <sup>2</sup>

3R) Delete Section 5.2.20.1

3. THAT Interim Control By-law 103-2020 **BE REPEALED** when the amending by-laws that implement the Official Plan Amendment and Zoning By-law amendments are in force.

4. THAT the City of Windsor Intensification Guidelines **BE ADOPTED** as the Design Guidelines referenced in the Official Plan to evaluate Infill and Intensification development proposals.

Carried.

Report Number: S 64/2022  
Clerk's File: Z/14384 & Z/13872

**Clerk's Note:**

1. The recommendation of the Standing Committee and Administration are **not** the same.
2. Please refer to Item 7.1. from the Development & Heritage Standing Committee Meeting held June 6, 2022.
3. To view the stream of this Standing Committee meeting, please refer to:  
<http://csg001-harmony.sliq.net/00310/Harmony/en/PowerBrowser/PowerBrowserV2/20220608/-1/7334>

**Subject: Multi-Residential Interim Control By-law Study - Proposed Official Plan Amendment and Zoning By-law Amendment**

**Reference:**

Date to Council: June 6, 2022  
Author: Laura Strahl, MCIP, RPP  
Planner III - Special Projects  
lstrahl@citywindsor.ca  
519-255-6543 x6396  
Planning & Building Services  
Report Date: May 20, 2022  
Clerk's File #: Z/14384 Z/13872

**To:** Mayor and Members of City Council

**Recommendation:**

1. THAT the reports titled "MRICBL Background Study" prepared by Municipal Planning Consultants, dated April 2022, attached as Appendix A to Report S64/2022 **BE ACCEPTED**.

2. THAT Volume 1: The Primary Plan of the City of Windsor Official Plan ("Official Plan") **BE AMENDED** as follows:

2A) Change the text of the Official Plan as follows:

- a) "Commercial Corridor" changes to "Mixed Use Corridor"
- b) "Commercial Centre" changes to "Mixed Use Centre"
- c) "Mixed Use" changes to "Mixed Use Node"; and

THAT Schedule A-1: Special Policy Areas of the Official Plan **IS AMENDED** by adding the Mature Neighbourhoods designation as shown on Schedule A-1-1 attached to this report; and,

THAT Schedule D: Land Use of the Official Plan **IS AMENDED** by changing the names of the designations in the Legend as follows:

- i. "Commercial Corridor" changes to "Mixed Use Corridor"
- ii. "Commercial Centre" changes to "Mixed Use Centre"
- iii. "Mixed Use" changes to "Mixed Use Node"; and

THAT Schedule D: Land Use of the Official Plan **IS FURTHER AMENDED** by changing the existing land use designations to the Mixed Use Corridor, Mixed Use Centre, and Mixed Use Node designations as shown on Schedule D-1 attached to this report.

2B) Add the following to the Chapter 6 – Land Use:

<i>RESIDENTIAL INTENSIFICATION</i>	6.1.14	To direct residential intensification to those areas of the City where transportation, municipal services, community facilities and goods and services are readily available
--	--------	--

2C) Delete 6.3.1.3 and replace it with the following:

<i>INTENSIFICATION, INFILL &amp; REDEVELOPMENT</i>	6.3.1.3	To promote residential redevelopment, infill and intensification initiatives in appropriate locations in the City.
--	---------	--

2D) Delete 6.3.2.1 and replace it with the following:

<i>PERMITTED USES</i>	6.3.2.1	Uses permitted in the Residential land use designation identified on Schedule D: Land Use include Low Profile and Medium Profile dwelling units.  High Profile Residential Buildings shall be directed to locate in the City Centre, Mixed Use Centres and Mixed Use Corridors.
---------------------------	---------	---

2E) Delete 6.3.2.3 and replace it with the following:

<i>TYPES OF LOW PROFILE HOUSING</i>	6.3.2.3	For the purposes of this Plan, Low Profile housing development is further classified as follows:  (a) small scale forms: single detached, semi-detached, duplex and row and multiplexes with up to 8 units.
---	---------	---

2F) Delete 6.3.2.4 and replace it with the following:

<i>LOCATIONAL CRITERIA</i>	6.3.2.4	Residential intensification shall be directed to the Mixed Use Nodes and areas generally within 200 metres of those Nodes. Within these areas mid-profile buildings, up to 4 storeys in height shall be permitted. These taller buildings shall be designed to provide a transition in height and massing from low-profile areas. Residential development shall be located where:  (a) there is access to a collector or arterial road;  (b) full municipal physical services can be provided;  (c) adequate community services and open spaces are available or are planned; and  (d) public transportation service can be provided.
--------------------------------	---------	---

2G) Delete 6.3.2.5 (c) and replace it with the following:

- (c) In existing neighbourhoods, compatible with the surrounding area in terms of scale, massing, height, siting, orientation, setbacks, parking and amenity areas.

In Mature Neighbourhoods as shown on Schedule A-1, compatible with the surrounding area, as noted above, and consistent with the streetscape, architectural style and materials, landscape character and setback between the buildings and streets;

2H) Delete 6.3.2.5 (f) and replace it with the following:

- (f) facilitating a gradual transition from Low Profile residential development to Medium and/or High profile development and vice versa, where appropriate. In accordance with Design Guidelines approved by Council.

2I) Delete 6.3.2.9 and replace it with the following:

*NEIGHBOURHOOD  
COMMERCIAL  
EVALUATION  
CRITERIA*

6.3.2.9

Neighbourhood Commercial uses shall be encouraged to locate in Mixed Use Nodes as shown on Schedule J. Ideally these uses would form part of a multi-use building with residential uses located above or behind the non-residential uses on the street front.

At the time of submission, the proponent shall demonstrate to the satisfaction of the Municipality that a proposed Neighbourhood Commercial development within a designated Residential area is:

- (a) feasible having regard to the other provisions of this Plan, provincial legislation, policies and appropriate guidelines and support studies for uses:
  - (i) within or adjacent to any area identified on Schedule C: Development Constraint Areas and described in the Environment chapter of this Plan;
  - (ii) adjacent to sources of nuisance, such as noise, odour, vibration and dust;
  - (iii) within a site of potential or known contamination;



- (iv) where traffic generation and distribution is a provincial or municipal concern;
  - (v) adjacent to heritage resources; and
  - (vi) where market impact is identified as a municipal concern;
- (b) in keeping with the goals, objectives and policies of any secondary plan or guideline plan affecting the surrounding area;
  - (c) compatible with the surrounding area in terms of scale, massing, height, siting, orientation, setbacks, parking and landscaped areas;
  - (d) capable of being provided with full municipal physical services and emergency services; and
  - (e) provided with adequate off-street parking.

2J) Delete 6.3.2.17 and replace it with the following:

*HERITAGE  
CONSERVATION*

6.3.2.17

Council shall encourage the retention, restoration and sensitive renovation of historic and/or architecturally significant residential buildings in accordance with the Heritage Conservation chapter of this Plan.

Infill and intensification within Mature Neighbourhoods, shown on Schedule A-1, shall be consistent with the built form, height, massing, architectural and landscape of the area. Council will adopt Design Guidelines to assist in the design and review of development in these areas.

2K) Delete 6.3.2.29 and replace it with the following:

*COMPATIBLE  
ADDITIONAL  
UNITS*

6.3.2.29

The creation of additional units through renovation or redevelopment in existing residential neighbourhoods shall be done in a manner that is compatible and complimentary to the character of the neighbourhood. The Zoning By-law will establish regulations for height, density, and massing that will preserve the character of stable neighbourhoods. Council will adopt Design Guidelines to assist in the design and review of development applications within existing stable neighbourhoods.

2L) Delete 6.5 and replace it with the following:

## 6.5 Commercial

Commercial lands provide the main locations for the purchase and sale of goods and services. In order to strengthen Windsor's economy, ensure convenient access and address compatibility concerns, Commercial land uses are provided under three designations on Schedule D: Mixed Use Centre, Mixed Use Corridor and Mixed Use Nodes.

Over the lifetime of this Plan, the Mixed Use Centres will evolve to become vibrant mixed-use commercial and residential areas. Ideally, the predominant form of new or redeveloped housing should be medium and high-density residential buildings with ground floor and possibly second floor commercial uses and upper floor residential dwellings.

The following objectives and policies establish the framework for development decisions in all Commercial areas.

2M) Add the 6.5.1.8 to the Chapter 6 – Land Use:

<i>RESIDENTIAL INTENSIFICATION</i>	6.5.1.8	To promote residential intensification with medium and high profile buildings to meet the housing needs of the City in appropriate areas where municipal services, transit and employment are in proximity.
--	---------	---

2N) Delete 6.5.2.2 and replace it with the following:

<i>RESIDENTIAL AND ANCILLARY USES</i>	6.5.2.2	Medium and high profile residential uses either as stand-alone buildings or part of a commercial-residential mixed use buildings shall be integrated within the Mixed Use Centres in a manner that creates a mixed-use community in a modern and attractive urban environment.
---	---------	--

Institutional uses, community, cultural, recreational and entertainment facilities shall be permitted in stand-alone buildings, or in mixed-use buildings/developments. Hotels, institutional uses, community, cultural, recreational and entertainment facilities may be located on individual sites, or form part of a larger, comprehensively planned retail commercial centre.

In addition to the uses permitted above, Council may permit ancillary Open Space and Major Institutional uses in areas designated as Mixed Use Centre on Schedule D: Land Use without requiring an amendment to this Plan provided that:

- (a) the ancillary use is clearly incidental and secondary to, and complementary with, the main commercial use; and

- (b) the development satisfies the policies for the proposed land use.

2O) Add 6.5.2.6 (i) to 6.5.2.6:

- (i) Council will adopt Design Guidelines that will assist in the design and review of development applications in a manner that will ensure implementation of these policies.

2P) Delete 6.5.3 and replace it with the following:

The Mixed Use Corridor land use designation is intended for areas which are designed for vehicle oriented Mixed Use uses. Mixed Use Corridors take the form of Mixed Use strips along Arterial and Collector roads within Windsor. These Mixed Use Corridors are expected to provide people-oriented employment and to accommodate higher density/intensity development, while maintaining a broad mix of land uses that support investment in transit and the achievement of complete communities.

2Q) Delete 6.5.3.1 and replace it with the following:

<i>PERMITTED USES</i>	<b>6.5.3.1</b>	Uses permitted in the Mixed Use Corridor land use designation are primarily retail, wholesale store (added by OPA 58, 24 07 2006) and service oriented uses and, to a lesser extent, office uses.
		Medium and High profile residential uses either as stand-alone buildings or part of a commercial-residential mixed use buildings shall be throughout the Corridors.

2R) Delete 6.5.3.3 and replace it with the following:

<i>STREET PRESENCE</i>	<b>6.5.3.3</b>	Council will encourage Mixed Use Corridor development to provide a continuous street frontage and presence. Accordingly, development along a Mixed-Use Corridor shall be: <ul style="list-style-type: none"><li>(a) no more than four storeys in height, except on lands immediately adjacent to an intersection with a Class I or Class II Arterial Road or Class I or Class II Collector Road where the height of buildings shall generally not exceed the width of the road right-of-way abutting the development site; and</li></ul>
----------------------------	----------------	--

- (b) Notwithstanding the identified maximum building height, the City may consider additional height, where the City is satisfied that the proposed height achieves compatible development, and where appropriate transitions to abutting lower scale development are established. Appropriate transitions may be achieved through the implementation of regulatory techniques including, but not limited to new height limitations, enhanced building setbacks and step backs, enhanced landscape buffers and planting requirements and/or the implementation of an angular plane. Permissions for taller buildings may be established through a site specific zoning By-law Amendment;
- (c) encouraged to locate the buildings at the street frontage lot line with parking accommodated at the rear of the site.

2S) Add 6.5.3.8 (f) to 6.5.3.8:

- (i) Council will adopt Design Guidelines that will assist in the design and review of development applications in a manner that will ensure

2T) Delete 6.9 and replace it with the following:

The lands designated as “Mixed Use Nodes” on Schedule D: Land Use provide the main locations for compact clusters of commercial, office, institutional, open space and residential uses. These areas are intended to serve as the focal point for the surrounding neighbourhoods, community. As such, they will be designed with a pedestrian orientation and foster a distinctive and attractive area identity.

The following objectives and policies establish the framework for development decisions in Mixed Use Nodes areas.

2U) Delete 6.9.2.2

2V) Delete 6.9.2.3 and replace it with 6.9.2.2:

<i>LOCATIONAL CRITERIA</i>	6.9.2.2	Mixed Use Nodes development shall be located where: <ul style="list-style-type: none"><li>(a) there is access to Class I or Class II Arterial Roads or Class I Collector Road;</li><li>(b) full municipal physical services can be provided;</li><li>(c) public transportation service can be provided; and</li><li>(d) the surrounding development pattern is compatible with Mixed Use Nodes development.</li></ul>
--------------------------------	---------	---

2W) Delete 6.9.2.5(b) and replace it with 6.9.2.4 (b):

- (b) the mass, scale, orientation, form, and siting of the development achieves a compact urban form and a pedestrian friendly environment. Building should not exceed 4 storeys in height;

2X) Add 8.7.2.3 (j) to the Chapter 8 – Urban Design:

- (j) Council may adopt Design Guidelines that will assist in the design and review of applications for development in accordance with the policies noted above.

2Y) Add 9.3.8 to Chapter 9 – Heritage Conservation:

<i>RECOGNIZE MATURE NEIGHBOURHOODS AS HERITAGE RESOURCE</i>	9.3.8	Schedule A-1 illustrates Mature Neighbourhoods in the City. These areas are not designated as Heritage Areas or Heritage Conservation Districts. However, the areas reflect the cultural heritage of the City and should be protected. When considering the development of these areas, the policies of Section 9.3.7(d) shall be applied.
---	-------	--

3. THAT Zoning By-law 8600 **BE AMENDED** as follows:

3A) Delete Section 10.1.5.4 and substitute with a new Section 10.1.5.4 as follows:

	Duplex Dwelling	Semi-Detached Dwelling	Single Unit Dwelling
.4 Main Building Height - maximum	9.0 m	9.0 m	9.0 m

3B) Add Section 10.1.5.10 as follows:

	Duplex Dwelling	Semi-Detached Dwelling	Single Unit Dwelling
.10 Gross Floor Area - maximum	400 m <sup>2</sup>	400 m <sup>2</sup>	400 m <sup>2</sup>

3C) Delete Section 10.2.5.4 and substitute with a new Section 10.2.5.4 as follows:

	Duplex Dwelling	Semi-Detached Dwelling	Single Unit Dwelling
.4 Main Building Height - maximum	9.0 m	9.0 m	9.0 m

3D) Add Section 10.2.5.10 as follows:

	Duplex Dwelling	Semi-Detached Dwelling	Single Unit Dwelling
.10 Gross Floor Area - maximum	400 m <sup>2</sup>	400 m <sup>2</sup>	400 m <sup>2</sup>

3E) Delete Section 10.3.5.4 and substitute with a new Section 10.3.5.4 as follows:

		Duplex Dwelling	Semi-Detached Dwelling	Single Unit Dwelling
.4	Main Building Height - maximum	9.0 m	9.0 m	9.0 m

3F) Add Section 10.3.5.10 as follows:

		Duplex Dwelling	Semi-Detached Dwelling	Single Unit Dwelling
.10	Gross Floor Area - maximum	400 m <sup>2</sup>	400 m <sup>2</sup>	400 m <sup>2</sup>

3G) Delete Section 10.4.5.4 and substitute with a new Section 10.4.5.4 as follows:

		Duplex Dwelling	Semi-Detached Dwelling	Single Unit Dwelling
.4	Main Building Height - maximum	9.0 m	9.0 m	9.0 m

3H) Add Section 10.4.5.10 as follows:

		Duplex Dwelling	Semi-Detached Dwelling	Single Unit Dwelling
.10	Gross Floor Area - maximum	400 m <sup>2</sup>	400 m <sup>2</sup>	400 m <sup>2</sup>

3I) Delete Section 10.5.5.4 and substitute with a new Section 10.4.5.4 as follows:

.4	Main Building Height - maximum	9.0 m
----	--------------------------------	-------

- 3J) Add  
 Section .10 Gross Floor Area - maximum 400 m<sup>2</sup>  
 10.5.5.  
 10 as follows:

- 3K) Delete Section 11.1.5.4 and substitute with a new Section 11.1.5.4 as follows:

	Duplex Dwelling	Semi-Detached Dwelling	Single Unit Dwelling
.4 Main Building Height - maximum	9.0 m	9.0 m	9.0 m

- 3L) Add Section 11.1.5.10 as follows:

	Duplex Dwelling	Semi-Detached Dwelling	Single Unit Dwelling
3M) Delete Section .10 Gross Floor Area - maximum 400 m <sup>2</sup> 11.2.5 and substitute with a new Section 11.2.5 as follows:	400 m <sup>2</sup>	400 m <sup>2</sup>	400 m <sup>2</sup>

## 11.2.5 PROVISIONS

### .1 Duplex Dwelling

.1 Lot Width – minimum	12.0 m
.2 Lot Area – minimum	360.0 m <sup>2</sup>
.3 Lot Coverage – maximum	45.0%
.4 Main Building Height – maximum	9.0 m
.5 Front Yard Depth – minimum	6.0 m
.6 Rear Yard Depth – minimum	7.50 m
.7 Side Yard Width – minimum	1.20 m
.10 Gross Floor Area – maximum	400 m <sup>2</sup>

### .2 Semi-Detached Dwelling

.1 Lot Width – minimum	15.0 m
.2 Lot Area – minimum	450.0 m <sup>2</sup>
.3 Lot Coverage – maximum	45.0%
.4 Main Building Height – maximum	9.0 m
.5 Front Yard Depth – minimum	6.0 m
.6 Rear Yard Depth – minimum	7.50 m
.7 Side Yard Width – minimum	1.20 m



.10	Gross Floor Area – maximum	400 m <sup>2</sup>
.3	Single Unit Dwelling	
.1	Lot Width – minimum	9.0 m
.2	Lot Area – minimum	270.0 m <sup>2</sup>
.3	Lot Coverage – maximum	45.0%
.4	Main Building Height – maximum	9.0 m
.5	Front Yard Depth – minimum	6.0 m
.6	Rear Yard Depth – minimum	7.50 m
.7	Side Yard Width – minimum	1.20 m
.10	Gross Floor Area – maximum	400 m <sup>2</sup>
.4	Double Duplex Dwelling or Multiple Dwelling	
.1	Lot Width – minimum	18.0 m
.2	Lot Area – minimum	540.0 m <sup>2</sup>
.3	Lot Coverage – maximum	45.0%
.4	Main Building Height – maximum	9.0 m
.5	Front Yard Depth – minimum	6.0 m
.6	Rear Yard Depth – minimum	7.50 m
.7	Side Yard Width – minimum	1.80 m
.10	Gross Floor Area – maximum	400 m <sup>2</sup>
.5	Townhome Dwelling	
.1	Lot Width – minimum	20.0 m
.2	Lot Area – per <i>dwelling unit</i> – minimum	200.0 m <sup>2</sup>
.3	Lot Coverage – maximum	45.0%
.4	Main Building Height – maximum	9.0 m
.5	Front Yard Depth – minimum	6.0 m
.6	Rear Yard Depth – minimum	7.50 m
.7	Side Yard Width – minimum	1.50 m
.10	Gross Floor Area – maximum	400 m <sup>2</sup>

3N) Delete Section 11.3.5.4 and substitute with a new Section 11.3.5.4 as follows:

	Semi-Detached Dwelling	Single Unit Dwelling	Townhome Dwelling
.4 Main Building Height - maximum	9.0 m	9.0 m	9.0 m

3O) Add Section 11.3.5.10 as follows:

Semi-	Single Unit	Townhome
-------	-------------	----------

	Detached Dwelling	Dwelling	Dwelling
.10 Gross Floor Area - maximum	400 m <sup>2</sup>	400 m <sup>2</sup>	400 m <sup>2</sup>

3P) Delete Section 11.4.5.4 and substitute with a new Section 11.4.5.4 as follows:

	Semi-Detached Dwelling	Single Unit Dwelling
.4 Main Building Height - maximum	9.0 m	9.0 m

3Q) Add Section 11.4.5.10 as follows:

	Semi-Detached Dwelling	Single Unit Dwelling
.10 Gross Floor Area - maximum	400 m <sup>2</sup>	400 m <sup>2</sup>

3R) Delete Section 5.2.20.1

4. THAT Interim Control By-law 103-2020 **BE REPEALED** when the amending by-laws that implement the Official Plan Amendment and Zoning By-law amendments are in force.
5. THAT the City of Windsor Intensification Guidelines **BE ADOPTED** as the Design Guidelines referenced in the Official Plan to evaluate Infill and Intensification development proposals.

### Executive Summary:

On July 13th, 2020, Council approved Interim Control By-law 103-2020 that prohibits the use on all lands, buildings, and structures for a Group Home, Shelter, Lodging House, and a Dwelling with five or more dwelling units, other than those exempted in the Interim Control By-law in order to conduct a study. Council approved an extension to By-law 99-2021 on May 18, 2022 for an additional year. The Interim Control By-law will expire on July 13, 2022.

The City hired a consortium of consultants made up of Municipal Planning Consultants, The Planning Partnership and The Altus Group (the “consultants”) to undertake the study. The main purpose of the Multi-Residential Interim Control By-law (MRICBL) Study is to determine the following:

- The appropriate locations within the city that can accommodate additional residential density;
- How to appropriately guide growth to those geographic areas;
- The extent to which a designated area can accommodate growth;
- How to ensure compatibility within the existing neighbourhood context; and
- The MRICBL Study also includes a review of Group Homes, Lodging Houses and Shelters to bring in compliance with the Human Rights Code.

The consultants have completed three reports that contain recommendations for the City to consider implementing to achieve the purposes of the MRICBL Study. Some of these recommendations will be implemented with the adoption of the OPA and Zoning Bylaw amendments contained in the recommendations of this report, while others will form part of an ongoing work program for the City.

The proposed Official Plan Amendment (OPA) implements a number of the recommendations that start to put the policy direction around infill and intensification in place. This policy direction focus intensification to areas that can support it and where it is most appropriate. The majority of intensification will be guided to “Intensification Priority Areas” made up of Mixed Use Centres, Mixed Use Corridors and Mixed Use Nodes, all of which will be identified on a new Official Plan Schedule D1.

Compatibility of proposed development within existing neighbourhoods is also addressed in the amendments by providing some direction about defining and describing it, as well as making it a requirement of all new development. The OPA also provides the policy foundation for adopting the Intensification Guidelines as a way to help design and evaluate development proposals for compatibility with surrounding neighbourhood.

The proposed ZBA focuses primarily on addressing the compatibility of infill development proposals in existing mature/stable neighbourhoods.

This report also recommends the adoption of the Windsor Intensification Guidelines to provide direction for the design of future residential projects that respect the unique character of Windsor’s existing neighbourhoods. The Urban Design Guidelines will provide predictability for applicants, the City, and stakeholders, by providing consistent direction about the criteria for the design of proposed development in Intensification Areas. The provisions, and examples in the Intensification Guidelines will be used as the foundation of design for intensification projects, and will be used in the assessment development proposals.

The MRICBL Study includes a review of definitions of Group Home, Lodging House and Shelter in Zoning By-law 8600 and 85-18. These definitions have been problematic in the way they are defined in the zoning by-law and need to be addressed for compliance with the Ontario Human Rights Code. Staff are continuing to work with the consultants to develop appropriate definitions and zoning requirements that are consistent with the OHRC and what the *Planning Act* can enable. A subsequent ZBA will be coming to Standing Committee and Council in the near future to address the definitional issues.

Additionally, staff have been reviewing opportunities within the existing zoning categories to allow more “as of right” housing options. A subsequent ZBA will be coming to Standing Committee and Council to “pre-zone” some of the Intensification Priority Areas to provide for more housing options.

### **Background:**

Section 38(1) of the Planning Act permits a municipality to pass an interim control by-law (ICBL) that prohibits the use of land, buildings or structures for such purposes as set out in the by-law. This in effect “freezes” development (that is described by the by-law) on the lands for a period not to exceed one year. An ICBL is an important planning tool that allows the municipality to rethink its current land use policies by suspending development that may end up conflicting with any new policy that may be developed. However, in order to enact an ICBL, Council must direct that a review or study be undertaken with respect to the existing land use policies in question.

On July 13th, 2020, Council approved Interim Control By-law 103-2020 that prohibits the use on all lands, buildings, and structures for a Group Home, Shelter, Lodging House, and a Dwelling with five or more dwelling units, other than those exempted in the Interim Control By-law in order to conduct a study. No appeals were received. This will allow Administration to review and, if deemed appropriate, implement the findings of the said study.

On May 18, 2021, Council approved By-law 99-2021 that extended Interim Control By-law 103-2020 for an additional year. The Interim Control By-law will expire on July 13, 2022.

The City hired a consortium of consultants made up of Municipal Planning Consultants, The Planning Partnership and The Altus Group (the “consultants”) to undertake the study. The consultants have now completed the background studies and drafted amendments to the City’s Official Plan and Zoning By-law 8600 to implement the recommendations of the background reports.

The purpose of this report is to recommend amendments to the Official Plan and Zoning By-law 8600 and By-law 85-18 that implement the findings of the Multi-Residential Interim Control By-law (MRICBL) Study.

### **Discussion:**

The main purpose of the MRICBL Study is to determine the following:

- The appropriate locations within the city that can accommodate additional residential density;
- How to appropriately guide growth to those geographic areas;
- The extent to which a designated area can accommodate growth;
- How to ensure compatibility within the existing neighbourhood context; and
- The MRICBL Study also includes a review of Group Homes, Lodging Houses and Shelters to bring in compliance with the Human Rights Code.

To address the above items, the consultants completed three reports:

*Demographics and Economic Analysis (Altus Group)* (attached as Appendix A) – This report provides demographic and economic analysis on factors driving intensification and infill demand in the City to inform planning policy recommendations for necessary changes to the City of Windsor Official Plan and Zoning By-law to achieve the goal of increased intensification within the City's existing built-up area.

*The Multi-Residential Interim Control Bylaw Study Background Report (Municipal Planning Consultants and The Planning Partnership)* (attached as Appendix B) – This report reviews relevant legislation, and City of Windsor policy documents and makes recommendations to address the main purposes of the study.

*Intensification Guidelines* (The Planning Partnership) (attached as Appendix C) – This document contains design guidelines that are intended as a framework that outlines the characteristics of various design concepts and principles associated with infill and intensification. The intent is to guide new development to become distinctive, while contextually compatible with existing neighbourhoods.

Each report contains recommendations for the City to consider implementing to achieve the purposes of the MRICBL Study. Some of these recommendations will be implemented with the adoption of the OPA and Zoning Bylaw amendments contained in the recommendations of this report, while others will form part of an ongoing work program for the City. The key findings of the above reports are as follows:

### **The Demographic Report:**

The Demographic Report was prepared by the Altus Group. This report provides demographic and economic analysis on factors driving intensification and infill demand in the City to inform planning policy recommendations for necessary changes to the City's Official Plan and Zoning By-law with the goal of increasing intensification within the City's existing built-up area. The analysis contained in the report was used to inform the policy recommendations made in the subsequent consultant reports.

The following is a summary of the findings of the Demographic Report:

- The City needs more housing in the inner areas of the City, with populations in a majority of the City declining due to a lack of new housing options and shrinking average household sizes;
- The City is not seeing enough purpose-built rental housing constructed to meet demand;
- Access to retail, transit and other community amenities can bolster the market for new residential development, making an area attractive to prospective new households;
- Similarly, adding residential uses near existing retail clusters can improve the viability of those retail environments. The practice of redeveloping major retail centres for a mix of uses including residential, as well as other community amenities such as parks, community centres, and even additional retail is growing across Ontario and Canada;
- The introduction of residential uses in close proximity to established retail centres can help to add value for retailers and allow for a mix of uses that make for convenient shopping for new residents, as well as help boost demand for additional retail uses in some cases;
- The City needs to account for prospective growth in post-secondary enrolment in forecasting housing needs; and,
- Given the significant under supply of industrial space in the Province and Southwestern Ontario, the City should be seeking to retain as many occupied and vacant employment lands as possible, unless there are compelling reasons to allow residential uses on them.

### **The Multi-Residential Interim Control Bylaw Study Background Report:**

Enabling infilling and intensification in the city can help to achieve a number of Provincial and municipal objectives. As such, the main purpose of the study prepared by Municipal Planning Consultants and The Planning Partnership is to

- Determine the appropriate locations within the City that can accommodate additional residential density;
- How to appropriately guide growth to those geographic areas;
- Determine to what extent a designated area can accommodate growth; and,
- How to ensure compatibility within the existing neighbourhood context.

A comprehensive approach to planning for residential intensification within a definable urban structure framework provides greater clarity and certainty for the developers looking to construction infill and/or higher density projects, and the neighbourhoods that will become home to these projects. This means that there is a clear responsibility for the City to define where intensification initiatives are appropriate and desirable, and equally important, where those intensification activities need to be more significantly planned for and managed.

There is an increasing awareness that the character of existing and historic or mature communities in Windsor is vitally important. The proposed framework encourages intensification in the appropriate areas while also ensuring the compatibility of the development with the surrounding neighbourhood. As such, there has been as much emphasis on ensuring compatible development as there has been on exploring new ways for the city to grow.

In order to develop the framework, and the subsequent recommendations, it is important to understand a number of fundamental principles and the legislative authority under the Planning Act for planning for housing. All regulations of the City's planning instruments must implement planning regulations in accordance with the Ontario Human Rights Code. Section 2.1 of the Code which states:

*Every person has a right to equal treatment with respect to the occupancy of accommodation without discrimination because of race, ancestry, place of origin, colour, ethnic origin, citizenship, creed, sex, sexual orientation, age, marital status, family status, disability or the receipt of public assistance.*

The City cannot use the *Planning Act* to regulate the number of residents in a dwelling unit, the tenure of a dwelling unit, or the relationship to one another of the people living in the dwelling unit. It cannot implement policies or regulations that discriminate, even inadvertently, against anyone based on the any of the reasons cited above.

The City can control the land uses permitted on a lot, the number of dwelling units on a lot, the number and size of buildings on a lot, the location of the buildings on a lot and the height and massing of buildings on a lot and parking requirements. It can also require that new development be compatible with existing neighbourhoods, and implement ways to evaluate this. The Multi-Residential Interim Control Bylaw Study Background Report focuses on a policy framework that the City can implement under its Planning Act authority, while also ensuring compliance with Ontario's Human Rights Code. The following is a summary of the conclusions found within the Multi-Residential Interim Control Bylaw Study Background Report:

- Stand-alone medium and high profile buildings should be permitted in the Regional Centres (designation name to exclude "Commercial"), with a policy framework that ensures appropriate transitions to adjacent communities;
- Medium profile combined (mixed) use buildings should be permitted in the Regional Centres and the Corridors with a policy framework that ensures appropriate transitions to adjacent communities;

- Combined use buildings up to 4 storeys high should be permitted in the Neighbourhood Nodes;
- The Residential policies should include consideration of modestly scaled intensification projects within 50 m of a Neighbourhood Node;
- Mature Neighbourhoods should be identified on Schedule G to the Official Plan;
- Intensification in these areas should be limited to development of a consistent character to what presently exists in those areas in terms of front and side-yard setbacks, height and density;
- The low density Residential Zones should include maximum gross floor area limits and reduce maximum height to 9 metres;
- The minimum dwelling unit size should be eliminated from the Zoning By-law;
- The Residential policies should define limits to intensification that will ensure that re-development for intensification is compatible with the existing built form;
- Additional policies regarding parking and landscaping requirements should be included in the Official Plan;
- The City should amend the Official Plan and Zoning By-law to ensure compliance with the Human Rights Code for various forms of Special Needs Housing; and,
- The Official Plan should enable Council to adopt Design Guidelines and implement those guidelines through architectural control in the development process. Design Guidelines will be implemented through a combination of Associated Official Plan design-focused policies, the Zoning By-law and Site Plan Approval, and potentially through urban/architectural design control processes.

Recommendation 1 is to adopt The Multi-Residential Interim Control Bylaw Study Background Report as the required study from passing the Interim Control By-law and as the supporting documentation for the proposed Official Plan Amendment and Zoning Bylaw Amendments.

## **Proposed Official Plan Amendment**

The proposed Official Plan Amendment (OPA) implements a number of the recommendations above. The OPA puts the policy direction around infill and intensification in place. This policy direction focus intensification to areas that can support it and where it is most appropriate – places that have access to transit, goods and services, infrastructure, etc.

The majority of intensification will be guided to “Intensification Priority Areas” made up of Mixed Use Centres, Mixed Use Corridors and Mixed Use Nodes, all of which will be identified on a new Official Plan Schedule D1.

Compatibility of proposed development within existing neighbourhoods is also addressed in the amendments by providing some direction about defining and describing it, as well as making it a requirement of all new development. The OPA also



provides the policy foundation for adopting the Intensification Guidelines as a way to help design and evaluate development proposals for compatibility with surrounding neighbourhood.

The Official Plan (OP) is implemented by the Zoning Bylaw, so adopting the OPA will allow for the Zoning Bylaw Amendments to be consistent with the OP. The policy framework will also provide guidance for the development of subsequent Zoning Bylaw Amendments that will implement the remaining recommendations contained in the consultant's report.

Recommendation 2 is recommending that the OP be amended in the manner described in the recommendation.

### **Proposed Zoning By-law Amendment**

The proposed Zoning By-law Amendment (ZBA) addresses a number of the recommendations coming out of the Multi-Residential Interim Control Bylaw Study Background Report and begins to implement the policy framework contained within the updated OP.

The proposed ZBA focuses primarily on addressing the compatibility of infill development proposals in existing mature/stable neighbourhoods. The Low Profile Neighbourhoods have existing intensification rights as a result of the Additional Dwelling Unit policies which now permit up to three units on a property. The consultants concluded that the current zoning provisions, particularly the building massing provisions, for the lower profile zoning categories are too permissive and fail to preserve the character of the surrounding neighbourhoods.

As a result, the recommended ZBA reduces the maximum building height from 10 metres to 9 metres in the RD1.1, RD1.2, RD1.3, RD1.4, RD1.5, RD2.1, RD2.2, RD2.3, and RD2.4. It will also limit the total maximum gross floor area to 400m<sup>2</sup> (~4,305 ft<sup>2</sup>) for a property. These changes are intended to make sure that the scale and height of new development is more consistent with what is currently found within many neighbourhoods.

Recommendation 3 describes the proposed amendments to the Comprehensive Zoning By-law and recommends that Zoning By-law be amended.

### **Provincial Policy Statement, 2020**

The Provincial Policy Statement (PPS) provides direction on matters of provincial interest related to land use planning and development and sets the policy foundation for regulating the development and use of land in Ontario.

The vision of the PPS focuses growth and development within urban settlement areas, that land use must be carefully managed to accommodate appropriate development to meet the full range of current and future needs, while achieving efficient development patterns. Planning authorities are encouraged to permit and facilitate a range of housing options, including residential intensification, to respond to current and future needs. Land use patterns should promote a mix of housing, including opportunities for infill and

intensification that support a broad range of housing options. Specific Provincial Policy that applies to this OPA and ZBA are summarized below.

Policy 1.1.1 of the PPS states:

*“Healthy, liveable and safe communities are sustained by:*

- a) promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term;*
- b) accommodating an appropriate affordable and market-based range and mix of residential types (including single-detached, additional residential units, multi-unit housing, affordable housing and housing for older persons), employment (including industrial and commercial), institutional (including places of worship, cemeteries and long-term care homes), recreation, park and open space, and other uses to meet long-term needs;*
- e) promoting the integration of land use planning, growth management, transit-supportive development, intensification and infrastructure planning to achieve cost-effective development patterns, optimization of transit investments, and standards to minimize land consumption and servicing costs;”*

The proposed OPA and ZBA is supportive of an efficient development approach that will have no adverse impact on the financial well-being of the City, land consumption, and servicing costs, and it accommodates an appropriate range of residential uses, and optimizes investments in transit.

Policy 1.1.3.1 of the PPS states:

*“Settlement areas shall be the focus of growth and development.”*

Policy 1.1.3.2 of the PPS states:

*“Land use patterns within settlement areas shall be based on densities and a mix of land uses which:*

- a) efficiently use land and resources;*
- b) are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion;*
- e) support active transportation;*
- f) are transit-supportive, where transit is planned, exists or may be developed;”*

Policy 1.1.3.3 of the PPS states:

*Planning authorities shall identify appropriate locations and promote opportunities for transit-supportive development, accommodating a significant supply and range of housing options through intensification and redevelopment where this can be accommodated taking into account existing building stock or areas, including brownfield sites, and the availability of suitable existing or planned infrastructure and public service facilities required to accommodate projected needs.*

The OPA and ZBA promotes a land use that makes efficient use of land and existing infrastructure, and identifies appropriate locations and promote opportunities for transit-

supportive development and a range of housing options through intensification. Active transportation options and transit services have been taken into consideration in recommending the appropriate locations for additional infill and intensification.

Policy 1.1.3.3 of the PPS states:

*Appropriate development standards should be promoted which facilitate intensification, redevelopment and compact form, while avoiding or mitigating risks to public health and safety.*

The OPA provides development standards that will help to facilitate appropriate intensification and development that is compatible with the surrounding neighbourhood.

### **Windsor Intensification Guidelines**

Building on the principles of ‘compatible’ development and guided by the policies articulated in the Official Plan (OP), the objective of the Intensification Guidelines is to provide direction for the design of future residential projects that respect the unique character of Windsor’s neighbourhoods.

The design guidelines (see Appendix C) are intended to guide new development to become distinctive, while relating harmoniously to the use, scale, architecture, streetscapes, and neighbourhoods of Windsor, as well as meeting the needs of its citizens and visitors. The Urban Design Guidelines will provide predictability for applicants, the City, and stakeholders, by providing consistent direction about the criteria for the design of proposed development in Intensification Areas. The provisions, and examples in the Intensification Guidelines will be used as the foundation of design for intensification projects, and will be used in the assessment development proposals.

Recommendation 5 is to adopt the City of Windsor Intensification as the Design Guidelines referenced in the Official Plan to evaluate Infill and Intensification development proposals.

### **Future Work Program**

There are few areas that were identified in the Background Report that are still being reviewed and will be part of the Planning Department’s short-term work programme. It is anticipated that there will be additional Zoning By-law Amendments required to further support intensification across the city.

The MRICBL Study includes a review of definitions of Group Home, Lodging House and Shelter in Zoning By-law 8600 and 85-18. These definitions have been problematic in the way they are defined in the zoning by-law and need to be addressed for compliance with the Ontario Human Rights Code. Staff are continuing to work with the consultants to develop appropriate definitions and zoning requirements that are consistent with the OHRC and what the *Planning Act* can enable. A subsequent ZBA will be coming to Standing Committee and Council in the near future to addresses the definitional issues.

Additionally, staff have been reviewing opportunities within the existing zoning categories to allow more “as of right” housing options. A subsequent ZBA will be coming to Standing Committee and Council to “pre-zone” some of the Intensification Priority Areas to provide for more housing options.

### **Risk Analysis:**

Given that the Recommendations in this report represent a fundamental change to how the City of Windsor proposes to guide and regulate infill and intensification opportunities throughout the city, there is a significant chance that the recommended OPA and/or ZBA(s) will be appealed to the Ontario Land Tribunal (OLT).

In the event of an appeal to the OLT, the Interim Control By-law will remain in place until such time that all appeals have been resolved.

### **Financial Matters:**

There are no short term financial implications with adoption of these recommendations. The implementation of the recommendations from this report and subsequent Zoning By-law Amendments will provide more housing options in a fiscally responsible way.

### **Consultations:**

An in-person Open House was held at All Saints Church on May 26, 2022 to consult the public on the draft Official Plan Amendment and draft Zoning By-law Amendment.

A special meeting of the Development & Heritage Standing Committee was held on May 9, 2022 to discuss the Background Report and preliminary direction coming from the recommendations. This meeting was open to the public and saw one delegate offer their support for the direction of the study.

A virtual public engagement session was held on April 5, 2022 from 6-8pm to present the findings and recommendations from the background report to interested members of the public. Eighteen (18) people attended the session and provided feedback. The feedback from the session was positive.

### **Planner's Opinion:**

It is the Planner's opinion that the proposed OPA and ZBA related to infill development and intensification are consistent with the Provincial Policy Statement and represent good planning.

### **Conclusion:**

The recommendations above come from the extensive background review and analysis conducted by the consultants hired by the City to prepare the study that is required with the passing of an Interim Control By-law. The consultants provided an infill and intensification framework and a number of recommendations that will be implemented by adopting the OPA and ZBA proposed above, as well as, subsequent Zoning Bylaw Amendments.

## Planning Act Matters:

I concur with the above comments and opinion of the Registered Professional Planner.

Neil Robertson, MCIP, RPP

Thom Hunt, MCIP, RPP

Manager of Urban Design

City Planner

I am not a registered Planner and have reviewed as a Corporate Team Leader

JP

## Approvals:

Neil Robertson	Manager of Urban Design / Deputy City Planner
Thom Hunt	City Planner / Executive Director, Planning & Development Services
Dana Paladino	Acting Commissioner, Legal & Legislative Services
Jelena Payne	Commissioner, Economic Development & Innovation
Shelby Askin Hager	Chief Administration Officer (A)

## Notifications:

Name	Address	Email

## Appendices:

- 1 Appendix A – The ALTUS Background Report
- 2 Appendix B – The Multi-Residential Interim Control Bylaw Study Background Report
- 3 Appendix C – The City of Windsor Intensification Guidelines

# City of Windsor Multi-Residential Interim Control By-law Study

Independent Real Estate Intelligence

**DRAFT REPORT**

**January 30, 2022**



# **City of Windsor Multi-Residential Interim Control By-law Study**

Prepared for:

**City of Windsor**

Prepared by:

**Altus Group Economic Consulting**

33 Yonge Street Toronto Ontario M5E 1G4

Phone: (416) 641-9500 Fax: (416) 641-9501

[economics@altusgroup.com](mailto:economics@altusgroup.com)

[altusgroup.com](http://altusgroup.com)

January 30, 2022

# **DRAFT**

## EXECUTIVE SUMMARY

---

Altus Group Economic Consulting was retained by the City of Windsor to assist in undertaking research into determining appropriate locations in the City that can accommodate additional residential density.

This report provides demographic and economic analysis on factors driving intensification and infill demand in the City to inform planning policy recommendations for necessary changes to the City's Official Plan ("OP") and Zoning By-law to achieve the goal of increased intensification within the City's existing built-up area.

The analysis contained in this report will be used by the City's planning consultants (Municipal Consultants) to bring policy recommendations for the City to promote and encourage growth within the City.

The City experienced a growth in population between 2001-2005, however, it lost population each year over the 2006-2011 period. Since 2012, the City's population has grown, with the rate of growth increasing in the most recent past five years.

The composition of growth has changed in Windsor over the last two decades. When shown by four-year period, the net inflows to the City from domestic (from other provinces or parts of Ontario) and international sources (immigration, non-permanent residents) is roughly 4,500 persons per year between 2017-2020, nearly two-and-a-half times what it was in the prior four-year period (2013-2016). These more recent trends are markedly different than the net outflows seen during the period two four-year periods from 2005-2008 and 2009-2012.

Population losses and gains have not been evenly distributed throughout the City. Of the 52 Census Tracts ("CTs") in the City of Windsor, a total of 43 CTs (or 83%) lost population over the 2006-2016 period. Of the nine (9) CTs that gained population, six (6) were located on the eastern, western, or southern edges of the City's boundary.

The population Inner Area of the City shrunk by nearly 4%, or 5,300 persons, with only one CT in the Inner Area seeing population growth, while the Outer Area outside of the blue line in the Figure below, grew by 6%, or roughly 5,900 persons.



Declining household sizes is also one major driver spurring the need for housing. The City needed 3,030 new homes over the 2006-2016 period just to maintain in the 2006 population levels, exclusive of any growth that occurred to 2016. The shrinking household sizes is also related to an aging population, with age brackets between 45-90+ all seeing increases of persons between 2006-2016, while age brackets below 45 saw losses.

Since 2001, average prices for single-detached units in the City have increased by 232%, from \$177,500 in 2001 to \$588,400 in 2020. While the average prices for single-detached units in Ontario have increased by a similar rate (239%) since 2001, the gap between the average prices in Ontario and the City have increased from \$86,000 in 2001 to over \$300,000 in 2020.

After rising from 3.0% in 2001 to a high of 15.0% in 2008, the vacancy rate in the City has steadily fallen to a low of 2.5% in 2017, and has been below 4.0% in each of the past five years. As expected during periods of high vacancy rates, rents were relatively unchanged over the 2001-2014 period, ranging between \$652 and \$703 per month over the 14-year period. As vacancy rates declined over the 2011-2017 period and have stayed low since, rents have increased to a 20-year high of \$933 per month, an increase of 33% since 2014. The significant decline in vacancy rates and corresponding increase in average rents is indicative of a rental housing market that is in need of additional supply

To understand the overall housing market demand, we have undertaken an analysis of post-secondary enrolment trends for the institutions located in the City. It was projected that total post-secondary student enrolment in Windsor will increase by another 3,500 students, or another 17.7%, between Fall 2020 and Winter 2025.

Currently, there is only one formal student housing space for every 14 students as of the 2019-2020 school year but by the 2024-2025 school year, the City is expected to have 1 formal student housing space for every 10 students. Should there be any delays to the proposed new student housing developments, the City is expected to have 1 formal student housing space for every 16 students.

The recent acceleration of population growth in the City (and the broader Essex County area) has had implications for the price of new housing.

Despite the increase in demand, the amount of new housing constructed in the City has not increased substantially.

It is estimated that the City will see an increase in housing demand in the next 20 years, with total net housing demand forecasted to be:

- 5,432 single/semi-detached units;
- 1,443 townhouse units;
- 3,012 apartment units per year, including 2,317 rental apartments, and 694 condominium apartments; and
- 145 ‘other’ dwellings (such as accessory apartments, etc.)

Based on our analysis of the demographic, and real estate market trends affecting the City, the broader region and the Province of Ontario as a whole, our recommendations are as follows:

- The City needs more housing in the inner areas of the City, with populations in a majority of the City declining due to a lack of new housing options and shrinking average household sizes;
- The City is not seeing enough purpose-built rental housing constructed to meet demand;
- Access to retail, transit and other community amenities can bolster the market for new residential development, making an area attractive to prospective new households;
- Similarly, adding residential uses near existing retail clusters can improve the viability of those retail environments. The practice of redeveloping major retail centres for a mix of uses including residential, as well as other community amenities such as parks, community centres, and even additional retail is growing across Ontario and Canada;
- The City needs to account for prospective growth in post-secondary enrolment in forecasting housing needs.

## TABLE OF CONTENTS

	Page
<b>EXECUTIVE SUMMARY .....</b>	<b>i</b>
<b>1 INTRODUCTION .....</b>	<b>1</b>
1.1 Background .....	1
1.2 Interim Control By-law .....	1
1.3 Approach .....	1
<b>2 POPULATION AND HOUSING TRENDS.....</b>	<b>2</b>
2.1 Population .....	2
2.2 Housing.....	11
2.3 Other Considerations – Population and Housing .....	17
<b>3 EMPLOYMENT AND ECONOMIC TRENDS .....</b>	<b>26</b>
3.1 Employment Trends.....	26
3.2 Non-Residential Market Trends .....	29
<b>4 GROWTH FORECASTS AND HOUSING OUTLOOK .....</b>	<b>35</b>
4.1 Housing Propensity Model (Altus Group Forecasts).....	35
4.2 Summary of City of Windsor Housing Demand Forecast .....	40
<b>5 RECOMMENDATIONS AND CONCLUSIONS.....</b>	<b>42</b>
5.1 Summary of Factors Driving Demand for Housing in Windsor .....	42
5.2 Recommendations and Findings Regarding Most Suitable Locations for Infill and Intensification.....	43

# **1 INTRODUCTION**

---

## **1.1 BACKGROUND**

Altus Group Economic Consulting was retained by the City of Windsor to assist in undertaking research into determining appropriate locations in the City that can accommodate additional residential density.

This report provides demographic and economic analysis on factors driving intensification and infill demand in the City to inform planning policy recommendations for necessary changes to the City's Official Plan ("OP") and Zoning By-law to achieve the goal of increased intensification within the City's existing built-up area.

## **1.2 INTERIM CONTROL BY-LAW**

In July 2020, the City of Windsor Council approved interim control by-law (ICBL) 103-2020 that allows the City to reconsider current land use policies in respect of high-density dwellings such as group homes, shelters, lodging houses, residential care facilities and buildings with five or more dwelling units.

## **1.3 APPROACH**

This study reviews trends relating to population, housing and employment growth in the City, including how the City has changed over the past 10-20 years.

The analysis contained in this report will be used by the City's planning consultants (Municipal Consultants) to bring policy recommendations for the City to promote and encourage growth within the City.

## 2 POPULATION AND HOUSING TRENDS

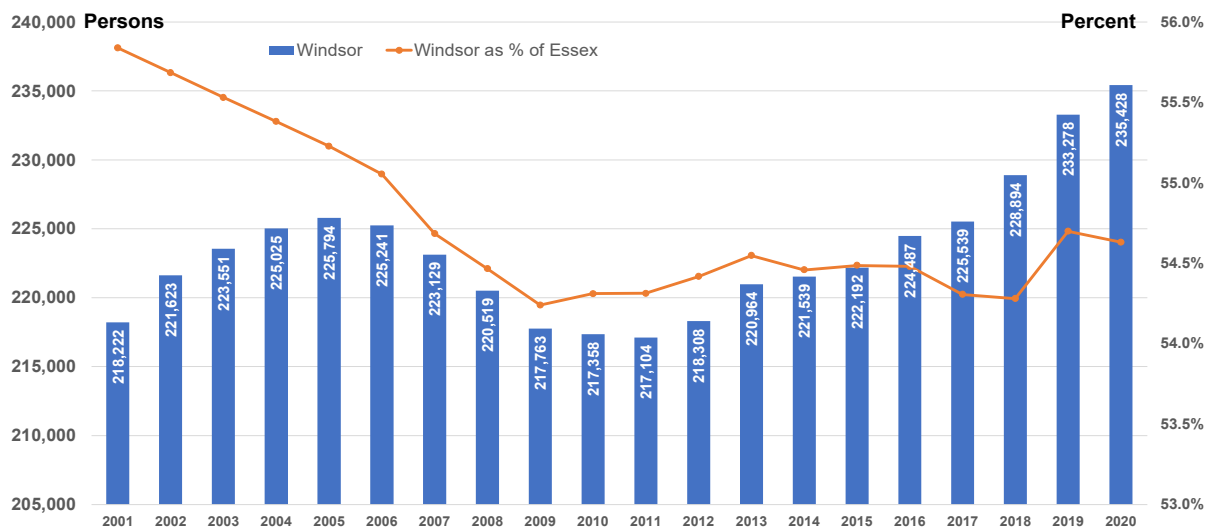
This section of the report reviews historic trends related to the population and composition of population in the City of Windsor.

### 2.1 POPULATION

#### 2.1.1 Change in Population

Figure 1 shows the population of the City of Windsor in total, and as a share of total Essex County population over the 2001-2020 period, as estimated by Statistics Canada. Over the full 20-year period of analysis, the City grew by 7.9%, or 17,200 persons, while the population in Essex County increased by 10.3%, or 40,100 persons. The share of Essex County population in the City of Windsor fell from nearly 56% in 2001 to a low of 54.2% in 2009, but since that time the City's share of County population has remained relatively steady at over 54%.

Figure 1 **Population, City of Windsor, 2001-2020**



Source: Altus Group Economic Consulting based on Statistics Canada Population Estimate, 2001-2020

The City experienced a growth in population between 2001-2005, while it lost population each year over the 2006-2011 period. However, since 2012, the City's population has grown, with the rate of growth increasing in the past five years.

### 2.1.2 Change in Population by Age Group

Figure 2 provides a population breakdown of the number of people by age group in the City of Windsor over the 2006 to 2016 period. The City of Windsor has seen significant losses of population in the age groups of ages 0-14, 25-34, and 35-44, while it has seen large relative population gains in the age groups of ages 55-64, 64-74, and 85-90+. As a population ages, there will be a corresponding increase in number of dwelling units required per person (as persons aged 0-14 will not generate any demand for housing in-and-of-itself).

Overall, the City's population has only grew by 0.3% over the 2006-2016 period, or a total of 710 people, between 2006-2016, however, the distribution of losses and gains has not been even across the demographic spectrum. While the City saw a decline in the number of children and young adults (aged 15-24 and 25-44), it gained persons in the older adults (aged 45-64) and seniors (aged 65+).

Figure 2

#### Census Population by Age Group, City of Windsor, 2006-2016

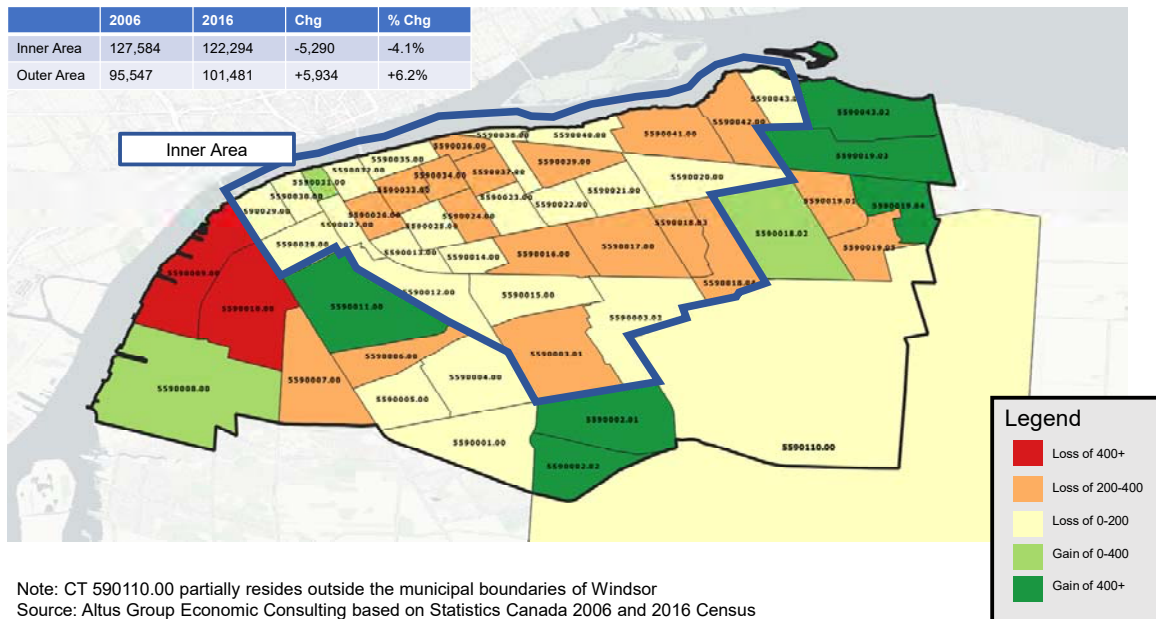
Age Group	2006	2011	2016	Change 2006-2016	
		Persons		Total	Percent
0-14	39,480	36,270	35,425	(4,055)	(10.3)
15-24	29,490	28,510	29,350	(140)	(0.5)
25-34	31,175	26,360	27,490	(3,685)	(11.8)
35-44	33,090	28,680	26,390	(6,700)	(20.2)
45-54	29,795	31,795	31,180	1,385	4.6
55-64	22,435	26,090	29,065	6,630	29.6
65-74	15,535	16,980	20,680	5,145	33.1
75-84	11,800	11,505	11,970	170	1.4
85-90+	3,680	4,700	5,640	1,960	53.3
Total	216,480	210,890	217,190	710	0.3

Source: Altus Group Economic Consulting based on Statistics Canada Population Census 2006, 2011, 2016

### 2.1.3 Change in Population by Area of City

Of the 52 Census Tracts ("CTs") in the City of Windsor, a total of 43 CTs (or 83%) lost population over the 2006-2016 period. Of the nine (9) CTs that gained population, six (6) were located on the eastern, western, or southern edges of the City's boundary.

Figure 3

**Population Changes by Census Tract, City of Windsor, 2006-2016**

The Inner Area population of the City, demarcated by the thick blue line, over the 2006-2016 period, shrunk by nearly 4%, or 5,300 persons, with only one CT in the Inner Area seeing population growth, while the Outer Area, outside of the blue line in the Figure below, grew by 6%, or roughly 5,900 persons.

Understanding the amount of population loss in built-up areas of the City is important to recognising that a certain proportion of the dwelling unit intensification being considered through this planning exercise will be to regain and retain population in the Inner Area part of the City. The decline in population in existing built-up areas of the City is driven by a combination of not enough new housing being constructed combined with declining average household sizes, meaning that a certain amount of development will be required just to maintain the population in existing areas of the City.

#### 2.1.4 Average Household Sizes

Windsor has seen the number of people per unit (“PPU”) of households decrease by 3.3% between 2006-2016. This resulted in the population in existing households declining by over 7,000 persons, meaning that the City

needed 3,030 new homes over the 2006-2016 period just to maintain the 2006 population levels, exclusive of any growth that occurred to 2016.

Figure 4

#### Household Size by Dwelling Type, City of Windsor, 2006-2016

Dwelling Type	2006	2016	Change	% Change
	Persons Per Unit (PPU)			Percent
Single-detached house	2.72	2.63	(0.08)	-3%
Semi-detached house	2.68	2.61	(0.07)	-3%
Row house	2.56	2.44	(0.13)	-5%
Apartment	1.70	1.64	(0.07)	-4%
Total	2.42	2.34	(0.08)	-3%

Source: Altus Group Economic Consulting based on Statistics Canada 2006 and 2016 Census

The City's 2020 DC Study, authored by Hemson Consulting, forecasted continued steady decline in average household sizes to 2.27 persons per unit by 2041, a further 3.4% decline in average household size.

Figure 5

#### Forecasted Total Population, Households, and Household Size, City of Windsor, 2020-2041

Mid-Year	Census Population	Total Occupied Households	Average Household Size
	Persons	Dwellings	Persons / Unit
2020	220,991	94,593	2.35
2041	239,989	105,903	2.27
Change 2020-2041	18,998	11,310	(0.08)
		Percent	
% Change 2020-2041	8.6%	12.0%	-3.4%

Source: Altus Group Economic Consulting based on City of Windsor Development Charge Background Study, 2020

## 2.1.5 Sources of Population Change

### 2.1.5.1 International Immigration

International immigrants are defined by Stats Canada as:

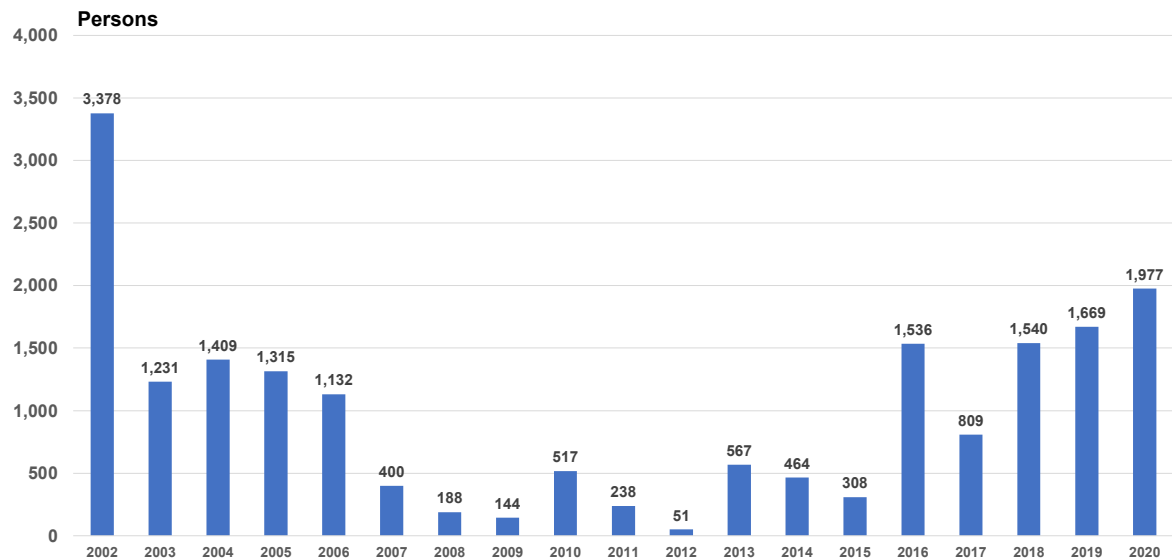
“...persons who are, or who have ever been, landed immigrants or permanent residents. Such persons have been granted the right to live in Canada permanently by immigration authorities. Immigrants who have obtained Canadian citizenship by naturalization are included in this category”

The statistics on immigration do not include people who hold study permits, which are counted as “non-permanent residents” which will be discussed separately later in this report.



Between 2009-2015, net international immigration<sup>1</sup> to the City progressively decreased from 3,400 to 300 net new persons per year. Beginning in 2016 through to the year 2020, net international immigration increased to approximately 1,500 to 2,000 people per annum (except for 2017). Four of the past five years have been the highest net inflow of persons to the City since 2002.

**Figure 6 Net International Immigration by Age Cohort, City of Windsor, 2002-2020**



Source: Altus Group Economic Consulting based on Statistics Canada Estimates of the Components of Demographic Growth, 2002-2020

Generally, net international immigration to the City has been led by adults between the ages of 25-34 and children between the ages of 0-14. The second largest immigrant age cohort to the City are young adults between the ages of 15-24 and adults aged 35-44. There has been some but much more limited numbers of net international immigration by people in the age cohorts of 45-54 and 55-64, however, there have been very few immigrants, in senior cohorts above 65+ years in age.

### 2.1.5.2 Interprovincial Migration

Interprovincial migration is the net movement of people between provinces or territories within Canada. In this case, it measures the net number of

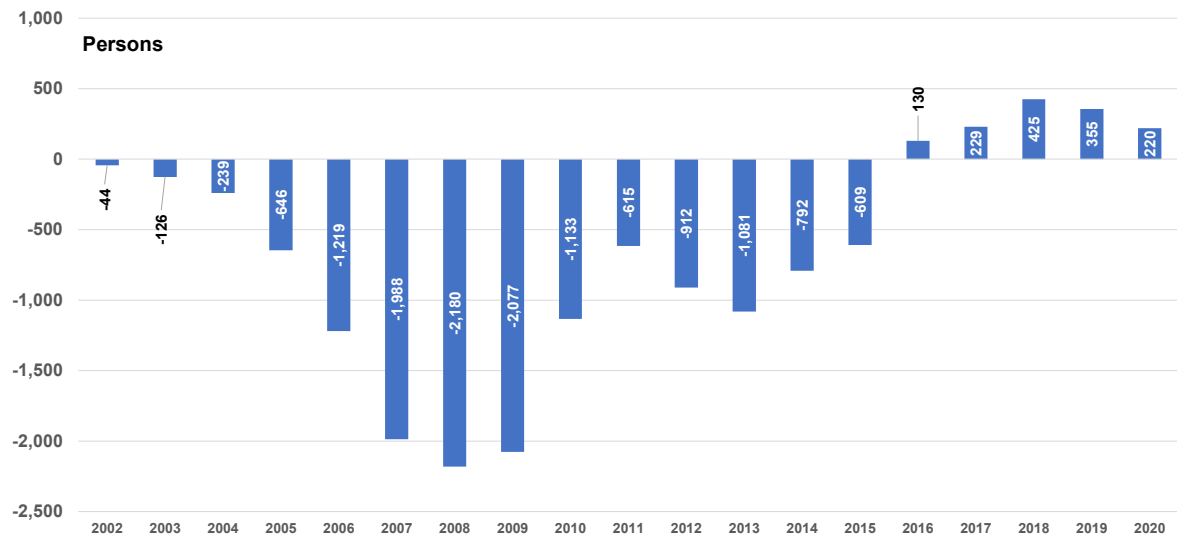
<sup>1</sup>Net immigration is immigration (people arriving in Canada as permanent residents) less emigration (permanent residents of Canada leaving to live outside of Canada)

people moving to/from Windsor from/to other provinces/territories outside of Ontario.

Over the 2002-2015 period, the City saw net outflows of people from the City to other provinces, however, since 2016, this statistic has become a net inflow of people from other provinces of Canada.

Figure 7

### Interprovincial Migration, City of Windsor, 2002-2020



Source: Altus Group Economic Consulting based on Statistics Canada Estimates of the Components of Demographic Growth, 2002-2020

#### 2.1.5.3 Intraprovincial Migration

Intraprovincial migration is the movement of people within the same province or territory. In this case, it represents the net number of people moving to/from Windsor from/to other parts of Ontario.

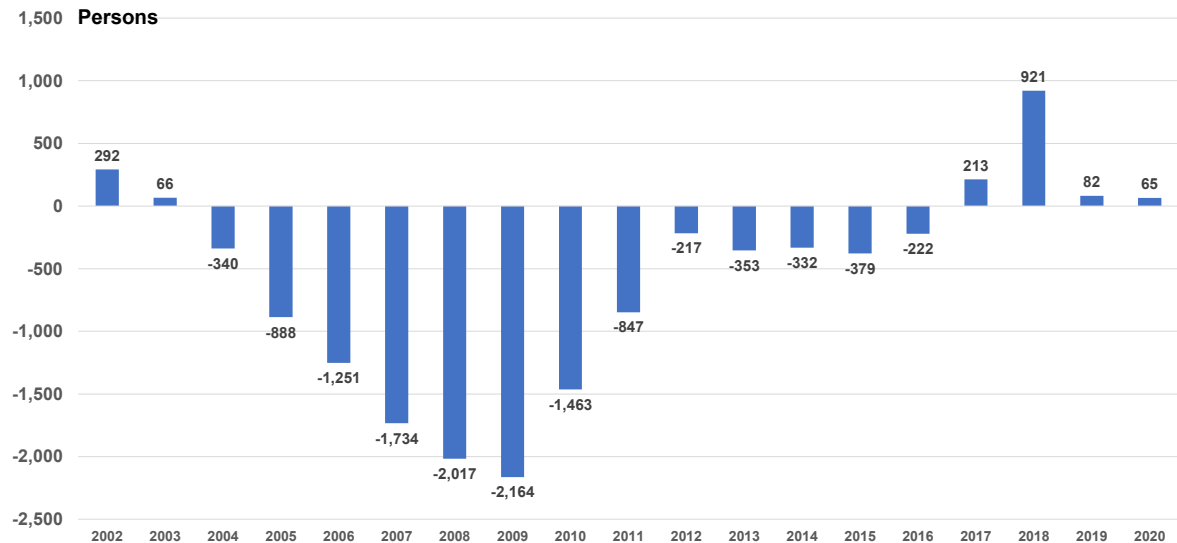
Windsor has seen net negative outflows to intraprovincial migration in every year where there is data available except more recently between 2017-2020.<sup>2</sup>

In total, the City has lost approximately 10,600 people between 2002-2020, with the largest share of this loss being made up of people in the age cohorts of between 15-44.

The two largest negative total outflow years between 2002-2020 were recorded in 2008 (2,000 persons) and 2009 (2,200 persons). However, since

those peak negative outflow years, the City has seen diminishing outflows of people with recorded positive inflows beginning in 2017 to the present.

**Figure 8 Intraprovincial Migration, City of Windsor, 2002-2020**



Source: Altus Group Economic Consulting based on Statistics Canada Estimates of the Components of Demographic Growth, 2002-2020

#### 2.1.5.4 Non-Permanent Residents

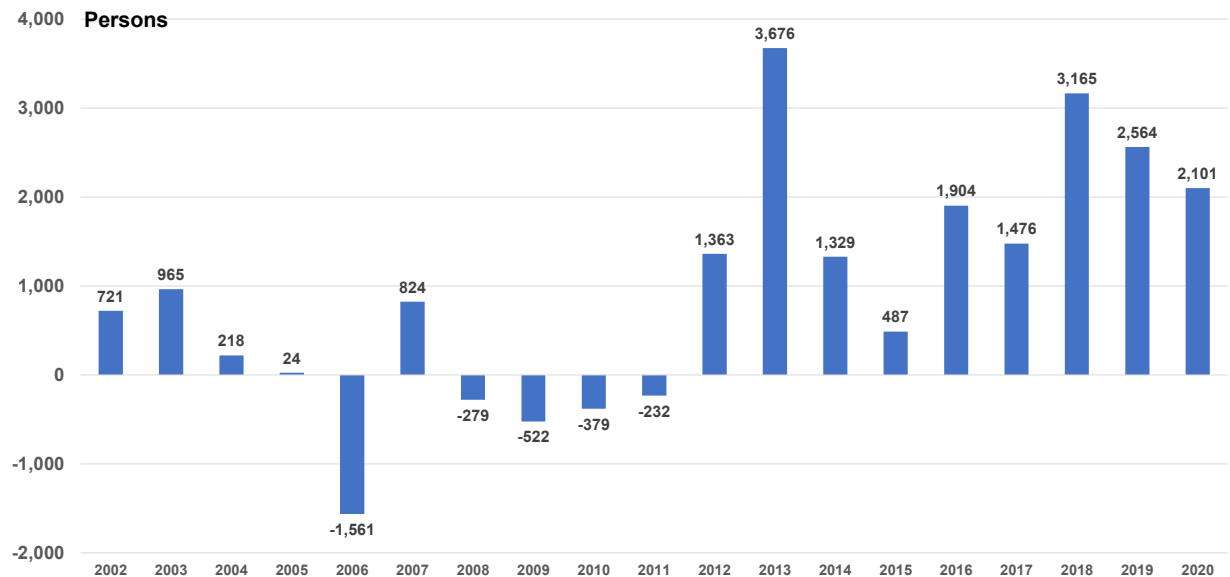
Statistics Canada defines non-permanent residents as:

persons from another country with a usual place of residence in Canada and who have a work or study permit or who have claimed refugee status (asylum claimants).

Family members living with work or study permit holders are also included, unless these family members are already Canadian citizens or landed immigrants/permanent residents.

After the City saw net outflows of non-permanent residents in five of the six years between 2006 and 2011, the City has since seen net inflows in each year over the 2012-2020 period, reaching a high of nearly 3,700 persons in 2013. The net inflow has been above 2,000 persons in each of the past three years (2018-2020).

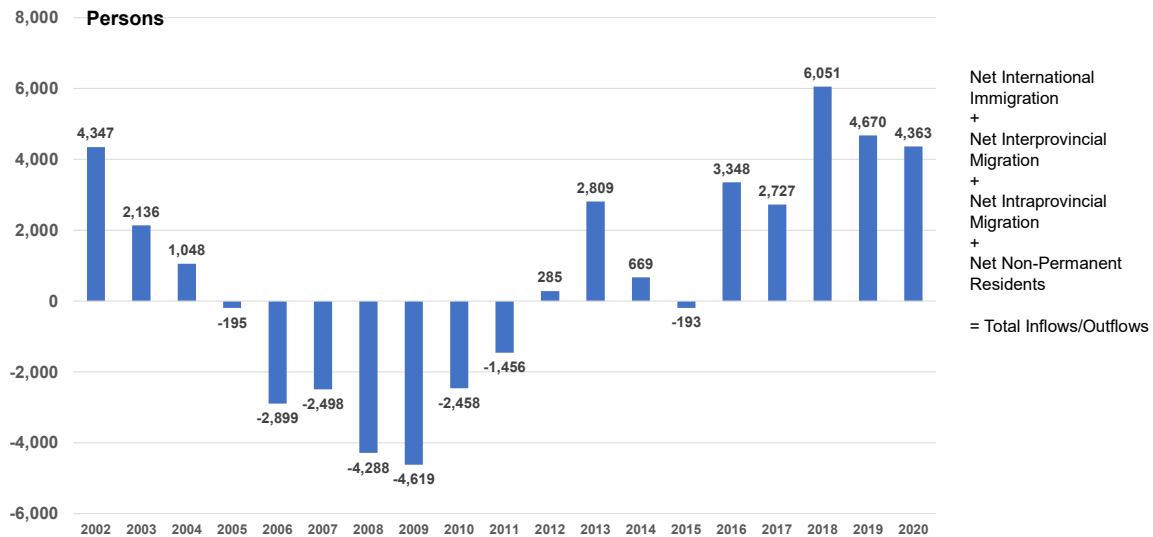
Figure 9

**Non-Permanent Residents, City of Windsor, 2002-2020**

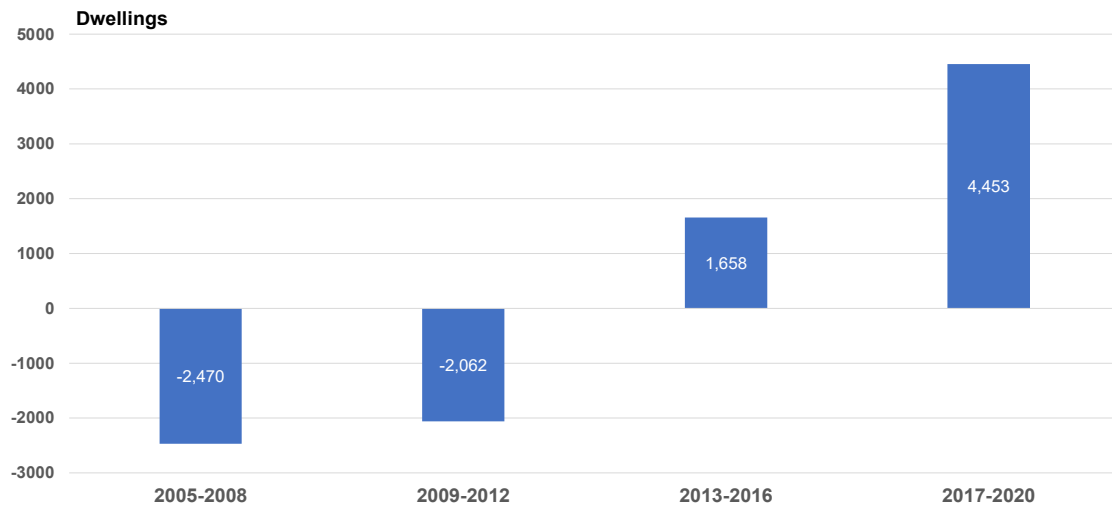
Source: Altus Group Economic Consulting based on Statistics Canada Estimates of the Components of Demographic Growth, 2002-2020

### 2.1.5.5 Conclusions Regarding Sources of Population Change

When the annual inflows/outflows from net international immigration, net interprovincial migration, net intraprovincial migration and net non-permanent residents, it is evident that the net inflows from international and domestic sources are increasing significantly, with the past five years (2016-2020) being five of the largest seven years of inflows to the City since 2002 (behind only 2013 and 2002).

**Figure 10 Total Inflows/Outflows, City of Windsor, 2002-2020**

When shown by four-year period, the net inflows to the City from domestic and international sources is roughly 4,500 persons per year, nearly two-and-a-half times what it was in the prior four-year period (2013-2016), and markedly different than the net outflows seen during the period two four-year periods from 2005-2008 and 2009-2012.

**Figure 11 Annual Net Inflows/Outflows, City of Windsor**  
*City of Windsor, 2005-2020, by 4-Year Period*

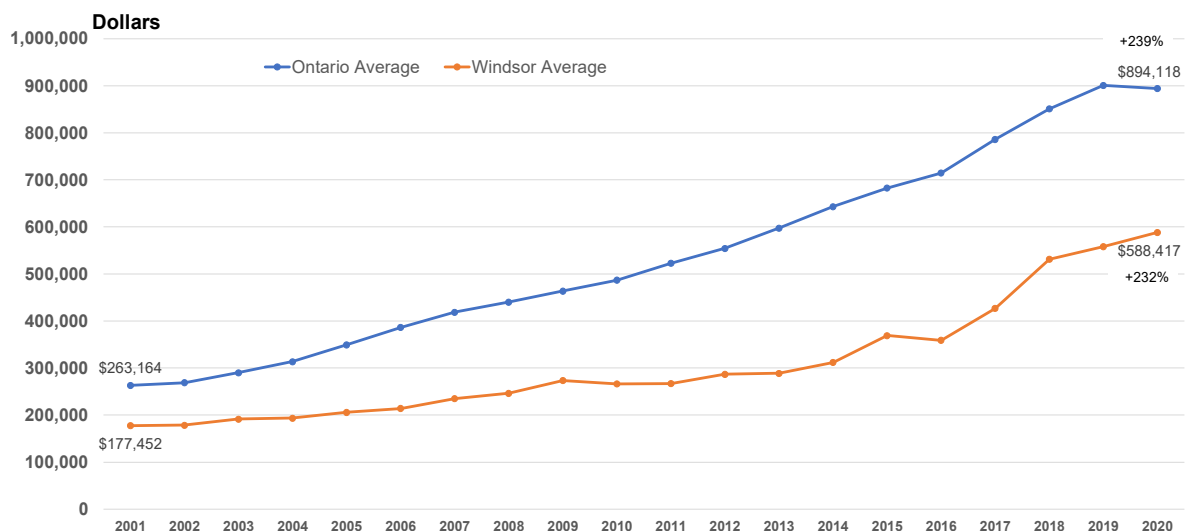
## 2.2 HOUSING

### 2.2.1 Housing Prices

Figure 12 shows the average price for absorbed (sold and completed) single-detached homes in the City of Windsor between 2001-2020, as reported by CMHC.<sup>3</sup>

Since 2001, average prices for single-detached units in the City have increased by 232%, from \$177,500 in 2001 to \$588,400 in 2020. While the average prices for single-detached units in Ontario have increased by a similar rate (239%) since 2001, the gap between the average prices in Ontario and the City have increased from \$86,000 in 2001 to over \$300,000 in 2020.

**Figure 12 Average Absorbed Single-Detached Price, Ontario and City of Windsor, 2001-2020**



Source: Altus Group Economic Consulting based on CMHC Housing Absorption Data, 2001-2020

### 2.2.2 Size of New Construction

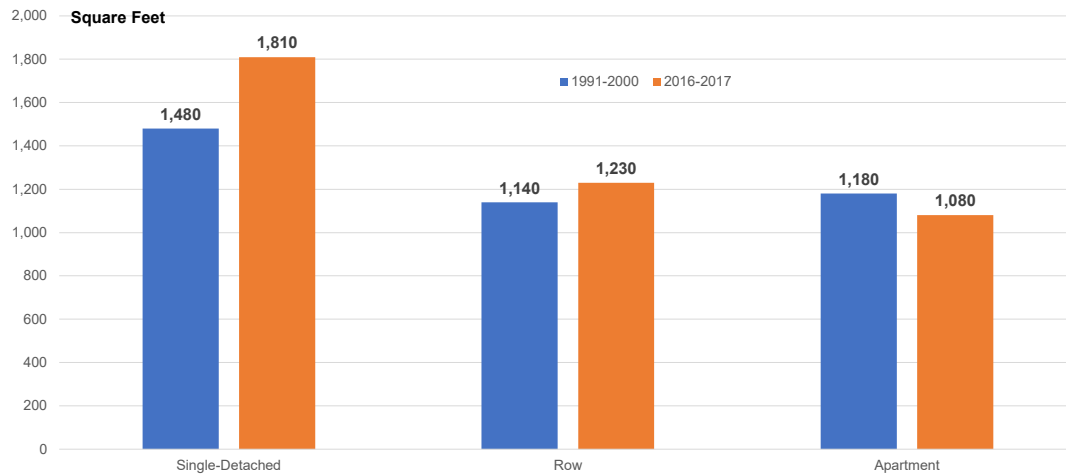
Figure 13 provides a breakdown of average above-grade living area by structure type for the City of Windsor.

The average size of a new single-detached unit in the City has grown from their average size in the 1990s by 22%, while the average townhouse unit has increased in size by 10%. The average size for apartment units has fallen by

<sup>3</sup> The data from CMHC does not control for unit sizes.

8%, but at 1,080 square feet is still on average larger than most new apartment units built elsewhere in Ontario.

**Figure 13 Average Above-Grade Living Area by Unit Type, by Period of Construction, City of Windsor**

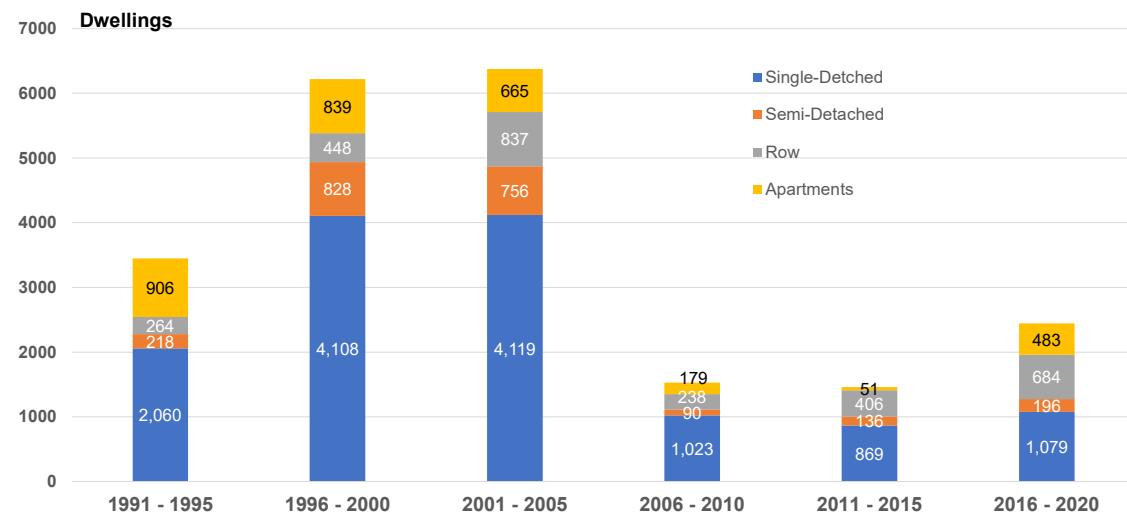


Source: Altus Group Economic Consulting based on Statistics Canada, Table 46-10-0028-01

### 2.2.3 Housing Completions by Unit Type

Figure 14 shows total housing completions by structure type in Windsor over the past 30 years, as broken out into separate five-year periods.

**Figure 14 Housing Completions by Structure Type City of Windsor, 1991-2020, by 5-Year Period**



Source: Altus Group Economic Consulting based on CMHC Housing Completions Data

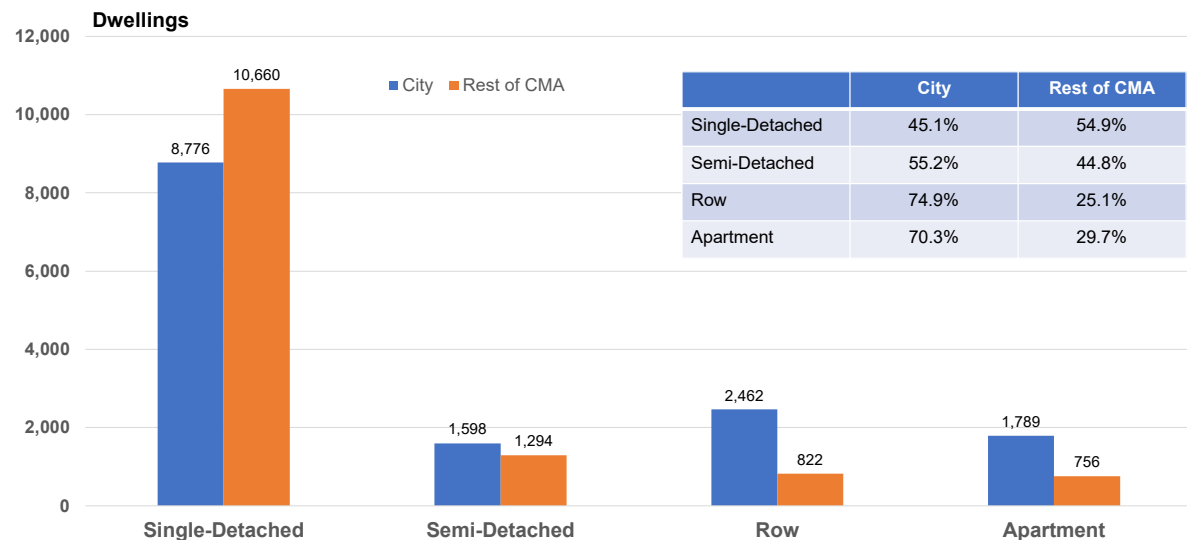
Since the 1996-2000 period, ground-oriented housing (single-detached and semi-detached) has progressively made up a smaller share of the total completed units in the City, falling from 79% of units in the 1996-2000 period, to 52% in the most recent five-year period (2016-2020).

Row housing (townhouses) has increased in importance, making up 7-16% of new housing completions in the City over the 1991-2010 period, but 28% in each of the past two five-year periods.

The total number of apartment completions in the City was highest during the 1991-2005 period, when 2,410 apartment units were completed or an average of 160 units per year. Over the next fifteen-year period, the City saw just 713 apartment units completed or under 48 units per year.

Since 1999, the City has received roughly 52% of the housing unit completions in the broader Windsor CMA, however, the share differs by unit type, with the City receiving lower than average shares of single-detached units, but higher than average shares of all other types of housing<sup>4</sup>, with the City's share of CMA completions for rows (74.9%) and apartments (70.3%) each exceeding 70%.

**Figure 15 Location of Housing Completions by Structure Type**  
*City of Windsor and Rest of Windsor CMA, 1999-2020*



Source: Altus Group Economic Consulting based on CMHC Housing Completions Data

<sup>4</sup> Semi-detached, row, apartment



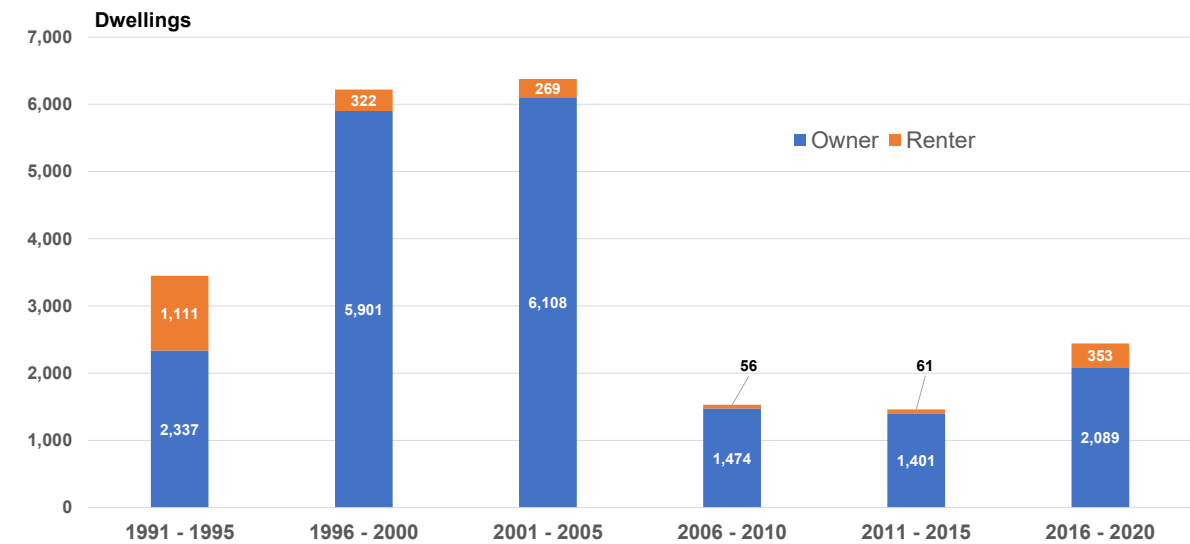
As the housing demand forecasts undertaken by Altus Group Economic Consulting are done for the Windsor CMA, the proportions of housing by structure type between the City and rest of CMA will be utilized to form assumptions for what proportion of future housing demand is likely to be directed towards the City of Windsor.

#### 2.2.4 Housing Completions by Tenure

Figure 16 shows total housing completions by tenure in the City of Windsor over the past 30 years, as broken out into separate five-year periods.

The share of housing completions that were rental tenure over the 1991-2020 period was just 10.1%, heavily driven by the 32% share seen in the 1991-1995 period. Since 1991-1995, just 5.9% of new housing completions in the City have been rental tenure, or only 1,061 rental units in total (or 42 units per year).

**Figure 16** **Housing Completions by Tenure**  
*City of Windsor, 1991-2020, by 5-Year Period*



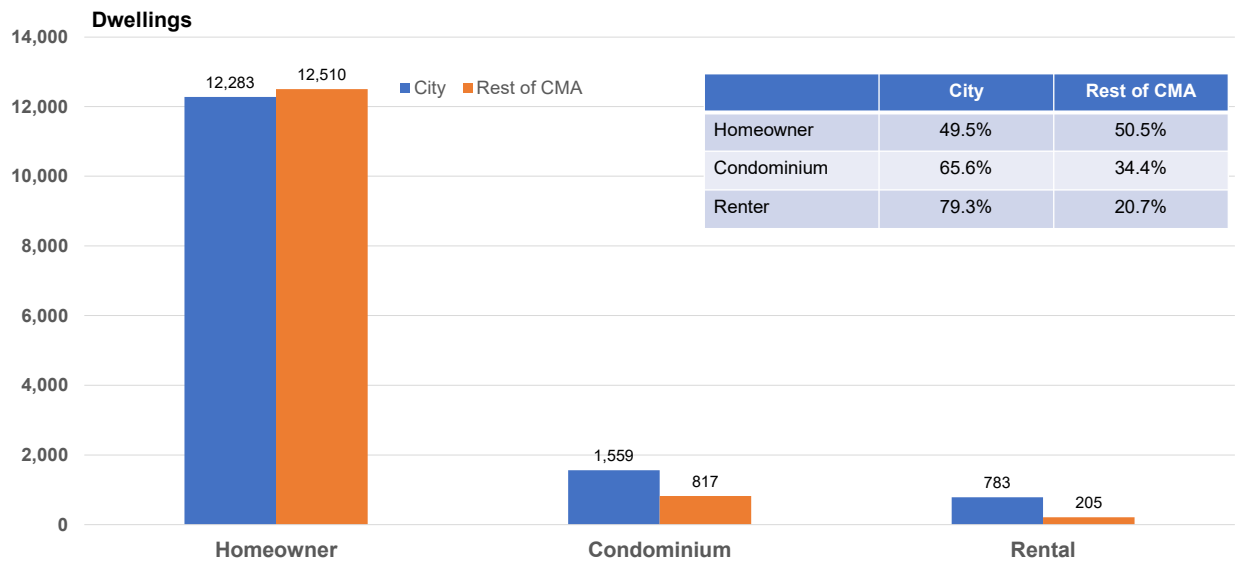
Source: Altus Group Economic Consulting based on CMHC Housing Completions Data, 1991-2020

Since 1999, the City has received roughly 52% of the housing unit completions in the broader Windsor CMA, however, like the differences in unit type, there are also significant differences in the forms of housing tenure that the City receives versus the rest of the CMA.

The City receives a lower-than-average shares of freehold ownership units (49.5%), but higher than average share of all other tenures, including

condominium (65.6%) and rental units (79.3%). To the extent that our housing demand forecast identifies specific unit types and tenures, these historic trends will be used to inform assumptions that allocate housing demand to the City or the rest of the Windsor CMA.

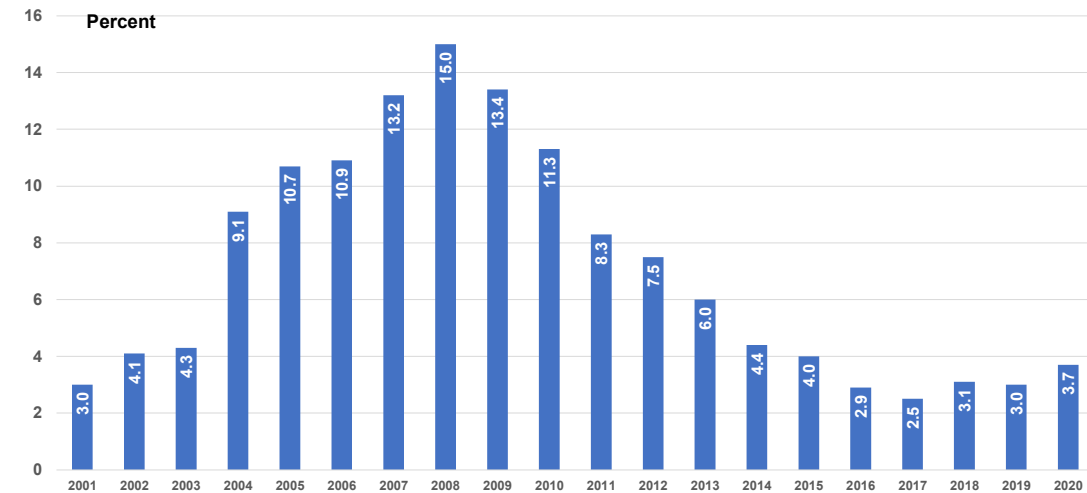
**Figure 17 Location of Apartment Housing Completions by Tenure**  
*City of Windsor and Rest of Windsor CMA, 1999-2020*



When the rental tenure units are broken down by unit type, the City receives a substantial share of the purpose-built rental housing completions within the CMA. Of the rental row houses, 93% were located in the City, while 82% of rental apartments were located in the City.

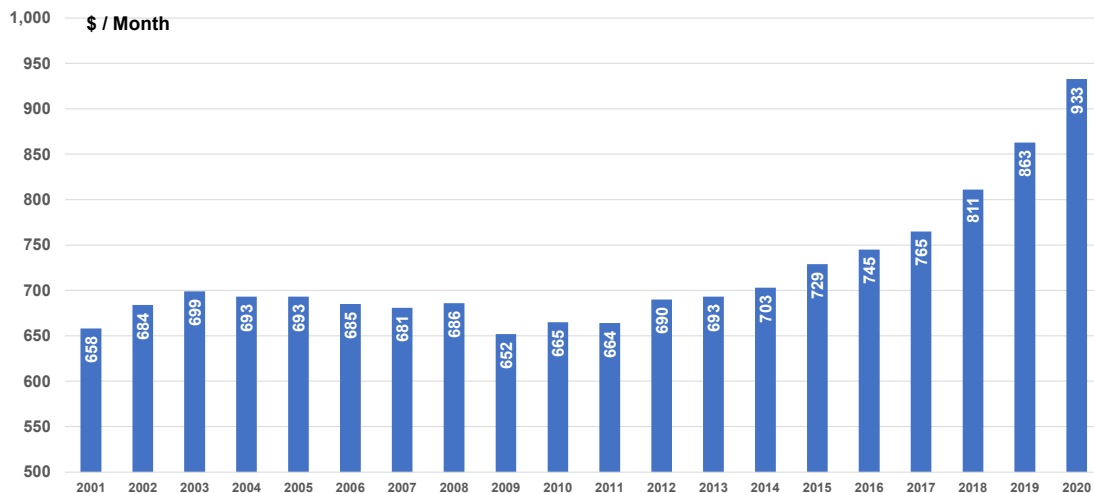
### 2.2.5 Rental Housing Market Trends

Figure 18 shows the 20-year trend in vacancy rates for private rental apartment units in the City of Windsor. After rising from 3.0% in 2001 to a high of 15.0% in 2008, the vacancy rate in the City has steadily fallen to a low of 2.5% in 2017, and has been below 4.0% in each of the past five years.

**Figure 18 Rental Apartment Vacancy Rate, City of Windsor**

Source: Altus Group Economic Consulting based on CMHC Rental Market data

Figure 19 shows the changes to average monthly rents for private apartment units in the City. As expected during periods of high vacancy rates, rents were relatively unchanged over the 2001-2014 period, ranging between \$652 and \$703 per month over the 14-year period. As vacancy rates declined over the 2011-2017 period and have stayed low since, rents have increased to a 20-year high of \$933 per month, an increase of 33% since 2014.

**Figure 19 Average Monthly Rents, Rental Apartments, City of Windsor**

Source: Altus Group Economic Consulting based on CMHC Rental Market data

The significant decline in vacancy rates and corresponding increase in average rents is indicative of a rental housing market that is in need of additional supply. Over the 2001 to 2020 period, the number of private rental apartments in the City has increased by just 296 units, from 14,218 units in 2001 to 14,516 units in 2020.

## 2.2.6 Seniors Housing Market Trends

Figure 20 provides the vacancy rate, total number of residence buildings, residents, and spaces for senior housing in Windsor over the 2019-2021 period. The vacancy rate in the City has risen from 8.7% in 2020 to 26.2% in 2021.

Figure 20

### Senior Housing, Vacancy Rates, Total Residences, Residents and Spaces, 2020-2021, City of Windsor

Year	Vacancy Rate Percent	Total Residences Buildings	Total Residents People	Total Spaces
2019	6.6	n.d	n.d	n.d
2020	8.7	10	1,087	1,113
2021	26.2	10	888	1,130
Change 2020-2021	17.5	-	(199)	17

Source: Altus Economic Consulting based on CMHC Senior Housing Data

Figure 21 provides the vacancy rate by rent range and unit type for senior housing in Windsor between 2019 and 2021.

Figure 21

### Vacancy Rate, by Rent Range and Unit Type, 2019-2021, City of Windsor

Year	Rent Range				
	Less than \$2,500	\$2,500-\$2,999	\$3,000-\$3,499	\$3,500-\$3,999	\$3,500-\$3,999
2019	6.8	12.4	6.6	n.d	4.0
2020	n.d	13.3	6.2	5.3	6.0
2021	32.9	28.0	27.0	21.1	22.9

Year	Unit Type			
	Ward/ Semi-Private	Bachelor/ Studio	1 Bedroom	2 Bedroom+
2019	n.d	9.5	4.6	n.d
2020	n.d	11.1	4.6	n.d
2021	n.d	25.2	21.7	n.d

Source: Altus Economic Consulting based on CMHC Senior Housing Data

Mirroring total vacancy rates, the rates by either rent range or unit type have grown significant since the onset of the COVID-19 pandemic. Generally,

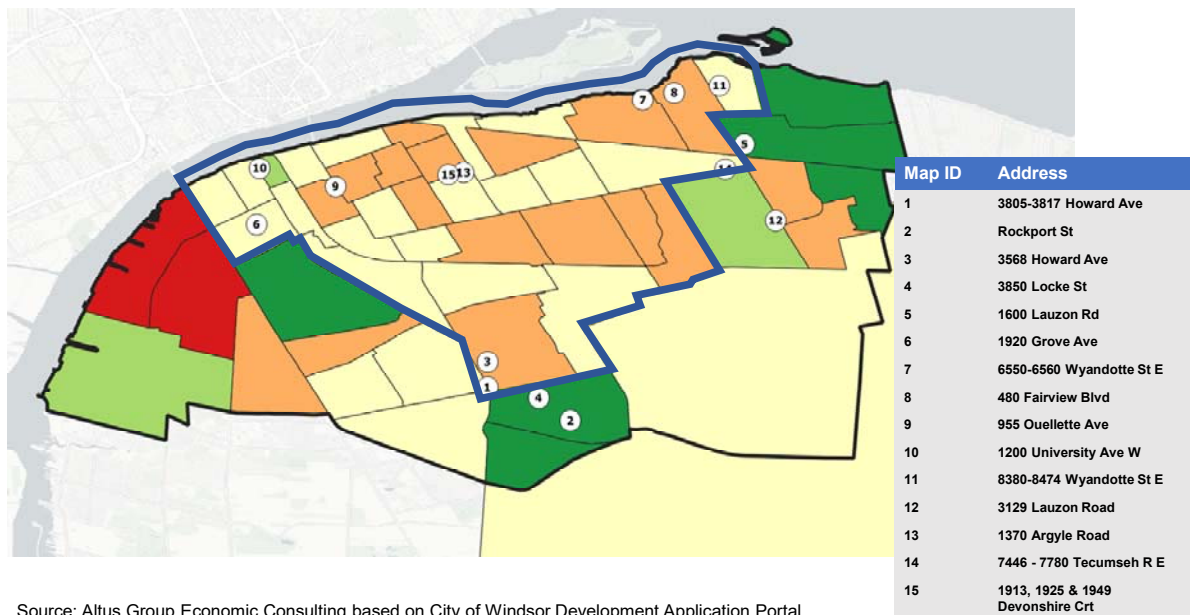
vacancy rates are higher for smaller rental units and those with lower rental prices.

## 2.3 OTHER CONSIDERATIONS – POPULATION AND HOUSING

### 2.3.1 Location of Development Applications

Of the major development applications in the City, there are 1,038 dwelling units proposed (for those with known unit counts), with the majority of these dwelling units proposed for the “Inner Area” where the population has been declining.

Figure 22 Major Residential Development Applications, City of Windsor



Of the 1,038 units in the active development applications, 81% are located in the Inner Area, including 89% of the apartments under proposal.

This is indicative of a high-density residential market willing and able to locate in the existing parts of the City, however, the quantum of new development will need to be increased to meet demand through supportive policies permitting and encouraging high-density residential projects in existing built-up parts of the City.

**Figure 23 Active Major Residential Developments, City of Windsor, Winter 2022**

Map ID	Address	Single	Semi	Row Dwelling Units	Apartment	Total
1	3805-3817 Howard Ave	-	-	-	54	54
2	Rockport St	80	-	-	-	80
3	3568 Howard Ave	13	-	-	-	13
4	3850 Locke St	-	18	-	-	18
5	1600 Lauzon Rd	n.d	n.d	n.d	n.d	n.d
6	1920 Grove Ave	-	40	-	-	40
7	6550-6560 Wyandotte St E	n.d	n.d	n.d	n.d	n.d
8	480 Fairview Blvd	-	-	-	15	15
9	955 Ouellette Ave	-	-	-	32	32
10	1200 University Ave W	-	-	-	133	133
11	8380-8474 Wyandotte St E	-	-	-	63	63
12	3129 Lauzon Road	-	-	-	96	96
13	1370 Argyle Road	-	-	-	81	81
14	7446 - 7780 Tecumseh R E	-	-	-	390	390
15	1913, 1925 & 1949 Devonshire Crt	-	-	-	23	23
<b>Total</b>		93	58	-	887	1,038

Note: Inclusive of development proposals greater than 5 dwelling units, but excludes residential and senior care facilities

Source: Altus Group Economic Consulting based on City of Windsor Development Application Portal

In their recent Economic Outlook for the City, CBRE indicated that the City of Windsor was primed to see growth within the existing built-up part of the City:

#### **Windsor Municipality Targets Density & Development**

The City of Windsor's aggressive push to incentivize new development and spur urban density in the core is expected to expand and drive developer interest. With robust fundamentals, further tax rebates and new community improvement plans expected, both new and long proposed projects could finally break ground or get announced in 2021.

### **2.3.2 Importance of Access to Transit**

Transit Windsor currently operates 15 different bus routes including a new limited-stop *express* route (Route 518X) from Tecumseh Mall to St. Clair College (via Devonshire Mall) intended to reduce travel times by over an hour each way between the eastern portions of the City and St. Clair College. This route is set to become a permanent corridor after experiencing strong ridership in its pilot phase – averaging 1,500 to 1,800 unique trips, or annualized ridership of over 90,000<sup>5</sup>. It is estimated that transit will see a 30% increase in ridership once the route is approved for expansion<sup>6</sup>. The success of this pilot is integral to the execution of the City's 2019 Transit Master Plan;

<sup>5</sup> City of Windsor (Dec 2021) – <https://www.citywindsor.ca/Newsroom/Pages/Proposed-2022-Budget-Delivers-Results-for-Transit-Operations.aspx>

<sup>6</sup> City of Windsor – 2022 Budget Issue Detail (Public), Page 250

*More Than Transit* (“TMP”) that prioritizes enhanced transit service and increased ridership.

The 518X joins an existing transit network which is “downtown” centric, with 10 routes originating or terminating in Downtown Windsor. There are also four routes that are interregional connecting to points outside of the City, including Downtown Detroit, the Town of LaSalle, the Town of Tecumseh and a limited service to the Municipality of Leamington.

Transit ridership has generally followed the population trends of Windsor – remaining relatively stagnant since 2007, at times declining, and rising slightly during the years prior to the pandemic.

However, Transit Windsor continues to face several challenges:

- The historical operating budget for Transit Windsor for the service shows that both revenues and expenses have been increasing over the past decade; however, expenditures continue to be roughly double the operating revenue generates;<sup>7</sup>
- Despite increasing costs, the City has been unable to increase revenue vehicle kilometres and hours;
- A review of Transit Windsor travel patterns shows that a gap exists between peak transit demand and service supply, as commute trips by all modes of travel are distributed across the City rather than being focused on the core, despite the majority of the network routes being directed there. As an example, less than 10% of morning peak period trips have a destination of Downtown Windsor, an outcome that contrasts with most other major municipalities<sup>8</sup>.

To improve the City’s transit infrastructure, the City has budgeted \$63 million for new capital projects over the next decade. In addition, following the success of the Express 518X, the next major service improvement is the addition of Route 418, estimated to cost about \$1 million annually to operate<sup>9</sup>. The route will provide express east-west service along Tecumseh Road and

<sup>7</sup> City of Windsor & Dillon Consulting (2019) – Transit Master Plan; *More Than Transit*, Page 7

<sup>8</sup> City of Windsor & Dillon Consulting (2019), Page 8

<sup>9</sup> Blackburn News (Dec 2021) - <https://blackburnnews.com/windsor/windsor-news/2021/12/02/budget-proposes-transit-windsor-investments-enough/>

the new St. Denis Athletic and Community Centre at the University of Windsor.

It will be important to focus intensification within the City of Windsor on areas already well-served by transit services, to leverage and optimize planned capital and operational investments.

### 2.3.3 Importance of Access to Amenities

There is a great deal of literature that reviews the impacts of various amenities on land values and home prices. Using land values and home prices acts as a proxy for reviewing the impact on housing demand from amenities, as growth in prices are an indicator of greater demand. These include:

- **Public Transit:** Access to public transit is positively correlated with higher land values and home prices<sup>10</sup>, however, geographic context needs to be accounted for. The impact from higher order transit has a wider but less sizable effect on low-rise communities, while in high rise transit orientated communities (“TOD”) the effect is greater but not as geographically dispersed.<sup>11</sup>
- **Parks and Open Spaces:** Access to public parks and open spaces has been found to have significant impact on nearby residential properties’ sale prices, particularly for housing types that lack self-contained green spaces (i.e., backyards), like apartments or townhomes.<sup>12</sup>
- **Libraries:** It was also found that being located within walking distance to a public library increased property values, with a nearly 8% property value increase for properties within 400 metres of a library.<sup>13</sup>

<sup>10</sup> Shanaka Herath. *Elevating the Value of Urban Location: A Consumer Preference-Based Approach to Valuing Local Amenity Provision*. Land. 2021

<sup>11</sup> Higgins and Kanaroglou. *Rapid transit, transit-oriented development, and the contextual sensitivity of land value uplift in Toronto*. Urban Studies. 2017

<sup>12</sup> McCord, McCluskey, Davis, et al. *Effect of public green space on residential property values in Belfast metropolitan Area*. Journal of Financial Management of Property and Construction. 2014

<sup>13</sup> Diamond, Gillen, et al. *The Economic Value of The Free Library in Philadelphia*. Fels Institute of Government at the University of Pennsylvania. 2010



- **Retail:** A 2007 study of a municipality in Washington State found that proximity to retail had a significant positive effect on residential values.<sup>14</sup>

#### 2.3.4 University and College Enrolment and Housing Options

The Census population for the City only includes permanent population, excluding non-permanent populations such as post-secondary students. However, while post-secondary students are not included in population counts, they do require housing for much of the year, and need to be considered in understanding the housing trends affecting a municipality.

Therefore, to understand the overall housing market demand, we have undertaken an analysis of post-secondary enrolment trends for the institutions located in the City – primarily the University of Windsor and St. Clair College primarily, but others as well.

Student enrollment for both St. Clair College and the University of Windsor has grown by a total of approximately 3,600 students between Fall 2012-Winter 2020 semesters.<sup>15</sup> However, the increase in student population has been greatest at the University of Windsor, which has seen a growth of approximately 3,200 full-time students, an increase of 23.1%.

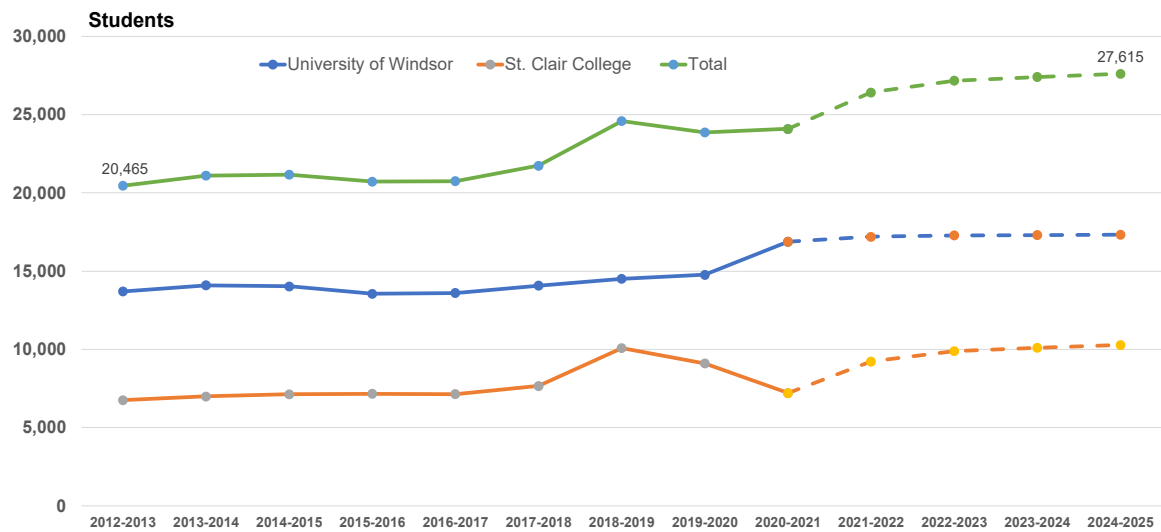
It was projected that total post-secondary student enrolment in Windsor will increase by another 3,500 students, or another 17.7%, between Fall 2020 and Winter 2025.

---

<sup>14</sup> Matthews. *Retail Proximity and Residential Values*. 2007

<sup>15</sup> See Appendix B **Error! Reference source not found.** for a yearly breakdown of student enrollment by higher education institution and by total.

**Figure 24 University and College Enrollment**  
**City of Windsor, 2012-2020 (actual) and 2021-2025 (projected)**



Note: Projections of student enrollment between 2020-2025 for St. Clair College have been discounted to account for campuses outside of Windsor  
 Source: Altus Group Economic Consulting based on University of Windsor, St. Clair College, and Ministry of Colleges and Universities

Figure 25 below provides a summary of both existing and future potential formal student housing in the City.

Currently, there is only one formal student housing space for every 14 students as of the 2019-2020 school year. Assuming that both the unnamed future residence hall at the University of Windsor with 450 spaces and the new International Student Residence Hall with 512 spaces at St. Clair College are completed by the 2024-2025 school year, the City is expected to have 1 formal student housing space for every 10 students. Should both student residences at the College and University be delayed beyond 2025, then the City is expected to have 1 formal student housing space for every 16 students.

Figure 25

---

**Existing and Potential Future Student Housing, City of Windsor**

Existing Housing	
Educational Institution and Resident Halls	Spaces
St. Clair College Windsor Campus	408
<b>Total</b>	<b>408</b>
University of Windsor	
Alumni Hall	337
Laurie Hall	150
Cartier Hall	153
Residence West <sup>1</sup>	270
<b>Total</b>	<b>910</b>
Potential Future Housing	
St. Clair College International Student Residence	512
University of Windsor Unnamed Residence Hall	450
Total Existing Student Housing	1,726
Total Potential Student Housing	962
<b>Total Student Housing</b>	<b>2,688</b>

<sup>1</sup> Closed for 2021-2022 semester

Source: Altus Group Economic Consulting based on University of Windsor, St. Clair College

---

Based on discussions with the University of Windsor, it was identified that most student who choose to dwell in student residences are typically first-year bachelor program students from outside the Windsor Region. As these students mature into second and subsequent school years, they typically move into student housing in the nearby neighbourhoods or in other parts of City.

In devising housing need forecasts, the City should ensure that current and future post-secondary students are captured in the estimated housing demand.

### 2.3.5 Commuting Flows

Figure 26 below shows, for persons with a usual place of work, the place of work for persons who reside in the City of Windsor, and the place of residence for people who work within the City. In total, the number of people working in the City at a usual place of work (93,565 persons) is significantly greater than the number of working persons who live in the City

(76,425 persons), meaning that there is a net inflow of 17,140 persons from areas surrounding the City.

Figure 26

---

**Commuting Flow, City of Windsor, 2016**


---

Municipality	2016		
	Persons Working in Windsor	Place of Work for Persons Living in Windsor	Net Inflow / (Outflow)
		<i>Persons</i>	
Windsor, CY	60,310	60,310	-
LaSalle, T	8,430	1,750	6,680
Amherstburg, T	4,445	760	3,685
Lakeshore, T	7,230	3,970	3,260
Essex, T	2,980	740	2,240
Kingsville, T	1,830	660	1,170
Chatham-Kent, MU	860	450	410
Leamington, MU	850	715	135
Tecumseh, T	6,210	6,425	(215)
All Other	420	645	(225)
Total	93,565	76,425	17,140

Source: Altus Group Economic Consulting based on Statistics Canada, Catalogue no. 98-400-X2016325

---

The greatest net inflow of workers into the City is from LaSalle, where 8,430 people live and work in the City of Windsor, but only 1,750 persons who live in the City of Windsor and commute to LaSalle, for a net inflow of nearly 6,700 persons.

There are also significant net inflows from Amherstburg (3,685 persons), Lakeshore (3,260 persons), Essex (2,240 persons) and Kingsville (1,170 persons).

### 3 EMPLOYMENT AND ECONOMIC TRENDS

This section of the report reviews trends in employment in the City, as well as trends affecting the retail and industrial sectors in particular.

#### 3.1 EMPLOYMENT TRENDS

##### 3.1.1 Change in Employment

In 2006, the City of Windsor had 114,785 jobs, but over the 2006-2016 period, the number of jobs in the City has declined by 8% or nearly 9,000 jobs. The decline in nearly 10,000 jobs with a usual place of work was offset somewhat by an increase of 1,450 jobs with 'no fixed' place of work, which may include construction workers, delivery drivers, and other occupations without a usual place of work.

Figure 27

**Total Employment in City of Windsor, 2006-2016**

	2006	2016	Change 2006 -2016	
		<i>Jobs</i>		<i>Percent</i>
Work at Home	3,765	3,325	(440)	-12%
No Fixed Place of Work	7,090	8,540	1,450	20%
Usual Place of Work	103,930	93,960	(9,970)	-10%
Total Employment	114,785	105,825	(8,960)	-8%

Source: Altus Economic Consulting based on Statistics Canada Census, 2006 and 2016

##### 3.1.2 Change in Employment by Sector

Figure 28 below shows the change in employment in the City by industry sector over the 2006-2016 period, where the City saw a decline of nearly 10,000 jobs. The largest decline was seen in the manufacturing sector, which lost 27% of the jobs the sector had in 2006, or a decline of nearly 6,900 jobs in the 10-year period.

Figure 28

**Change in Jobs Located in City of Windsor by Industry, 2006-2016**

Industry	2006	2016	Change 2006-2016	% Change
		<i>Jobs</i>		<i>Percent</i>
11 Agriculture, forestry, fishing and hunting	190	130	(60)	-32%
21 Mining and oil and gas extraction	295	245	(50)	-17%
22 Utilities	535	490	(45)	-8%
23 Construction	2,475	2,020	(455)	-18%
31-33 Manufacturing	25,235	18,340	(6,895)	-27%
41 Wholesale trade	2,700	1,995	(705)	-26%
44-45 Retail trade	13,150	11,445	(1,705)	-13%
48-49 Transportation and warehousing	3,765	3,465	(300)	-8%
51 Information and cultural industries	1,490	1,230	(260)	-17%
52 Finance and insurance	3,310	3,835	525	16%
53 Real estate and rental and leasing	1,385	1,155	(230)	-17%
54 Professional, scientific and technical services	3,970	3,995	25	1%
55 Management of companies and enterprises	50	45	(5)	-10%
56 Administrative and support, waste management, remediation, etc.	2,705	3,775	1,070	40%
61 Educational services	7,740	8,030	290	4%
62 Health care and social assistance	11,415	13,900	2,485	22%
71 Arts, entertainment and recreation	4,990	3,120	(1,870)	-37%
72 Accommodation and food services	9,440	8,425	(1,015)	-11%
81 Other services (except public administration)	4,405	3,540	(865)	-20%
91 Public administration	4,675	4,795	120	3%
<b>Total</b>	<b>103,930</b>	<b>93,960</b>	<b>(9,970)</b>	<b>-10%</b>

Source: Altus Group Economic Consulting based on 2006 and 2016 Census

Other sectors seeing large losses were the arts/entertainment/recreation sector (-37% or 1,870 jobs), the accommodation/food service sector (-11% or 1,015 jobs), and retail (-13% or 1,705 jobs).

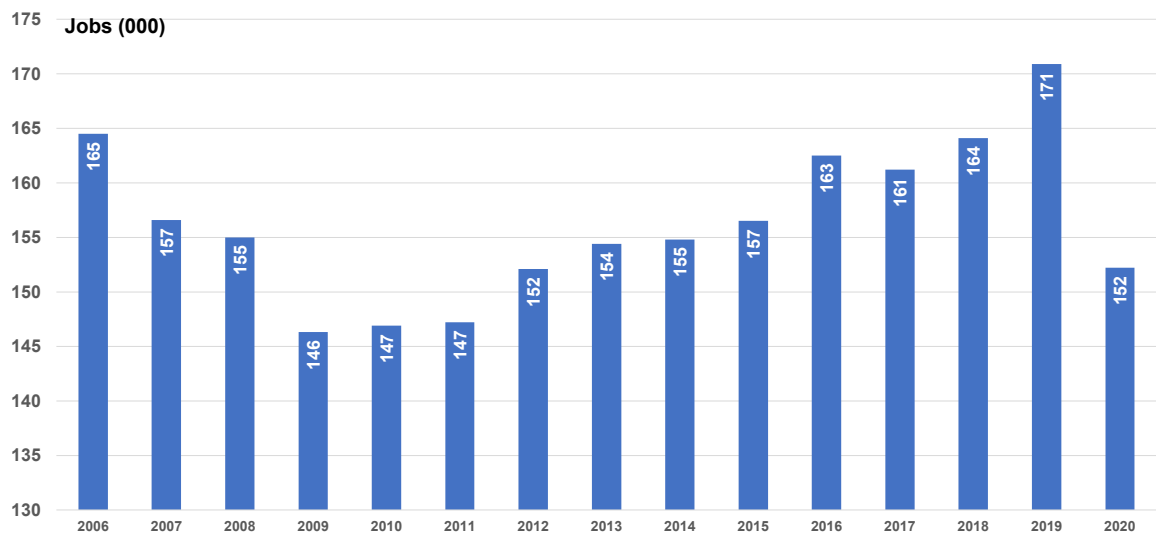
The only sectors that saw increased employment in the City were the health care sector, the administrative and support sector and educational services.

### 3.1.3 Employment Rate and Unemployment Rate

Figure 29 provides labour force characteristic statistics between 2006-2020 for the Windsor Census Metropolitan Area ("CMA")<sup>16</sup>. Note, census metropolitan areas are the lowest level of geographic analysis for publicly available employment data that is provided by Statistics Canada.

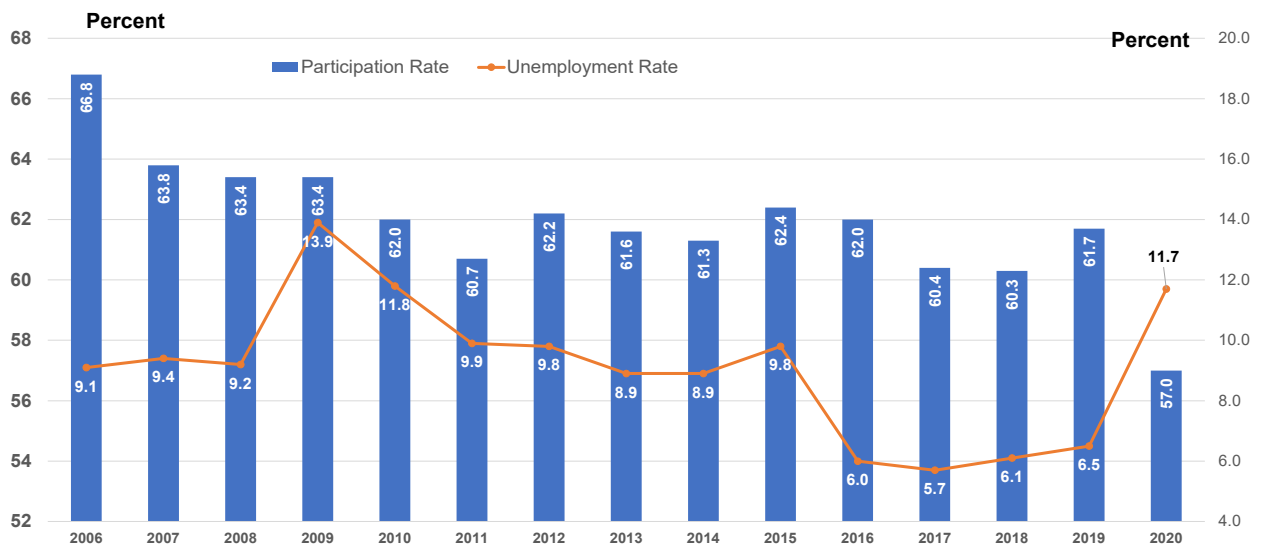
Over the 2009-2019 period, total employment in the Windsor CMA increased by nearly 25,000 jobs, from 146,000 jobs in 2009 to 171,000 jobs in 2019. Employment levels fell to 152,000 jobs in 2020 due to impacts from the COVID-19 pandemic.

<sup>16</sup> Includes the City of Windsor along with the Towns of Amherstburg, LaSalle, Lakeshore and Tecumseh.

**Figure 29 Total Employment, Windsor CMA, 2006-2020**

Source: Altus Group Economic Consulting based on Statistics Canada, Table 14-10-0385-01

The participation rate (number of persons aged 15+ with work or looking for work as % of all persons aged 15+) among City residents has fallen from roughly 67% in 2006 to 62% in 2019 before falling further in 2020 to approximately 57%.

**Figure 30 Trends in Labour Participation Rate and Unemployment Rate, Windsor CMA, 2006-2020**

Source: Altus Group based on Statistics Canada, Table 14-10-0385-01

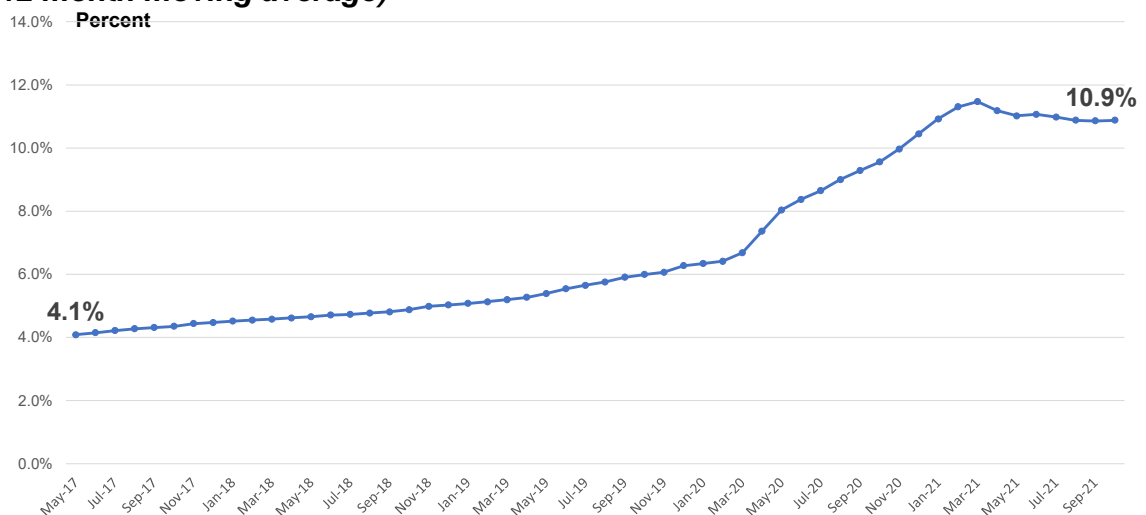
The unemployment rate in the Windsor CMA since 2009 had been on a general declining trend, settling at roughly 6% since 2016. However, in 2020, due to the impacts from the COVID-19 pandemic, the unemployment rate increased to 11.7%, although this is expected to return to long-term historic averages once the economic effects of the pandemic ease in 2021 and beyond.

## 3.2 NON-RESIDENTIAL MARKET TRENDS

### 3.2.1 Retail Market

Ecommerce as a share of all retail sales in Canada has grown from 4.1% of sales in mid-2017 to 10.9% in late-2021, with the growth accelerated by the pandemic.

**Figure 31 E-Commerce as % of Retail Sales, Canada, 2017-2021  
(12 month moving average)**



Source: Altus Group Economic Consulting based Statistics Canada, Table 20-10-0072-01

A continued increase in the shift towards online retail will push fulfilment of retail demand towards the industrial sector, where online orders are processed through warehouse and distribution centres, and away from traditional retail store formats. It is estimated that with every \$1 billion in online e-commerce sales, an additional 1.25 million square feet of industrial space is required.<sup>17</sup>

<sup>17</sup> CBRE, Market Outlook Report, 2021



CBRE forecasts that e-commerce will grow another 58% in the next five years, and this growth may alone be responsible for an additional 40 million square feet of demand for industrial and warehousing space throughout Canada.

Analyses done on retail vacancy rates in major Canadian urban centres has generally found that retail spaces in smaller neighbourhood centres have fared better than more isolated, regional shopping centres such as enclosed malls. The average vacancy rate among “Regional Centres”, which generally consists of enclosed malls is over 9%. Meanwhile, the vacancy rate for power centres (which typically include hardware stores and general retailers like Wal-Mart or Canadian Tire) is lower at 3.1%, while community/neighbourhood retail also has maintained a low vacancy rate at 3.7%.

Figure 32

#### Vacancy Rates by Type of Retail Location

	Community, Neighbourhood and Strip Plaza	Power Centre <i>Percent</i>	Regional Centre
Vancouver	3.6%	1.4%	n.a.
Calgary	4.0%	3.0%	17.0%
Edmonton	3.0%	1.7%	6.9%
Winnipeg	5.5%	4.1%	14.5%
Toronto	2.8%	2.9%	5.0%
Ottawa	3.2%	2.4%	10.6%
Montreal	4.7%	5.3%	7.9%
Halifax	6.0%	6.2%	23.5%
Average	3.7%	3.1%	9.3%

Note: Power Centre is generally big-box format retail locations

Note: Regional Centre is generally enclosed malls

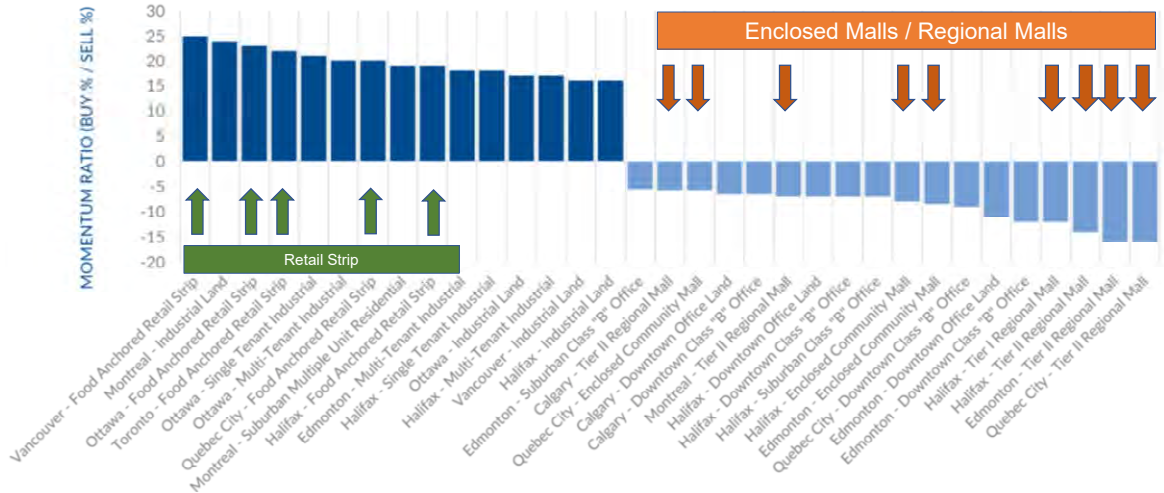
Source: Altus Group Economic Consulting based on CBRE, Canada Retail Report, Spring 2021

Similar to the findings by CBRE, Altus Group’s Investment Trends Survey found that out of the 128 combinations of product types (various types of office, retail, industrial types) and location across Canada, both “Enclosed Malls” and “Regional Malls” were among the least preferred asset types among investors. Combined large-scale retail development types comprised nine (9) of the 15 least preferred asset types across Canada, with a mix of regional malls and enclosed malls.

However, of the top 10 most preferred asset types, “food anchored retail strip” in various locations across made up five (5) of the top 10 most preferred assets.

Figure 33

**Product/Market Barometer - All Available Products (Q4 2021)**  
**Top 15 Preferred/15 Least Preferred**



The introduction of residential uses in close proximity to established retail centres can help to add value for retailers and allow for a mix of uses that make for convenient shopping for new residents, as well as help boost demand for additional retail uses in some cases.

The addition of residential uses to established shopping centres is a burgeoning trend occurring across Canada, as well as Ontario, with the following shopping centres seeking to add residential uses to transform isolated commercial areas into mixed-use community hubs.

Figure 34

Shopping Centre	Municipality	Proposed Additional Uses
Yorkdale Shopping Centre	Toronto	1,500 rental residential units, hotel, additional retail space
CF Shops at Don Mills	Toronto	2,800 residential units
Bayview Village	Toronto	1,130 residential units and 147,100 square feet of additional retail space

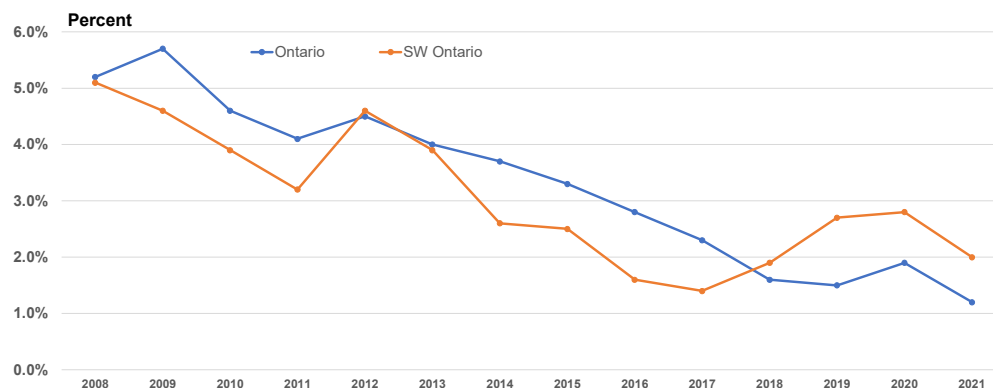
Shopping Centre	Municipality	Proposed Additional Uses
Cloverdale Mall	Toronto	Refurbished retail uses, plus addition of residential, parks, open spaces.
Promenade Mall	Vaughan	1,000 residential units, office space, hotel
Agincourt Mall	Toronto	4,000 residential units, office space, additional commercial space, child care centre, etc.
Square One	Mississauga	Significant number of high-density residential units
Westgate Shopping Centre	Ottawa	Five residential buildings, 20,000 square feet of retail
<i>Source: Retail Council of Canada, Canadian Shopping Centre Study, 2019</i>		

### 3.2.2 Industrial Market

As of year-end 2021, the industrial market across Ontario is severely constrained by a lack of supply, with vacancy rates across the entirety of Ontario nearing 1.0%, with just 12.7 million square feet (out of a total of 1.07 billion square feet province-wide) available as of Q4 2021.

Figure 35

#### Trends in Industrial Vacancy Rates, Ontario and SW Ontario



\*SW Ontario includes Brantford, Cambridge, Guelph, Hamilton, Waterloo Region, London, St. Catharines, Stratford, Woodstock  
Source: Altus Data Studio

While Altus Group's "Southwestern Ontario" submarket area does not extend far enough to include the City of Windsor, it does cover an area from Niagara Region to the London area, with the same Province-wide trends evident in the SW Ontario submarket, including vacancy rates of just 2.0% and only 2.37 million square feet (out of 120.5 million square feet total) available to be leased.

In essence, vacancy rates in the range of 1-2% represent 'full occupancy' as there will always be an expectation of 'structural' vacancy consisting of companies moving to new buildings, going out of business, awkward available space sizes or configurations that have limited marketability, etc. that can leave buildings vacant for short periods of time.

The vacancy rate in the SW Ontario submarket is consistent with the findings of CBRE which specifically studies the Windsor industrial market. CBRE found that the availability rate for industrial in the City as of year-end 2020 was just 2.2%, down from 2.6% the year prior. The net asking rents, owing to the relative lack of supply, increased from \$6.85 per sf in 2019 to \$7.75 per sf in 2020, an increase of 13% in just one year.

CBRE found that the industrial sector in the City could soon significantly expand and build upon the various employment lands ready and available for development in the City.

#### **Automotive & Manufacturing Tailwinds to Drive Investment**

2020 carried with it a series of multibillion-dollar announcements to invest in Southwestern Ontario's automotive industry from Ford, General Motors and Stellantis, which should solidify the region's economic future. With these tailwinds, an influx of corporate investment across Windsor's manufacturing sectors is anticipated to follow in the years ahead as the electric and autonomous vehicle market takes shape.

The expectations of a 'tight' industrial market in the City is expected to continue, given a modest development pipeline:

#### **Record Tight Industrial Market Here to Stay**

Windsor's industrial market ended the year on a high note and recorded an eye catching 2.2% availability rate, which decreased 100 bps from a year prior to a new record-low. Despite an anticipated minor softening of demand in 2021, availability is expected to remain near current levels as a result of the modest development pipeline which has virtually no speculative development.

Given the significant under supply of industrial space in the Province and Southwestern Ontario, the City should be seeking to retain as many occupied and vacant employment lands as possible, unless there are compelling reasons to allow residential uses on them.

## **4 GROWTH FORECASTS AND HOUSING OUTLOOK**

---

This section provides a high-level overview of the housing demand forecast prepared by Altus Group Economic Consulting for housing by unit type and tenure in the Windsor CMA, and how much of this estimated demand can be expected to accrue to the City of Windsor.

### **4.1 HOUSING PROPENSITY MODEL (ALTUS GROUP FORECASTS)**

#### **4.1.1 Approach and Methodology**

Based on modelling from Altus Group Economic Consulting, estimates of housing demand by unit type in the Windsor CMA have been undertaken.

The Altus Group Housing Demand Model is a multi-dimensional propensity-based model taking demographic inputs (from the separate cohort survival population growth model) and analysing among historical and projected propensities for the interplay between age of household maintainers, household type, household tenure, and household structure type. In all the model considers some 780 demand propensities.

Our approach to forecasting these propensities is to model the historical pattern on propensities and evaluate trend that is significantly different than zero (no change) those that are not significantly different than zero are projected to be held constant. Typically, this accounts for some 80-90% of propensities - for those that show a significant trend up or down over the historical period, an analysis is done to evaluate the drivers of that trend and factors related to those drivers are evaluated for the forecast.

The sections of the report below will summarize some of the key findings from the housing demand model. However, the detailed forecasts will be provided in an appendix to this report.

#### **4.1.2 Windsor CMA Population Forecast**

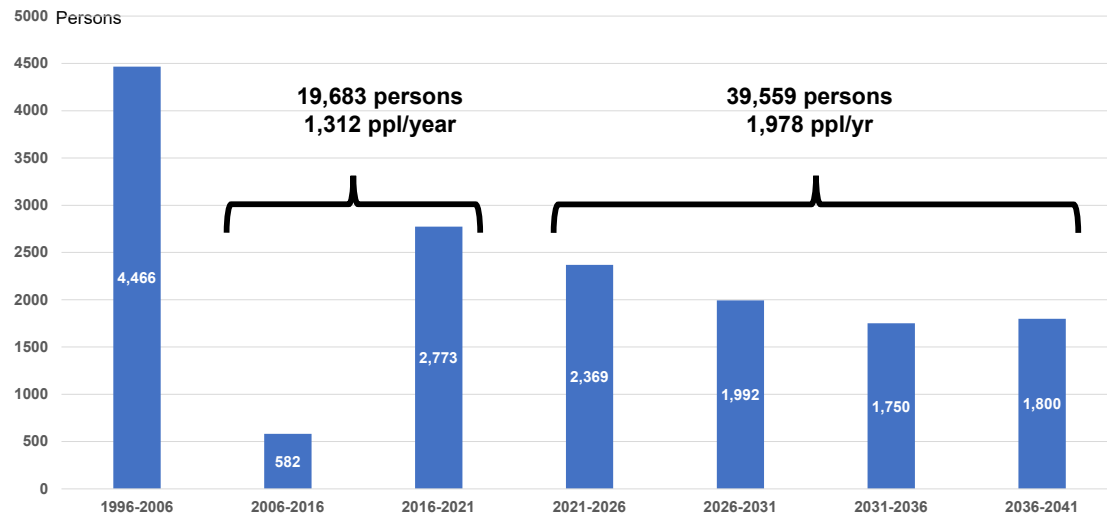
Figure 36 depicts the population forecast in the Windsor CMA based on the Altus Group model.

In total, it is expected that the Windsor CMA will grow by nearly 39,600 persons over the 2021-2041 period, or approximately 1,978 persons per year. This compares to the average annual rate of growth over the 2006-2021 period of 1,312 persons per year. Therefore, the amount of population growth

forecasted over the 2021-2041 period represents an increase in population growth of approximately 51% over the preceding 15-year period.

Figure 36

**Forecasted Change to Windsor CMA Population**  
**City of Windsor, 2006-2021 (actuals) and 2021-2041 (forecast), Annual Averages**



Source: Altus Group Economic Consulting

#### 4.1.3 Windsor CMA Housing Demand by Structure Type

The figure below provides the detailed results from the Altus Group forecast of housing demand by structure and tenure in the Windsor CMA over the 2021-2041 period.

In total, it is forecast that housing demand will equate to approximately 891 units per year over the 20-year period, with the first ten years of the forecast period most heavily weighted, with each five-year period from 2021-2031 seeing annual housing demand in the CMA exceed 1,000 units per year.

The distribution of housing demand growth by unit consists of 58.8% single-detached, 7.1% semi-detached, 10.8% row houses, and 21.7% apartment units.

Of the apartment unit demand, nearly three-quarters is for rental apartment units (15.8% of total housing demand), with the remaining apartment demand being for condominium apartment units.

**Figure 37 Potential Housing Demand by Structure Type, Windsor CMA (Draft 1.14.22) 1996-2041**  
**Base Scenario**

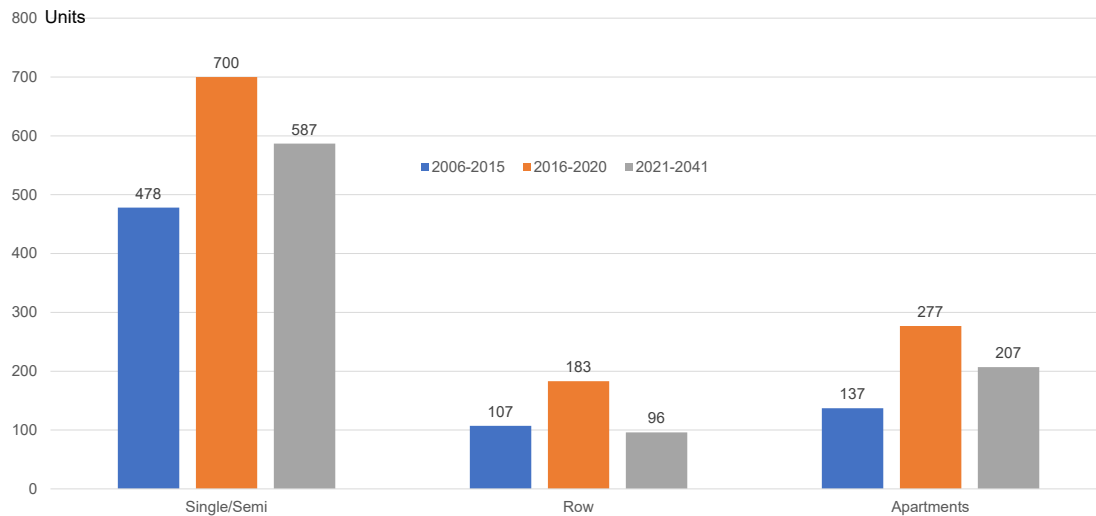
				Apartments				
	Single Detached	Semi Detached	Row	Total	Owner	Renter	Other	Total
Households				Occupied Dwelling units				
1996	72,100	2,995	4,485	21,505	1,775	19,730	4,740	105,825
2006	86,690	5,250	6,310	23,475	4,065	19,410	4,065	125,790
2016	91,045	5,670	7,375	24,455	3,595	20,860	4,340	132,885
2021	93,963	6,250	8,289	25,140	3,860	21,280	4,357	137,999
2026	96,601	6,684	9,026	26,525	4,178	22,348	4,430	143,267
2031	99,566	7,047	9,618	27,668	4,484	23,184	4,497	148,396
2036	102,214	7,331	10,022	28,410	4,739	23,671	4,543	152,520
2041	104,449	7,512	10,214	29,010	4,918	24,092	4,636	155,821
Annual Household Growth				Occupied Dwelling units				
1996-06	1,459	226	183	197	229	(32)	(68)	1,997
2006-16	436	42	107	98	(47)	145	28	710
2016-21	584	116	183	137	53	84	3	1,023
2021-26	528	87	147	277	64	213	15	1,054
2026-31	593	73	118	229	61	167	13	1,026
2031-36	529	57	81	148	51	97	9	825
2036-41	447	36	38	120	36	84	19	660
2021-41	524	63	96	194	53	141	14	891
Distribution of Households				Percent				
1996	68.1	2.8	4.2	20.3	1.7	18.6	4.5	100.0
2006	68.9	4.2	5.0	18.7	3.2	15.4	3.2	100.0
2016	68.5	4.3	5.5	18.4	2.7	15.7	3.3	100.0
2026	67.4	4.7	6.3	18.5	2.9	15.6	3.1	100.0
2036	67.0	4.8	6.6	18.6	3.1	15.5	3.0	100.0
Distribution of Growth				Percent				
1996-06	73.1	11.3	9.1	9.9	11.5	(1.6)	(3.4)	100.0
2006-16	61.4	5.9	15.0	13.8	(6.6)	20.4	3.9	100.0
2016-21	57.1	11.3	17.9	13.4	5.2	8.2	0.3	100.0
2021-26	50.1	8.2	14.0	26.3	6.0	20.3	1.4	100.0
2026-31	57.8	7.1	11.5	22.3	6.0	16.3	1.3	100.0
2031-36	64.2	6.9	9.8	18.0	6.2	11.8	1.1	100.0
2036-41	67.7	5.5	5.8	18.2	5.4	12.8	2.8	100.0
2021-41	58.8	7.1	10.8	21.7	5.9	15.8	1.6	100.0

Source: Altus Group Economic Consulting, Housing Demand Model

Figure 38 shows the average annual housing demand by unit type for the City over the 2021-2041 period, and as compared to the 2006-2015 and 2016-2020 periods.



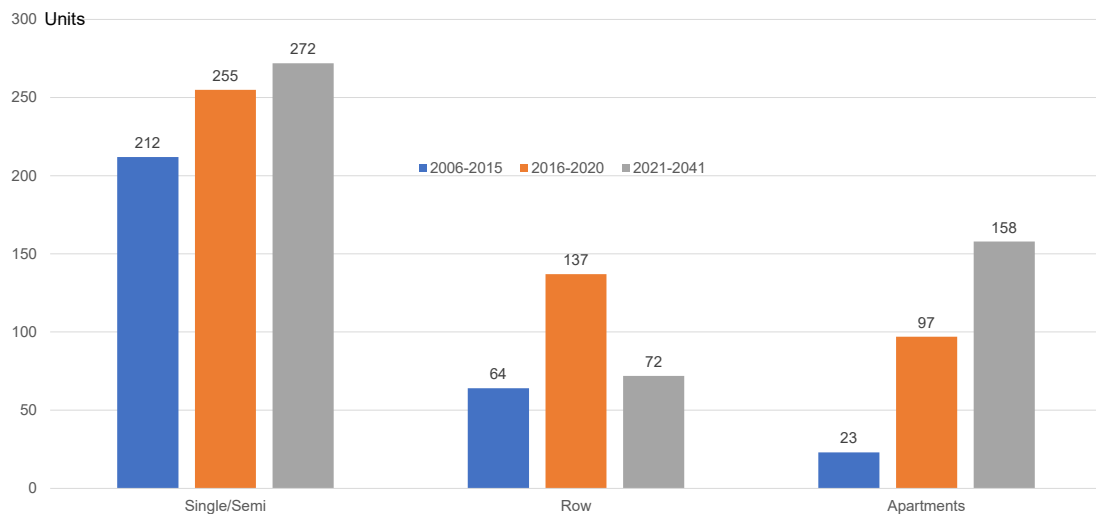
**Figure 38** **Forecasted Housing Demand by Unit Type, Windsor CMA, 2021-2041, Annual Averages**



Source: Altus Group Economic Consulting

Based on the historic share of housing completions by type in the City versus the rest of the Windsor CMA, and the housing forecast by unit type for the CMA, it is expected that the City would receive approximately 56.3% of the forecasted housing demand in the Windsor CMA over the 2021-2041 period.

**Figure 39** **Forecasted Housing Demand by Unit Type, City of Windsor, 2021-2041, Annual Averages**



Source: Altus Group Economic Consulting

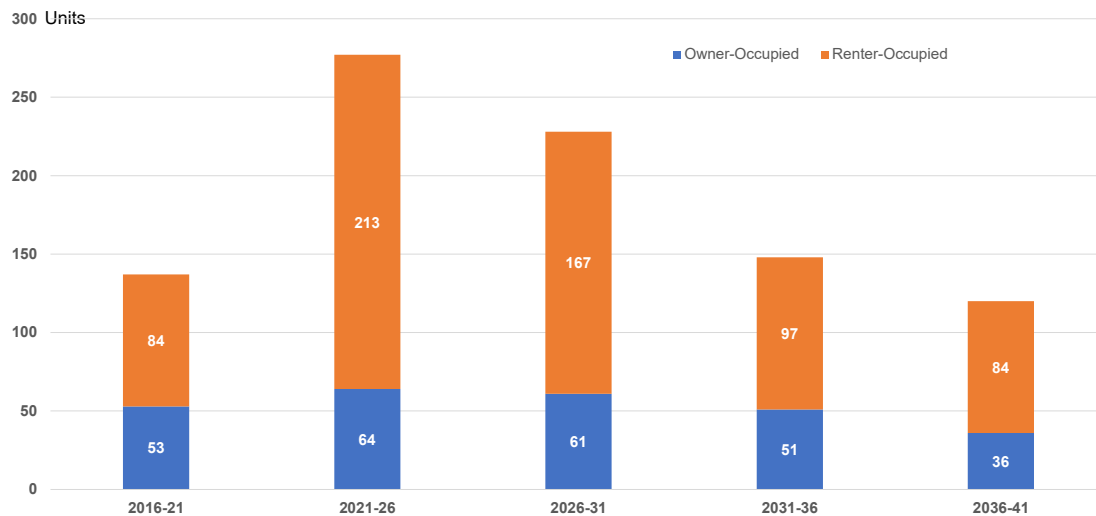
#### 4.1.4 Windsor CMA and City Apartment Housing Demand by Tenure

Figure 40 shows how the annual Windsor CMA housing forecast of apartment dwelling demand of 194 units per year changes over time, and how it is broken down by tenure (ownership or rental).

The demand for owner-occupied apartments in the CMA over the 2021-2041 period ranges from 36 to 64 units per year, or an average of 53 units per year. This is consistent with the demand for owner-occupied apartment units that were added in the City over the 2016-2021 period (53 units per year).

Figure 40

#### Forecasted Apartment Unit Demand by Tenure Windsor CMA, 2016-2021 (actuals) and 2021-2041 (forecast), Annual Averages

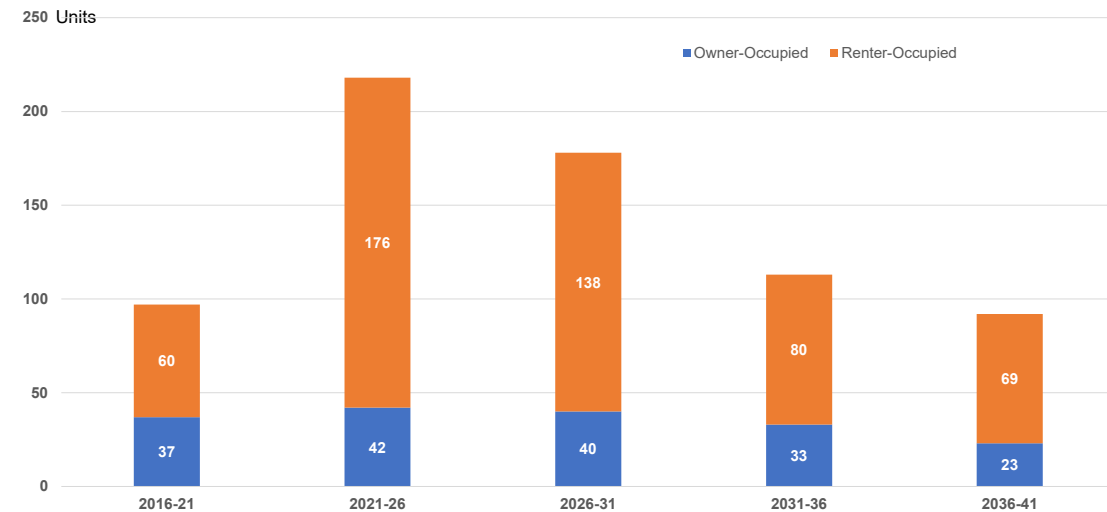


Source: Altus Group Economic Consulting

The driver of future apartment unit demand is for renter-occupied units, which ranges from 84 to 213 units per year over the 2021-2041 period, and averages 141 units per year, up 67% from the amount seen over the 2016-2021 period (84 units per year).

It is estimated that a significant proportion of the apartment unit demand (owner or rental in tenure) will be located in the City of Windsor. Based on historic trends, of the 194 apartment units forecast for the CMA each year over the 2021-2041 period, roughly 158 units will be located in the City (or 78% of all apartment housing demand in the CMA).

**Figure 41** **Forecasted Apartment Unit Demand by Tenure**  
**City of Windsor, 2006-2021 (actuals) and 2021-2041 (forecast), Annual Averages**



Source: Altus Group Economic Consulting

A significant proportion of the growth in apartment units (both condominium and rental tenure) can be expected to be located within the City's existing built-up area.

## 4.2 SUMMARY OF CITY OF WINDSOR HOUSING DEMAND FORECAST

The table below summarizes the City of Windsor housing forecast by unit type over the 2021-2041 period. In total, housing demand is estimated to be 10,032 occupied dwelling units.

Note that these forecasts only incorporate housing demand for people with their primary residence in the City. Any additional housing demand from increased post-secondary enrolment, temporary foreign workers, etc., should be over and above the housing demand forecasts summarized below.

**Figure 42 Potential Housing Demand by Structure Type, City of Windsor 2021-2041**  
Base Scenario

	Single Family			Apartment			Other	Total
	Singles	Semis	Rows	Total	Condo	Renter		
CMA Forecasts (annual)				<i>Occupied Dwelling units</i>				
2021-2026	528	87	147	277	64	213	15	1,054
2026-2031	593	73	118	229	61	167	13	1,026
2031-2036	529	57	81	148	51	97	9	825
2036-2041	447	36	38	120	36	84	19	660
Annual Average 2021-2041	524	63	96	194	53	141	14	891
				<i>Percent</i>				
City Share	45%	55%	75%	78%	66%	82%	52%	
City Forecasts (annual)				<i>Occupied Dwelling units</i>				
2021-2026	238	48	111	218	42	176	8	622
2026-2031	268	40	89	178	40	138	7	581
2031-2036	239	31	61	114	33	80	5	450
2036-2041	202	20	29	93	23	69	10	353
Annual Average 2021-2041	237	35	72	151	35	116	7	502
City Forecasts (total)								
2021-2026	1,191	240	553	1,088	209	880	38	3,110
2026-2031	1,339	200	443	890	201	689	35	2,907
2031-2036	1,195	157	303	568	167	401	24	2,248
2036-2041	<u>1,010</u>	<u>100</u>	<u>144</u>	<u>465</u>	<u>117</u>	<u>347</u>	<u>48</u>	<u>1,766</u>
Total 2021-2041	4,735	697	1,443	3,012	694	2,317	145	10,032

Source: Altus Group Economic Consulting, Housing Demand Model

## **5 RECOMMENDATIONS AND CONCLUSIONS**

---

### **5.1 SUMMARY OF FACTORS DRIVING DEMAND FOR HOUSING IN WINDSOR**

The City of Windsor's population is growing, however the inner-areas of the City have seen a decline in population even while the rest of the City was growing, and the City needs to encourage and promote intensification and infill to ensure that populations in existing parts of the City do not continue to decline. The decline in existing population in existing areas of the City is caused by a relative lack of new development and declining average household sizes.

In the past 5-10 years, the City has seen a steadily increasing amount of persons arriving in the City from international sources (immigration, non-permanent residents), as well as domestic sources (from other provinces, or elsewhere in Ontario), and in the case of domestic flows into or out of the City, Windsor has seen a reversal of long periods of net outflow to other parts of Canada and Ontario, and is now seeing net inflows from the rest of Canada.

The recent acceleration of population growth in the City (and the broader Essex County area) has had implications for the price of new housing, with average single-detached housing prices increasing by 232% since 2001, most of that price growth being in the last five years when both international and domestic sources of population growth increased dramatically.

Despite the increase in demand, the amount of new housing constructed in the City has not increased substantially, well below the amounts constructed during the 1996-2005 period, and only marginally higher than the period of 2006-2015 when only 300 new housing units were being completed per year.

Like elsewhere in Ontario, the City has seen very little purpose-built rental housing constructed, with just 6% of the new housing completions in the City since 1996 being rental in tenure.

Consistent with the lack of rental housing construction in the City, the rental vacancy rate has declined significantly over the past ten years, falling from above 13% in several years, to below 4% in each of the past five years. The

reduced vacancy rates in purpose-built rental apartments has caused apartment rental rates per month to increase by 33% since 2014.

The City imports a net 17,000 persons per day (as of 2016) from other parts of Essex County and the broader region, who come into the City for their place of employment, with the largest inflows to the City coming from LaSalle, Amherstburg, Lakeshore and the Town of Essex.

Perhaps owing to the relative lack of new housing production, the City has seen a significant decline in jobs located in the City, with nearly 9,000 fewer jobs located in the City in 2016 than there were in 2006.

Adding more housing options in the City of all types can help to reduce the need for in-commuters to fill jobs located in the City and make the City more attractive to employers by providing a sufficient locally-residing workforce.

The continued growth of e-commerce could impact the viability of ‘bricks and mortar’ retail options, however, studies have shown that retail oriented around neighbourhoods and communities are faring better than more isolated separated forms of retail (enclosed malls). Directing residential growth towards major retail centres can help add vitality to these areas of the City, support the existing retail in the City, and help build complete communities.

The industrial market across Ontario is at all-time low vacancy rates, with just 1.0% of industrial space across the Province vacant and available. The tightness of the industrial market, combined with emerging manufacturing investment in Michigan, means that the City should not rely on large-scale employment land conversion to add residential dwelling units in the built-up areas of the City. In fact, the shift towards online retail pushes a lot of demand for ‘space’ into the industrial sector, with every \$1 billion in e-commerce sales equating to a need for an additional 1.25-million square feet of industrial space.

## **5.2 RECOMMENDATIONS AND FINDINGS REGARDING MOST SUITABLE LOCATIONS FOR INFILL AND INTENSIFICATION**

Based on our analysis of the demographic, and real estate market trends affecting the City, the broader region and the Province of Ontario as a whole, our recommendations are as follows:

- The City needs more housing in the inner areas of the City, with populations in a majority of the City declining due to a lack of new housing options and shrinking average household sizes;
- The City is not seeing enough purpose-built rental housing constructed to meet demand;
- Access to retail, transit and other community amenities can bolster the market for new residential development, making an area attractive to prospective new households;
- Similarly, adding residential uses near existing retail clusters can improve the viability of those retail environments. The practice of redeveloping major retail centres for a mix of uses including residential, as well as other community amenities such as parks, community centres, and even additional retail is growing across Ontario and Canada;
- The City needs to account for prospective growth in post-secondary enrolment in forecasting housing needs;

It is estimated that the City will see an increase in housing demand in the next 20 years, with annual housing demand forecasted to be:

- 5,432 single/semi-detached units;
- 1,443 townhouse units;
- 3,012 apartment units per year, including 2,317 rental apartments, and 694 condominium apartments; and
- 145 'other' dwellings (such as accessory apartments, etc.)

In order to meet the housing demand forecast, particularly the likely demand for apartment dwellings in existing built-up parts of the City, the City should ensure that more sufficient land is designated and available for development to act as a contingency factor and to ensure that there is adequate choice in possible development sites for the entirety of the forecast period.

# **Appendix A**

## **Detailed Data**



Figure A- 1

**Total Housing Completions by Structure Type, City of Windsor, 1991-2020**

Year	Single-Detached	Semi-Detached	Row Units	Apartments	Total
1991	260	12	47	429	748
1992	329	116	120	226	791
1993	393	20	52	76	541
1994	489	46	26	16	577
1995	589	24	19	159	791
1996	783	156	48	115	1,102
1997	864	94	40	297	1,295
1998	775	158	63	16	1,012
1999	773	206	123	75	1,177
2000	913	214	174	336	1,637
2001	839	144	119	67	1,169
2002	1,003	186	148	151	1,488
2003	841	212	183	124	1,360
2004	831	124	180	83	1,218
2005	605	90	207	240	1,142
2006	444	20	74	16	554
2007	180	28	30	8	246
2008	130	8	14	139	291
2009	107	16	47	16	186
2010	162	18	73	0	253
2011	106	12	39	12	169
2012	199	26	120	16	361
2013	161	16	81	6	264
2014	199	54	67	17	337
2015	204	28	99	0	331
2016	271	64	89	3	427
2017	231	44	162	84	521
2018	120	52	166	13	351
2019	206	20	71	18	315
2020	251	16	196	365	828

Source: Altus Economic Consulting based on CMHC Housing Completions Data

Figure A- 2

**Total Housing Completions by Structure Type, City of Windsor, Five-Year Period 1991-2020**

5 Year Period	Single-Detached	Semi-Detached	Row Units	Apartments	Total
1991-1995	2,060	218	264	906	3,448
1996-2000	4,108	828	448	839	6,223
2001-2005	4,119	756	837	665	6,377
2006-2010	1,023	90	238	179	1,530
2011-2015	869	136	406	51	1,462
2016-2020	1,079	196	684	483	2,442

Source: Altus Economic Consulting based on CMHC Housing Completions Data

Figure A- 3

**Percentage Housing Completions by Structure Type, City of Windsor, Five-Year Periods 1991-2020**

5 Year Period	Single-Detached	Semi-Detached	Row Percent	Apartments	Total
1991-1995	59.7	6.3	7.7	26.3	100.0
1996-2000	66.0	13.3	7.2	13.5	100.0
2001-2005	64.6	11.9	13.1	10.4	100.0
2006-2010	66.9	5.9	15.6	11.7	100.0
2011-2015	59.4	9.3	27.8	3.5	100.0
2016-2020	44.2	8.0	28.0	19.8	100.0

Source: Altus Economic Consulting based on CMHC Housing Completions Data

Figure A- 4

**Total Housing Completions by Tenure, City of Windsor, 1991-2020**

Year	Homeowner	Rental	Condo Units	Co-Op	Total
1991	270	469	9	0	748
1992	349	302	4	136	791
1993	465	76	0	0	541
1994	557	20	0	0	577
1995	634	108	49	0	791
1996	959	75	68	0	1,102
1997	970	181	144	0	1,295
1998	990	22	0	0	1,012
1999	1,102	2	73	0	1,177
2000	1,296	42	299	0	1,637
2001	1,092	22	55	0	1,169
2002	1,322	54	112	0	1,488
2003	1,236	0	124	0	1,360
2004	1,119	52	47	0	1,218
2005	880	141	121	0	1,142
2006	538	16	0	0	554
2007	228	8	10	0	246
2008	144	16	131	0	291
2009	121	16	49	0	186
2010	176	0	77	0	253
2011	118	11	40	0	169
2012	237	22	102	0	361
2013	207	6	51	0	264
2014	278	20	39	0	337
2015	295	2	34	0	331
2016	416	5	6	0	427
2017	435	36	50	0	521
2018	338	13	0	0	351
2019	293	18	4	0	315
2020	412	281	135	0	828

Source: Altus Economic Consulting based on CMHC Housing Completions Data

**Figure A- 5 Total Housing Completions by Tenure, City of Windsor, Five-Year Period 1991-2020**

5 Year Period	Homeow ner	Rental	Condo <i>Units</i>	Co-Op	Total
1991-1995	2,275	975	62	136	3,448
1996-2000	5,317	322	584	-	6,223
2001-2005	5,649	269	459	-	6,377
2006-2010	1,207	56	267	-	1,530
2011-2015	1,135	61	266	-	1,462
2016-2020	1,894	353	195	-	2,442

Source: Altus Economic Consulting based on CMHC Housing Completions Data

**Figure A- 6 Percentage Housing Completions by Tenure, City of Windsor, Five-Year Period 1991-2020**

5 Year Period	Homeow ner	Rental	Condo <i>Percent</i>	Co-Op	Total
1991-1995	66.0	28.3	1.8	3.9	100.0
1996-2000	85.4	5.2	9.4	-	100.0
2001-2005	88.6	4.2	7.2	-	100.0
2006-2010	78.9	3.7	17.5	-	100.0
2011-2015	77.6	4.2	18.2	-	100.0
2016-2020	77.6	14.5	8.0	-	100.0

Source: Altus Economic Consulting based on CMHC Housing Completions Data

**Figure A- 7 Total Apartment Completions by Structure Size, City of Windsor, 2006-2020**

Year	1 to 49	50 to 99	100 to 199 <i>Units</i>	200+	Total
2006	16	-	-	-	16
2007	8	-	-	-	8
2008	16	-	123	-	139
2009	16	-	-	-	16
2010	-	-	-	-	-
2011	12	-	-	-	12
2012	16	-	-	-	16
2013	6	-	-	-	6
2014	17	-	-	-	17
2015	-	-	-	-	-
2016	3	-	-	-	3
2017	34	50	-	-	84
2018	13	-	-	-	13
2019	18	-	-	-	18
2020	93	-	272	-	365

Source: Altus Economic Consulting based on CMHC Housing Completions Data

Figure A- 8 **Total Apartment Completions by Structure Size, City of Windsor, Five-Year Periods 2006-2020**

5 Year Period	1 to 49	50 to 99	100 to 199	200+	Total
			<i>Units</i>		
2006-2010	56	-	123	-	179
2011-2015	51	-	-	-	51
2016-2020	161	50	272	-	483
<b>2006-2020</b>	<b>268</b>	<b>50</b>	<b>395</b>	<b>-</b>	<b>713</b>

Source: Altus Economic Consulting based on CMHC Housing Completions Data

Figure A- 9 **Percentage Apartment Completions by Structure Size, City of Windsor, Five-Year Periods 2006-2020**

5 Year Period	1 to 49	50 to 99	100 to 199	200+	Total
			<i>Percent</i>		
2006-2010	31.3	-	68.7	-	100.0
2011-2015	100.0	-	-	-	100.0
2016-2020	33.3	10.4	56.3	-	100.0
<b>2006-2020</b>	<b>37.6</b>	<b>7.0</b>	<b>55.4</b>	<b>-</b>	<b>100.0</b>

Source: Altus Economic Consulting based on CMHC Housing Completions Data

Figure A- 10 **Absorbed Single Detached Prices, City of Windsor, 1991-2020**

Year			Year-Over-Year Change	
	Median	Average	Median	Average
	<i>Dollars</i>		<i>Percentage</i>	
1991	140,000	160,428		
1992	135,000	150,318	-3.6%	-6.3%
1993	140,000	157,607	3.7%	4.8%
1994	155,000	169,878	10.7%	7.8%
1995	155,000	164,334	0.0%	-3.3%
1996	150,000	164,723	-3.2%	0.2%
1997	160,000	175,903	6.7%	6.8%
1998	150,000	169,148	-6.3%	-3.8%
1999	150,000	172,564	0.0%	2.0%
2000	150,000	163,992	0.0%	-5.0%
2001	160,000	177,452	6.7%	8.2%
2002	165,000	178,461	3.1%	0.6%
2003	170,000	191,731	3.0%	7.4%
2004	175,000	193,467	2.9%	0.9%
2005	180,000	205,982	2.9%	6.5%
2006	200,000	214,232	11.1%	4.0%
2007	215,000	234,982	7.5%	9.7%
2008	220,000	246,370	2.3%	4.8%
2009	245,000	273,579	11.4%	11.0%
2010	235,000	266,250	-4.1%	-2.7%
2011	250,000	267,039	6.4%	0.3%
2012	252,500	286,718	1.0%	7.4%
2013	267,500	288,863	5.9%	0.7%
2014	280,000	312,031	4.7%	8.0%
2015	340,000	369,230	21.4%	18.3%
2016	330,000	359,038	-2.9%	-2.8%
2017	420,000	426,843	27.3%	18.9%
2018	520,000	531,455	23.8%	24.5%
2019	555,000	558,172	6.7%	5.0%
2020	580,000	588,417	4.5%	5.4%

Source: Altus Economic Consulting based on CMHC Housing  
Absorption Data

**Figure A- 11 Average Absorbed Singled Detached Homes In Ontario and Select Municipalities**

Year	Ontario	Windsor	London	Kitchener	Hamilton	Toronto
	<i>Dollars</i>					
2001	263,164	177,452	214,492	227,021	231,706	564,140
2002	269,081	178,461	220,386	239,285	235,128	551,918
2003	290,124	191,731	240,688	236,186	265,749	520,823
2004	313,664	193,467	253,763	245,259	301,256	570,836
2005	349,663	205,982	276,860	271,456	331,422	609,595
2006	386,261	214,232	281,845	305,086	344,357	890,223
2007	418,785	234,982	296,745	338,060	340,989	919,300
2008	440,309	246,370	326,504	338,603	377,445	944,011
2009	463,661	273,579	352,167	349,037	399,266	940,566
2010	486,918	266,250	352,936	388,675	422,685	1,233,587
2011	522,909	267,039	354,114	413,267	419,949	1,252,512
2012	554,319	286,718	365,827	444,673	460,698	1,202,158
2013	597,562	288,863	383,963	485,782	470,423	1,577,146
2014	643,190	312,031	415,157	445,975	489,281	1,739,480
2015	682,769	369,230	435,469	452,732	487,077	2,032,261
2016	714,706	359,038	455,346	470,937	468,228	1,976,205
2017	786,091	426,843	536,395	503,552	457,194	1,846,322
2018	851,038	531,455	629,224	669,668	530,351	1,990,584
2019	900,979	558,172	656,733	740,994	572,245	1,889,558
2020	894,118	588,417	654,956	751,338	622,407	1,914,339
Change	630,954	410,965	440,464	524,317	390,701	1,350,199
2001-2020	239.8%	231.6%	205.4%	231.0%	168.6%	239.3%

Source: Altus Economic Consulting based on CMHC Housing Absorption Data

**Figure A- 12 Median Absorbed Singled Detached Homes In Ontario and Select Municipalities**

Year	Ontario	Windsor	London	Kitchener	Hamilton	Toronto
	<i>Dollars</i>					
2001	250,000	160,000	195,000	200,000	205,000	500,000
2002	250,000	165,000	200,000	220,000	220,000	400,000
2003	270,000	170,000	215,000	195,000	232,500	340,000
2004	290,000	175,000	225,000	210,000	280,000	380,000
2005	325,000	180,000	250,000	250,000	300,000	405,000
2006	350,000	200,000	260,000	270,000	320,000	800,000
2007	375,000	215,000	270,000	290,000	320,000	805,000
2008	395,000	220,000	300,000	300,000	350,000	885,000
2009	405,000	245,000	315,000	315,000	355,000	900,000
2010	425,000	235,000	320,000	340,000	390,000	995,000
2011	440,000	250,000	330,000	380,000	390,000	1,095,000
2012	480,000	252,500	335,000	425,000	440,000	995,000
2013	495,000	267,500	355,000	445,000	450,000	1,340,000
2014	515,000	280,000	380,000	415,000	475,000	1,790,000
2015	520,000	340,000	400,000	412,500	480,000	2,000,000
2016	570,000	330,000	427,500	450,000	455,000	2,000,000
2017	610,000	420,000	500,000	475,000	395,000	1,892,500
2018	650,000	520,000	600,000	625,000	490,000	2,000,000
2019	700,000	555,000	650,000	700,000	490,000	1,950,000
2020	700,000	580,000	600,000	700,000	580,000	1,990,000
Change	450,000	420,000	405,000	500,000	375,000	1,490,000
2001-2020	180.0%	262.5%	207.7%	250.0%	182.9%	298.0%

Source: Altus Economic Consulting based on CMHC Housing Absorption Data

Figure A- 13

**Population, City of Windsor & County of Essex, 2001-2020**

Year	Windsor	Essex	Year-Over Year Change	
	<i>People</i>		Windsor	Essex
			<i>Percent</i>	
2001	218,222	390,809		
2002	221,623	397,995	1.6	1.8
2003	223,551	402,572	0.9	1.2
2004	225,025	406,321	0.7	0.9
2005	225,794	408,840	0.3	0.6
2006	225,241	409,126	(0.2)	0.1
2007	223,129	408,034	(0.9)	(0.3)
2008	220,519	404,870	(1.2)	(0.8)
2009	217,763	401,476	(1.2)	(0.8)
2010	217,358	400,207	(0.2)	(0.3)
2011	217,104	399,724	(0.1)	(0.1)
2012	218,308	401,171	0.6	0.4
2013	220,964	405,080	1.2	1.0
2014	221,539	406,797	0.3	0.4
2015	222,192	407,791	0.3	0.2
2016	224,487	412,050	1.0	1.0
2017	225,539	415,308	0.5	0.8
2018	228,894	421,687	1.5	1.5
2019	233,278	426,474	1.9	1.1
2020	235,428	430,945	0.9	1.0
Change	17,206	40,136	7.9	10.3
2001-2020				

Source: Altus Economic Consulting based on Statistics Canada  
Population Estimate, 2001-2020

Figure A- 14

**International Immigration by Age Cohort, City of Windsor, 2002-2020**

Year	0-14	15-24	25-34	35-44	45-54	55-64	65-74	75-84	85-90+	Total
	<i>Persons</i>									
2002	1,017	644	1,240	739	240	140	97	24	2	4,143
2003	642	443	792	446	169	117	67	14	-	2,690
2004	755	485	923	541	192	121	82	21	-	3,120
2005	807	532	893	587	174	56	49	14	-	3,112
2006	749	487	907	539	215	112	64	8	5	3,086
2007	594	468	742	432	195	92	44	16	-	2,583
2008	550	442	682	410	199	121	63	26	-	2,493
2009	436	342	566	306	174	103	68	16	2	2,013
2010	502	404	501	335	195	98	55	9	-	2,099
2011	498	314	469	350	180	92	54	12	4	1,973
2012	363	267	391	296	146	98	75	28	6	1,670
2013	436	295	482	311	178	122	103	26	2	1,955
2014	378	322	496	276	167	121	71	32	2	1,865
2015	301	221	464	227	127	83	44	13	2	1,482
2016	846	381	723	439	210	121	78	22	1	2,821
2017	487	205	592	302	124	88	55	27	2	1,882
2018	529	345	857	365	163	108	65	33	6	2,471
2019	521	359	964	376	142	124	83	34	11	2,614
2020	598	379	1,023	345	164	105	60	16	1	2,691
Total	11,009	7,335	13,707	7,622	3,354	2,022	1,277	391	46	46,763

Source: Altus Economic Consulting based on Statistics Canada Estimates of the Components of Demographic Growth, 2002-2020



Figure A- 15 Emigration by Age Cohort, City of Windsor, 2002-2020

Year	0-14	15-24	25-34	35-44	45-54	55-64	65-74	75-84	85-90+	Total
<i>Persons</i>										
2002	102	89	247	186	70	43	15	13	-	765
2003	215	114	500	371	128	76	29	25	1	1,459
2004	330	217	502	403	147	85	18	9	-	1,711
2005	373	163	574	421	153	80	23	10	-	1,797
2006	383	188	601	441	197	117	20	5	2	1,954
2007	478	178	653	512	202	99	32	26	3	2,183
2008	477	307	634	498	233	118	22	16	-	2,305
2009	408	197	516	403	206	110	20	9	-	1,869
2010	374	168	416	319	170	92	27	16	-	1,582
2011	401	152	425	336	232	116	48	25	-	1,735
2012	396	152	407	314	176	86	53	29	6	1,619
2013	302	148	344	253	187	97	35	17	5	1,388
2014	298	165	349	248	200	101	26	14	-	1,401
2015	239	136	289	196	160	82	46	22	4	1,174
2016	241	155	331	225	176	95	41	20	1	1,285
2017	208	98	259	176	166	93	43	24	6	1,073
2018	178	111	237	157	124	72	30	18	4	931
2019	180	112	242	160	125	74	30	18	4	945
2020	138	85	177	122	96	55	25	15	1	714
Total	5,721	2,935	7,703	5,741	3,148	1,691	583	331	37	27,890

Source: Altus Economic Consulting based on Statistics Canada Estimates of the Components of Demographic Growth, 2002-2020

Figure A- 16 Net Immigration by Age Cohort, City of Windsor, 2002-2020

Year	0-14	15-24	25-34	35-44	45-54	55-64	65-74	75-84	85-90+	Total
<i>Persons</i>										
2002	915	555	993	553	170	97	82	11	2	3,378
2003	427	329	292	75	41	41	38	(11)	(1)	1,231
2004	425	268	421	138	45	36	64	12	-	1,409
2005	434	369	319	166	21	(24)	26	4	-	1,315
2006	366	299	306	98	18	(5)	44	3	3	1,132
2007	116	290	89	(80)	(7)	(7)	12	(10)	(3)	400
2008	73	135	48	(88)	(34)	3	41	10	-	188
2009	28	145	50	(97)	(32)	(7)	48	7	2	144
2010	128	236	85	16	25	6	28	(7)	-	517
2011	97	162	44	14	(52)	(24)	6	(13)	4	238
2012	(33)	115	(16)	(18)	(30)	12	22	(1)	-	51
2013	134	147	138	58	(9)	25	68	9	(3)	567
2014	80	157	147	28	(33)	20	45	18	2	464
2015	62	85	175	31	(33)	1	(2)	(9)	(2)	308
2016	605	226	392	214	34	26	37	2	-	1,536
2017	279	107	333	126	(42)	(5)	12	3	(4)	809
2018	351	234	620	208	39	36	35	15	2	1,540
2019	341	247	722	216	17	50	53	16	7	1,669
2020	460	294	846	223	68	50	35	1	-	1,977
Total	915	555	993	553	170	97	82	11	2	18,873

Source: Altus Economic Consulting based on Statistics Canada Estimates of the Components of Demographic Growth, 2002-2020

Figure A- 17

**Interprovincial Migration by Age Cohort, City of Windsor, 2002-2020**

Year	0-14	15-24	25-34	35-44	45-54	55-64	65-74	75-84	85-90+	Total
<i>Persons</i>										
2002	33	(60)	(8)	14	10	(18)	(8)	(4)	(3)	(44)
2003	(39)	(61)	(8)	14	-	(22)	(8)	1	(3)	(126)
2004	(30)	(106)	(33)	(7)	(23)	(30)	(7)	-	(3)	(239)
2005	(126)	(173)	(168)	(84)	(41)	(38)	(10)	(3)	(3)	(646)
2006	(300)	(281)	(348)	(186)	(47)	(43)	(9)	-	(5)	(1,219)
2007	(406)	(491)	(626)	(283)	(102)	(75)	9	(10)	(4)	(1,988)
2008	(318)	(656)	(698)	(323)	(114)	(80)	14	(1)	(4)	(2,180)
2009	(269)	(643)	(670)	(315)	(122)	(84)	25	4	(3)	(2,077)
2010	(27)	(322)	(479)	(212)	(62)	(42)	19	(2)	(6)	(1,133)
2011	20	(209)	(299)	(124)	(17)	(15)	28	5	(4)	(615)
2012	(56)	(309)	(343)	(194)	-	(11)	(1)	(1)	3	(912)
2013	(108)	(308)	(387)	(217)	(45)	(40)	11	10	3	(1,081)
2014	(15)	(328)	(251)	(147)	(28)	(29)	1	2	3	(792)
2015	55	(228)	(297)	(179)	24	(8)	8	3	13	(609)
2016	225	(50)	(56)	(53)	43	12	(1)	-	10	130
2017	289	31	(170)	(48)	73	44	5	3	2	229
2018	202	(29)	104	68	59	36	(11)	(3)	(1)	425
2019	186	(5)	53	42	39	22	10	4	4	355
2020	165	(33)	13	24	29	13	6	1	2	220
Total	(519)	(4,261)	(4,671)	(2,210)	(324)	(408)	81	9	1	(12,302)

Source: Altus Economic Consulting based on Statistics Canada Estimates of the Components of Demographic Growth, 2002-2020

Figure A- 18

**Intraprovincial Migration by Age Cohort, City of Windsor, 2002-2020**

Year	0-14	15-24	25-34	35-44	45-54	55-64	65-74	75-84	85-90+	Total
<i>Persons</i>										
2002	52	140	(234)	312	99	(49)	13	(28)	(13)	292
2003	(31)	109	(246)	310	65	(87)	-	(37)	(17)	66
2004	(61)	33	(437)	205	50	(81)	(4)	(31)	(14)	(340)
2005	(181)	(210)	(554)	136	40	(89)	13	(30)	(13)	(888)
2006	(320)	(255)	(623)	56	15	(93)	14	(30)	(15)	(1,251)
2007	(390)	(242)	(629)	(409)	(125)	28	76	(1)	(42)	(1,734)
2008	(387)	(313)	(724)	(455)	(157)	23	71	(14)	(61)	(2,017)
2009	(415)	(363)	(818)	(521)	(178)	-	136	31	(36)	(2,164)
2010	(186)	(337)	(621)	(399)	(78)	72	125	12	(51)	(1,463)
2011	(92)	(164)	(410)	(279)	(41)	91	93	3	(48)	(847)
2012	174	(153)	(305)	(144)	102	69	27	11	2	(217)
2013	143	(285)	(268)	(122)	72	44	44	17	2	(353)
2014	105	(296)	(314)	(148)	127	88	73	27	6	(332)
2015	106	(275)	(317)	(150)	95	65	65	25	7	(379)
2016	193	(198)	(375)	(182)	140	92	74	28	6	(222)
2017	381	(138)	(235)	(95)	109	101	61	23	6	213
2018	415	(171)	(10)	28	231	205	152	54	17	921
2019	381	(445)	(207)	(81)	171	152	76	27	8	82
2020	387	(478)	(207)	(80)	172	155	80	30	6	65
Total	274	(4,041)	(7,534)	(2,018)	909	786	1,189	117	(250)	(10,568)

Source: Altus Economic Consulting based on Statistics Canada Estimates of the Components of Demographic Growth, 2002-2020

Figure A- 19 **Non-Permanent Residents by Age Cohort, City of Windsor, 2002-2020**

Year	0-14	15-24	25-34	35-44	45-54	55-64	65-74	75-84	85-90+	Total
<i>Persons</i>										
2002	138	494	65	37	(6)	(4)	(1)	(2)	-	721
2003	152	540	187	94	23	(11)	(10)	(8)	(2)	965
2004	31	306	(32)	(35)	(16)	(20)	(13)	(3)	-	218
2005	23	229	(111)	(54)	(35)	(10)	(11)	(7)	-	24
2006	(180)	(329)	(862)	(169)	(29)	13	(6)	(3)	4	(1,561)
2007	96	446	104	117	66	4	(11)	3	(1)	824
2008	180	134	(150)	(152)	(188)	(88)	(6)	(8)	(1)	(279)
2009	(45)	198	(147)	(234)	(205)	(70)	(12)	(4)	(3)	(522)
2010	(72)	140	(182)	(141)	(77)	(33)	(9)	(6)	1	(379)
2011	(31)	106	(176)	(92)	(18)	(25)	1	1	2	(232)
2012	10	907	363	164	(35)	(27)	(13)	(3)	(3)	1,363
2013	64	2,311	938	289	89	2	(14)	(4)	1	3,676
2014	106	1,316	(202)	83	34	(23)	9	7	(1)	1,329
2015	(10)	10	(237)	271	309	140	8	(4)	-	487
2016	(27)	442	(7)	796	559	153	(7)	(7)	2	1,904
2017	158	2,038	(89)	(210)	(255)	(152)	(14)	2	(2)	1,476
2018	105	5,261	79	(848)	(972)	(419)	(44)	3	-	3,165
2019	236	3,212	(100)	(318)	(294)	(139)	(30)	(1)	(2)	2,564
2020	70	2,653	(375)	(28)	(119)	(73)	(23)	(4)	-	2,101
Total	1,004	20,414	(934)	(430)	(1,169)	(782)	(206)	(48)	(5)	17,844

Source: Altus Economic Consulting based on Statistics Canada Estimates of the Components of Demographic Growth, 2002-2020

Figure A- 20 **University and College Enrollment, City of Windsor, 2012-2025**

School Year (Fall/Winter)	University of Windsor	St. Clair College <i>Students</i>	Total
2012-2013	13,710	6,755	20,465
2013-2014	14,103	7,004	21,107
2014-2015	14,028	7,135	21,163
2015-2016	13,560	7,164	20,724
2016-2017	13,610	7,148	20,758
2017-2018	14,078	7,662	21,740
2018-2019	14,506	10,086	24,592
2019-2020	14,769	9,102	23,871
2020-2021	16,880	7,213	24,093
2021-2022	17,200	9,228	26,428
2022-2023	17,285	9,888	27,173
2023-2024	17,305	10,108	27,413
2024-2025	17,331	10,284	27,615
Change 2012-2020	3,170 23.1%	458 6.8%	3,628 17.7%
Change 2020-2025	451 2.7%	3,071 42.6%	3,522 14.6%

Note: Student enrollment projections for St. Clair College between 2020-2025 have been discounted to account for campuses outside of Windsor

Source: Altus Economic Consulting based on University of Windsor, St. Clair College, and Ministry of Colleges and Universities



# **MULTI-RESIDENTIAL INTERIM CONTROL BYLAW STUDY BACKGROUND REPORT**



**MUNICIPAL PLANNING  
CONSULTANTS**

APRIL 20, 2022

## TABLE OF CONTENTS

<b>1.0</b>	<b>Introduction .....</b>	<b>1</b>
<b>2.0</b>	<b>Policy Review .....</b>	<b>2</b>
2.1	<i>Planning Act, RSO 1990, c.13,) (as amended) .....</i>	2
2.2	<i>Provincial Policy Statement (2020).....</i>	4
2.3	<i>City of Windsor Official Plan .....</i>	6
2.4	<i>Zoning By-law .....</i>	11
<b>3.0</b>	<b>Licensing Special Needs Housing under the Municipal Act ....</b>	<b>17</b>
<b>4.0</b>	<b>Market Overview Summary .....</b>	<b>17</b>
<b>5.0</b>	<b>Directing Intensification .....</b>	<b>18</b>
5.1	<i>Transit .....</i>	18
5.2	<i>Goods and Services.....</i>	19
5.3	<i>Municipal Services and Infrastructure.....</i>	19
5.4	<i>Development Constraints .....</i>	19
5.5	<i>Community Improvement Areas .....</i>	20
5.6	<i>Mature Neighbourhoods .....</i>	20
<b>6.0</b>	<b>Character Precincts .....</b>	<b>21</b>
6.1	<i>City Centre .....</i>	22
6.3	<i>Corridors .....</i>	23
6.4	<i>Neighbourhood Nodes .....</i>	23
6.5	<i>Low Profile Neighbourhoods.....</i>	23
6.6	<i>Mature Neighbourhoods .....</i>	23
<b>7.0</b>	<b>Policy Direction .....</b>	<b>23</b>
7.1	<i>Change and Investment.....</i>	24
7.2	<i>Protection and Promotion.....</i>	25
7.3	<i>Defining Compatible Development.....</i>	26
7.4	<i>Special Needs Housing .....</i>	27
<b>8.0</b>	<b>Summary of Recommendations.....</b>	<b>28</b>

**FIGURES:**

- Figure 1 - Windsor Official Plan Schedule J
- Figure 2 - Windsor Official Plan Schedule D
- Figure 3 - Windsor Official Plan Schedule E
- Figure 4 - Approved Exceptions to ICBL
- Figure 5 - As of Right Lodging House Zones
- Figure 6 - Permitted Combined Use
- Figure 5 - As of Right Lodging House Zones
- Figure 6 - Permitted Combined Use
- Figure 7 - Intensification Location Factors
- Figure 8 - Windsor Character Precincts
- Figure 9 - Tecumseh Rd and Lauzon Parkway Regional Centre
- Figure 10 - Tecumseh Rd Corridor



## 1.0 Introduction

The City of Windsor passed Interim Control By-law 103-2020 on July 13, 2020 to prohibit the use on all lands, buildings, and structures for a Group Home, Shelter, Lodging House, and a Dwelling with five or more dwelling units, other than those excepted by the Interim Control By-law, in order to allow the municipality to review and, if deemed appropriate, implement the findings of the review.

There is a desire across Ontario to see existing communities intensify over time because, in accordance with Provincial Policy, intensification delivers on a number of key planning principles, including:

- A more efficient use of land and investments in municipal infrastructure, typically based on an urban structure of higher density centres and corridors;
- The establishment of transit supportive forms of development that will support transit system investment and promote more mobility options within the City; and,
- The delivery of a broader mix of housing types, including housing that is more affordable than the traditional housing stock.

Intensification also causes great anxiety where it is proposed within or immediately adjacent to any of the City's existing residential neighbourhoods. Conversations about community character and impacts of change dominate Council's deliberations on development applications that affect residential communities.

The main purpose of the study is to:

- Determine the appropriate locations within the City that can accommodate additional residential density;
- How to appropriately guide growth to those geographic areas;
- The extent to which a designated area can accommodate growth; and,
- How to ensure compatibility within the existing neighbourhood context.

In September 2021 the City retained the consulting team comprised of Municipal Planning Consultants, The Planning Partnership and Altus Group to complete this study. The result of this work will be changes to the City of Windsor Official Plan and

Zoning By-law to build a foundation within municipal planning documents to detail a rationale for where density makes sense within the City. The end products will be amendment to the City's Official Plan and Zoning By-laws that are required to implement the recommendations in this report, as may be approved by Council. In addition, the products will include Design Guidelines to assist staff and Council in their review and assessment of intensification proposals to ensure compatibility within the community.

## **2.0 Policy Review**

The hierarchy of Planning legislation and policy in the Province of Ontario requires the Official Plan to have regard for matters of Provincial Interest and be consistent with Policy Statements issued by the Province. The Province strongly supports intensification and infilling within the existing urban areas and requires the City of Windsor to enable and promote this form of development. The following is a brief summary of the legislation and Policies that are intended to guide decisions regarding these matters.

### **2.1 Planning Act, RSO 1990, c.13,) (as amended)**

Section 2 of the Planning Act requires all decisions on Official Plans and Zoning By-laws to have regard for the following matters of Provincial Interest as they relate to housing and communities:

- (e) *the supply, efficient use and conservation of energy and water;*
- (f) *the adequate provision and efficient use of communication, transportation, sewage and water services and waste management systems;*
- (g) *the minimization of waste;*
- (h) *the orderly development of safe and healthy communities;*
- (h.1) *the accessibility for persons with disabilities to all facilities, services and matters to which this Act applies;*
- (i) *the adequate provision and distribution of educational, health, social, cultural and recreational facilities;*
- (j) *the adequate provision of a full range of housing, including affordable housing;*
- (k) *the adequate provision of employment opportunities;*
- (l) *the protection of the financial and economic well-being of the Province and its municipalities;*
- (m) *the co-ordination of planning activities of public bodies;*
- (n) *the resolution of planning conflicts involving public and private interests;*
- (o) *the protection of public health and safety;*



- (p) *the appropriate location of growth and development;*
- (q) *the promotion of development that is designed to be sustainable, to support public transit and to be oriented to pedestrians;*
- (r) *the promotion of built form that,*
  - (i) *is well-designed,*
  - (ii) *encourages a sense of place, and*
  - (iii) *provides for public spaces that are of high quality, safe, accessible, attractive and vibrant; and*
- (s) *the mitigation of greenhouse gas emissions and adaptation to a changing climate.*

Enabling infilling and intensification in the City has been demonstrated to achieve these objectives where this form of development is properly planned and managed.

The key issue in considering how the City can appropriately manage/regulate the development and use of various forms of housing under the legislative authority of the Planning Act starts with a recognition of a number of fundamental principles, including:

- The regulations of the City's planning instruments must begin with an understanding of the responsibility to implement planning regulations in accordance with the Human Rights Code. Section 2.1 of the Code states:

*"Every person has a right to equal treatment with respect to the occupancy of accommodation without discrimination because of race, ancestry, place of origin, colour, ethnic origin, citizenship, creed, sex, sexual orientation, age, marital status, family status, disability or the receipt of public assistance."*

- The City cannot regulate the occupancy of a dwelling unit. In other words, the number of residents in a dwelling unit cannot be controlled.
- The City cannot regulate the tenure of a dwelling unit. In other words, whether the unit is owned or rented, and whether that rental is short-term or long-term.
- The City can control the land uses permitted on a lot, the number of dwelling units on a lot, the number of buildings on a lot, the location of the buildings on a lot and the height and massing of buildings on a lot and parking requirements.

## **2.2 Provincial Policy Statement (2020)**

The Provincial Policy Statement (PPS) is the only policy statement issued under Section 3 of the Planning Act that is in effect within the City of Windsor. The PPS is a wide reaching document, addressing all of the planning issues identified in Section 2 of the Act and providing direction to municipalities for implementation of the policies. Consideration of policies related to intensification include environmental, social, health and safety, financial and cultural issues. These matters are addressed in the PPS and to a great extent have already been implemented in the City of Windsor Official Plan. Key to the consideration of this matter are the policies that direct the City to undertake specific actions and include policies in the City's Official Plan to encourage and promote intensification. The following provides a brief summary of those policies.

### *1.0 Building Strong Healthy Communities*

*Ontario's long-term prosperity, environmental health and social well-being depend on wisely managing change and promoting efficient land use and development patterns. Efficient land use and development patterns support sustainability by promoting strong, liveable, healthy and resilient communities, protecting the environment and public health and safety, and facilitating economic growth.*

#### *1.1.1 Healthy, liveable and safe communities are sustained by:*

- a) promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term;*
- b) accommodating an appropriate affordable and market-based range and mix of residential types (including single-detached, additional residential units, multi-unit housing, affordable housing and housing for older persons), employment (including industrial and commercial), institutional (including places of worship, cemeteries and long-term care homes), recreation, park and open space, and other uses to meet long-term needs;*
- c) avoiding development and land use patterns which may cause environmental or public health and safety concerns;*
- d) avoiding development and land use patterns that would prevent the efficient expansion of settlement areas in those areas which are adjacent or close to settlement areas;*
- e) promoting the integration of land use planning, growth management, transit-supportive development, intensification and infrastructure planning to achieve cost-effective development patterns, optimization of transit investments, and standards to minimize land consumption and servicing costs;*

- f) improving accessibility for persons with disabilities and older persons by addressing land use barriers which restrict their full participation in society;*
- g) ensuring that necessary infrastructure and public service facilities are or will be available to meet current and projected needs;*
- h) promoting development and land use patterns that conserve biodiversity; and*
- i) preparing for the regional and local impacts of a changing climate.*

*1.1.2 Sufficient land shall be made available to accommodate an appropriate range and mix of land uses to meet projected needs for a time horizon of up to 25 years, informed by provincial guidelines. However, where an alternate time period has been established for specific areas of the Province as a result of a provincial planning exercise or a provincial plan, that time frame may be used for municipalities within the area.*

*Within settlement areas, sufficient land shall be made available through intensification and redevelopment and, if necessary, designated growth areas.*

*1.1.3.2 Land use patterns within settlement areas shall also be based on a range of uses and opportunities for intensification and redevelopment in accordance with the criteria in policy 1.1.3.3, where this can be accommodated.*

*1.1.3.3 Planning authorities shall identify appropriate locations and promote opportunities for transit-supportive development, accommodating a significant supply and range of housing options through intensification and redevelopment where this can be accommodated taking into account existing building stock or areas, including brownfield sites, and the availability of suitable existing or planned infrastructure and public service facilities required to accommodate projected needs.*

*1.1.3.4 Appropriate development standards should be promoted which facilitate intensification, redevelopment and compact form, while avoiding or mitigating risks to public health and safety.*

*1.1.3.5 Planning authorities shall establish and implement minimum targets for intensification and redevelopment within built-up areas, based on local conditions. However, where provincial targets are established through provincial plans, the provincial target shall represent the minimum target for affected areas.*

*1.1.3.6 New development taking place in designated growth areas should occur adjacent to the existing built-up area and should have a compact form, mix of uses and densities that allow for the efficient use of land, infrastructure and public service facilities.*

The provincial support for intensification is strong and the requirement for the City to accommodate and plan for this form of development is very clear.

## 2.3 City of Windsor Official Plan

The Official Plan for the City was originally approved in 2002 but has been the subject of significant amendments to keep the Plan updated. Chapter 3 of the Plan includes a broad Development Strategy for the City and was included in the Plan by amendment in 2012. At that time the 2005 PPS was in effect and had many of the same or similar policies regarding infilling and intensification as the 2020 PPS.

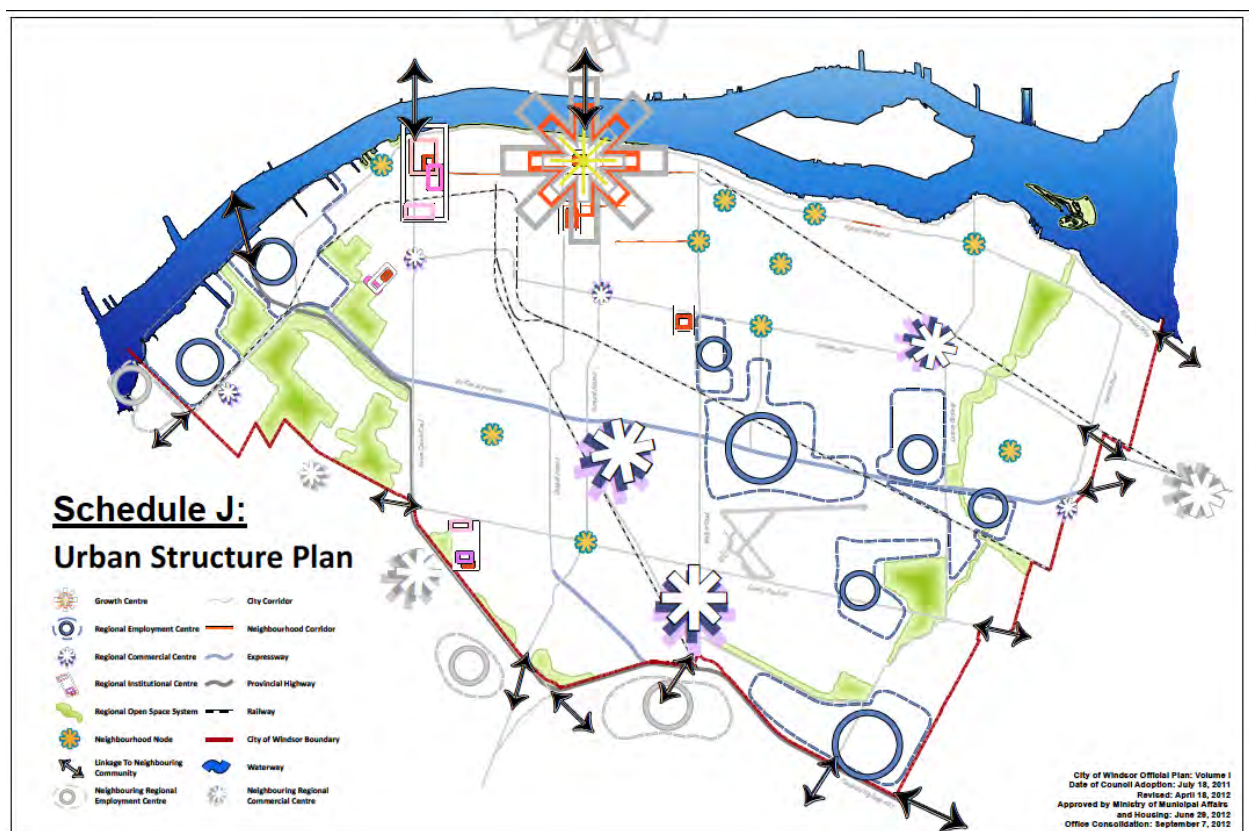


Figure 1 – Windsor Official Plan Schedule J

Schedule J to the Official Plan, the Urban Structure plan was added to the Official Plan at the same time and is shown here as Figure 1. It acts as an overlay designation and refers back to the policies in Section 6 and designations on Schedule D (Land Use) and Schedule E (City Centre Planning District) of the Plan.

Schedule J identifies a number Nodes in the City. The following policies describe the characteristics and functions of the Nodes:

### 3.3.1 Nodes

*Nodes in this context are existing or future locations of concentrated activity on the Urban Structure Plan that serve the societal, environmental and economic needs at a neighbourhood and/or regional scale. The most successful nodes are the ones that exhibit a wide variety of land uses, including higher density residential and employment uses, and have access to frequent public transit service. Smaller scale community and neighbourhood nodes play an important role in providing services to the surrounding neighbourhoods, providing a range of housing opportunities and, providing a recognized sense of place for these neighbourhoods.*

There are a hierarchy of Nodes identified in the Plan. This hierarchy is summarized in the following policy excerpts from the Plan:

*3.3.1.1 Growth Centres are the highest in the hierarchy of nodes in Windsor due to their scale, density, range of uses, function and current or future identity. Growth Centres should be planned:*

- (a) To serve as focal areas for investment in institutional and regionwide public services, as well as commercial, recreational, cultural and entertainment uses;*
- (b) To accommodate and support major transit infrastructure;*
- (c) To serve as high density major employment centres;*
- (d) To accommodate a significant share of households and employment growth; and,*
- (e) To accommodate a minimum density of 200 residents and 200 jobs per net hectare;*

The minimum density for new residential-only development is 80 units per net hectare.

While these policies do not specifically promote intensification of these Growth Centres, they clearly support higher density residential uses within the identified Nodes.

*3.3.1.2 Major Activity Centres are second in the hierarchy of nodes in Windsor.*

*The following comprise Windsor's Major Activity Centres:*

- (a) Regional Commercial Centres;*
- (b) Regional Institutional Centres;*
- (c) Regional Employment Centres; and*
- (d) Regional Open Space System.*

Future residential development and redevelopment at Major Activity Centres should be medium (30 units per net hectare) to high-density (80+ units per net hectare). Residential intensification is desired at or near Major Activity Centres.

Regional Employment Centres and Regional Open Space System areas are not appropriate for residential development. However, the existing policies enable residential development to occur at significant densities in the and Regional Commercial Centres. This concept is supported by the following:

*In the future these nodes should function as vibrant mixed-use commercial-residential neighbourhoods serving a higher density of population. Ideally, the predominant form of new or redeveloped housing should be medium and high-density residential buildings with ground floor and possibly second floor commercial uses and upper floor residential dwellings.*

The Urban Structure Plan also identifies a hierarchy of Corridors within the City, including City Corridors and Neighbourhood Corridors. Residential development along City Corridors include medium and high profile developments of between 14 and 58 metres (16 storeys) in height. There are no provisions for intensification of residential uses within the Neighbourhood Corridors.

The Neighbourhood policies in Chapter 3 permit a mix of low and medium density development within the Neighbourhood areas as follows:

*The three dominant types of dwellings in Windsor's neighbourhoods are single detached, semi-detached and townhouses. The density range for Windsor's neighbourhoods is between 20 to 35 units per net hectare. This density range provides for low and some medium-density intensification to occur in existing neighbourhoods. Multiple dwelling buildings with medium and high-densities are encouraged at nodes identified in the Urban Structure Plan.*

The policies in Chapter 3 of the Plan are implemented in greater detail in Chapter 6, and on Schedules D and E of the Plan. It is noted that many of the policies in Chapter 6 of the Plan were enacted before Chapter 3 and Schedule J were approved.



There are three Land Use designations that permit residential uses; Residential, Mixed Use (Schedule D) and City Centre Planning District (Schedule E).

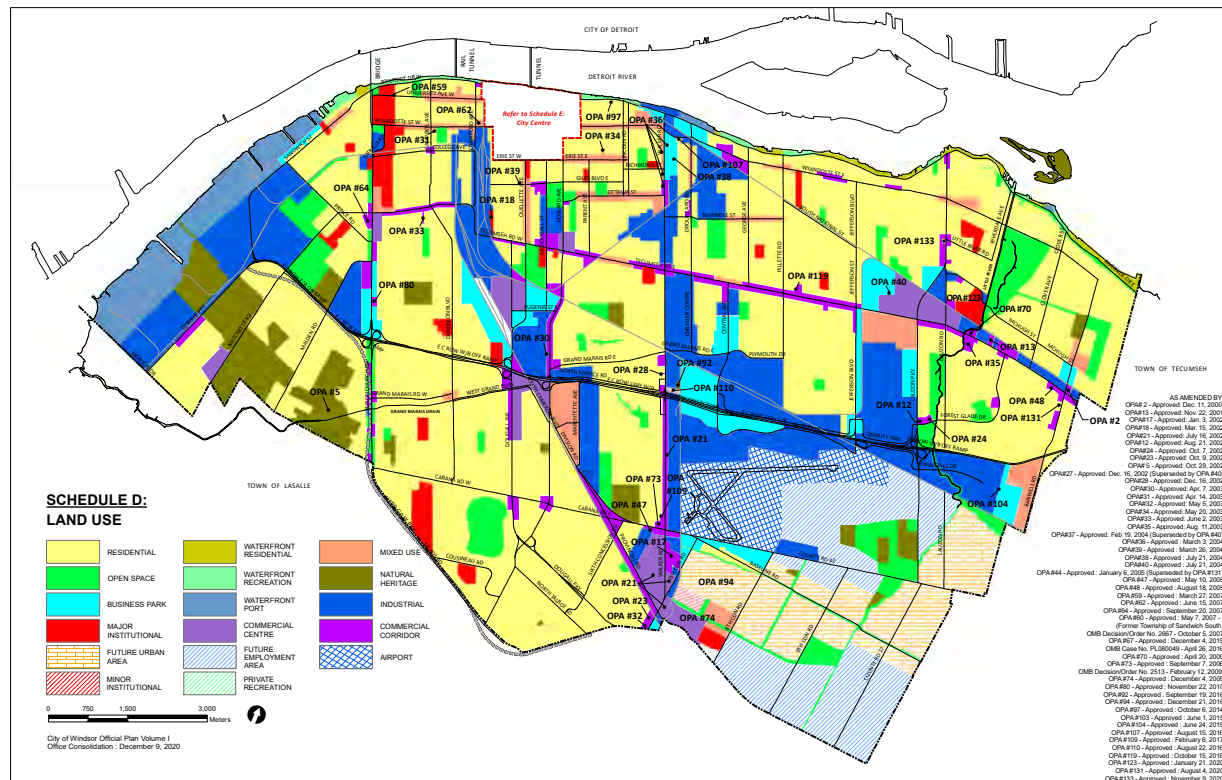


Figure 2 - Windsor Official Plan Schedule D

The Residential policies in the Plan permit low, medium and high profile residential uses subject to the following policies:

6.3.1.3 To promote selective residential redevelopment, infill and intensification initiatives.

6.3.2.5 At the time of submission, the proponent shall demonstrate to the satisfaction of the Municipality that a proposed residential development within an area having a Neighbourhood development pattern is:

- (b) in keeping with the goals, objectives and policies of any secondary plan or guideline plan affecting the surrounding area;
- (c) compatible with the surrounding area in terms of scale, massing, height, siting, orientation, setbacks, parking and amenity areas;
- (d) provided with adequate off street parking;

- (e) *capable of being provided with full municipal physical services and emergency services; and*
- (f) *facilitating a gradual transition from Low Profile residential development to Medium and/or High profile development and vice versa, where appropriate.*

Apart from these policies, there is little direction provided to direct higher densities in the Residential designation. This is why the Urban Structure policies in Section 3 are of assistance.

The City updated the Residential policies in 2020 (OPA 130) to incorporate the permissions for secondary residential units as required by the Province in Bill 108. The policies now permit additional units in a single detached, semi-detached, or rowhouse dwelling (the primary dwelling unit) or a building accessory to the primary dwelling unit located on the same lot. These policies enable significant small-scale intensification in the City

The Mixed Use areas are multi-functional areas which integrate compatible commercial, institutional, open space and residential uses. Low profile residential uses are not permitted in these areas however there are no policies related to permitted density or height. Mixed Use Areas include Corridors and Centres. Often there are Commercial Centres in proximity to Mixed Use Areas (Devonshire Mall and Tecumseh Mall areas). A number of the Mixed Use areas are also in proximity to the Nodes and on City Corridors shown on Schedule J. The criteria for evaluation residential uses in the Mixed Use area are the same as identified in Section 6.3.2.5, quoted above.

Within the Regional Commercial Centre Nodes shown on Schedule J, there are Commercial Centre and Commercial Corridor designations shown on Schedule D to the Plan. While residential uses are described and density policies are included in Chapter 3 of the Plan, residential uses are not permitted in the Commercial Centre or Commercial Corridor policies in Chapter 6 of the Plan. Therefore, it would be necessary to amend the Official Plan, Chapter 6 and Schedule D, in order to implement the policies in Chapter 3 and Schedule J.

The City Centre Planning District is identified as a Growth Centre on Schedule J. Schedule E identifies the land use designations associated with the policies in Chapter 6. The designations that permit residential uses include the Residential Areas and the Mixed Use Areas.



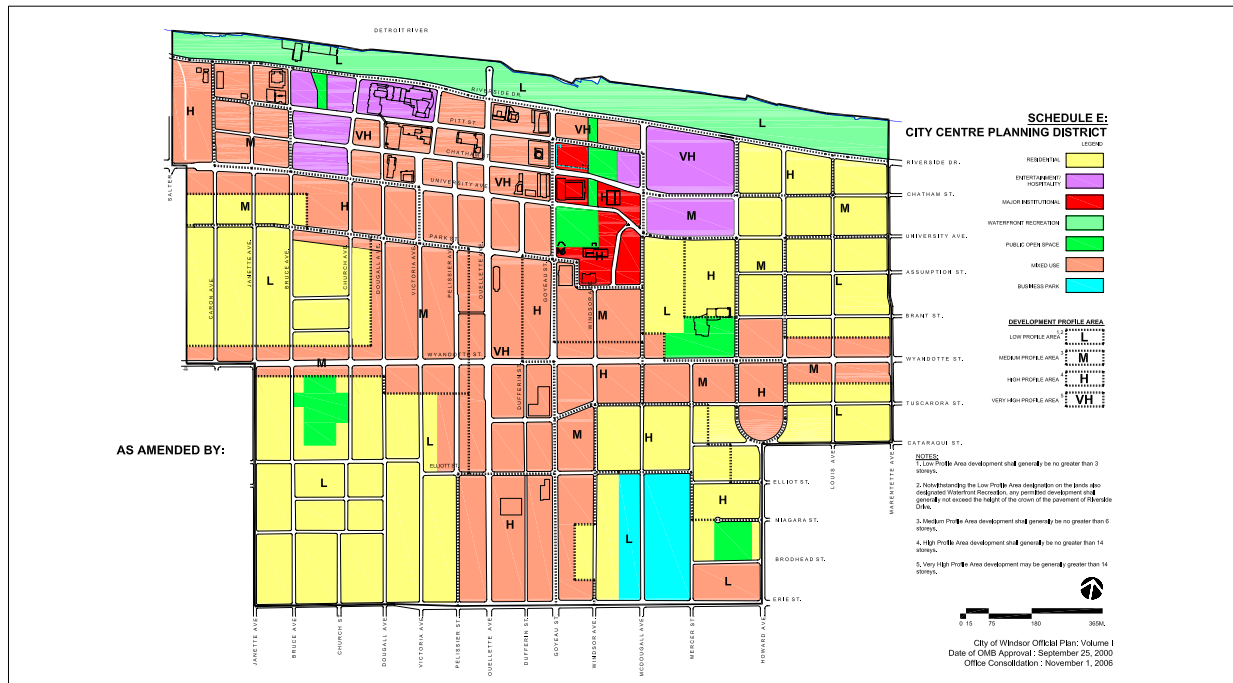


Figure 3 - Windsor Official Plan Schedule E

Schedule D and the policies in Chapter 6 of the Plan assign the following height/density provisions for this area:

- (a) Low Profile Area (L) where development is generally no greater than three (3) storeys in height and up to 8 units;
- (b) Medium Profile Area (M) where development is generally no greater than six (6) storeys in height;
- (c) High Profile Area (H) where development is generally no greater than fourteen (14) storeys in height; and
- (d) Very High Profile Area (VH) where development is generally greater than fourteen (14) storeys in height.

## 2.4 Zoning By-law

Most of the City is regulated under Zoning By-law 8600. It was initially approved in 1986 and has been amended on many occasions through the years. That part of the City that was Annexed from the Town of Tecumseth in 1985 is subject the Zoning By-law 85-18.

Zoning enables development to occur immediately, subject to compliance with other applicable law. For this reason, most land is zoned to permit the existing use as well

as a range of uses permitted by the Official Plan. The City passed Interim Control By-law 103-2020 in July, 2020 to prohibit the use on all lands, buildings, and structures for a Group Home, Shelter, Lodging House, and a Dwelling with five or more dwelling units that would otherwise be permitted by the Zoning By-law. Since that time the City has approved a number of exceptions to the ICBL to permit multiple residential uses. The location of these exceptions is shown on Figure 4.

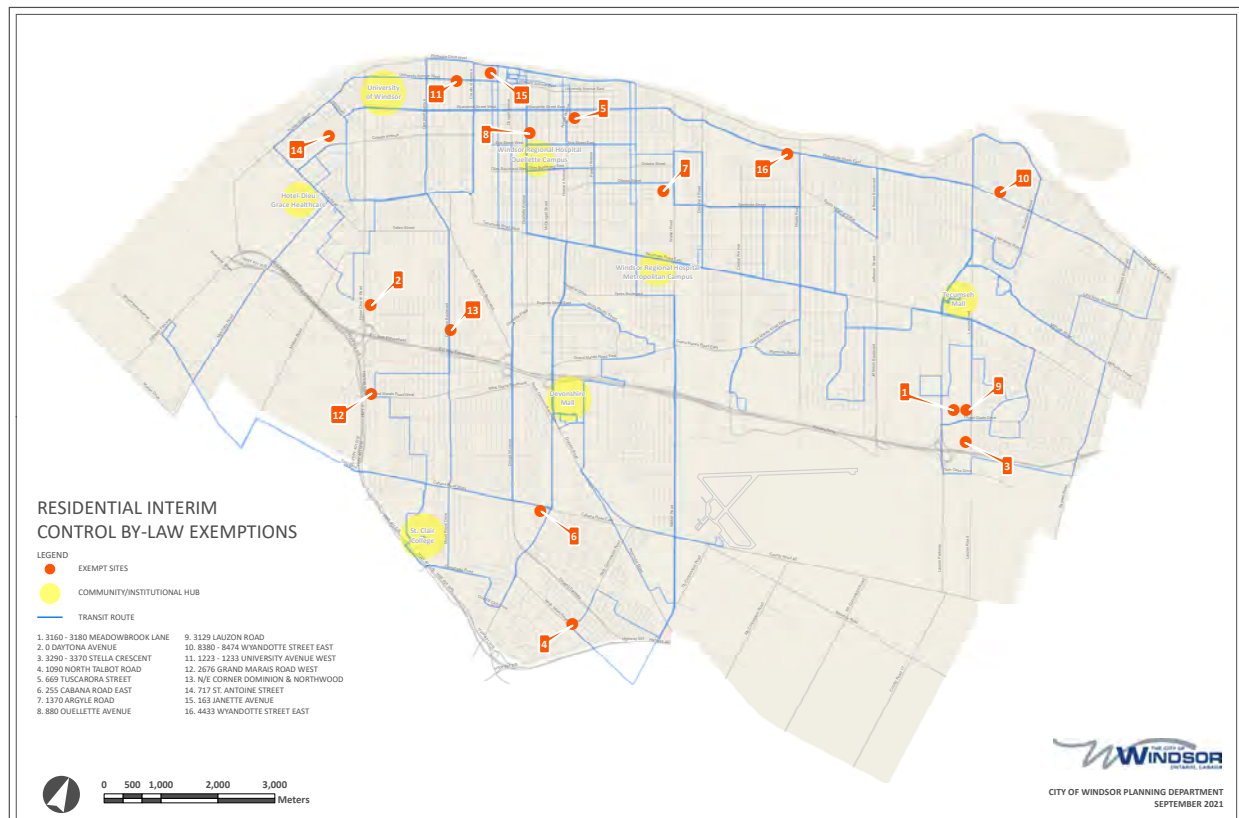


Figure 4 - Approved Exceptions to ICBL

Most intensification projects in the City occur through a zoning by-law amendment process. This allows for a full review and public consultation of the proposal before the zoning is in place. This process would occur for most of the developments of 5 or more units that are currently subject to the Interim Control By-law (ICBL). For this reason, this section of the report is focused on those other forms of housing that are currently restricted under the ICBL; group home, shelter and lodging house. We have also added the definition of fraternity house for reasons described below. These uses are defined in By-law 8600 as follows:

*FRATERNITY OR SORORITY HOUSE means a dwelling used exclusively for the accommodation of students of a college or university who are also members of a chartered fraternity or sorority.*

*GROUP HOME means a dwelling that is:*

- 1. For the accommodation of six to ten persons, exclusive of staff;*
- 2. For persons living under supervision in a single housekeeping unit and who require a group living arrangement for their well-being; and*
- 3. Licensed or funded by the federal, provincial or municipal government. A correctional institution, fraternity or sorority house, hospital, hotel, lodging house, private home day care, religious residence or residential care facility is not a group home.*

*LODGING HOUSE means a dwelling in which a minimum of four persons, not including staff, are provided with lodging for hire, with or without meals. A correctional institution, fraternity or sorority house, group home, hospital, hotel, private home day care, religious residence or residential care facility is not a lodging house.*

*SHELTER means a lodging house used exclusively for the provision of temporary accommodation to individuals who are in need of ancillary health care, counselling and social support services.*

The preferred term to use is Special Needs Housing. The Social Housing Reform Act (2002 s.2) defined Special Needs Housing as;

*A unit that is occupied by or is made available for occupancy by a household having one or more individuals who require accessibility modifications or provincially-funded support services in order to live independently in the community;*

The reason for including the definition of Fraternity House in this report it that appears to be contrary to Section 35 of the *Planning Act*, and more recent decisions on the Human Rights Code in that it regulated who can live in the dwelling based on their relationship (students and members of a chartered fraternity or sorority). The definition should be deleted and the use should be considered a Lodging House for the purposes of the Zoning By-law.

Lodging houses are permitted in many of the Residential District Zones. Figure 5 illustrates where these zones are located in the City. In addition there have been many amendments to the By-law to permit lodging houses in other residential Zones within the City.

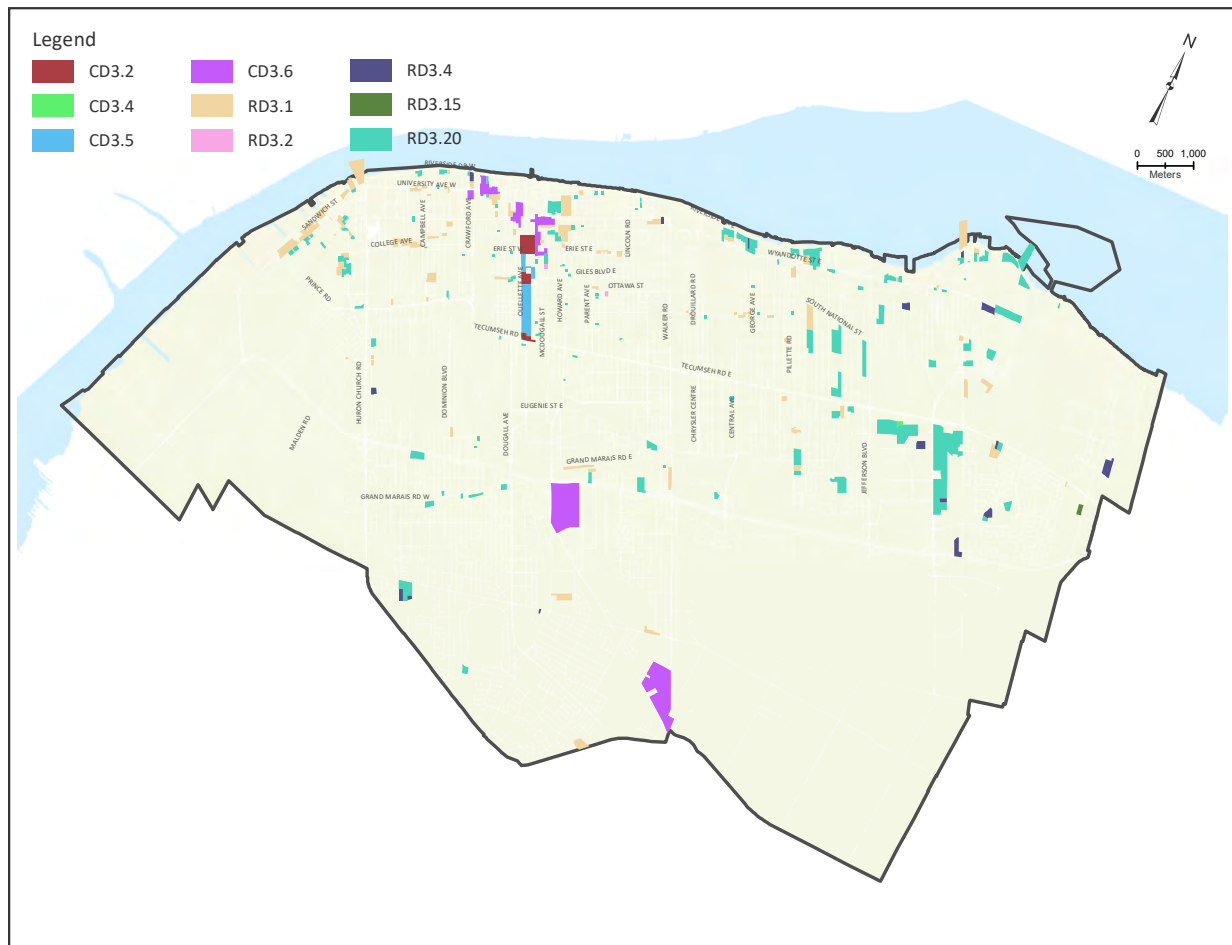


Figure 5 - As of Right Lodging House Zones

Group Homes have been recognized as residential uses and, under the provisions of the Human Rights Code, cannot be treated any differently than any other residential use in the Zoning By-law.

Shelters are only permitted as of right in the Institutional District 6 (ID1.6) Zone. However, the definition of 'shelter' includes a lodging house, which is permitted in many RD3 and CD3 Zones. A minor adjustment to the By-law could correct this potential problem.

Most of Windsor's low density residential areas are in the R1 and R2 zones. The Zone standards are very common for urban areas; 9 m frontage for singles, 12 m frontage

for duplex, lot coverage of 45% and maximum height of 10 m. While these zone standards are typical, they do not reflect the existing built form of many areas.

In those neighbourhoods with primarily 1 or 1 1/2 story houses, the By-law permits 3 stories with a flat roof and a building of 486 sq m (5,225 sq ft), excluding the basement. On a 12 m lot the By-law permits up 607 sq m (6530 sq ft).

After the Province enacted Bill 108 in 2019, the More Homes, More Choice Act, the City was required to amend its Zoning By-law to permit up to three dwelling units on a single lot. Where a duplex is a permitted use up to six units are now permitted. With the zone regulations described above, it is possible to build a duplex with six units, each unit being over 100 sq m – the typical size for a three bedroom unit. These types of buildings are current being constructed in the residential areas in proximity to the University. There is concern that this size of the buildings and the density are not in character with the neighbourhood. In order to address this issue it is necessary to amend the zoning By-law to limit the size of the buildings, perhaps through greater limits on height and gross floor area

It is also noted that the City's Zoning By-law includes a minimum dwelling unit size of 40 sq m. Staff note a number of variance requests to reduce this requirement. The Building Code specifies the minimum size for specific rooms in a dwelling. Many municipalities have eliminated a zoning requirement and relied upon the Building code to regulate dwelling unit size, thereby enabling smaller units without planning approval.

The Commercial District Three (CD3.1 and CD3.2) Zones permit mixed use developments as of right. The maximum height is limited to the longest length of an exterior lot line – usually frontage. This zoning promotes the development of combined use buildings in those areas. The large retail centres are presently zoned CD3.3 and do not permit residential uses. It is reasonable to expect that, in the event that the Official Plan was changed to permit residential uses in these areas, the approval process would require an amendment to the Zoning By-law.

Many of the Corridors in the City are zoned in the Commercial District Two (CD2) Zones. The zone permits dwelling units as part of a 'combined use building' (not defined). The Zones impose 14 m height limit on buildings which is just enough for ground floor retail and three residential floors. To build a 5 storey building an amendment to the By-law would be required as a 5 storey building with ground floor retail would be 20 m high.

The CD1 zone also permits residential dwellings but establishes a 7.5 m height limit, providing only for two storey buildings. To build a 3 storey building the height limit would need to be 12 m. The CD2 and CD3 zones permit residential dwellings in a combined use building but restrict the residential units to floors above the commercial uses. The City advises that a number of amendments to the By-law have been considered to permit residential units on main floors, behind commercial units.

Areas where combined use buildings are permitted as of right in the Zoning By-law are shown on Figure 6.

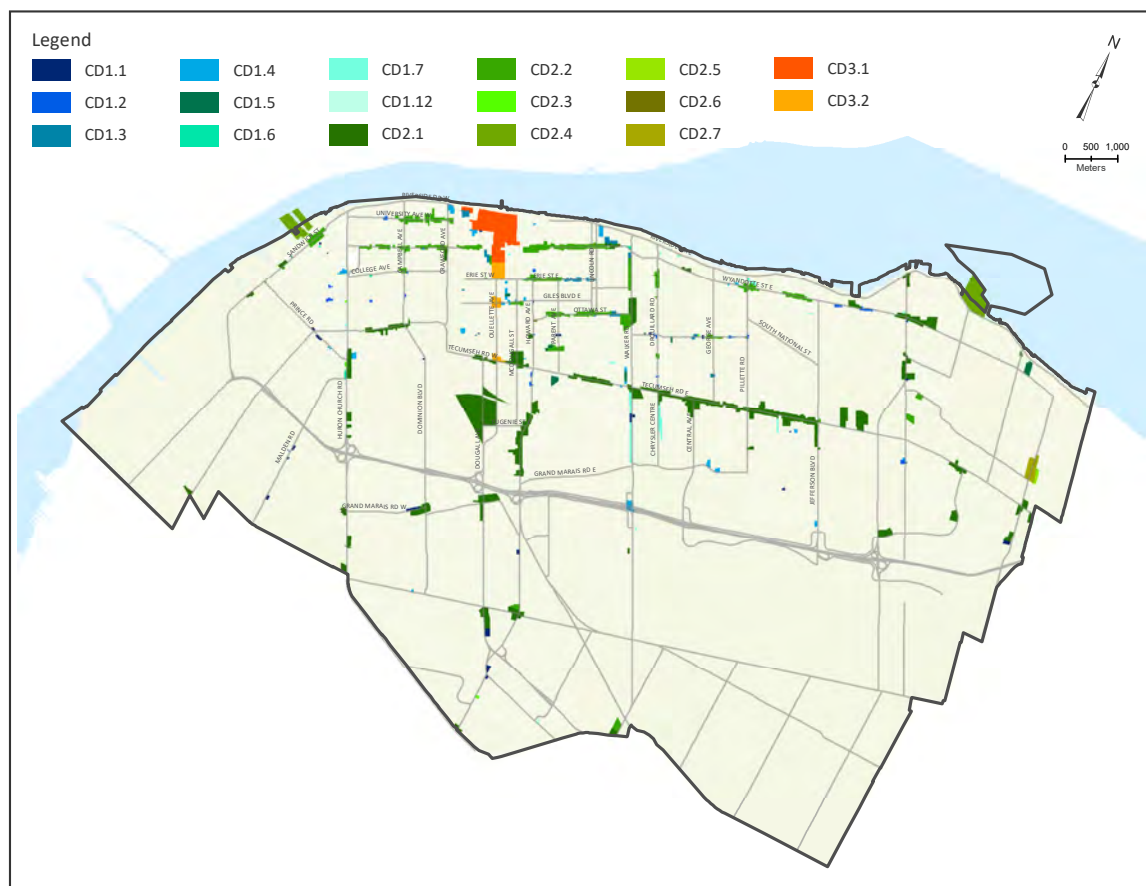


Figure 6 - Permitted Combined Use

The Zoning By-law plays a crucial role in implementing the framework for various forms of Special Needs Housing, as articulated in the Official Plan. It is crucial that the Zoning By-law permit Special Needs Housing in all zones that permit residential uses, subject to the built form regulations of the individual zone, again, keeping in mind that there should be no reference to elements of occupancy or tenure. This is absolutely crucial



because if there is to be consideration of a licensing program under the Municipal Act, the use must be identified as a permitted use in the Zoning By-law.

### **3.0 Licensing Special Needs Housing under the Municipal Act**

The Planning Act is a very poor regulatory instrument in dealing with the management of various forms of Special Needs Housing. On the other hand, the Municipal Act (2001) gives municipalities the specific authority to license, regulate and govern some forms of Special Needs Housing operating within the municipality. This includes the authority to pass licensing by-laws covering the business of renting residential units and operating rooming, lodging or boarding houses/group homes.

It is suggested that the City consider for those forms of Special Needs Housing that are not within a Commercial or Institutional Zone, the implementation of a licensing program that appropriately manages the number of occupants/staff, ensures appropriate inspections by fire and building officials and establishes a regime to ensure the health and safety of staff and residents of those facilities. A licensing regime may also regulate behavioural elements of some forms of Special Needs Housing, where there is the potential for non-compliance and/or nuisance. Most municipalities do not charge significant licensing fees in order to encourage participation in the licensing program.

Of course, the establishment of a licensing program requires a commitment to enforcement and, potentially the need to levy fines or other orders that affect granting of permits, inspection and the management of health and safety issues as well as behavioural issues.

### **4.0 Market Overview Summary**

Altus Consulting Group has prepared a Market Overview for housing in the City of Windsor (2022) as part of this Study. The following is a summary of their findings.

1. The City needs more housing in the inner areas of the City, with populations in a majority of the City declining due to a lack of new housing options and shrinking average household sizes;
2. The City is not seeing enough purpose-built rental housing constructed to meet demand;
3. Access to retail, transit and other community amenities can bolster the market for new residential development, making an area attractive to prospective new households;

4. Similarly, adding residential uses near existing retail clusters can improve the viability of those retail environments. The practice of redeveloping major retail centres for a mix of uses including residential, as well as other community amenities such as parks, community centres, and even additional retail is growing across Ontario and Canada;
5. The City needs to account for prospective growth in post-secondary enrolment in forecasting housing needs.

For greater detail, refer to the full Altus Report (Attachment 2).

## **5.0 Directing Intensification**

There is an increasing awareness that the character of existing and historic or mature communities in Windsor is vitally important. The image of a community is created by the buildings and landscape elements which frame and contain spaces that are viewed from streets and sidewalks. A comprehensive approach to planning for residential intensification in a definable urban structure means that there is a clear responsibility of the City to define where intensification initiatives are appropriate and desirable within the City, and equally important, where those intensification activities need to be more significantly managed.

In addition to reviewing the Land Use designations, associated policies and Zoning within the City, the following additional factors have been considered:

- Transit
- Goods and Services
- Municipal Services
- Development Constraints
- Community Improvement Areas
- Mature/Historic Neighbourhoods

### **5.1 Transit**

The transit system in Windsor is a bus system. Promoting intensification and affordable housing in proximity to transit is critical to reducing the reliance and costs of operating a motor vehicle. The CAA estimates the cost of new automobile ownership in Canada to be about \$1,100/month. Owning a used car is less expensive however eventually maintenance costs balance payment costs. Locating density in proximity to transit (within 500 m) reduces household costs and municipal costs and is significantly better for the environment.



## **5.2 Goods and Services**

The ability to walk to sources of essential goods and services reduces costs, pollution, energy consumption and promotes a healthier life-style. Having goods and services within 500 m to one's residence provides these benefits. In addition, the residential uses in proximity to the commercial uses supports the vitality of the commercial uses. For these reasons, mixed-use areas provide excellent opportunities for intensification and affordable housing.

## **5.3 Municipal Services and Infrastructure**

In order to support higher densities at moderate costs, it is important to locate that development in areas that are already serviced at a level that can support the additional uses. Intensification should be directed to areas in proximity to roads that can accommodate additional traffic, sanitary and water services that can accommodate additional flow, and stormwater management facilities that can accommodate additional flow.

Consideration must also be given to areas where community services and institutions are reasonably close. Proximity to parks and schools is important for providing for quality of life and recreation. Major institutions provide services and well as employment.

## **5.4 Development Constraints**

Natural development constraints are identified as Natural Heritage Areas in the Official Plan. All forms of development should avoid these areas.

Flooding is a major constraint in Windsor as the land is very flat and the natural soils have low permeability. Major flooding events have happened through the City in recent years. The high water table in the City also makes it difficult and expensive to create underground parking areas in many parts of the City.

Large areas within the City are also recognized as having High Archaeological Potential. While not an absolute constraint, the costs to assess, document and perhaps recover features or leaving them in place, can considerably extend the approval time and costs for new development.

## **5.5 Community Improvement Areas**

Community Improvement Areas in the City are places where the City has already undertaken studies and determined that development should be encouraged. The Municipal Act prohibits municipalities from providing funding in the form of grants, low cost loans or reduced municipal fees for properties without first completing a Community Improvement Plan. Community Improvement Areas also have targeted municipal expenditures for improvements to infrastructure, streetscape, parkland and other community facilities to encourage redevelopment and investment.

## **5.6 Mature Neighbourhoods**

There are areas in the City where intensification needs to be limited in order to protect the character and cultural significance of the neighbourhood. Provincial policies mandate that some intensification shall be permitted in all residential areas - primarily in the form of additional residential units. However, care must be taken in the neighbourhoods that have cultural and historic significance to ensure that what intensification is permitted is done so with the greatest consideration of the potential impact on the character of those neighbourhoods. The examples of as-of-right development permitted in the RD1 and RD2 zones in Section 2.4 illustrate the need to change some zoning provisions in order to protect low density residential neighbourhoods.

These factors have been combined on a single map of the city (Figure 7) in order to assist in the establishment of areas that should be considered as priority intensification areas.

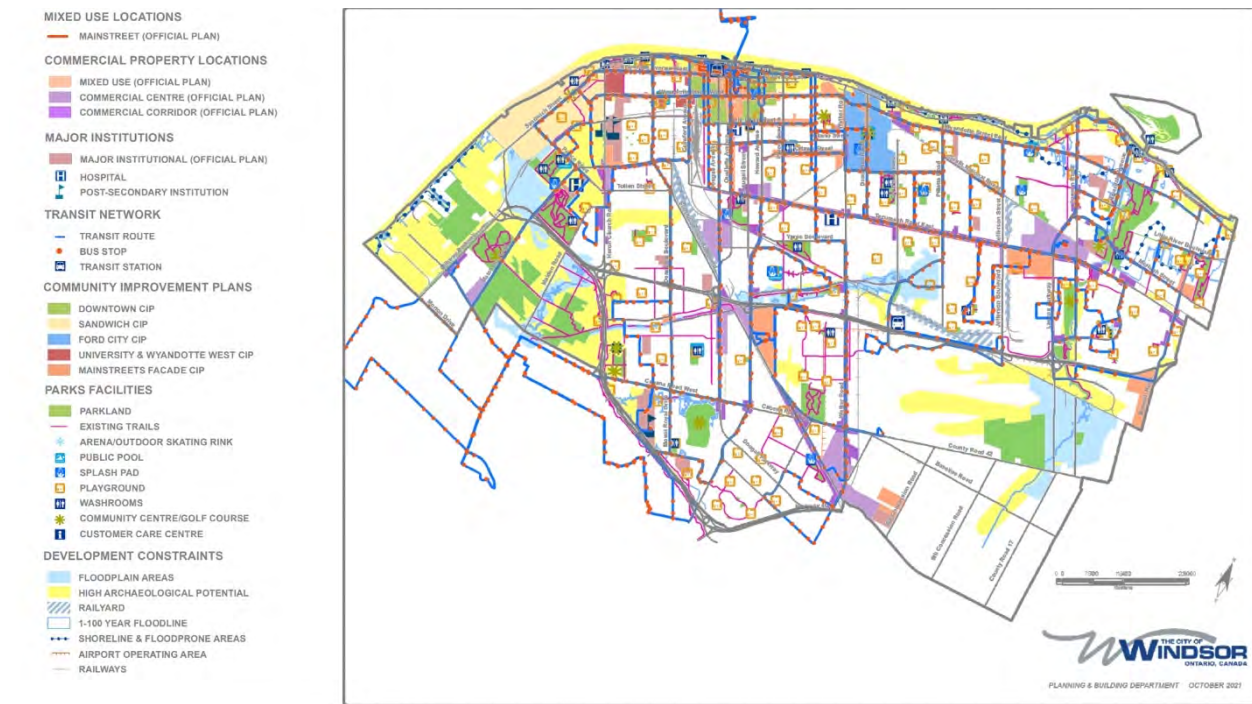


Figure 7 - Intensification Location Factors

Based on these location factors, the existing City Centre, Regional Commercial Nodes and Corridors appear to be best suited to accommodating the majority of intensification activity. These areas should be identified as Intensification Priority Areas in the Official Plan. The official Plan should clearly identify that the majority of intensification in the City should be directed to these areas. Intensification in Neighbourhood Nodes can also produce affordable units through intensification while re-enforce the local function of those areas.

## 6.0 Character Precincts

A review of the Official Plan Location Factors and the built form in the City indicates that there are five residential and residential/commercial mixed use areas that have distinct characteristics. These areas are shown on Figure 8 and described below.

Each of these Precincts will require different policies and Design Guidelines to guide intensification.

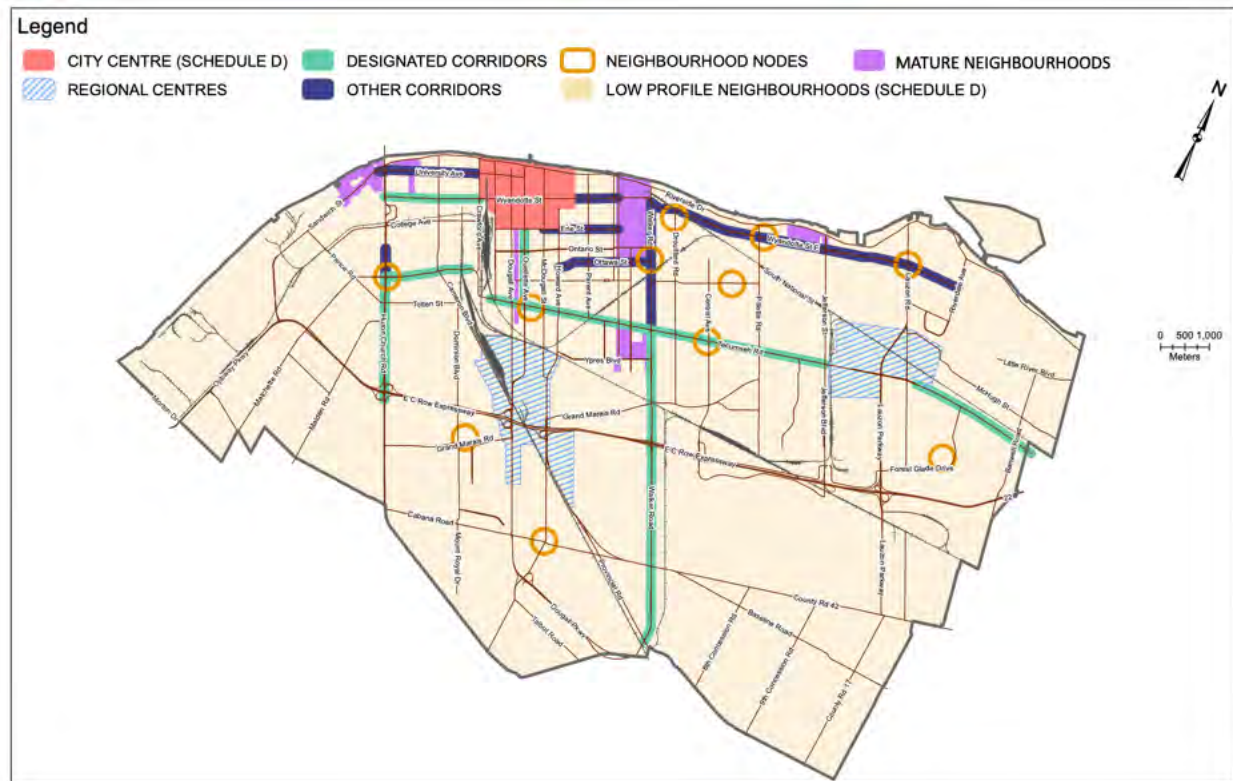


Figure 8 - Windsor Character Precincts

## 6.1 City Centre

This area is shown on Schedule E to the Official Plan and well described in that document. It consists of medium to high profile buildings, ground floor retail/office uses and generally reflects a post-war architectural style. The Official Plan directs higher density residential uses to this area as part of the mixed use character and includes appropriate Design Criteria.

## 6.2 Regional Nodes

These areas are shown graphically on Schedule J to the Official Plan. Schedule D to the Official Plan shows the areas as Mixed Use, Commercial Centres, Commercial Corridors and Business Parks. Chapter 3 of the Official Plan acknowledges that over time these areas will become mixed use communities, however this is not reflected in Chapter 6 of the Plan. The areas are large commercial centres with generally low profile buildings (less than 3 stories), vast parking lots and adjacent to major transportation routes.

### **6.3 Corridors**

The Corridors connect Regional Centres, the City Centre and Employment Areas with four-lane roads and major transit routes. They are referred to as Commercial Corridors in Chapter 6 of the Plan and on Schedule D. However, they also include low and medium profile residential uses and local commercial uses.

### **6.4 Neighbourhood Nodes**

Neighbourhood Nodes typically occur at intersections of arterial or collector roads. They are mixed use areas with local commercial uses, some specialty retail and dining and low to medium profile (under 5 stories) residential uses. The goods and services provided are local serving and oriented to pedestrian traffic. There is limited off street parking.

### **6.5 Low Profile Neighbourhoods**

Most of the residential areas in the City are designated as Residential areas on Schedule D to the Official Plan. However, these areas also contain some medium profile and high profile buildings as the Official Plan permits all densities within the Residential designation. Most of the medium and high profile buildings are in the vicinity of Corridors and Neighbourhood nodes within the Residential designation. Many of the low profile neighbourhoods that were reviewed reflect the architectural style of the 1960's and 1970's, primarily bungalow or 1 1/2 story buildings on large lots with mature landscapes

### **6.6 Mature Neighbourhoods**

There are a number of neighbourhoods in the City that warrant special consideration due to their historic and culturally significant character. These communities include Walkerville, Old Town and Sandwich. These neighbourhoods were designed and built in the 1920's and 1930's and have a consistent architectural style that reflects the prosperous City that Windsor became in that era.

## **7.0 Policy Direction**

The passing of the ICBL is an indication that the City believes it does not have suitable policies or regulations to address appropriate infilling and intensification within the City. Updated policies are required to provide greater direction to Council, Staff and the public when considering proposals for multiple residential developments. The



following provides general direction for how the current planning policies and regulations should be modified to provide this direction.

## 7.1 Change and Investment

Maintaining historic neighbourhoods requires investment, however that investment is of a smaller scale, primarily directed toward maintenance. The City has Community Improvement Plans that provide incentives for maintaining the architecture integrity of historic buildings while promoting investment in those areas.

Investment in change is more significant as it often requires purchase and removal of existing development and investment in infrastructure to support large scale buildings. The City directs the most significant change and investment to be directed to the City Centre and the least change to occur in Historic and Stable Neighbourhoods. The following graphic depicts the scale of investment in change anticipated in the City's Official Plan.

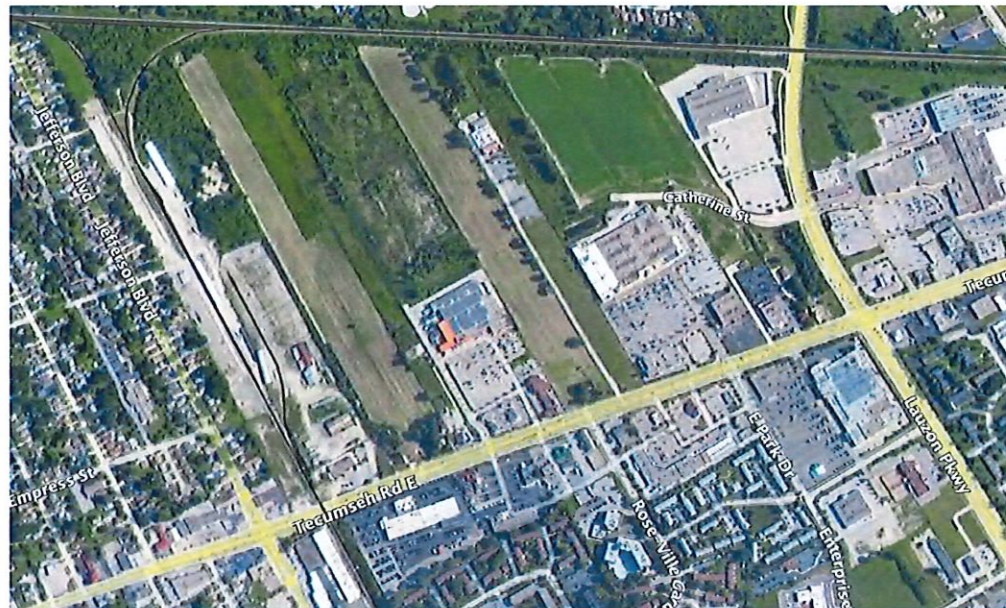
### Investment in Change

City Centre - Regional Centres - Corridors - Neighbourhood Nodes - Residential Neighbourhoods - Mature Neighbourhoods

The Official Plan Policies should clearly direct the greatest degree of change and investment in intensification to the City Centre, followed by Regional Centres then Corridors.

Figure 9 -  
Tecumseh  
Rd and  
Lauzon  
Parkway  
Regional  
Centre

The current  
Regional  
Commercial Centres have tremendous capacity to accommodate large scale housing projects. They typically have sufficient vacant lands to accommodate buildings and





parking. The Centres could also use additional residential support for the commercial uses. These areas should be identified as Intensification Priority Areas.

The corridors in the City, and to a lesser extent the Neighbourhood Nodes can accommodate mid-rise residential and commercial mixed use buildings through the redevelopment of presently underutilized commercial sites. Recent changes to the retail market will put added pressure on old shopping centres and plazas to redevelop in order to ensure that the site remains viable.



Figure 10 - Tecumseh Rd Corridor

The Corridors and Nodes should also be identified as Intensification Priority Areas in the Official Plan.

## 7.2 Protection and Promotion

In order to promote or facilitate investment, planning policies must reduce uncertainty. Similarly, in order to protect existing neighbourhoods and their individual character, policies must clearly protect those areas. These objectives should also be reflected in the City's Intensification Strategy.

In developing this Strategy, the policies need to reflect the degree of protection and the degree of promotion or facilitation. The following graphic depicts how these directions relate to the Character Precincts in the City.

City Centre - Regional Centres - Corridors - Neighbourhood Nodes - Residential Neighbourhoods - Mature Neighbourhoods



The current Official Plan policies in Chapter 3 of the Plan clearly express the intent to promote investment in the City Centre and promote change in the Regional Commercial Centres. Additional policies to increase the extent of promoting change and development in the Corridors would assist in directing intensification to those Areas.

Clear policies are required to protect the character of stable Residential Neighbourhoods and especially the identified Mature Neighbourhoods. In these areas, detailed Design Guidelines would greatly assist the City to protect the character while not entirely preventing any change,

### **7.3 Defining Compatible Development**

All communities evolve over time, and one of the most important challenges for decision makers is to establish an approach to development approval that ensures that change is understood on the basis of "Compatible Development". The concept and definition of compatible development is intended to ensure that all new development within the City is appropriately integrated into the existing built form and landscape and enhances the image, livability and character of the entire City. The starting point is to consider the tested definition of "Compatible Development", as follows:

*"Compatible development means development that may not necessarily be the same as, or even similar to the existing buildings in the vicinity, but, nonetheless, enhances an established community and coexists with existing development without causing any undue, adverse impact on surrounding properties."*

"Compatible Development" is an overarching principle of good planning, applicable throughout the City of Windsor, and its definition needs to be clearly understood, and applied in different ways, in different contexts throughout the City. This definition raises a variety of key phrases that require further definition:

Development in the vicinity - the concept of vicinity can be flexible. Within this neighbourhood context, the definition of vicinity should vary by the scale of development. There are generally two key scales of development/ redevelopment that must be considered, including:

- Major redevelopment, where land assembly and significant development intensification are proposed – likely in a townhouse or apartment form. The vicinity here should be extensive, perhaps community based; and,



- Minor redevelopment, where land assembly is not necessarily required, but existing dwellings are demolished and replaced by new intensified development. The vicinity here should include properties within 150 metres in all directions. This vicinity could be reduced further if the anticipated impacts are considered to be more immediate - within a few properties on either side and across the street of a proposal.

Enhance an established community - this is a general phrase that needs to be articulated. In order to pass this test, the nature and character of the defined vicinity needs to be considered and clearly articulated. Clear statements about those attributes that define the character of that vicinity are required to assist in the determination of what form of building can "enhance" that character, and what form of building may be "detrimental". Further, community investment is an important factor to consider where new and significant investment within a District may be both necessary and desirable.

"Coexistence without undue, adverse impact on surrounding properties" - this is quite an onerous test, usually related to easily identifiable/quantifiable impacts like shadow, privacy, traffic and parking problems. In some instances, the concept of "visual impact" may be established as an important development review criteria. Visual impact analysis will need to be tied to the attributes that define the area's character, either on a District-wide or defined vicinity basis.

#### **7.4 Special Needs Housing**

The Official Plan and Zoning By-law need to be amended to provide a definition of Special Needs Housing, and remove all other definitions that related to how a dwelling unit is to be used, particularly where a specific group of people are specifically recognized - like students, or seniors - or where the number residents and/or relationship among residents is identified. It is appropriate for the definition of Special Needs Housing to be inclusive of a host of types of Special Needs Housing so that direction can be appropriately provided in the Zoning By-law.

- Permit in all designations where residential uses are permitted, specific identification of Special Needs Housing, subject to meeting the built-form policies and regulations of the designation.
- Define the differences among forms of Special Needs Housing that are institutional in nature (long-term care homes, for example), and potentially commercial (short-term accommodations, for example) and identify where those uses are appropriately accommodated.

## 8.0 Summary of Recommendations

The next phase of this project will be a comprehensive implementation strategy that will consider a host of recommendations that are geared to:

- Prioritizing the City's centres corridors and nodes for intensification through supportive Official Plan policies.
- Reducing the risks inherent to the planning approvals process by pre-zoning identified centres and corridors for intensification and mixed-use development. Pre-zoning will identify the appropriate permitted land uses and will establish appropriate built form, massing and transition regulations.
- Reducing the cost of development by right-sizing parking standards and parkland dedication requirements and identifying areas where financial incentives may be considered.

The corollary is also true, the comprehensive implementation strategy will provide a host of recommendations that will provide planning tools aimed at regulating intensification initiatives within the City's established neighbourhoods

Subject to the endorsement of these recommendations, amendments to the Official Plan and Zoning By-law would be prepared for consideration by the City:

1. Stand-alone medium and high profile buildings should be permitted in the Regional Centres (designation name to exclude "Commercial"), with a policy framework that ensures appropriate transitions to adjacent communities.
2. Medium profile combined (mixed) use buildings should be permitted in the Regional Centres and the Corridors with a policy framework that ensures appropriate transitions to adjacent communities.
3. Combined use buildings up to 4 storeys high should be permitted in the Neighbourhood Nodes.
4. The Residential policies should include consideration of modestly scaled intensification projects within 50 m of a Neighbourhood Node.
5. Mature Neighbourhoods should be identified on Schedule G to the Official Plan. Intensification in these areas should be limited to development of a consistent

character to what presently exists in those areas in terms of front and side-yard setbacks, height and density.

6. The low density Residential Zones should include maximum gross floor area limits and reduce maximum height to 10 m.
7. The minimum dwelling unit size should be eliminated from the Zoning By-law
8. The Residential policies should define limits to intensification that will ensure that re-development for intensification is compatible with the existing built form. Additional policies regarding parking and landscaping requirements should be included in the Official Plan.
9. The City should amend the Official Plan and Zoning By-law to ensure compliance with the Human Rights Code, and to set a policy framework for the City to establish and enforce a licensing program for various forms of Special Needs Housing.
10. The Official Plan should enable Council to adopt Design Guidelines and implement those guidelines through architectural control in the development process. Design Guidelines will be implemented through a combination of Associated Official Plan design-focused policies, the Zoning By-law and Site Plan Approval, and potentially through urban/architectural design control processes.

Respectfully submitted

**Jim Dymont BES, MCIP, RPP**  
**Municipal Planning Consultants**

**Ron Palmer, BES, MCIP, RPP**  
**The Planning Partnership**





City of Windsor

# Intensification Guidelines

APRIL 2022 • DRAFT

 The Planning  
Partnership



Prepared by:



For the City of Windsor

April 2022  
Draft

# TABLE OF CONTENTS

<b>1</b>	<b>Introduction</b>	<b>1</b>
1.1	Design Guidelines Context	1
1.2	Purpose of the Design Guidelines	1
1.3	What are Design Guidelines	2
1.4	How Will They Be Used	2
1.5	Applicability	3
1.6	Submissions	3
1.7	Compatible Development	3
<b>2</b>	<b>Stable and Mature Neighbourhoods</b>	<b>6</b>
2.1	Understanding Community Character	7
2.2	General Guidelines for all Development	9
2.3	Modest Infill Development	13
2.4	Townhouse Development	15
2.5	Low Profile Apartments	17
2.6	Guidelines for Road Rights-of-way	18
<b>3</b>	<b>Centres, Nodes and Corridors</b>	<b>20</b>
3.1	Centres, Nodes and Corridors	21
3.2	General Guidelines for all Development	22
3.3	Site Planning	23
3.4	Built Form Guidelines	26

## Appendix A - Urban Design Brief Terms of Reference







# 1. INTRODUCTION

## 1.1 Design Guidelines Context

The City of Windsor is located in the southernmost portion of Ontario, situated on the south bank of the Detroit River and Lake St. Clair, one mile across from Detroit, Michigan. The City currently covers approximately 146.9 square kilometres and is the chief port of entry between Canada and the United States.

There is a desire across Ontario to see existing communities intensify over time to assist with delivering on a number of key planning principles, including:

- A more efficient use of land and investments in municipal infrastructure, typically based on an urban structure of higher density centres and corridors;
- The establishment of transit supportive forms of development that will support transit system investment and promote more mobility options; and,
- The delivery of a broader mix of housing types, including housing that is more affordable than the traditional housing stock.

The current planning paradigm in the City plans for the City's greatest height and density along its major corridors and in its nodes while promoting compatibility and stability in the surrounding low density neighbourhoods. Significant intensification is being directed to Intensification Priority Areas resulting in new sets of challenges and opportunities for the City including compatible development and heritage conservation.

The City's Intensification Priority Areas have substantial potential to accommodate intensification in a residential and mixed use form and will be directed to:

- Regional Centres;
- Corridors;
- Neighbourhood Nodes; and,
- Stable and Mature Neighbourhoods.

## 1.2 Purpose of the Design Guidelines

Guided by the community vision articulated in the Official Plan (OP), and building on the principles of 'compatible' development, the objective of the Urban Design Guidelines is to provide direction for the design of future uses that respect the unique character of Windsor's neighbourhoods.

The design guidelines are intended as a framework that outlines the salient characteristics of various design concepts and principles. The intent is to guide new development to become distinctive, while relating harmoniously to the use, scale, architecture, streetscapes, and neighbourhoods of Windsor, as well as meeting the needs of its citizens and visitors. The Urban Design Guidelines will provide predictability for applicants, the City, and stakeholders, by providing consistent direction about the criteria for the design of proposed development in Intensification Areas.

The provisions, and examples in the Urban Design Guidelines should be used as the foundation of design for intensification projects in the City and will be used in the assessment development proposals.

Meeting the requirements of the guidelines does not preclude the necessity to design specific site elements to function properly, be of high quality construction, and with appropriate attention to details that ensure that site improvements can be properly maintained.

**Note.** Illustrations and photographs shown throughout this guideline document demonstrate examples of how the guidelines can be applied and are not intended to exclude other designs that meet the intent of the Guidelines.



## 1.3 What are Design Guidelines

Good urban design contributes to the vitality and health of a community; aesthetics, architecture, and compatibility; and to vibrant and successful public spaces. The Urban Design Guidelines for intensification in Windsor are a set of recommendations intended to guide development to achieve a desired level of prescribed quality for intensification.

Urban Design Guidelines address the relative height, massing and articulation of elements (buildings and landscapes), and their relationship to one another and to their surroundings. These 'qualitative' aspects of physical form work in combination with zoning parameters to lend shape and 'character' to a neighbourhood.

Urban Design Guidelines are statements that include design guidance, criteria, standards and codes for how to shape the built environment, both the individual elements, as well as how these should be spatially arranged and relate to one another. Urban Design Guidelines address diverse scales of development, from site specific to city-wide. Design Guidelines typically address the design of buildings, landscape features and their organization within a defined area, as well as their relationship to their surroundings - built and natural.

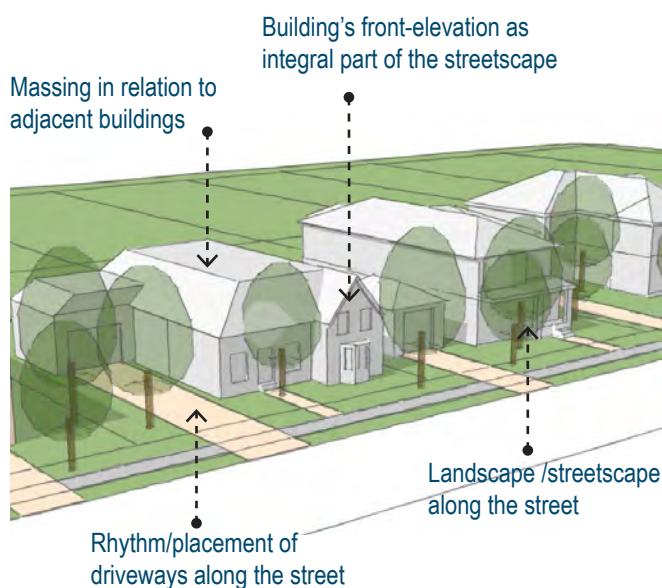


Diagram generally illustrating the contextual considerations for new buildings in a Neighbourhood.

## 1.4 How Will They be Used?

The Urban Design Guidelines shall apply to all intensification projects subject to review and Planning approval by the City through subdivisions, condominiums, and site plan control applications as permitted under the Planning Act and the Zoning By-law, and in some instances, Committee of Adjustment.

The Zoning by-law establishes clear regulations for lot coverage, parking, setbacks, and height - the 'quantitative' aspects of a neighbourhood's physical form. While zoning regulates how buildings sit within a lot/block, it represents only one of the planning tools that may be used to guide and shape development. These guidelines are not intended to duplicate the Zoning By-law, but instead, work in conjunction with the zoning standards to ensure enhancements of development through qualitative, context related design.

The Urban Design Guidelines will be used to evaluate development applications in order to ensure that a high level of urban design is achieved. The Urban Design Guidelines will be used by:

- City Council and Committees;
- City staff and external agencies;
- The development industry including but not limited to developers, consultants, and property owners; and
- The public for a greater awareness of the benefits of urban design in their neighbourhoods.

Guidelines, as opposed to Official Plan policies or Zoning By-law regulations, are a qualitative test that require interpretation. Development applications will be assessed on a basis of compatibility with adherence to the spirit, if not the letter, of the guidelines. The test is "consistency" rather than "conformity". Consistency in terms of agreement, or in keeping with, the intent of the guidelines and avoiding contradiction. As such, these Urban Design Guidelines are intended as a reference. They indicate the City's expectations with respect to the character, quality, and form of new development in Windsor's centres, corridors, nodes, and neighbourhoods.

## 1.5 Applicability

Compliance with the provisions of the Urban Design Guidelines does not preclude compliance with other development regulations associated with an application as required by the City or other applicable jurisdiction.

Where provisions of the Urban Design Guidelines conflict due to the characteristics of a proposal, the more restrictive shall apply and/or an alternative design solution(s) may be required that meets the intent of the Urban Design Guidelines.

## 1.6 Submissions

To assist decision makers, stakeholders, and community members in understanding proposals applicants shall submit an **Urban Design Brief** in support of a development application. The Urban Design Brief will provide the design rationale for the building, landscape, and site design elements of the proposed development.

The Urban Design Brief shall describe the project and demonstrate to the City how their proposal is consistent with the Urban Design Guidelines, including any additional written materials, graphic illustrations, and diagrams necessary to demonstrate compliance with the Urban Design Guidelines.

The Urban Design Brief shall outline how the design considerations of the guidelines have been met, how the development responds harmoniously to the specific context, and how it is complementary to the character of the surrounding neighbourhood in terms of building placement, building design, height, massing, materials, heritage considerations, etc.

Further information see **Appendix A** for the Urban Design Brief Terms of Reference.

## 1.7 Compatible Development

All communities evolve over time, and one of the most important challenges for decision makers is to establish an approach to development approval that ensures that change is understood on the basis of “Compatible Development”.

The intent for intensification in Windsor is to encourage compatible design that does not deviate substantially from an established pattern, without requiring an identical design, architectural style, or material palette for every dwelling or building in a neighbourhood. It is important that intensification integrates with the existing context and co-exists in harmony with no undue physical or functional adverse impact on existing or proposed development in the area.

The concept and definition of compatible development is intended to ensure that all new development within the City is appropriately integrated into the existing built form and landscape and enhances the image, livability, and character of the entire City.

The starting point is to consider the tested definition of “Compatible Development”, as follows:

***“Compatible development means development that may not necessarily be the same or similar to the existing buildings in the vicinity, but, nonetheless, enhances an established community and coexists with existing development without causing any undue adverse impact on surrounding properties.”***

“Compatible Development” is an overarching principle of good planning, applicable throughout the City of Windsor, and its definition needs to be clearly understood, and applied in different ways, in different contexts throughout the City. This definition raises a variety of key phrases that require further definition:

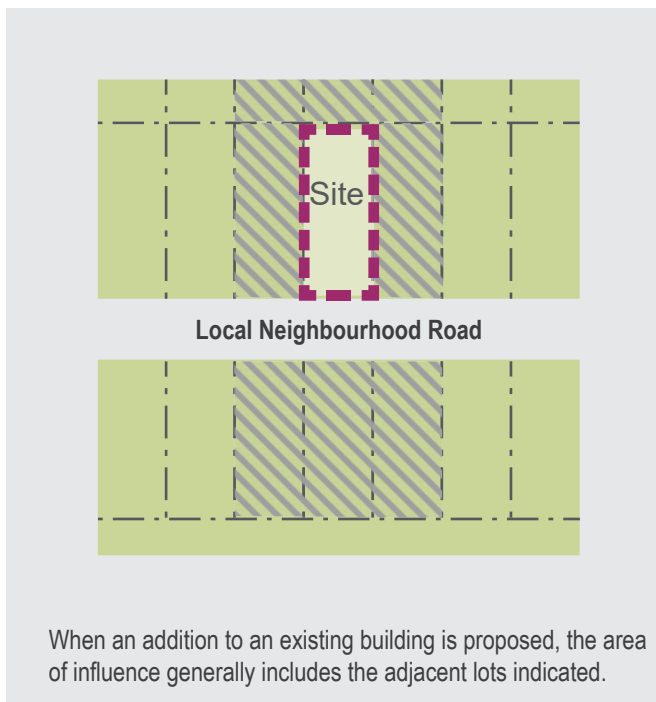
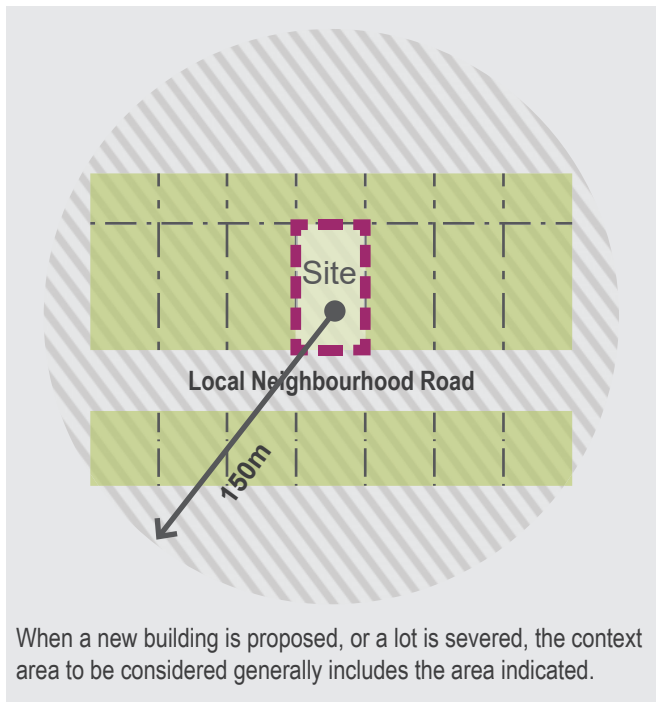
- **Development in the vicinity** - the concept of vicinity can be flexible. Within this neighbourhood context, the definition of vicinity should vary by the scale of development. There are generally two key scales of development/redevelopment that must be considered, including:
  - Major redevelopment, where land assembly and significant development intensification are proposed – likely in a townhouse or apartment form. The vicinity here should be extensive, perhaps community based; and,
  - Minor redevelopment, where land assembly is not necessarily required, but existing dwellings are demolished and replaced by new intensified development. The vicinity here should include properties within 150 metres in all directions. This vicinity could be reduced further if the anticipated impacts are considered to be more immediate - within a few properties on either side and across the street of a proposal.
- **Enhance an established community** - this is a general phrase that needs to be articulated generally. In order to pass this test, the nature and character of the defined vicinity needs to be considered and clearly articulated. Clear

statements about those attributes that define the character of that vicinity are required to assist in the determination of what form of building can “enhance” that character, and what form of building may be “detrimental”. Further, community investment is an important factor to consider where new and significant investment within a neighbourhood may be both necessary and desirable; and,

- **Coexistence without undue, adverse impact on surrounding properties** - this is quite an onerous test, usually related to easily identifiable/quantifiable impacts like shadow, privacy, traffic, and parking problems. In some instances, the concept of “visual impact” may be established as an important development review criteria. Visual impact analysis will need to be tied to the attributes that define the area’s character, either on a community-wide or defined vicinity basis.

In determining compatibility, an area of influence in the vicinity of the new development shall be used. New development should be compatible with the existing development within its area of influence. The scale of new development determines the appropriate scale of the area of influence.

Scale of new development	Area of influence for determining compatibility
<b>Major redevelopment</b> - land assembly; significant intensification	neighbourhood or community based
<b>Minor redevelopment</b> - conversion, demolition, or redevelopment of existing dwelling or property	streetscape/block or 150 metres in all directions
<b>Renovation/Addition</b> - renovation or addition to dwelling on an existing lot	existing dwelling and immediate neighbours (including across the street)



Diagrams generally illustrating the area of influence, in relation to the scale of building proposed.

The focus of compatible development within a defined **Centre, Node or Corridor** is less about protecting community character as new development is specifically promoted to change the character of these centres and corridors, and more focused on ameliorating undue, adverse impacts on adjacent properties.

As such, the following considerations should be taken into account to ensure compatibility where intensified development is proposed within an identified **Centre, Node or Corridor**:

- Consider the height and massing of nearby buildings, and where appropriate, incorporate buffers and/or transitions in height and density to adjacent properties;
- Provide, or permit the reduction of on-site amenity space that is reflective of the evolving urban and mixed use context;
- Implement appropriately urban streetscape patterns, including block lengths, setbacks, and building separations; and,
- Ensure capacity exists and that there are no adverse impacts on the City's sewer, water, storm water management, and transportation systems.

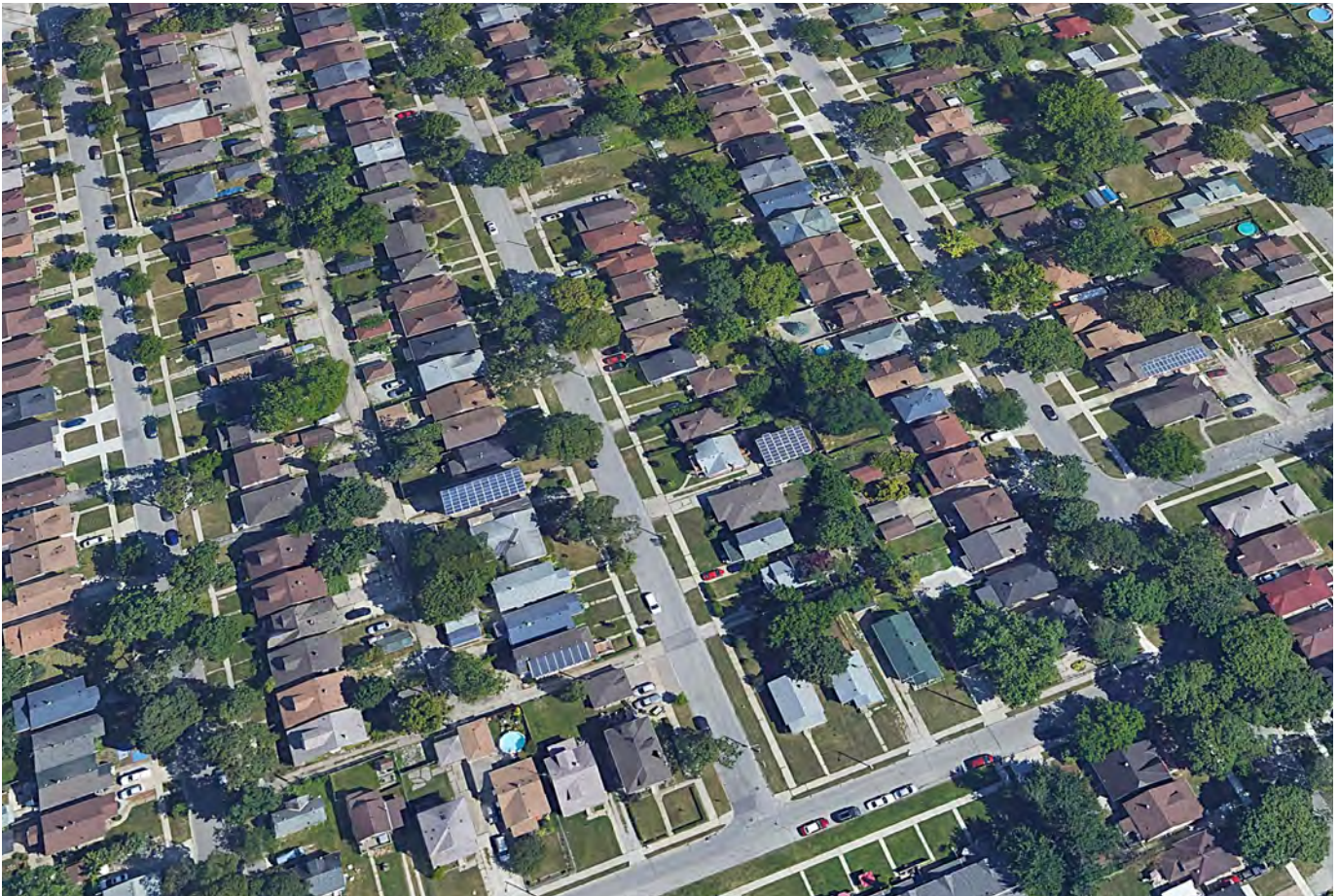
### Consistent Development

Throughout the Urban Design Guidelines, the terms consistent and consistency are used. Consistent refers to responding harmoniously to a specific context and being complementary to the existing area.

It is the intent of the Urban Design Guidelines to ensure that intensification in the Mature Neighbourhoods reflects a consistency of style. Within a locality of consistent character there are usually predominant building materials, textures, and ranges of colour, particularly in detail and decoration. Good infill buildings should recognise characteristic materials, textures, and colours used locally and in adjacent buildings. These should be re-interpreted and incorporated as part of the new building.



## 2. STABLE & MATURE NEIGHBOURHOODS



Throughout a number of Windsor's **Stable and Mature Neighbourhoods** there is a growing trend of dwellings being renovated, enlarged, or replaced by new dwellings, which are often significantly larger or conflict with the existing character of the community. Due to this trend, special consideration must be placed in a number of neighbourhoods in the City due to their historic and culturally significant character.

The City's objective for these evolving and historic neighbourhoods is to promote new construction that recognizes and enhances the neighbourhoods unique character as it is defined based on elements of urban design, streetscape, architecture, and landscape which contribute positively to their evolving image.

The purpose of the Urban Design Guidelines is to implement the Official Plan Vision for Stable and Mature Neighbourhoods by identifying the key attributes that contribute to the character of the area and providing a framework to guide the design of additions, new buildings, and landscapes that:

- Reconcile compatibility with diversity, while avoiding both monotony and harsh contrasts;
- Respect the architectural character of the neighbourhood;
- Promote a contextual design approach that considers the adjacent and surrounding development and fosters pedestrian scaled/oriented streetscapes;
- Encourage appropriate flexibility, innovation, and diversity in design, intrinsic to evolving communities; and,
- Recognize and implement the existing standards and guidelines for the conservation of Cultural Heritage Resources, where appropriate.



## 2.1 Understanding Neighbourhood Character

The **Stable and Mature Neighbourhoods** design guidelines are intended to address the changes occurring in these neighbourhoods so that compatibility and consistency can be achieved within the existing context and neighbourhood character.

### Stable Neighbourhoods

Achieving compatibility in Stable Neighbourhoods is not about replicating the existing form or reproducing architectural styles or details of nearby buildings. Rather, the focus is to direct how new development can be designed to maintain and preserve neighbourhood character.

New development in Stable Neighbourhoods should be designed to respond to the basic neighbourhood patterns and reoccurring characteristics, such as lot patterns; placement and orientation; scale, height, and massing of dwellings; existing vegetation; topography; and other common or distinctive elements.

### Mature Neighbourhoods

New development in Mature Neighbourhoods should also achieve compatibility similar to Stable Neighbourhoods, but the focus in these areas is to be consistent with the architectural style, building elements, and materials of existing dwellings in the surrounding neighbourhood.

The intent is to maintain and protect the existing historic and culturally significant character of these neighbourhoods. Colour schemes and materials should be inspired by, and carefully coordinated, with surrounding buildings for visual harmony and consistency with the architectural style of the buildings, as well as the neighbourhood.

Materials and colours of surrounding buildings need not be simply copied but used as a point of reference. Modern materials can be used if their proportions and details are harmonious within the surrounding historic context. Colour, texture and tonal contrast can be unifying elements.

### Neighbourhood Character

Character means the collective qualities and characteristics that distinguish a particular area or neighbourhood. In a general sense, the character of the City's Stable and Mature Neighbourhoods is defined by the comfortable scale of the buildings and the streets, the street trees and landscape features, and the feeling of history invoked by the inventory of period appropriate and historic homes.

In many of Windsor's neighbourhoods there is a diversity of building forms, housing types, streetscapes, and landscape features. Diversity is an element to be celebrated as a defining factor within each of these neighbourhoods.



Three storey apartment buildings on Argyle Street in the Walkerville neighbourhood.

The character of the City's Stable and Mature Neighbourhoods is defined generally by the following elements:

- **Architecture** - Architectural styles, in some cases vary dramatically, while in other neighbourhoods, convey consistency. While a rigorous adherence to a particular form or style is neither desirable nor realistic there are key elements of all building designs that can be used to ensure that different forms and styles can co-exist alongside one another in a compatible and complementary manner;
- **Heritage** - The inventory of heritage buildings within the Stable and Mature Neighbourhoods is a key contributor to the character of the neighbourhoods. To maintain the historic character of these areas, the design of both new development and additions must complement the heritage character and be context-specific to avoid detracting from the existing built fabric.
- **Lot Size/Frontage** - Streets that display the most diversity in terms of lot size and street frontage are not necessarily negative in terms of community character. Varying lot sizes and frontages can accommodate a diversity of housing types and built forms. To support this variety and diversity as a positive attribute, it is important to ensure that the development is appropriate for the site and within the context of the surrounding built form;
- **Setbacks** - Front and side yard setbacks are character giving elements within these neighbourhoods that establish both the building's relationship with the street, and the visual separation between buildings. Consistency in building setbacks, regardless of built form, is a key character giving element of any street;
- **Streets** - On a street by street basis, right-of-way and pavement widths are considered important to the image of a Stable and Mature Neighbourhood and are directly related to the adjacent scale of development, with a desire to maintain existing relationships among pavement width, boulevard treatment, and the interface between the street and the adjacent buildings;
- **Street Trees and Landscaping** - The protection of mature street trees and the enhancement and maintenance of front yard landscapes in all Stable and Mature Neighbourhoods is a crucial objective in maintaining its positive character; and,
- **Parking** - Dealing with the issue of parking is often a flashpoint in the conversation about residential intensification. Parking must be appropriately accommodated on the site of any specific residential development, and that parking supply may be augmented by on-street parking, or in parking spaces provided in communal facilities. A lack of parking supply, with too much reliance on on-street parking has a significant negative impact on community character and may impact the functional operation of the street network.



Gateway to historic Sandwich Town



## 2.2 General Guidelines for all Development

The intent for development within Windsor's Stable and Mature Neighbourhoods is to maintain the Low Profile built form character of the area and ensure a sensitive integration of new development, additions, or renovations to adjacent properties.

Low Profile development in the Stable and Mature Neighbourhoods includes single-detached, semi-detached, duplex, townhouses, and apartments that are generally no greater than three (3) storeys in height.

### 2.2.1 Site Orientation

The relationship between buildings through placement on the lot is important to ensure a consistent neighbourhood 'feel' and to define and frame the street while imparting the sense of openness and enclosure.

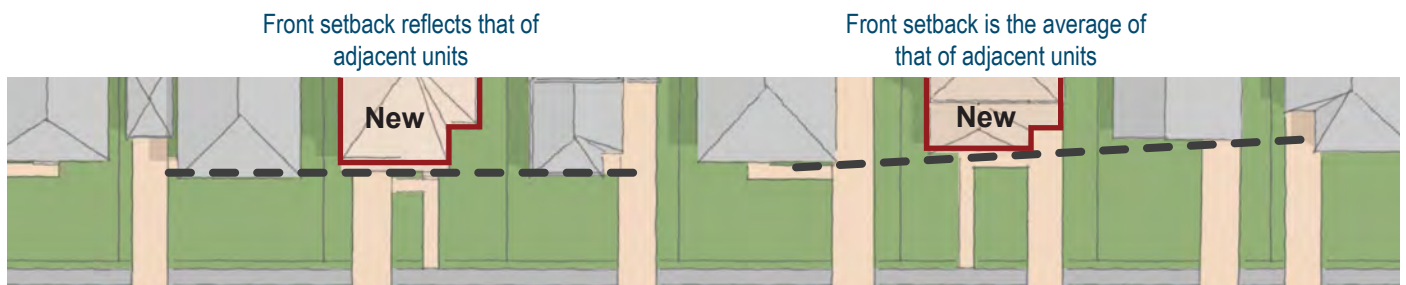
The Zoning By-law establishes clear regulations for front yard setbacks and interior/exterior side yard setbacks. The objectives of the Urban Design Guidelines in directing the relationship of the building to lot lines are to:

- Maintain consistent spacing between dwellings; and,
- Allow a measure of privacy between neighbours by providing space for light and landscaping.

1. Consider building placement and siting on a property in relation to the street and the property's neighbours to reinforce the positive characteristics of the existing streetscape.
2. Ensure the scale of Low Profile buildings is compatible and sensitively integrated with residential buildings in the immediate vicinity in terms of building mass, height, setbacks, orientation, privacy, landscaping, shadow casting, accessibility, and visual impact.
3. Locate dwellings close to the street edge to frame the streetscapes, however, this will depend on the setbacks to houses on either side of the site.
4. Maintain consistent front yard setbacks along the street. New development should have a set back equal to the predominant setback (70%+) on the street (+/- 1.0m), or a distance that is the average of those on either side of the development site (+/- 1.0m).
5. Provide side yard setbacks that reflect those of adjacent homes, or are the average distance of those on either side of the development, in accordance with existing zoning standards, to a minimum of 1.2 metres.



Generally consistent spacing between buildings



Front yard setback approaches



6. Consider rear yard privacy issues when extending a home towards the rear property line or building a new dwelling by:
  - a. Minimizing extensions beyond the adjacent dwellings rear wall;
  - b. Limit direct conflict with new windows on the side elevations with existing windows on the abutting building;
  - c. Minimizing the location of second floor balconies on rear and side elevations or providing privacy screening on the side of the balcony; and,
  - d. Providing fencing that effectively screens the rear amenity and minimizes its exposure to/ from adjacent properties, where appropriate.
7. Limit blocks of street townhouses to a maximum of 8 units, with 6 units preferred. The length of the townhouse blocks should not exceed 50 metres, unless it is essential to the architectural style of the townhouse block.
8. Orient blocks of attached townhouse units to the street with integrated front garages accessed from the street. For rear lane townhouses an attached or detached garage will be located at the rear of the block and accessed from a lane.

## 2.2.2 Developments within Heritage Contexts

1. Locate and design buildings to respect and complement the scale, character, form, and siting of on-site and surrounding cultural heritage resources.
2. Ensure that conceptual design and massing of development or redevelopment projects are compatible with adjacent listed heritage buildings and/or sites.
3. New buildings located adjacent to built cultural heritage resources will be compatible with existing historical building types, colours, and material palettes having regard for modern building designs, techniques, and materials.

## 2.2.3 Access & Parking

Garages and driveways should be located and sized based on the established pattern of the neighbourhood. The objectives of the Urban Design Guidelines in directing the location and width of garages and driveways are to:

- Prioritize the location of a garage off an open and travelled alley;
  - Ensure that garage doors do not dominate the front facade of the house;
  - Minimize the garage and driveway presence on the streetscape;
  - Direct parking to the side or rear of a building to ensure the front yard can be landscaped; and,
  - Maintain a consistent garage type and driveway width along the street.
1. Place garages behind the front wall of the dwelling or at the side or rear of the lot, unless the predominant location of the garage on other houses on the streetscape are at the front of the house or not at the side or rear.
  2. Townhouses should be serviced with access to the garage or parking from the rear of the unit. The front yard is best fully landscaped, with a single width driveway leading to the parking or garage area at the rear.
  3. Ensure rear lane accessed garages are complementary in design and building material with the principal dwelling.
  4. Where there is no option for rear access parking, the garage on the front face of the dwelling unit should not dominate the streetscape.
  5. Set back detached garages from the main front wall of the dwelling. Ensure detached garages are similar in material and architectural character to the dwelling.
  6. Ensure front-facing garages attached to the main dwelling do not occupy more than 50% of the building's width. For semi-detached, duplex, and townhouse units, pair garages to allow for more substantial front yard green space

## 2.2.4 Landscaping

The objectives of the Urban Design Guidelines with respect to landscape are to:

- 7. Locate and space driveways to reinforce the rhythm along a street and to allow for street trees to be planted in the boulevard.
  - 8. Ensure the garage door does not protrude beyond the front wall of the townhouse. Building design should include elements to reduce the dominance of the garage doors by, for example:
    - a. Single car garages only (2.7 m door width);
    - b. Including a habitable room over the garage;
    - c. Articulating the front door with a porch; and,
    - d. Integrating the design of the roof over the garage with that of the townhouse units.
  - 9. Parking for detached, semi-detached, and townhouse dwellings is only permitted in the front or exterior side yard and only on a driveway or a parking pad.
  - 10. For Low Profile apartments, locate visitor parking, loading, and service areas in areas of low public visibility in side or rear yards and set back from the front facade of the building.
- Maintain the green landscape character of the neighbourhood;
  - Plan for the urban canopy;
  - Screen views to rear yard parking; and,
  - Preserve mature trees.
1. Preserve existing mature trees where possible. The planting of new trees is encouraged to provide a continuous canopy over the street and to replace any canopy lost to new development.
  2. Enhance the bio-resiliency of the area through planting of native, non-invasive trees and shrubs.
  3. Include landscaped areas in front of buildings that provide a transition from private to public areas. A minimum of 50% of the front yard zone should include soft landscaping areas (non-paved areas supporting grass, groundcovers, trees and/ or shrubs).
  4. Encourage permeable paving for new walkways and driveways to reduce run-off to storm sewers and soften the streetscape appearance.



Garages set back from the main dwelling.



Landscaped areas provide a transition from private to public areas.

5. Where the predominant (70%+) existing streetscape character has design elements such as low stone walls, low permeable fences, planting and/or other landscaping at the front of the lot, ensure new development provides similar elements.
6. Maintain the green character of the front yards and avoid monotony of treatment over large extents of development. The front yards of units in a new townhouse development should have a coordinated landscape design that should include fences/hedges, and street trees in the boulevard.
7. Ensure front yard hedges or fencing that are used to define the edge of private property are no more than 1.2 metres high to maintain visibility to the street.
8. Screen the parking lots of apartments from abutting residents and street view through the use of landscape buffers and/or fencing that is consistent with the building's architectural style.
9. Consider outdoor amenity areas in the form of second floor decks or rooftop patios for townhouses with an attached garage in the rear as an alternative to traditional rear yard amenity areas.
10. Provide outdoor amenity space for apartment units either individually or in a shared space.
1. Ensure building materials reflect and complement the existing materials in the area and are high quality, durable, and easily maintained.
2. Ensure the materials selected are consistent for a building's facade and any walls that are publicly visible.
3. Recommended building materials include brick masonry, stone masonry, wood, or stucco; one or two of these materials should be selected as base materials and may be complemented by a wider range of accent materials.
4. For additions or renovations to an existing building, incorporate materials and colours that are consistent with and complement the main building.
5. Ensure material changes on exposed elevations occur at transition points, such as a change of plane.
6. Ensure rear and side walls exposed to public view are of a similar composition to the front wall.
7. Colour should be selected from the heritage palette. In most cases the predominant colours throughout the City's historic neighbourhoods are subdued. The preferred colours are those within a traditional palette.
8. Traditional high quality building materials are encouraged. The traditional building materials utilized within each historic neighbourhood should be identified and are to be encouraged for new development.
9. Ensure material changes on exposed elevations occur at transition points, such as a change of plane.

## 2.2.5 Materials

The variety of building materials contributes to the interest along the street and to the varied architectural character of the neighbourhood.

The objectives of the Urban Design Guidelines for renovations, additions, and new construction are to:

- Ensure high quality materials are used;
- Preserve the variety of design, colour and building materials within a range that enhances the character of the neighbourhood; and,
- Ensure that while buildings will inevitably change over time, they will maintain the cohesive visual character of the street.

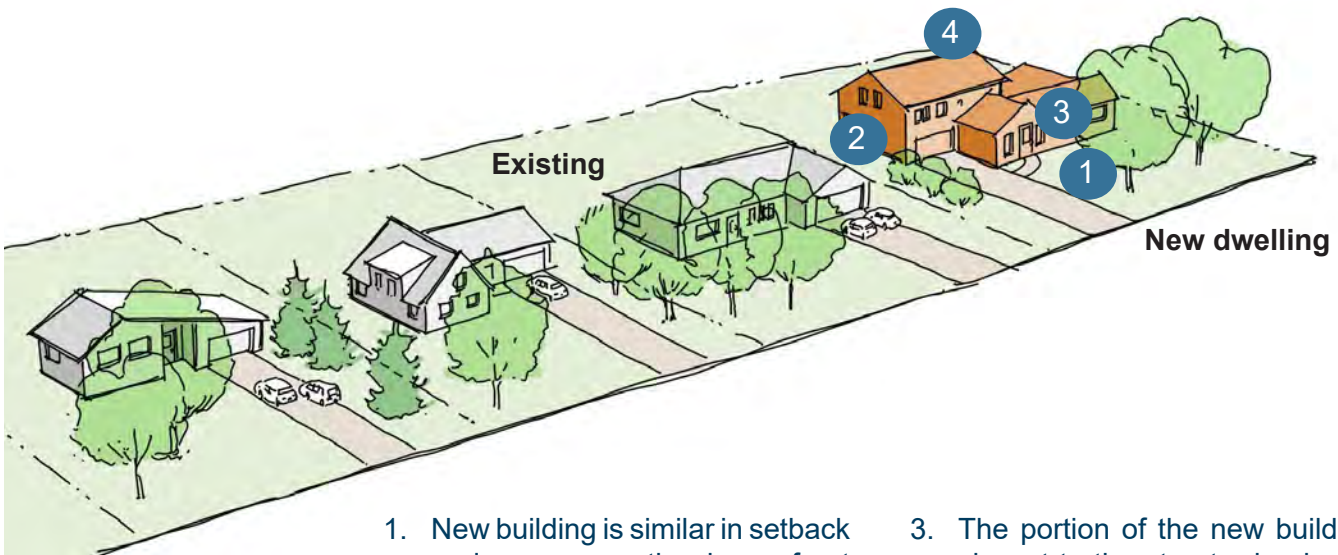


## 2.3 Modest Infill Development

The intent for development of **single lot infill** is to encourage compatible design that does not deviate substantially from an established pattern, without requiring an identical design, architectural style, or material palette for every dwelling or building in a neighbourhood. It is important that infill development integrates with the existing context and co-exists in harmony with no undue physical or functional adverse impact on existing or proposed development in the area.

### 2.3.1 General Guidelines

1. Infill development in the form of architecture for renovations and new construction shall:
  - a. Ensure development is sensitively integrated with the existing context and character of the neighbourhoods identity.
  - b. Preserve the variety of design, colour and construction materials within a range that enhances the character of the neighbourhood; and,
  - c. Maintain compatible architectural character in the design of roofs, windows, doors, porches and signs.
2. Ensure the architecture of a new dwelling is consistent with the architectural style and era in which its neighbourhood was built.
3. Design the architecture of an addition to be consistent with the original architecture of the existing dwelling.
4. On second-story additions and new two-story dwellings, maintain architectural continuity of materials and detailing around all sides of the dwelling, especially where the dwelling backs onto and is visible from adjacent streets or other public areas.
5. Ensure solar access by designing a new dwelling or addition to not adversely affect the availability of daylight falling on neighbouring properties. Design the location, scale, and massing of an addition or new dwelling to have regard for the amount of shadow upon neighbours' rear yard areas.



1. New building is similar in setback and preserves the large front yard and mature trees.
2. New building is similar in side yard setback.
3. The portion of the new building closest to the street edge has a similar low profile to the existing buildings; taller portions are set back.
4. Similarity in massing and roofline elements.

Demonstration of compatible infill.

## 2.3.2 Building Design

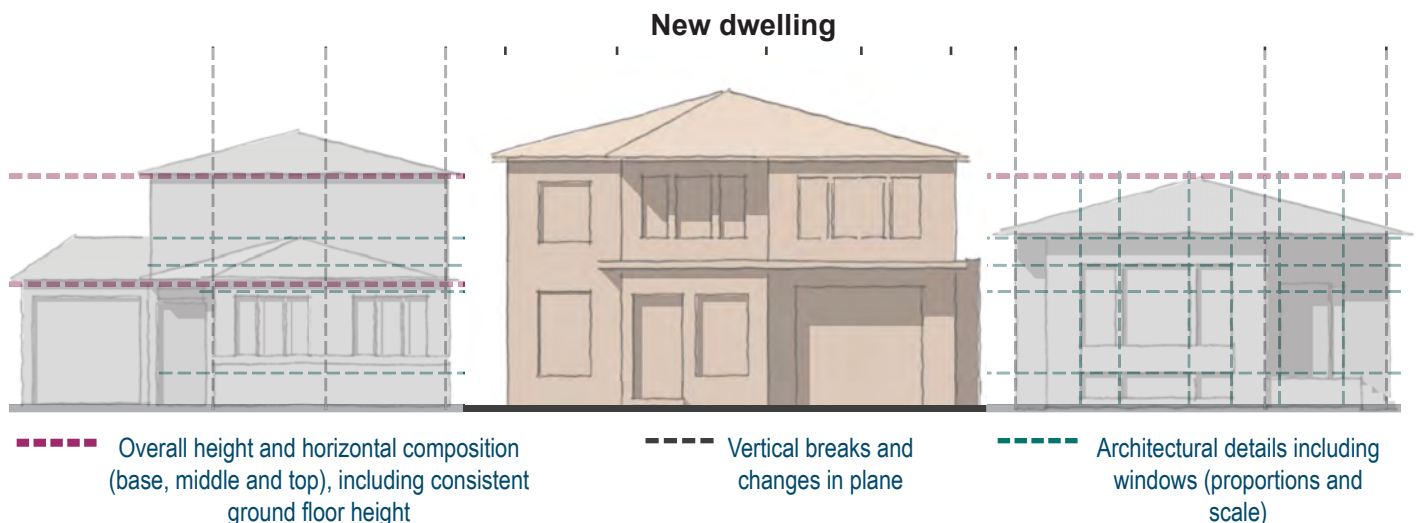
### 2.3.2.1 Massing & Elevation Articulation

The objectives of the Urban Design Guidelines in directing the relationship of the building elevation and entrance to the street are to:

- Encourage a variety of architectural forms and styles that reflect the evolution of the neighbourhood while enhancing its character.
  - Promote “eyes on the street” and a strong presence of the main elevation on the street;
  - Ensure that the prominence of the front entrance is maintained and consistent with the surrounding neighbourhood; and,
  - Ensure the entrance remains the main feature of the house and is oriented to and clearly visible from the street.
1. Design dwellings to have articulated elevations, especially those exposed to streets and open spaces. Articulated elevations might include changes in plane, projections, enhanced fenestration, highlighted entrances, complementary materials, among other architectural elements.
  2. Design the building envelope, and individual architectural elements within the building, to reference the architectural treatment of buildings in the neighbourhood. The goal is not

to replicate buildings of the neighbourhood, but to ensure new development relates to them by incorporating similarities in design language to promote compatibility. Massing and architectural elements to be considered include:

- a. Similar building shape (square, rectangular, L-shaped, etc.);
  - b. Roof lines with similar massing, pitches and articulation (gable, hipped, shed, flat, use of dormers, etc.);
  - c. Similar principal building massing elements (bays, projections, first floor height, building height, entry features, etc.);
  - d. Similar architectural features (porches, stoops, chimneys, columns, frieze boards, etc.);
  - e. Important datum lines (cornice, base courses, string courses, window alignment, bays, etc.); and,
  - f. Similar proportions (bays, windows, garage, etc.).
3. Ensure the new building is generally consistent in height and massing with adjacent buildings along the streetscape.
  4. Provide appropriate transitions in height to existing adjacent buildings and ensure no new building is more than 1.5 storeys or 4.5 metres higher than the adjacent dwellings.



The design of a new dwelling reflects the proportions of those adjacent to it.

## 2.4 Townhouse Development

5. Where possible, maintain the existing lot grading and the neighbourhood's characteristic first floor height.
6. Avoid mixing historic architectural elements with other architectural style elements.
7. Contemporary designs may be considered provided they exhibit consistency with the massing and articulation guidelines in this section and are not located within a heritage context or adjacent to a heritage dwelling.

### 2.3.2.2 Porches and Entry Features

1. Ensure the main entrance faces the street, with the door in a prominent position. The front door should be clearly visible and approachable from the street.
2. Front porches are encouraged as features that increase the prominence of the front entrance.
3. Encourage weather protection elements at the main entrance and design to complement the overall design of the dwelling.

Townhouses in Windsor's Stable and Mature Neighbourhoods are considered a popular choice for their ability to provide housing at greater densities than traditional single detached dwellings. In these neighbourhoods, the general appearance and placement of townhouses is characteristically different from the existing forms of development. Of special concern for townhouse development is the dominance of front facing garages.

The architectural character of new townhouse units has the potential to exert a greater impact on stable and mature neighbourhoods than that of single-detached or semi-detached dwelling units. Townhouse developments typically present a large unified extent of building face exposed to the street. Their massing characteristics could easily have an overwhelming effect that may be out of character with the neighbourhood.

The intent of these Guidelines is to translate the characteristics of more historic buildings found in the Stable and Matures Neighbourhoods to the townhouse form. The objectives of the Urban Design Guidelines with respect to townhouses are to:

- Ensure a form and character that is compatible with the dominant single detached housing in the neighbourhoods;
- Ensure that new developments do not impact adjacent residents due to, e.g., loss of privacy or sunlight;
- Ensure that the landscape treatment of the front yards contributes to sustaining the lush and green landscape character of the neighbourhood; and,
- Ensure that the street view is not dominated by garages.

Maximum 1.5 storeys difference  
between adjacent dwellings

Consistent height

Appropriate transition to  
lower dwellings



Illustration demonstrating the approach to height variation and transition between dwelling types.



## 2.4.1 Building Design

### 2.4.1.1 Massing & Elevation Articulation

1. Building mass should be compatible with buildings in the immediate vicinity of the development. Generally, the building foot print should not exceed 35% of the lot area. In addition, 40% of the lot area should be dedicated to landscaped open space exclusive of parking facilities and driveways.
2. Maintain the traditional range of building heights. Townhouses should not exceed three storeys. Consideration of height will depend on the height of housing in the immediate vicinity of the development.
3. Articulate the elevation of the townhouse block in a manner that provides variation between units with common characteristics that visually unites the block.
4. The main entrance should face the street, with the door in a prominent position. The front door should be clearly visible and approachable from the street.
5. For units flanking a window street, the main front door should be visible from, and oriented to, the exterior side elevation of the dwelling with access to the sidewalk. Ensure the entries are articulated through the use of entry features such as projecting porches facing the street.

6. The roofline should feature modulation of roof planes and use of dormer windows to avoid monotony.
7. Utilize variety in the design of roofs through the use of traditional gables and dormers, or more contemporary designs that include cantilevers and parapet details to break up the massing of units within a block. The main roof should appear as one roof where possible and reflect the architectural style of the unit block.

### 2.4.1.2 Porches and Entry Features

1. Front porches are encouraged as features that increase the prominence of the front entrance. The composition of wall elements should support the location and definition of the main entrance.
2. Housing in the Stable and Mature Neighbourhoods is characterized by front doors that have a direct relationship to the street grade. Avoid a finished floor elevation of the first floor and the front door at a second floor height up a full set of stairs.
3. The elevation of the front door should be no more than 1.5 m above grade.



Townhouse development in Walkerville neighbourhood.

## 2.5 Low Profile Apartments

### 2.4.1.3 Utility Meters and Mechanical Equipment

1. Where possible, locate utilities and meters in interior side or rear yards, away from public view.
2. Locate utility and service meters discreetly by:
  - a. Integrating into the design of the building;
  - b. Screening through landscaping;
  - c. Recessing or enclosing in the porch entry or landing;
  - d. Installing below porch slabs and porch steps;
  - e. Grouping in one location in a wall recess, enclosure or, where appropriate, a small roof overhang; and,
  - f. Screening meters on exposed elevations by integrating them into a wall or below porches and steps, providing complementary landscaping, or placing them behind a change in plane towards the rear of the elevation.

There is some demand for development of Low Profile apartment buildings in the stable and mature neighbourhoods. The City has a number of good examples of existing low profile walk up apartments that are compatible in scale and landscape character with adjacent lower density forms of housing. However, there is a very fine balance between allowing new low profile apartments within the context of a historic neighbourhood and maintaining the character of existing built and landscape form.

The intent of these Guidelines is to translate the characteristics of the historic Low Profile apartment buildings found in Windsor to a more modern apartment building form. Similar to the objectives for townhouses, the objectives of Urban Design Guidelines with respect to low profile apartment buildings are to:

- Ensure a mass, height and character that is compatible with the character of the neighbourhood;
- Ensure that new buildings do not impact adjacent residents such as through loss of privacy or sunlight;
- Ensure that the landscape treatment of the front yards contributes to sustaining the lush and green landscape character of the neighbourhood; and,
- Ensure that the street view is not dominated by parking.



Three storey apartment building in Riverside neighbourhood.



## 2.5.1 Building Design

### 2.5.1.1 Massing & Elevation Articulation

1. Compatible building height will vary depending on the specific conditions of the buildings in the immediate context of the site of the apartment building. Low Profile apartments shall have a maximum height of 3 storeys in the existing Stable and Mature Neighbourhoods.
2. Design the building and the site layout to consider overall form, massing and proportions, and rhythm of major repetitive building elements to create a streetscape that is pedestrian scale.
3. Orient buildings to face the street. The front face of the building should be articulated with windows and/or balconies.
4. Locate and orient primary building entrances to public roads, and design to be visible and accessible to the public. The main door of the building should be clearly visible from the street and be articulated with special architectural treatment.

### 2.5.1.2 Mechanical Equipment

1. All mechanical penthouses should be designed and clad with materials that complement the main building façades.
2. Locate mechanical rooms to the centre of the building rooftop and integrate into the rooftop design so they are not visible from the public realm.

## 2.6 Guidelines for Road Rights-of-way

The streets and streetscapes within Windsor's historic neighbourhoods display elements that provide an important overall character to the neighbourhood. The prominent tree canopies, often joining above the street, are a foreground to many of the houses providing a park-like character. Sidewalks are present on most streets at least on one side and provide a safe pedestrian environment.

These Guidelines direct the streetscape treatment in the boulevard of the right-of-way of the street. The guidelines include consideration of special paving patterns and materials, planting, lighting, and street furniture. The objectives of the Design Guidelines with respect to streetscape are to:

- Maintain the streetscape character in the historic neighbourhoods;
- Maintain the pedestrian character of the streets; and,
- Protect existing street trees and enhance canopies.

### 2.6.1 Roads and Sidewalks

1. Road improvements and maintenance of utilities should be completed in a manner that preserves and enhances the character of the City's mature neighbourhoods. Care must be taken to ensure that road improvements do not create a new suburban road type. Trees should be carefully pruned when required.



Example of a street in Windsor with sidewalks on both sides of the street and on-street parking.

## 2.6.2 Street Trees

2. The pavement width should be kept as narrow as possible to accommodate two travel lanes and on street parking on at least one side.
  3. Existing informal road edges such as grassed verges and road side drainage swales add character to the historic neighbourhoods and should be preserved, based on input from residents on the street and the City's engineers.
  4. Provide sidewalks on a least one side of the street with a grassed boulevard/verge. Sidewalks throughout the historic neighbourhoods should be poured concrete or concrete pavers.
1. Protect the existing street trees, replace dead trees, and plant trees to complete the existing gaps. Support the re-establishment of a complete street tree canopy.
  2. Ensure that there is sufficient space adjacent to the street and sufficient soil medium to sustain long-term growth and healthier tree life.
  3. Plant deciduous street trees in the centre of the grass boulevard at the edge of the pavement and spaced 8 to 10 metres on-centre to form a continuous canopy at maturity.
  4. Trees should be native, broad leaf species with a straight trunk. A variety of species should be selected for street trees to avoid a mono-culture. Refer to the City of Windsor tree guide.

## 2.6.3. Utilities

1. Locate poles, lights, signs, transformers, and mail boxes along the street tree line to minimize clutter and disruption of the street's character and pedestrian circulation.



Large canopy trees provide shade over the street.



### 3. CENTRES, NODES AND CORRIDORS



The City's urban structure of centres, nodes and corridors continues to evolve with higher density development, including opportunities for higher density forms of residential development. It will be important to ensure that undue, adverse impacts are not created on surrounding low profile neighbourhoods. Ensuring compatibility between new and existing uses will be a foundational criteria in determining the appropriate built form within centre, nodes and along corridors moving forward.

The purpose of the Urban Design Guidelines for Centres, Nodes and Corridors is to:

- Manage the transition between new, higher intensity development and existing lower density residential neighbourhoods;
- Manage the scale and massing of new development when considering a more intensified form of development;
- Mitigate any adverse effects on adjacent built form and the comfort and use of the open spaces and streets; and,
- Respect the prominent heritage fabric of adjacent residential neighbourhoods.

## 3.1 Centres, Nodes and Corridors

The intent for development within Windsor's Centres, Nodes and Corridors is to ensure a sensitive transition to adjacent properties and appropriate height, scale and massing of new development.

**Regional Centres** are defined as large scale sites that are integrated with, or connected to sites that accommodate larger scale retail centres. Development in Regional Centres is anticipated to accommodate Medium and High Profile built forms, on large vacant sites and/or within existing underutilized parking lots.

**Neighbourhood Nodes** are located at Collector Road intersections and serve the local neighbourhood with retail and mixed use buildings.

**Corridors** are located along Arterial or Collector Roads and are expected to accommodate Low and Medium Profile built forms that include mixed use, retail, office, and residential development.

### Transition

Transition can be achieved through the regulations of the Zoning By-law through setbacks and height control. Through transition, the guidelines will consider:

- Buffering that typically includes fencing and/or landscape plantings that abut property lines where the transition is most sensitive.
- Mitigating issues of overlook/privacy, shadow impacts, and concerns about the visual impact of new buildings that are not the same character as the adjacent neighbourhood.
- Applying setbacks, angular plane, or linking the height of buildings with the width of the road right-of-way.

### Height

Minimum and maximum building heights vary across the Centres, Nodes and Corridors and are outlined under Section 3.4 of the guidelines and defined in the Zoning By-law. In addressing height, these guidelines seek to:

- Protect and maintain established stable and mature residential areas.
- Ensure buildings form an appropriately scaled and designed street wall that reinforces the desired character at the street level.
- Ensure appropriate height taking into consideration existing and permitted heights; proportional relationships to streets; and, visual and physical impacts on pedestrians and adjacent areas.

### Scale and Massing

As in height, the scale and massing of buildings (the size of buildings) must be designed to:

- Provide a respectful adjacency to other buildings and open spaces.
- Consider how the building fits within its context.
- Create a comfortable "human scale" experience along the streetscape and allow for physical and visual permeability.

## 3.2 General Guidelines for all Development

All development shall ensure excellence in design, be designed to achieve a high degree of environmental sustainability, and demonstrate high quality architectural detailing, in accordance with the following guidelines.

### 3.2.1 Regional Centres

Regional Centres are large scale sites that are intended to intensify with Medium and High Profile development. They are a focus for intensification and should be planned to evolve into highly active urban places with mixed use buildings, residential, commercial services, and office uses that are well served by transit.

1. Design Regional Centres to serve the area and provide for a pedestrian oriented mix of land uses and functions including residential, commercial, office, small-scale employment, recreation, entertainment, and culture.
2. Ensure buildings address the street and provide a development pattern that supports a range of uses.
3. Line the perimeter of the development site with a distinct edge of buildings and open spaces.
4. Ensure development is complementary to adjacent development in terms of overall massing, orientation, setback and exterior design, particularly character, scale and appearance.
5. Design parking lots to be internal to the site, located to the side or rear of buildings.

### 3.2.2 Corridors

Corridors can accommodate a full range of residential, office, recreational, entertainment cultural, and community uses and facilities over time. Corridors are the connective spines of the City and intensification is envisioned to develop as mixed use and transit supportive.

1. Locate Low and Medium Profile forms of development and mixed uses along the Corridors and at gateways to create areas of community focus.
2. Ensure buildings relate to adjacent streets, particularly at transit stops. Block patterns should be permeable, providing access and frontage among buildings along the Corridors.
3. Design parking lots with planting strips and landscaped traffic islands, medians, or bump-outs to break up the expanse of hard surface areas.
4. Design buildings to be compatible with, and sensitively integrated with the surrounding land uses and built forms. Ensure appropriate transition to adjacent uses and built forms.

### 3.2.3 Neighbourhood Nodes

Neighbourhood Nodes are located at the intersections of Collector Roads and serve the convenience needs of surrounding local neighbourhoods within walking distance.

1. Design the nodes as mixed use areas with local commercial uses, specialty retail and dining and Low to Medium Profile residential uses.
2. Ensure new buildings are compatible in scale and function to the neighbourhood setting.
3. Frame the street edge with a consistent building setback.
4. Ensure primary entrances to buildings are clearly visible and located on a public road or onto a public open space for reasons of public safety and convenience.
5. Provide on-street parking by using lay-by parking with resident parking provided at the rear of the building and accessed from a lane.



### 3.3 Site Planning

Site planning plays an important role in how a development is experienced and how it functions, including elements such as building orientation, site access, and landscaping. The following will guide new development to continue Windsor's development pattern of walkable and interconnected neighbourhoods.

#### 3.3.1 Placement and Orientation

1. Arrange all development to address the street by lining streets with building front facades, active uses, and public spaces. Reinforce and maintain existing setbacks by aligning the building base of new development with adjacent building bases, or by placing the building at the average distance between those of adjacent properties.
2. Use prominent built form to address gateways and other key locations. On larger sites, create 'paired' corner buildings on either side of a street to emphasize a sense of entry or to distinguish one street district from another
3. Provide a safe, clear, and accessible site circulation system for pedestrians, cyclists, and vehicles, including connections to the surrounding street network, public sidewalks, transit stops, and parking areas.
4. Create a pedestrian-scaled environment by arranging buildings to create comfortable and protected pedestrian spaces that provide a sense of enclosure.
5. Provide mid-block pedestrian connections for development blocks over 200 metres in length to support pedestrian movement.
6. Enhance wayfinding by using buildings as gateways and landmarks, public spaces as focal points, and streetscapes to frame significant views.
7. On sites with multiple High Profile towers, provide mid-block pedestrian connections through the podium to enhance permeability, break-up the podium, and create additional corner conditions.
8. Ensure all pedestrian connections and entrances are visible and universally accessible. Distinguish walkways from driveways through a change in material or by using a planted or sodded edge.
9. Where multiple towers exist on a site, arrange the buildings to provide a gradual and appropriate transition in height to adjacent established or planned uses.



### 3.3.2 Access, Parking, and Servicing

1. Provide access to parking, servicing, and loading from the rear of the building, or from a laneway where possible. On corner sites, provide access from secondary streets provided the entrance facilities are well integrated into the rest of the frontage.
2. Provide a variety of parking options, including on-street parking, structured parking, and screened at-rear parking courtyards. Avoid the use of large surface parking areas, where possible.
3. Design surface parking into small courtyards by using walkways, public art, or landscaped strips.
4. Screen surface parking lots from view from roads, open spaces, and adjacent residential areas with low fencing, architectural features, landscaping or other mitigating design measures, such as lowered parking surfaces with landscaped buffers.
5. Incorporate pedestrian walkways and landscaping into surface parking areas along primary vehicular routes to enable safe, barrier free, and direct movement to principal building entrances and the sidewalk.
6. Consider above grade parking structures where feasible in efforts to conserve land and reduce heat island effect. Incorporate active uses at-grade for above grade parking structures facing onto any Arterial or Collector Road, where possible.
7. Avoid vehicular site access from higher order roads. Provide access from local roads or rear lanes where possible.
8. Consolidate vehicular entrances to serve multiple buildings in order to minimize the number of interruptions to the street wall and sidewalk network. Limit the number of accesses from the same street to two.
9. Locate and screen parking, loading, utilities, and servicing areas away from public view through a combination of soft and hard landscaping, as well as other integrated architectural elements such as walls and pergolas.
10. Integrate facilities for handling, storing, and separating waste and recycling into the building design and screen from public view through landscaping and architectural elements.
11. Provide accessible and secure bicycle racks and parking at retail, commercial, and employment developments, as well as at other key locations to promote active transportation.



Parking lot with smaller courts, plantings, and decorative paving.



Landscaped islands and pedestrian walkways with distinct paving and plantings provide safe crossing through the parking lot.



### 3.3.3 Landscaping

Landscaping design should reinforce the structure of the site with a focus on creating a safe, comfortable, and animated pedestrian environment.

Landscaped Buffers are linear green open spaces that serve to provide an appealing and 'soft' transitional interface between new development areas in Centres, Nodes, along Corridors and the backyards of exiting low density established areas. Buffers serve to minimize any noise, light, and visual impacts associated with denser and more urban developments.

1. Develop a comprehensive strategy for planting, built features, fencing, walls, paving, lighting, signage, and site furnishings.
2. Base planting strategies on year-round interest, hardiness, drought, salt and disease tolerance, and bio-diversity.
3. Preserve, protect, and incorporate existing healthy and mature trees into the site's landscape design.
4. Minimize the use of hard, paved areas to reduce surface run-off and heat island effect. Consider permeable paving wherever possible.
5. Utilize high-quality, durable materials for all landscape features such as paving, fences, walls, planters, site furniture, and shade structures.
6. Design landscaped buffers to incorporate lush landscaping including the use of trees and plantings, such as evergreens, that retain their foliage in all seasons to provide a visual barrier as well as some sound attenuation.
7. Design landscaped buffers to be environmentally sustainable with respect to stormwater management, plant species, bio-diversity, and extent of maintenance requirements.
8. Consider green roofs for Medium and High Profile buildings. This will assist with reducing heat island effects and improving air quality and noise insulation.
9. Incorporate a combination of soft landscaping, planters, and trees along non-residential frontages to delineate and differentiate private open spaces, entrances, and individual units at grade.
10. Appropriate planting conditions such as soil depth, volume, and growing mediums must be provided for successful landscapes.
11. Design lighting to avoid light spill onto abutting properties and adjacent residential neighbourhoods.



Street tree planting to buffer the sidewalk from the street.



Landscaped buffer with a variety of trees and shrubs.



## 3.4 Built Form Guidelines

### Low Profile Buildings

Low Profile buildings are generally no greater than three (3) storeys. Low Profile buildings can be townhouses, apartment buildings, or mixed use buildings with retail and commercial at grade and residential above.



Example of a three storey mixed use building and townhouses.

### Medium Profile Buildings

A Medium Profile building is any building generally no greater than six (6) storeys in height. A Medium Profile building can be a landmark, a prominent destination, or a focal point of a community that provides a transition between stable neighbourhoods and High Profile buildings.



Example of a 6 storey residential building.

### High Profile Buildings

A High Profile building is a multi-storey structure generally no more than fourteen (14) storeys in height.

Given the characteristics of a High Profile development, the condition of each site will ultimately define its possibilities. The built form of a High Profile development can be designed to include one or a combination of the following:

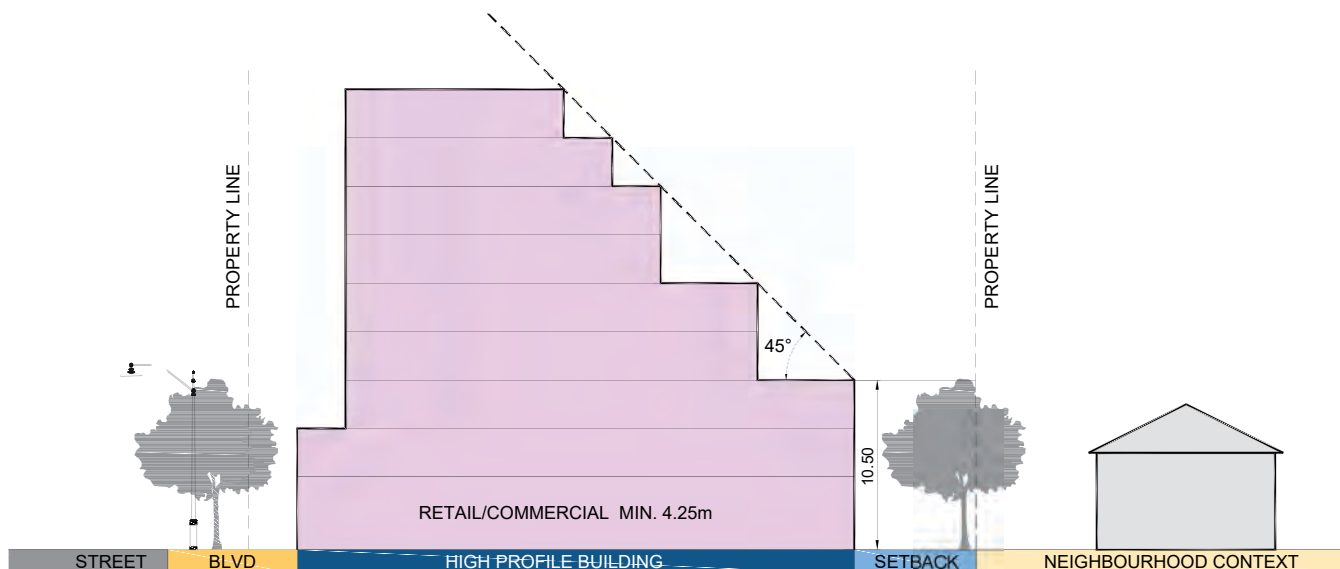
- Slab - large-horizontal structure
- Tower - slim structure
- Tower(s) over podium



Example of a 9 storey residential building with ground floor retail.

### 3.4.1 General Guidelines

1. Concentrate the greatest heights and massing along the frontage of an Arterial or Collector Road.
2. Ensure the scale of Medium and High Profile buildings is compatible and sensitively integrated with surrounding residential uses in terms of building mass, height, setbacks, orientation, privacy, landscaping, shadow casting, accessibility, and visual impact.
3. To demonstrate mitigation of potential shadow or wind impacts on existing or proposed pedestrian routes, public spaces, and adjacent development technical studies may be required including a wind study and/or sun/shadow study.
4. For Medium and High Profile buildings, ensure development transition requirements are met using a combination of the following:
  - a. Separate Medium and High Profile buildings from low profile buildings with a Local Road;
  - b. Locate less dense and lower scale residential buildings in locations adjacent to existing low density neighbourhoods;
  - c. Require a minimum 7.5 metre rear yard setback where Medium and High Profile development abuts low profile properties;
- d. Mitigate the actual and perceived massing impacts of a Medium or High Profile building by breaking up the mass horizontally and vertically, through the creative incorporation of changes in materials, balcony and floor plate design, architectural features, and unit/amenity locations;
- e. Provide rear and side step-backs for upper storeys to provide contextually appropriate transitions from the Medium and High Profile buildings to the surrounding low profile neighbourhoods; and,
- f. Provide high quality landscape treatment such as decorative fencing, trees, shrubs, grassed areas, and berming.
5. Angular planes can be used as a tool to evaluate the massing and height transition of a proposed High Profile building to low profile neighbourhoods to ensure appropriate skyview, light, and separation. Consider a 45 degree angular plane, measured from a height of 10.5 metres at the 7.5 metre setback, to determine the maximum height of the building.
6. Ensure new development is compatible with adjacent and neighbouring development by siting and massing new buildings to avoid undue adverse impacts on adjacent properties particularly in regard to adequate privacy conditions for residential buildings and their outdoor amenity areas.



Angular plane diagram - 45 degree angular plane taken from a height of 10.5m at the 7.5 metre setback.

7. Locate and orient primary building entrances to public roads, and design to be visible and accessible to the public in order to support public transit and for reasons of public safety and convenience.
8. Design sites with multiple buildings to reflect a consistent architectural theme. Similar building elements could include colours, materials, signage, and the base and top of buildings. Design individual buildings to offer visual interest and variety in design through architectural features.

### 3.4.2 Low Profile Buildings

1. All Low Profile buildings shall demonstrate design excellence and compatibility with the surrounding context. Ensure architectural detailing, landscape treatments, colour, and building materials are representative of the highest quality possible.
2. The height difference between adjacent Low Profile buildings on the same block should not vary by more than 1 storey to maintain a consistent street wall.
3. For Low Profile residential and mixed use buildings locate and orient windows, decks, and balconies to limit overlook into nearby windows and amenity spaces of adjacent properties while enabling “eyes on the street” for common public areas.



Example of three storey mixed use buildings at a node location.

4. Limit continuous residential forms such as stacked townhouse buildings to 3 to 8 units per block and the length of the townhouse block should not exceed 50 metres, unless it is essential to the architectural style of the building.
5. Locate garages at the rear of the building to be accessed from a lane or private drive.
6. When located at a corner, design buildings to address both street frontages and be massed towards the corner location for visual interest and to anchor the building.

### 3.4.3 Medium and High Profile Buildings

1. Medium and High Profile buildings may include commercial and office uses at grade and multi-unit residential above or behind. Design ground floors to be appealing to pedestrians and include uses that are more active in terms of pedestrian traffic, such as commercial/retail, personal service, and restaurant type uses on the ground floor.
2. Provide retail and service commercial uses on the ground floors of buildings to bring animation to the street and encourage pedestrian activity. Such uses should have a minimum 4.25 metre floor-to-ceiling height.



Use of step-backs to provide appropriate transition to adjacent uses.



3. Ensure residential entrances are clearly distinguished from the commercial entrances through building design and locate at the front or side of the building.
4. Locate visitor drop off areas at the side or rear of buildings with lane or private drive access.
5. Design interior courtyards to maximize sun exposure through the massing and location of tall building elements.
4. Provide a tower step-back of a minimum of 3 metres from the podium to differentiate between the building podium and tower and to ensure usable outdoor amenity space.
5. Consider an additional step-back for buildings taller than 8 storeys in height.
6. Provide a minimum separation distance of 25 metres between High Profile towers to maximize privacy and sky views, and to minimize the cumulative shadow impacts of multiple tall buildings. Balconies shall not be provided within this separation distance.

### 3.4.4 Building Design

#### 3.4.4.1 Height and Massing

Medium and High Profile buildings are generally comprised of a podium, tower, and top.

1. The height of the podium, and the tower step-backs above, should reflect the established streetwall. Ensure the height of the podium matches existing adjacent structures to reinforce the pedestrian scale of the streetscape.
2. Where no established streetwall exists, the minimum height of the podium shall be 3 storeys to frame the streetscape.
3. Where windows are proposed within the podium, provide an 11 metre separation distance between adjacent properties. Where no adjacent buildings exist, a 5.5 metre setback is appropriate. Where a continuous streetwall is desirable, no side-yard setbacks are necessary.
7. The top of the building defines the tower while further distinguishing a unique and interesting skyline. Design the top of buildings to include a variety of elements, such as step-backs, material variations, lighting, and other architectural elements to reinforce a strong presence at the top of the building.
8. Where possible, include outdoor amenity space within the top of the building, including balconies, patios, terraces, and rooftop gardens.
9. For developments with more than one building, provide a range of heights and establish a height hierarchy related to site conditions and context.

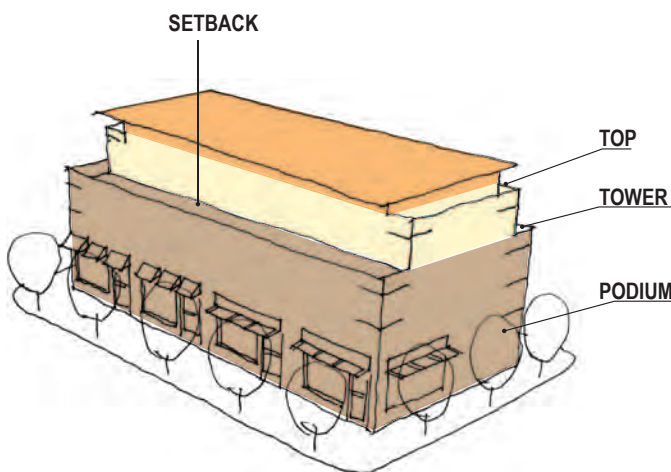


Diagram illustrating the building components of podium, tower and top.



The height of the podium on the mid-rise building should match the adjacent dwellings.

#### 3.4.4.2 Articulation and Architectural Features

1. To animate the public realm and promote safe environments encourage active uses at grade based on the street character (i.e., retail, commercial uses, day-care facilities, townhouses, etc).
2. Mitigate the actual and perceived impacts of Medium and High Profile buildings by breaking up the mass both vertically and horizontally through the creative incorporation of changes in materials, balcony and floor plate design, architectural features, and amenity locations.
3. Incorporate windows and balconies on all elevations, especially if exposed to public view.
4. Provide a high level of glazing at ground level, especially for those areas related to lobbies, common/amenity areas, and non-residential uses (i.e. commercial uses).
5. Encourage weather protective design at grade and at the podium level through canopies, arcades, and cantilevers. Canopies located on the ground floor should be at least 1.5 metres deep.
6. Avoid blank or long expansive elevations which are exposed to the public view. Where unavoidable, consider art or special wall treatments (i.e., screens, living walls, metallic or wooden textures).



Windows and balconies on the front elevation of the building.

#### 3.4.4.3 Exterior Materials

1. Ensure high quality and durable materials are used on all elements and elevations of the development.
2. Select materials to complement the architecture, character, size, and style of the building, as well as the streetscape.
3. Incorporate changes in materials to visually break-up the building massing.
4. Use reflective, low intensity colours on rooftops to reduce heat island effect and HVAC loads.
5. Minimize danger to migratory birds by:
  - a. Avoiding untreated reflective glass or clear glass that reflects trees and the sky;
  - b. Ensuring glass has visual markers and is not reflective within the first 12 metres of building height; and,
  - c. Locating and managing lighting to reduce reflections that might confuse migratory birds.



Changes in exterior materials lessen the visual impact of the building.



#### 3.4.4.4 Developments within Heritage Contexts

1. Locate and design buildings to respect and complement the scale, character, form and siting of on-site and surrounding cultural heritage resources.
2. Use existing heritage buildings to inform the site plan and podium layout and design.
3. Ensure building bases respect the scale of the surrounding historic fabric.
4. When an existing building is adapted or incorporated into the base of a High Profile building, maintain the size and shape of the original window openings and entrances.
5. Signage should add diversity and interest to the street and not overwhelm either the storefront or streetscape. Design building signage to be compatible and complement the architecture of the building in its scale, material, consistency, and design.
6. Design signage to be consistent with respect to materials, size, location (on a building), lettering and lighting, while also allowing some flexibility for tenant branding.
7. Direct signage lighting to limit light trespass to surrounding properties and to prevent light pollution.

#### 3.4.4.5 Signage

Signage plays an important role in the overall image of any area. Signs contribute to the quality of individual buildings and the overall streetscape, and reflect the unique characteristic of their context.

1. Integrate signage in the building design and ensure it complements the building's elevation, animates the ground level, and enhances the streetscape.
2. In Neighbourhood Nodes, design signage to be compatible with the character of the neighbourhood.
3. Ensure signage does not obscure windows, cornices, or other architectural elements.
4. Back-lit illuminated sign boxes are discouraged.
5. Projecting or hanging signs should be permitted to encroach over the street line provided that they do not project more than 1.0 metre from the building. There should be a minimum 2.4 metre clearance between the bottom of the sign and grade.



Lighting above signage is directed at the sign and complements the design of the building.



High quality signage is in keeping with the scale and material of the rest of the building.



Hanging signs encroach over the streetline and extend into the pedestrian realm.

### 3.4.4.6 Storefronts

1. Provide retail and service commercial uses on the ground floors of buildings to bring animation to the street and encourage pedestrian activity.
2. Locate entrances to stores at grade and design to be universally accessible, highly visible, and clearly articulated.
3. Provide spill-out space around the base of buildings for uses such as patios, street furniture, and special events.
4. Where retail uses are provided at-grade, ensure a significant amount of the building frontage on the ground floor and at the building base level is glass to allow views of the indoor uses and create visual interest for pedestrians. Clear glass is preferred to promote the highest level of visibility.
5. Provide awnings or canopies above windows and doors for weather protection.
6. Ensure storefronts on corner sites address both street frontages through entries and glazing.
7. Locate patios along primary streets in areas that maximize sun exposure and effectively animate the streetscape.

### 3.4.4.7 Mechanical Equipment

1. Screen rooftop mechanical equipment from view through architectural design that reflects the building's façade treatment. Add-on screening elements such as lattice are prohibited.
2. Design and clad mechanical penthouses with materials that complement the main building façades.
3. Locate mechanical rooms to the centre of the building rooftop and integrate into the rooftop design so they are not visible from public view.



An enhanced pedestrian realm along a commercial street with plantings, paving materials, and canopies projecting from the buildings.

# A appendix

## urban design brief terms of reference



## Purpose

An Urban Design Brief may be required to support a development proposal as part of a complete development application, such as an Official Plan Amendment, Zoning By-law Amendment, Draft Plan of Subdivision/Condominium, and/or Site Plan Control Application. This requirement will be identified by Planning Staff at the Pre-Consultation meeting.

An Urban Design Brief is intended to describe and illustrate the proposed design for a development proposal and demonstrate how the design meets the intent of the Urban Design Guidelines and other City design guidelines and policies.

Planning Staff will use the Urban Design Brief to assess the urban design aspects of development applications to ensure high quality design is achieved. The City is committed to urban design excellence that results in a complete, functional, sustainable, and attractive built environment consistent with Windsor's character and vision for the future, as outlined in the City's Official Plan.

The Urban Design Brief Terms of Reference has been prepared to standardize the City's expectation for Urban Design Brief submissions. The scope and level of detail expected in the Urban Design Brief will depend on the scale, site, nature, and complexity of the development proposal.

## Components of an Urban Design Brief

### 1.0 Existing Site Conditions and Surrounding Context

The Urban Design Brief should provide a description and analysis of the site and surrounding context. Photographs and a context map showing the subject site in relation to the existing neighbourhood should be included.

### 2.0 Applicable Design Guidelines and Policies

The Urban Design Brief should identify relevant urban design guidelines and policies from the following documents that are applicable to the proposed development:

- City of Windsor Official Plan
- Applicable Secondary Plans and Guidelines
- City of Windsor Intensification Urban Design Guidelines

### 3.0 Project Design Analysis

The Urban Design Brief should provide an analysis of the design rationale for the building, landscape, and site design elements of the proposed development and explain why the proposed development represents the optimum design solution. Discussion should consider the following:

- How the design of the proposed development meets the intent of the City's applicable urban design guidelines and policies;
- How the design addresses existing site conditions and constraints such as lot size, grading, or natural heritage features;
- How the design of the proposed development integrates with the existing neighbourhood and enhances its function and aesthetics; and,
- How the design of the proposed development will influence and integrate with future development in the neighbourhood.

### 4.0 Design Considerations

The Urban Design Brief should include a written description, plans, elevations, diagrams, and/or photographs to illustrate the design choices of the proposed development and site design. Depending on the scale of the development proposal explain how the applicable design considerations have been addressed:

- Street and block pattern (e.g., connectivity, pedestrian access);
- Lot sizes;
- Building orientation and site layout;
- Built form, height, scale, and massing;
- Building articulation and detailing;
- Building materials;
- Setbacks from adjacent properties and the street;
- Building step back (if applicable);
- Building transition to adjacent neighbourhoods;
- Heritage considerations (if applicable);
- Location of parking (surface or underground), driveways, ramps, drop-off areas;
- Access to transit;
- Bicycle parking/storage;
- Location of servicing, garbage, organics, and recycling storage and collection, and loading areas;
- Streetscape elements (e.g., boulevard design, landscaping, street furniture, public art, signage, lighting, etc.); and,
- On-site landscaping and buffering.



**From:** Andi Shallvari, CPA

**To:** Development and Heritage Standing Committee

**Re:** Amendment to zoning by laws 85-18 and 8600 (June 6, 2022) to reduce building height to 9 M and implement maximum gross floor area to 400 SQM (4300 SQF)

Statement: The proposed zoning changes of reducing building height to 9M and gross floor area to 4300 SQF will work against new developments of affordable housing and Missing Middle projects such as townhouses in core neighbourhoods around the City.

It is my opinion as a professional accountant (CPA), builder and housing provider that reducing the building height in all residential zones from 10 meters to 9 in addition to implementing a maximum GFA of maximum 4300 SQF in all residential buildings will eliminate a 3<sup>rd</sup> storey and at the same time reduce building footprint on a semi-detached, townhouse, 4 plex building forms, all known as Missing Middle. These restrictions will significantly impact Missing Middle building designs because it translates to a **48% loss** in living space in each dwelling (See calculations attached). For the last 7 years I have been focused only on Missing Middle typology because they provide great living options and are affordable. After reviewing the proposed zoning changes, it is my expert opinion that from a cost perspective the new proposed height reduction combined with a maximum limit on GFA of 4300 SQF will make it financially impossible to develop any future Missing Middle projects. The reality is that Windsor is growing, and all of us are seeing the high land values and increasing construction costs. The proposed zoning restriction indirectly will make new housing more costly on a per square footage of building cost. It will force and incentivise all private developers to only build two types of buildings:

- Luxury Single Family homes that will be priced to sell over \$1.5M
- High Rise Condominiums with very small units and floor plans

More so this new proposal will lead to private developers abandoning plans to develop and build much needed new housing in existing neighbours (Downtown, West End etc). Instead, the private capital will be allocated to fuel more suburban sprawl and farmland conversion outside Windsor. The cost of servicing sprawl is something that City Windsor should consider carefully here. A wrong zoning policy now will cause financial pains for many decades and a huge burden to future generations.

I consider myself a big supporter of Missing Middle developments because they provide:

- A variety of living options and alternative unit layouts (vs single family detached or condos)
- A nice transition into existing neighbourhoods from High Rise to Single Family Detached
- Affordable housing options for families with children (either owning or renting)

- They add gentle density on existing city infrastructure. They add more tax revenues to city and require less servicing costs (making them a great value for City and all Windsor taxpayers). Municipal services can be delivered more efficiently in areas with gentle density.

I would like the committee members to consider the following numbers and comparisons:

1. Selling prices of new condos average \$500-600/sqf compared to Missing Middle dwellings at \$360-400/sqf
2. Rental rates of new condo units average \$2.00-2.50/sqf compared to Missing Middle dwellings at \$1.47-1.56/sqf (based on 2 newly built 4 plexes offering all inclusive utilities and with \$0 rent increase for 5 years)

The point I am trying to make is that Housing Affordability will not improve but continue to get worse until we all work together to address and try to reduce the building costs on a per square footage for any new living space (costs such as land, materials, labour, permits, soft costs etc). Lately we are hearing the term affordability being used everywhere but it should be just a simple number, specifically the dollar (\$) cost/sqf of living space one can afford (to buy or rent). We already agreed to implement the Licensing regime which will add extra costs to rentals. Adding the height and GFA restrictions will reduce how much sqf of living space can be built on a vacant lot which will directly increase the cost/sqf of any new living space that will be built (This is simple economics). All these extra costs will be passed to the end user being either homeowner or renter. With this new proposal we will be setting up a City with very limited housing options (basically only 2 options, either the luxury detached home or the expensive small condo in a high rise).

I encourage the committee members to raise the following questions as it relates to the reduction of the proposed building height to 9M (from 10M) and limiting GFA to 4300 SQF on all building forms:

1. Based on last 4 years data of new condo buildings that are approved and being built, are there any figures on how many units will be considered affordable as it compares to the total number of units produced?
2. Has the city performed an assessment on how the new height reduction (to 9M) and limiting GFA on all buildings will influence and impact builder decisions as it relates to building costs and sale/rent prices that will be passed to the end consumer? Simply put what exactly are we trying to accomplish with this new proposal of reducing height and GFA? What are the goals? Were builders and housing providers consulted on the proposed changes?
3. Has the city performed any assessment on how this proposal could intensify suburban sprawl and more farmland turned into subdivisions due to developers allocating their capital towards financially viable projects?
4. Has the city performed an analysis of how the new height reduction (to 9M) and limiting GFA on all buildings impacts the following initiatives that City Windsor has put forward in the last 4 years:
  - a. Affordable Housing initiatives

- b. Incremental Development Workshop sponsored by City Windsor in Oct 2019 with its main goal to incentivise building Missing Middle (This program was very valuable and sparked a lot of interest in Missing Middle. The new zoning changes seem very contradictory and against what the City itself has been promoting in the last 3 years)
  - c. ADUs and requirements from Provincial Planning Statement to have 3 dwellings in 1 lot
  - d. Windsor Works - An Economic Development Strategy for the City's Future Growth
5. Has the City reviewed or considered what City of Ottawa has done to make their New Official Plan more flexible to build Missing Middle? **See Appendix 1 for Ottawa's 613 Flats** and the 6 building typologies they suggest for different infill lots.

In conclusion it is my opinion that the current proposal of reducing building height to 9M combined with limiting GFA to 4300 SQF in all residential neighbourhoods will eliminate Missing Middle developments which are the most affordable housing option available (either owning or renting). It also seems to go against the urbanization trends we are seeing in cities all over Canada (Ottawa, Edmonton, Waterloo, Guelph etc). Lastly it seems inconsistent with many initiatives that City Council has put forward in the last 4 years. I hope that a major decision like this one will be thoroughly discussed before proceeding with a significant change that will impact all new residential buildings in Windsor. Finally, my recommendations to the Committee are:

1. To prepare a study on how the proposed zoning changes impact housing affordability
2. To engage builders, developers and housing providers in order to put forward a plan that includes feedback from the industry. The average buyer or renter will not know how zoning rules impact building costs and most likely will blame the builder or the landlord for the high cost of their future home. The Committee should bring builders as housing providers in the discussion to better understand the difficulties, challenges and increasing costs we are facing daily on the building sector.

Sincerely,

Andi Shallvari, CPA

May 19, 2022 article from Windsor Star

<https://windsorstar.com/news/erosion-of-housing-affordability-in-ontario-during-pandemic-worst-in-five-decades>



## 613 Flats

The “613 Flats” are a play on words to identify a new type of residence that has six rooms in total, one of them being a bathroom and three of them being bedrooms. The 613 Flats are homes that could be built in established neighbourhoods to provide additional housing choice. They increase housing options while respecting the required amenity space, trees, soft landscaping and neighbourhood context.

### **Intensification.**

These units support one of the New Official Plan's primary goals of achieving more growth through intensification than urban expansion. These units are meant to attract families and larger households to existing neighbourhoods, where in the past they would find their housing needs satisfied mainly in new suburban communities.

### **15 Minute Neighbourhoods.**

613 Flats support the goal of creating 15-minute neighbourhoods by adding new housing that fits within the context of the neighbourhood. These new households will help support local businesses, schools and parks within a short walk from their home. Living in a walkable neighbourhood with these amenities helps promote better social, physical and mental health.

### **Trees and Landscaping.**

The 613 flats designs will require trees and soft landscaping to contribute to the existing character of the street and grow the City's urban tree canopy.

### **Flexibility.**

These examples of 613 Flats are examples of how neighbourhoods could evolve. There could be many other types of new homes through intensification. The New Official Plan sets the stage with high level policy to allow these new forms of homes while providing flexibility in design and shape across different Transect and neighbourhood needs.

### **New forms of homes will provide.**

- more housing choice at affordable price points
- help City residents move into neighbourhoods that were not a viable choice due to the lack of inventory
- provide greater housing choice for Ottawa residents across our many different and wonderful neighbourhoods



## Typology #1: 18mx30m Lot



Typology #1 illustrates a new housing idea that could integrate within the City's Neighbourhood Designations to contribute to the Official Plan's regeneration objectives. Allowing a greater number of units on an existing lot also addresses issues of housing affordability. This option provides four freehold units which can each accommodate three bedrooms.



1. Original Site  
18m X 30m Lot



2. Lot Distribution  
4 Private Lots & Parking



3. Built Form  
Single Building



4. Unit Distribution  
Four Freehold Units

### TYPOLGY BREAKDOWN

- This Typology is suitable for lots that are 18mX30m or more in size
- This Typology is divided into four freehold ownership units with common circulation easements
- Parking stalls are hidden from the public realm, at grade under the building footprint
- Each unit can accommodate three bedrooms for a total area of about 130sq.m. per unit
- This Typology also allows for the possibility of an additional 65sq.m. secondary dwelling units at grade
- This model would be most suitable in cases where creating a closer setback to the street is appropriate, such as Minor Corridors, or where existing setbacks are similar enough



## Typology #2: 18mx30m Lot



Typology #2 illustrates a new housing idea that could integrate within the City's Neighbourhood Designations in order to contribute to the Official Plan's regeneration objectives. Allowing a greater number of units on an existing lot also addresses issues of housing affordability. This new housing option provides three freehold units along with a corner retail/commercial unit to contribute to 15-minute neighbourhoods. Each residential unit can accommodate three bedrooms.



1. Original Site  
18m X 30m Corner Lot



2. Lot Distribution  
3 Private Lots & Parking



3. Built Form  
Single Building



4. Unit Distribution  
Three Freehold Units  
+ Retail

### TPOLOGY BREAKDOWN

- This Typology is suitable for corner lots that are 18mX30m or more in size
- This Typology is divided into three freehold ownership units with common circulation easements
- Parking stalls are hidden from the public realm, at grade under the building footprint
- Each unit can accommodate three bedrooms with two units of 158sq.m. and one unit of 130sq.m.
- This Typology also allows for the possibility of a 65sq.m retail space at grade



NEW  
Official Plan



## Typology #3: 15mx30m Lot



Typology #3 illustrates a new housing idea that could integrate within the City's Neighbourhood Designation in order to contribute to the Official Plan's regeneration objectives. Allowing a greater number of units on an existing lot also addresses issues of housing affordability. This new housing option provides four freehold units. Each residential unit can accommodate three bedrooms.



1. Original Site  
30m X 30m Lot



2. Lot Distribution  
Two - 15m X 30m Lots Each Divided  
Into 4 Private Lots & Parking



3. Built Form  
Two - 15m X 30m Lots Each Divided  
Into 4 Private Lots & Parking



4. Unit Distribution  
Four Freehold Units/15X30m  
Lot Eight New Units Total

### TPOLOGY BREAKDOWN

- This Typology is suitable for lots that are 30mX30m or more in size, or on lots that are 15mX30m
- This Typology is divided into four freehold ownership units with common circulation easements
- Parking stalls are provided at grade in the rear yard with access along the side
- Each unit can accommodate three bedrooms of about 121sq.m
- This Typology also allows for the possibility of 37sq.m secondary dwelling units in each basement



New  
Official Plan



## Typology #4: 15mx30m Lot



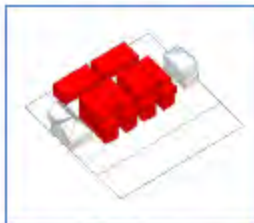
Typology #4 illustrates a new housing idea that could integrate within the City's Neighbourhood Designations to contribute to the Official Plan's regeneration objectives. Allowing a greater number of units on an existing lot also addresses issues of housing affordability. This new housing option provides six strata-units and two coach houses. Each of the strata-units unit can accommodate three bedrooms.



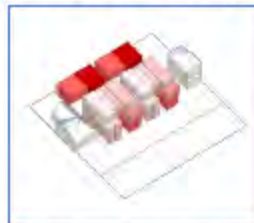
1. Original Site  
30m X 30m Lot



2. Lot Distribution  
Two - 15m X 30m Lots Each Divided  
Into 2 Private Lots & Parking



3. Built Form  
Single Building + Coach House/New  
Lot (2 buildings)



4. Unit Distribution  
Six Strata Units + 2 Coach  
Houses/ 15X30m Lot  
(16 New Units Total)

### TYPOLOGY BREAKDOWN

- This Typology is suitable for lots that are 30mX30m or 15mX30m or more in size
- This Typology is divided into two lots, each with three strata-units and one coach house
- Parking stalls are provided at grade in the rear yard under the coach house with access along the side
- Each lot can accommodate one, three bedroom unit of 121sq.m and two, two bedroom units of 79sq.m.
- This Typology also allows for the possibility of a 44sq.m coach house on each lot, in the rear yard



How  
Official Plan





## Typology #5: 12mx30m Lot



Typology #5 illustrates a new housing idea that could integrate within the City's Neighbourhood Designations in order to contribute to the Official Plan's regeneration objectives. Allowing a greater number of units on an existing lot also addresses issues of housing affordability. This new housing option provides three units that can each accommodate three bedrooms.



1. Original Site  
12m X 30m Lot



2. Lot Distribution  
Three Private Lots



3. Built Form  
Single Building



4. Unit Distribution  
Three Freehold Units

TYPOLOGY BREAKDOWN

- This Typology is suitable for lots that are 12mX30m or more in size
- This Typology is divided into three freehold lots with common circulation easements
- This typology does not allow for parking on site
- Each lot can accommodate three, three bedroom units of about 130sq.m



New  
Official Plan



## Typology #6: 9mx30m Lot



Typology #6 illustrates a new housing idea that could integrate within the City's Neighbourhood Designation in order to contribute to the Official Plan's regeneration objectives. Allowing a greater number of units on an existing lot also addresses issues of housing affordability. This new housing option provides three units that can each accommodate three bedrooms.



1. Original Site  
9m X 30m Lot



2. Lot Distribution  
Three Private Lots



3. Built Form  
Single Building



4. Unit Distribution  
Three Freehold Units

### TYPOLOGY BREAKDOWN

- This Typology is suitable for lots that are 9mX30m or more in size
- This Typology is divided into three freehold lots with common circulation easements
- This typology does not allow for parking on site
- Each lot can accommodate three, three bedroom units of about 121sq.m



How  
Official Plan

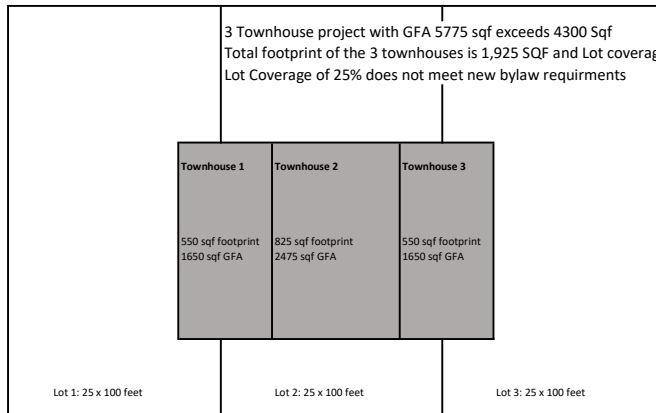


3 houses of 10,125 SQF combined GFA are allowed  
 Total footprint of the 3 houses is 3,375 SQF and Lot coverage ratio is 45%  
 (each detached house has 3,375 sqf GFA)



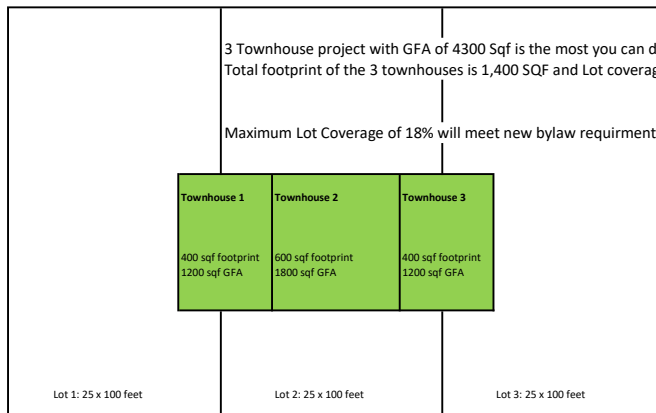
**Scenario 1**  
 Exact same land size (75x100 feet) offers different outcomes

3 Townhouse project with GFA 5775 sqf exceeds 4300 Sqf  
 Total footprint of the 3 townhouses is 1,925 SQF and Lot coverage ratio is 25%  
 Lot Coverage of 25% does not meet new bylaw requirements



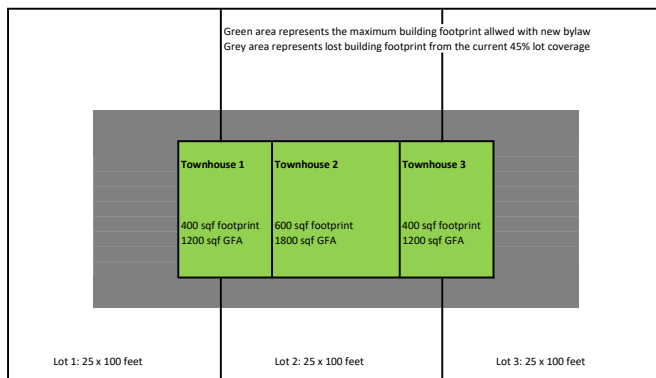
**Scenario 2**

3 Townhouse project with GFA of 4300 Sqf is the most you can do  
 Total footprint of the 3 townhouses is 1,400 SQF and Lot coverage ratio is 18%  
 Maximum Lot Coverage of 18% will meet new bylaw requirements



**Scenario 3**

Green area represents the maximum building footprint allowed with new bylaw  
 Grey area represents lost building footprint from the current 45% lot coverage



**Scenario 3.1**



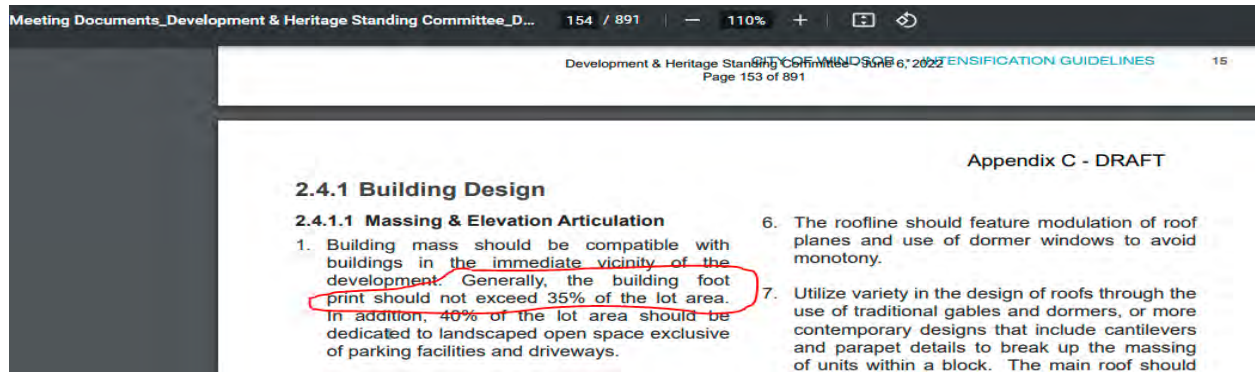
Originally submitted at June 6, 2022  
Development & Heritage Standing Committee  
Written Submission

From: Andi Shallvari, CPA

To: Development and Heritage Standing Committee

Re: Clarification of Lot coverage for infill developments (45% or 35% maximum)

I would like some clarification on lot coverage allowance after the new changes. The proposed zoning changes maintain lot coverage at 45%. However the new draft design guidelines limit lot coverage to a maximum of 35%. We need to have 1 maximum either 35% or 45% lot coverage. This very important number needs to be clarified so there is no confusion during the planning process.



Meeting Documents\_Development & Heritage Standing Committee\_D... 27 / 891 110% + -

Development & Heritage Standing Committee - June 6, 2022 Page 27 of 891

<b>.2 Semi-Detached Dwelling</b>	
.1 Lot Width – minimum	15.0 m
.2 Lot Area – minimum	450.0 m <sup>2</sup>
.3 Lot Coverage – maximum	45.0%
.4 Main Building Height – maximum	9.0 m
.5 Front Yard Depth – minimum	6.0 m
.6 Rear Yard Depth – minimum	7.50 m
.7 Side Yard Width – minimum	1.20 m

Page 11 of 24

<b>.5 Townhome Dwelling</b>	
.1 Lot Width – minimum	20.0 m
.2 Lot Area – per dwelling unit – minimum	200.0 m <sup>2</sup>
.3 Lot Coverage – maximum	45.0%
.4 Main Building Height – maximum	9.0 m



**Committee Matters: SCM 156/2022**

**Subject: Official Plan and Rezoning Amendments – Tunio Development – 3885 & 0 Sandwich Street - OPA 152 OPA [6504] Z-028/21 ZNG [6503] - Ward 2**

Moved by: Councillor Sleiman

Seconded by: Councillor Holt

Decision Number: **DHSC 399**

THAT the City of Windsor Official Plan, Volume II, Part 1-Special Policy Areas **BE AMENDED** by deleting and replacing Specific Policy Area 1.7 as follows:

**“1. 7 Sandwich Street and Chappell Avenue**

1.7.1 The property comprising Part of Lot 27, Registered Plan 40S, east side of Sandwich Street and Part of Lot 28, east Side of Sandwich Street, and Lot 28 west side corner of Sandwich Street, Registered Plan 40, located at the northeast corner of Sandwich Street and Chappell Avenue;

Site Specific Policy Direction 1.7.2

Notwithstanding Section **6.4.3.1 Industrial Policies- Permitted Uses** designation of the subject lands on Schedule D: Land Use in Volume I--The Primary Plan, the uses permitted in Section 6.9.2.1 shall be additional permitted uses; and,

THAT Zoning By-law 8600 **BE AMENDED** by changing the zoning of Part Lot 27, Registered Plan 40S, Lot 28 East Side, Registered Plan 40, and Lot 28 West Side, Registered Plan 40, situated at the northeast corner of Sandwich Street and Chappell Avenue (known municipally as 0 and 3885 Sandwich Street; Roll # 050-170-09700 and 050-170-09800) from Development Reserve District 1.1 (DRD1.1), Commercial District 2.1 (CD2.1) and Manufacturing District 1.4 (MD1.4) to Commercial District 2.1 (CD2.1) and by adding the following site specific provision:

**445. NORTHEAST SIDE OF SANDWICH STREET AND CHAPPELL AVENUE**

For the lands comprising Part Lot 27, Registered Plan 40S; Lot 28 East Side on Registered Plan 40; and Lot 28 West Side, Registered Plan 40:

The following uses are prohibited:

- Bakery
- Food Outlet-Drive-Through
- Gas Bar
- Place of Entertainment and Recreation
- Public Hall
- Restaurant with Drive-through

2. A *Multiple Dwelling* and *Dwelling Units in a Combined Use Building* shall be additional permitted main uses and shall be subject to the following provisions:

- a) Main Building Height – maximum 37.0 m
- b) Amenity Area – Per Dwelling Unit – minimum 5.0 m<sup>2</sup>
- c) For a *Combined Use Building*, all *dwelling units*, not including entrances thereto, are located above the non-residential uses
- d) Exposed flat concrete block walls or exposed flat concrete wall, whether painted or unpainted, are prohibited
- e) Required Number of Parking Spaces – minimum - 156

[ZDM 4; ZNG/6503]; and,

THAT Part Lot 27 on Registered Plan 40S (PIN 012580193) (Roll # 050-170-09800-0000) known municipally as 0 Sandwich Street and for Lots 28 East Side; & Lot 28 West Side; Corner on Registered Plan 40 (PIN 012580190) (Roll #170-09700-0000) known municipally as 3885 Sandwich Street, situated at the northeast corner of Sandwich Street and Chappell Avenue **BE CLASSIFIED** as a Class 4 area pursuant to Publication NPC-300 (MOECP Environmental Noise Guideline – Stationary and Transportation Sources – Approval and Planning); and,

THAT the Site Plan Approval Officer **BE DIRECTED** to incorporate the mitigation measures including warning clauses required for a Class 4 designation pursuant to Publication NPC-300 identified in the Acoustical Report prepared by Baird AE (Project No. 20-028), dated October 1, 2021, in the site plan approval and the site plan agreement; and,

THAT the City Planner or their designate **BE DIRECTED** to provide a copy of the Council Resolution approving the Class 4 area classification and a copy of any development agreement or site plan agreement for the subject lands that incorporates noise mitigation measures to the surrounding noise sources identified in the Acoustical Report prepared by Baird AE (Project No. 20-028), dated October 1, 2021.

Carried.

Report Number: S 65/2022  
Clerk's File: Z/14317

**Clerk's Note:**

1. The recommendation of the Standing Committee and Administration are the same.
2. Please refer to Item 7.3. from the Development & Heritage Standing Committee Meeting held June 6, 2022.
3. To view the stream of this Standing Committee meeting, please refer to:  
<http://csg001-harmony.sliq.net/00310/Harmony/en/PowerBrowser/PowerBrowserV2/20220608/-1/7334>

**Subject: Official Plan and Rezoning Amendments – Tunio Development  
– 3885 & 0 Sandwich Street - OPA 152 OPA[6504] Z-028/21 ZNG[6503] -  
Ward 2**

**Reference:**

Date to Council: 2022-06-06  
Author: Kevin Alexander, MCIP RPP  
Senior Planner Special Projects  
519-255-6543 x6732  
kalexander@citywindsor.ca  
Planning & Building Services  
Report Date: 2022-05-20  
Clerk's File #: Z/14317

**To:** Mayor and Members of City Council

**Recommendation:**

THAT the City of Windsor Official Plan, Volume II, Part 1-Special Policy Areas **BE AMENDED** by deleting and replacing Specific Policy Area 1.7 as follows:

**“1. 7 Sandwich Street and Chappell Avenue**

1.7.1 The property comprising Part of Lot 27, Registered Plan 40S, east side of Sandwich Street and Part of Lot 28, east Side of Sandwich Street, and Lot 28 west side corner of Sandwich Street, Registered Plan 40, located at the northeast corner of Sandwich Street and Chappell Avenue;

**Site Specific Policy Direction 1.7.2**

Notwithstanding Section **6.4.3.1 Industrial Policies- Permitted Uses** designation of the subject lands on Schedule D: Land Use in Volume I--The Primary Plan, the uses permitted in Section 6.9.2.1 shall be additional permitted uses; and,

THAT Zoning By-law 8600 **BE AMENDED** by changing the zoning of Part Lot 27, Registered Plan 40S, Lot 28 East Side, Registered Plan 40, and Lot 28 West Side, Registered Plan 40, situated at the northeast corner of Sandwich Street and Chappell Avenue (known municipally as 0 and 3885 Sandwich Street; Roll # 050-170-09700 and 050-170-09800) from Development Reserve District 1.1 (DRD1.1), Commercial District 2.1 (CD2.1) and Manufacturing District 1.4 (MD1.4) to Commercial District 2.1 (CD2.1) and by adding the following site specific provision:

#### 445. **NORTHEAST SIDE OF SANDWICH STREET AND CHAPPELL AVENUE**

For the lands comprising Part Lot 27, Registered Plan 40S; Lot 28 East Side on Registered Plan 40; and Lot 28 West Side, Registered Plan 40:

1. The following uses are prohibited:

Bakery

Food Outlet-Drive-Through

Gas Bar

Place of Entertainment and Recreation

Public Hall

Restaurant with Drive-through

2. *A Multiple Dwelling and Dwelling Units in a Combined Use Building* shall be additional permitted main uses and shall be subject to the following provisions:

- a) Main Building Height – maximum 37.0 m
- b) Amenity Area – Per Dwelling Unit – minimum 5.0 m<sup>2</sup>
- c) For a *Combined Use Building*, all *dwelling units*, not including entrances thereto, are located above the non-residential uses
- d) Exposed flat concrete block walls or exposed flat concrete wall, whether painted or unpainted, are prohibited
- e) Required Number of Parking Spaces – minimum - 156

[ZDM 4; ZNG/6503]; and,

**THAT** Part Lot 27 on Registered Plan 40S (PIN 012580193) (Roll # 050-170-09800-0000) known municipally as 0 Sandwich Street and for Lots 28 East Side; & Lot 28 West Side; Corner on Registered Plan 40 (PIN 012580190) (Roll #170-09700-0000) known municipally as 3885 Sandwich Street, situated at the northeast corner of Sandwich Street and Chappell Avenue **BE CLASSIFIED** as a Class 4 area pursuant to Publication NPC-300 (MOECP Environmental Noise Guideline – Stationary and Transportation Sources – Approval and Planning); and,

**THAT** the Site Plan Approval Officer **BE DIRECTED** to incorporate the mitigation measures including warning clauses required for a Class 4 designation pursuant to Publication NPC-300 identified in the Acoustical Report prepared by Baird AE (Project No. 20-028), dated October 1, 2021, in the site plan approval and the site plan agreement; and,

**THAT** the City Planner or their designate **BE DIRECTED** to provide a copy of the Council Resolution approving the Class 4 area classification and a copy of any development agreement or site plan agreement for the subject lands that incorporates noise mitigation measures to the surrounding noise sources identified in the Acoustical Report prepared by Baird AE (Project No. 20-028), dated October 1, 2021.

### **Executive Summary:**

N/A

### **Background:**

#### **Application Information:**

**Location:** Northeast Corner of Sandwich Street and Chappell Avenue;  
3885 Sandwich Street and 0 Sandwich Street;  
Roll No. 050-170-09700-0000, 050-170-09800-0000

**Ward:** 2      **Planning District:** Sandwich      **Zoning District Map:** 4

**Applicant:** Tunio Development (Khurram Tunio)

**Agent:** Pillon Abbs Inc. (Tracey Pillon-Abbs)

#### **Proposal:**

The applicant requests a Site Specific Amendment to the City's Official Plan (OPA) and Site Specific Amendment to Zoning By-law 8600 (ZBA) to allow for the construction of an eleven (11) storey Combined-Use Building (Mixed-Use) with one-hundred and fifty (150) affordable residential units, two (2) retail units, one-hundred and fifty-six (156) parking spaces including six (6) accessible parking spaces, eleven (11) bicycle spaces, and two (2) loading spaces. Access will be provided via two driveways from Sandwich Street and Chappell Avenue. Refuse bins are located within the building.

The applicant submitted the following studies:

- Planning Rational Report (PRR), by Pillon Abbs Inc. (October 15, 2021);



- Urban Design Brief, by Baird AE (September 2, 2021)
- Acoustical Report, by Baird AE. (October 1, 2021);
- A Geotechnical Investigation, by CT Soils & Materials Engineering Inc. (May 21, 2021);
- Traffic Impact Study (TIS), by Baird AE (October 5, 2020)
- Phase 1 (April 3, 2020) and Phase 2 (January 28, 2021) Site Assessment (ESA) by WOOD Environment & Infrastructure Solutions;
- Functional Servicing Report (FSR) for Storm and Sanitary, dated October 8<sup>th</sup>, 2020.

The revised PRR (See Appendix D) suggests designating the Site to a site specific “Industrial” use to permit a combined use building with commercial on the main floor and residential above. The Planning Department suggested that the PRR report be revised to consider a site specific Official Plan Amendment to permit uses identified in Section 6.9.2.1 as additional permitted uses and additional policies in the Provincial Policy Statement (PPS) should be considered with respect to Section 1.3.2.5 regarding the conversion of Employment Lands. Pilon Abbs Inc. submitted a revised PRR dated May 25<sup>th</sup>, 2022.

Based on feedback from Transportation Planning, the TIS provided by Baird AE requires a memo clarifying information related to minor changes to the preliminary site plan. In addition, due to the large deficiency in parking with respect to Zoning By-law 8600, a parking study is required. The Parking Justification Report was submitted on April 22, 2022. Transportation Planning reviewed the study but found some inaccuracies related to the interpretation of the parking requirements identified in Zoning By-law 8600. An updated memo regarding the TIS and a revised Parking Justification Report was submitted on May 25<sup>th</sup>, 2022.

Based on the updated Parking Justification Report the deficiency in parking was somewhat reduced by providing additional amenity space to be used by the residents. The applicant can provide 156 parking spaces based on the current design. The development will be deficient by 60 parking spaces based on the parking requirements set out in Section 24 (Parking, Loading and Stacking) in Zoning By-law 8600.

Although the parking is deficient based on By-law 8600 more than one space is provided per residential unit providing sufficient parking in the evening hours, which is considered the peak hours when parking is required for the residential units. Whereas the peak hours for commercial uses is typically weekends during the day. Therefore, while residents are working or performing other daily tasks during the day on-site parking and on street parking will be available to users of the commercial/retail space during the day.

The Acoustical Report prepared by Baird AE. (October 1, 2021) suggests a Class 2 area designation pursuant to NPC-300 (MOECP Environmental Noise guideline – Stationary and Transportation Sources – Approval and Planning). This is an acceptable designation, however, Class 4 would be the preferable designation requested by the Planning Department. The Class 4 area designation is a tool that allows a municipality to approve a noise sensitive land use with relaxed noise limit levels in an area of existing stationary noise sources to promote intensification.

The Planning Department suggested that the Acoustical Report should be revised to request a Class 4 area designation subject to the mitigation measures identified in the study, noise levels can be mitigated to levels identified in NPC-300 for a Class 4 area. It should also include a recommendation for the inclusion of the appropriate warning clause for a Class 4 area in addition to the warning clauses identified in the assessment. One of the Recommendations of this report request that the subject lands be classified as a Class 4 area pursuant to Publication NPC-300 (MOECP Environmental Noise Guideline – Stationary and Transportation Sources – Approval and Planning).

#### Site Information:

OFFICIAL PLAN	ZONING	CURRENT USE	PREVIOUS USE
Industrial	Commercial District 2.2 (CD2.1)  S.20(1)15  Development Reserve Residential District (DRD1.1)  Manufacturing District (MD1.4)	Vacant Land	Commercial  Residential
LOT FRONTAGE SANDWICH STREET	LOT FRONTAGE CHAPPELL AVENUE	AREA	SHAPE
94.31 m	82.34 m	6,694 m <sup>2</sup> / 0.67 ha	Irregular
309.42 ft	270.14 ft	83,587 sq ft / 1.92 ac	
All measurements are provided by applicant and are approximate.			

#### Neighbourhood Characteristics:

The surrounding land uses consist of a mix of residential, commercial, industrial, and transportation uses. In the City of Windsor, the Detroit River is considered north. However, in the Sandwich Town neighbourhood the Detroit River bends towards the east and therefore the Ambassador Bridge is considered north and the Detroit River is west. When taking into consideration the change in direction in Sandwich Town, to the north of the subject lands low-density residential uses are located along the east side of Sandwich Street with an Industrial use (Volmer/Fahrhall) is located on the west side of Sandwich Street across the street from the subject lands. Low-density residential uses are also located just east of the subject lands. A commercial use (Tim Hortons) with a drive-through is located south of the subject lands (across from Chappell Avenue). Other Industrial uses also exists southwest of the subject lands.

The Essex Terminal Railway is located west and south of the Industrial uses to the southwest of the subject lands.

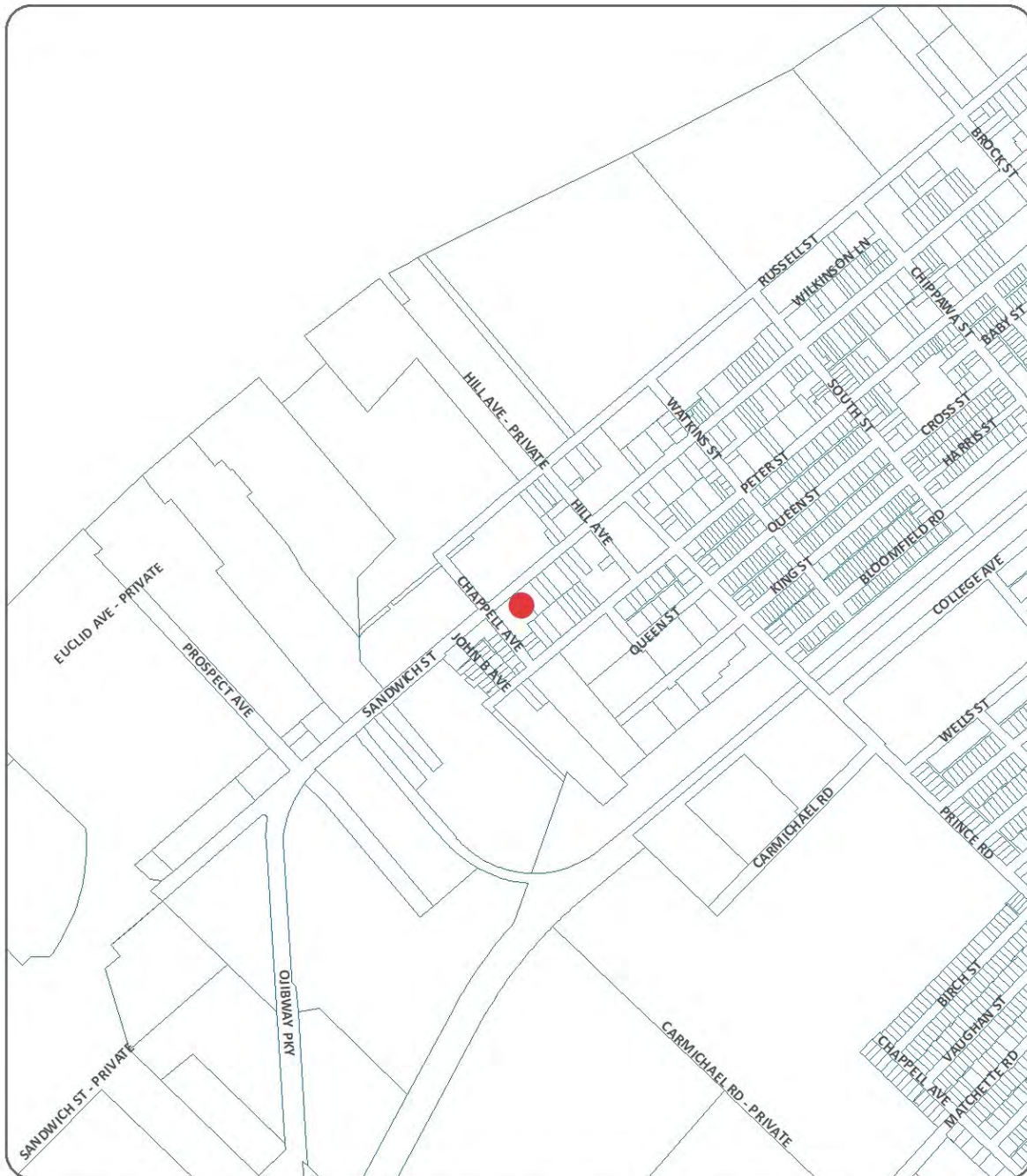
The Sandwich Street Corridor extends south towards Ojibway Parkway where mostly Industrial uses flank Sandwich Street on the east and west sides. However, residential uses do exist east of Sandwich Street along this route. The Sandwich Street corridor also extends north towards the Sandwich Main Street and Business Improvement Area (BIA). Along that route both single-family and multi-family residential uses, commercial, and industrial uses flank the east and west side of Sandwich Street.

Sandwich Street is classified as a Class II Arterial Road on Schedule F: Roads and Bikeways in the Official Plan. Chappell Avenue is classified as a Local Road.

Public transit is available via the Crosstown 2. The closest bus stop to the subject lands is located on Prince Road at the southeast corner of Peter Street. This bus stop is located approximately 450 metres from the subject lands falling outside of Transit Windsor's 400 metre walking distance guideline to a bus stop. However, the Council approved Transit Master Plan will introduce a new secondary route along Sandwich Street in the area of the subject lands and will likely site a bus stop on Sandwich Street at Chappell Avenue, thereby providing a direct service to the proposed development.

As part of the Sandwich Street Reconstruction Project new bike lanes and parking spaces are proposed on the west and east sides of Sandwich Street between Chappell Street and Hill Avenue.

**Figure 1: Key Map**



**KEY MAP - Z-028/21, ZNG-6503 & OPA 152, OPA-6504**

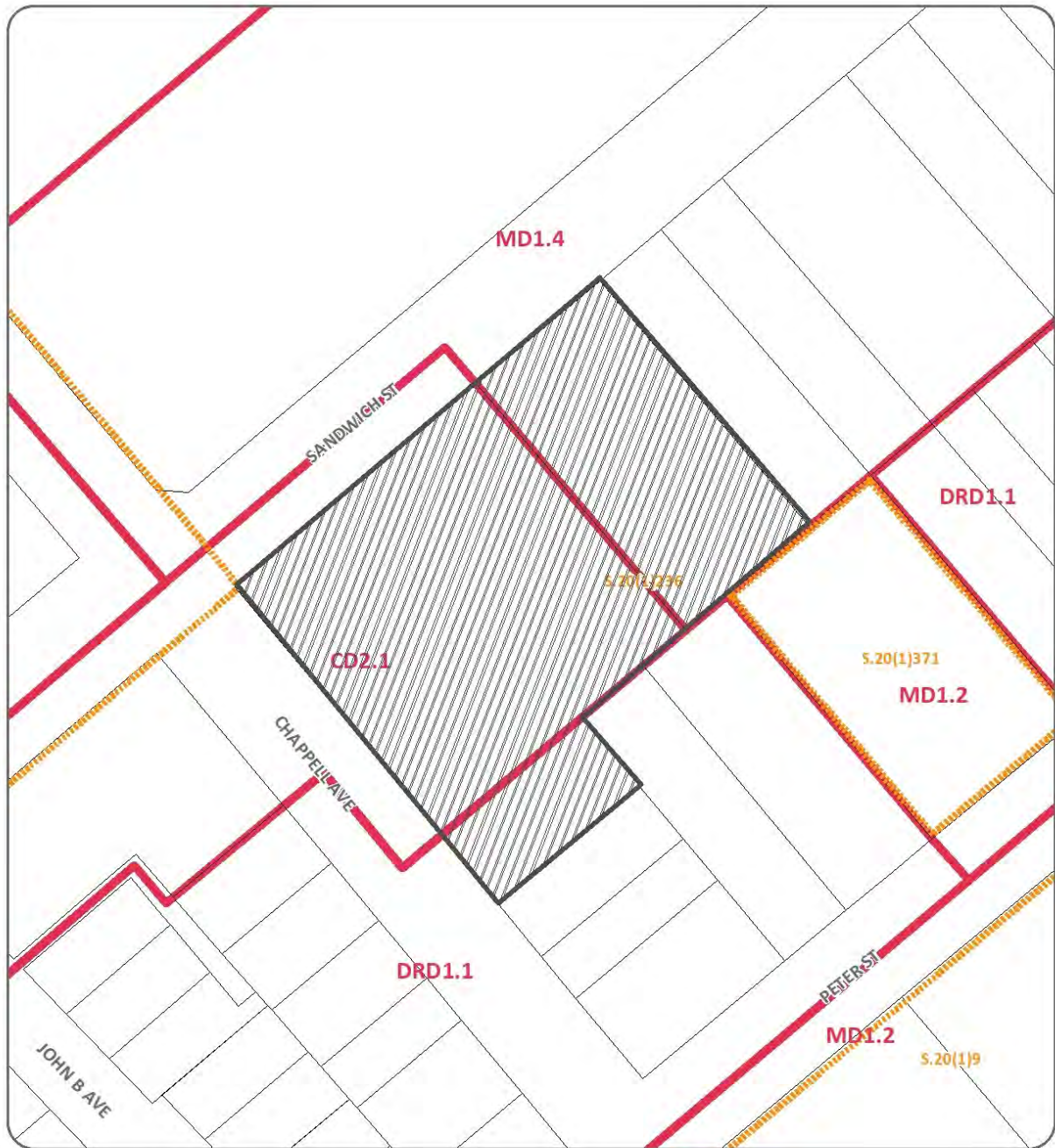


**● SUBJECT LANDS**

APPLICANT: TUNIO DEVELOPMENT

ADDRESS: 0 & 3885 SANDWICH STREET

**Figure 2: Subject Parcel – Rezoning**



PART OF ZONING DISTRICT MAP 4

N.T.S.

## REZONING

Applicant: Tunio Development



SUBJECT LANDS

PLANNING & BUILDING DEPARTMENT



DATE : FEBRUARY 2022  
FILE NO. : Z-028/21, ZNG-6503



**Figure 3: Neighborhood Map**



**NEIGHBOURHOOD MAP - Z-028/21, ZNG-6503 &  
OPA 152, OPA-6504**



SUBJECT LANDS

APPLICANT: TUNIO DEVELOPMENT  
ADDRESS: 0 & 3885 SANDWICH STREET

## Discussion:

### Provincial Policy Statement 2020

The Provincial Policy Statement (PPS) provides direction on matters of provincial interest related to land use planning and development and sets the policy foundation for regulating the development and use of land in Ontario. Relevant excerpts from the PPS are found in Appendix E.

The review of the PPS applies to both the Official Plan Amendment and the Zoning By-law amendment. Many of the policies identified below are also supported in the PRR.

Policy 1.1.1 of the PPS states:

*“Healthy, liveable and safe communities are sustained by:*

- a) promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term;*
- b) accommodating an appropriate range and mix of residential (including second units, affordable housing and housing for older persons), employment (including industrial and commercial), institutional (including places of worship, cemeteries and long-term care homes), recreation, park and open space, and other uses to meet long-term needs;*
- c) avoiding development and land use patterns which may cause environmental or public health and safety concerns;*
- e) promoting cost-effective development patterns and standards to minimize land consumption and servicing costs;”*

The construction of a combined use building represents an efficient development and land use pattern that will have no adverse impact on the financial well-being of the City of Windsor, land consumption, and servicing costs, and accommodates a residential use that is lacking in the surrounding area and that is in close proximity to commercial, employment, and institutional uses.

Policy 1.1.2 of the PPS states:

- 1.1.2 Sufficient land shall be made available to accommodate an appropriate range and mix of land uses to meet projected needs for a time horizon of up to 25 years.....Within settlement areas, sufficient land shall be made available through intensification and redevelopment and, if necessary, designated growth areas.....*

The proposed combined use building with 150 residential units above commercial retail space represents a form of intensification and redevelopment that will help the City of Windsor meet the full range of current and future residential needs. Within this existing settlement area, the site will provide for residential/commercial infill in the form of a new affordable housing choice and minor employment opportunities.

The amendments are consistent with Policy 1.1.1 and 1.1.2 of the PPS.



Policies 1.1.3.1, 1.1.3.2 and 1.1.3.3 of the PPS state:

- “1.1.3.1 Settlement areas shall be the focus of growth and development, and their vitality and regeneration shall be promoted.*
- 1.1.3.2 Land use patterns within settlement areas shall be based on densities and a mix of land uses which:*
- a) efficiently use land and resources*
  - b) are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion;*
  - e) support active transportation;*
  - f) are transit-supportive, where transit is planned, exists or may be developed*
- 1.1.3.3 Planning authorities shall identify appropriate locations and promote opportunities for intensification and redevelopment where this can be accommodated taking into account existing building stock or areas, including brownfield sites, and the availability of suitable existing or planned infrastructure and public service facilities required to accommodate projected needs.”*
- 1.1.3.4 Appropriate development standards should be promoted which facilitate intensification, redevelopment and compact form, while avoiding or mitigating risks to public health and safety.*
- 1.1.3.6 New development taking place in designated growth areas should occur adjacent to the existing built-up area and should have a compact form, mix of uses and densities that allow for the efficient use of land, infrastructure and public service facilities.*

The subject parcel is located within the settlement area. The requested amendments promote a land use that makes efficient use of land and existing infrastructure by permitting a mix of uses (commercial and multiple residential dwellings) on one site. Active transportation options such as bike lanes and transit services are located or planned adjacent to, or near, the subject lands. The subject location represents an appropriate location for intensification and redevelopment.

The construction of the proposed combined-use building will be built with a high standard of construction and will utilize modern building methods, which will conform to the Ontario Building Code concerning safety and energy efficiency. Through the development review process the building will be designed to address the Sandwich CIP Urban Design Guidelines. There will be no risk to the public as identified in the PRR, ESA, and TIS.

The proposed mixed-use development is located within the existing built-up well-established Sandwich Town neighbourhood and is considered a compact built form with commercial on the main floor and residential above. The proposed Eleven (11) storey

building allows for an efficient use of land utilizing existing infrastructure and public services.

The amendments are consistent with PPS Policies 1.1.3.1, 1.1.3.2, 1.1.3.3, 1.1.3.4, and 1.1.3.6.

Policy 1.3.2 Employment Areas states:

*1.3.2.3 Within employment areas planned for industrial or manufacturing uses, planning authorities shall prohibit residential uses and prohibit or limit other sensitive land uses that are not ancillary to the primary employment uses in order to maintain land use compatibility.*

*Employment areas planned for industrial or manufacturing uses should include an appropriate transition to adjacent non-employment areas.*

It is appropriate to recommend an OPA and ZBA to permit Multiple Dwelling Units in a Combined Use Building as an additional permitted use within the Industrial Land Use category for the following reasons:

- Most industrial uses within the Sandwich Town Neighbourhood are found on the west side of Sandwich Street and Sandwich Street provides an appropriate buffer.
- Part of the subject lands (3885 Sandwich Street, on Plan 40 (PIN 012580190) PT Lot 28 West Side; Corner; 050-170-09700-0000) located at the corner of Sandwich Street and Chappell Avenue was previously used for a commercial use for many years
- Part of the subject lands (3885 Sandwich Street, on Plan 40 (PIN 012580190) PT Lot 28 East Side; 050-170-09700-0000; and 0 Sandwich Street, Plan (PIN 012580193) 40S, PT Lot 27; Roll # 050-170-09800-0000) was previously used for residential purposes including a 6-family dwelling according to our records.
- The area is in transition and the property has remained vacant for at least fifteen (15) years where there has been no interest in redeveloping the subject lands for Industrial uses

*1.3.2.4 Planning authorities may permit conversion of lands within employment areas to non-employment uses through a comprehensive review, only where it has been demonstrated that the land is not required for employment purposes over the long term and that there is a need for the conversion.*

The conversion from Employment Lands is not being considered as part of a comprehensive review, but is a privately initiated transition.

*1.3.2.5 Notwithstanding policy 1.3.2.4, and until the official plan review or update in policy 1.3.2.4 is undertaken and completed, lands within existing employment areas may be converted to a designation that permits non-employment uses provided the area has not been identified as provincially significant through a provincial plan exercise or as regionally significant by a regional economic development corporation working together with affected upper and single-tier municipalities and subject to the following:*

- a) *there is an identified need for the conversion and the land is not required for employment purposes over the long term;*
- b) *the proposed uses would not adversely affect the overall viability of the employment area; and*
- c) *existing or planned infrastructure and public service facilities are available to accommodate the proposed uses.*

Based on recent Provincial housing policies (*More Homes for Everyone*) related to increasing more affordable housing supply options there is an identified need to provide *Multiple Dwelling Units in a Combined Use Building* as an additional permitted use in the existing Industrial land use category in the City's Official Plan. The Site is not required for employment purposes over the long term for the following reasons:

- The size and location, and immediate adjacent residential and commercial uses make the site no longer appropriate or desirable for industrial uses. This is further supported by the fact that the subject site has remained vacant for over fifteen (15) years and to our knowledge, there has been no interest in developing the subject lands for an industrial use.
- The site is in an area of transition where the most adjacent land uses are residential and commercial and Sandwich Street provides a buffer from the industrial uses along the west side of Sandwich Street.
- The recommended OPA and ZBA to amend the Official Plan and Zoning By-law 8600 is consistent with the Official Plan.
- Section 1.1.4 of the City's Official Plan identified the transfer of lands from the Town of Tecumseh as being sufficient to accommodate Employment Land growth through the 20-year planning period.
- The proposed use will not adversely affect the overall viability of the employment area because as identified the site has been vacant for over fifteen (15) years and has not been pursued as being desirable for industrial uses based on our records. Any new employment can occur in nearby industrial areas. The recommended OPA and ZBA to permit a mixed used development as an additional permitted use within Industrial Land Use category will have a minor impact on the overall vacant Employment Land Inventory.

- Infrastructure and public service facilities are available to accommodate the proposed uses as identified through the required studies.

The amendments are consistent with PPS Policies 1.3.2.3; 1.3.2.4; and 1.3.2.5.

Policy 1.4 of the PPS states:

- “1.4.1 To provide for an appropriate range and mix of housing types and densities required to meet projected requirements of current and future residents of the regional market area, planning authorities shall:*
- a. maintain at all times the ability to accommodate residential growth for a minimum of 15 years through residential intensification and redevelopment and, if necessary, lands which are designated and available for residential development; and*
  - b. maintain at all times where new development is to occur, land with servicing capacity sufficient to provide at least a three-year supply of residential units available through lands suitably zoned to facilitate residential intensification and redevelopment, and land in draft approved and registered plans.*
- 1.4.3 Planning authorities shall provide for an appropriate range and mix of housing types and densities to meet projected requirements of current and future residents of the regional market area by:*
- b. permitting and facilitating:*
    - 1. all forms of housing required to meet the social, health and well-being requirements of current and future residents, including special needs requirements; and*
    - 2. all forms of residential intensification, including second units, and redevelopment in accordance with policy 1.1.3.3;*
  - c. directing the development of new housing towards locations where appropriate levels of infrastructure and public service facilities are or will be available to support current and projected needs;*
  - d. promoting densities for new housing which efficiently use land, resources, infrastructure and public service facilities, and support the use of active transportation and transit in areas where it exists or is to be developed;”*

The proposed combined use building (mixed-use) with 150 residential units above commercial retail space represents a form of intensification and redevelopment. It will facilitate the municipality's ability to accommodate residential growth through intensification and redevelopment, will provide a form of housing that is appropriate in terms of range and mix, and will meet the social, health and well being of current and future residents. Appropriate levels of infrastructure, active transportation (walking, bike lanes and bike parking), and transit are available or will be available and community amenities such as community centres and parks exist along the Sandwich Main Street/BIA and within the Sandwich Neighbourhood.

The amendments are consistent with PPS Policy 1.4.

Policy 1.6.1 of the PPS states:

- 1.6.1 Infrastructure and public service facilities shall be provided in an efficient manner that prepares for the impacts of a changing climate while accommodating projected needs.*

As confirmed through comments from Public Works (Engineering & ROW) combined sanitary and storm sewers exist in the area and the development can connect to existing municipal services. As identified previously in the report Public transit is available via the Crosstown 2 and a secondary route along Sandwich Street in the area of the subject lands is proposed.

The amendments are consistent with PPS Policy 1.6

Policies 1.6.6 states:

- 1.6.6.2 Municipal sewage services and municipal water services are the preferred form of servicing for settlement areas to support protection of the environment and minimize potential risks to human health and safety. Within settlement areas with existing municipal sewage services and municipal water services, intensification and redevelopment shall be promoted wherever feasible to optimize the use of the services.*
- 1.6.6.7 Planning for stormwater management shall:*
- a) be integrated with planning for sewage and water services and ensure that systems are optimized, feasible and financially viable over the long term;*
  - b) minimize, or, where possible, prevent increases in contaminant loads;*
  - c) minimize erosion and changes in water balance, and prepare for the impacts of a changing climate through the effective management of stormwater, including the use of green infrastructure;*
  - d) mitigate risks to human health, safety, property and the environment;*
  - e) maximize the extent and function of vegetative and pervious surfaces; and*
  - f) promote stormwater management best practices, including stormwater attenuation and re-use, water conservation and efficiency, and low impact development.*

Combined sanitary and storm sewers exist in the area and the development can connect to existing municipal services.

A FSR has been completed identifying no negative impacts on the municipal system and will not add to the capacity in a significant way. However, a sewer servicing study will be required for sanitary and storm. ERCA has concerns with the potential impact of the quantity and quality of runoff in the downstream watercourse due to the future development of the site. A storm water management plan is required at the time of Site Plan Control. The proposed development is an efficient use of existing infrastructure in an already built-up area of the city. These studies will also help address PPS 2.2.1 related to water quantity and quality.

An ESA was also completed to mitigate any risk to health and safety.

The amendments are consistent with PPS Policy 1.6.6 and 2.2.1

Policies 1.6.7 states:

- 1.6.7.1 Transportation systems should be provided which are safe, energy efficient, facilitate the movement of people and goods, and are appropriate to address projected needs.*
- 1.6.7.2 Efficient use should be made of existing and planned infrastructure, including through the use of transportation demand management strategies, where feasible.*
- 1.6.7.4 A land use pattern, density and mix of uses should be promoted that minimize the length and number of vehicle trips and support current and future use of transit and active transportation.*

A TIS was provided by Baird AE and based on Transportation Planning's review requires a memo clarifying information related to minor changes to the preliminary site plan. In addition, due to the large deficiency in parking with respect to Zoning By-law 8600 a parking justification report was required. The Parking Justification Report was submitted on April 22, 2022. Transportation Planning reviewed the study but found some inaccuracies related to the interpretation of the parking requirements identified in Zoning By-law 8600. An updated memo regarding the TIS and a revised Parking Justification Report was submitted on May 25<sup>th</sup>, 2022.

The subject property is adjacent Sandwich Street (Class II Arterial in the Official Plan) and Chappell Avenue (local Road). There is sufficient width to accommodate the proposed development and no conveyance will be required.

The proposed development provides a good infill opportunity in an existing built-up area of the City and is an efficient use of the existing transportation network and provides the opportunity for additional ridership on the existing transit network. Active transportation options are proposed through the siting of sidewalks and bike lanes as part of a future Sandwich Street Road reconstruction project.

The amendments are consistent with PPS Policy 1.6.7.1, 1.6.7.2, and 1.6.7.4.

The proposed development can be appropriately designed and buffered via the implementation of the mitigation measures identified in the Acoustical Study and will not impact the long-term operation and economic role of the rail facilities. The rail corridor is within 300 metres from the proposed development. The amendments are consistent with PPS Policy 1.6.9.1.

Policies 3.0 states:

*Development shall be directed away from areas of natural or human-made hazards where there is an unacceptable risk to public health or safety or of property damage, and not create new or aggravate existing hazards.*

An Acoustical Report with mitigation measures and ESA have been completed identifying no natural or human-made hazards. However, the municipality will request a Class 4 area pursuant to Publication NPC-300 (MOECP Environmental Noise Guideline – Stationary and Transportation Sources – Approval and Planning) to ensure that existing Industrial uses existing and future facility needs are not impacted by the proposed mixed-use development (residential/commercial).

The amendments are consistent with PPS Policy 3.0



## Official Plan:

Relevant excerpts from the Official Plan are attached as Appendix F.

The subject property is located within the Sandwich Planning District, is designated Industrial on Schedule D: Land Use of the City of Windsor Official Plan, and is located within 300 metres of a Rail Corridor. We have circulated the Essex Terminal Railway. The Lou Romano Pollution Control Plant is over 500 metres away. Nevertheless, we have also circulated the Manager of the Lou Romano Pollution Control Plant.

Permitted uses in the Industrial designation include large physical sized facilities, outdoor storage of materials/products, multi-modal transportation facilities and service and repair facilities. Ancillary uses included open space, convenience stores and restaurants that serve employees in the industrial area, adult entertainment parlours, motor vehicle sales, club, athletic, and sports facilities, whole sale store, the sale of goods produced by an industrial use, and accessory to retail sale of building supplies and materials, home improvement products, and nursery products.

Notwithstanding the uses permitted in the Industrial designation (*Section 6.4.3.1 Industrial Policies*), the applicant is requesting a Site Specific Amendment to the City's Official Plan (OPA) to permit "mixed-use" as an additional permitted use within the Industrial designation.

Permitted uses in the Mixed Use designation include retail and service commercial establishments, offices, cultural, recreation and entertainment uses, and institutional, open space and residential uses, exclusive of small scale Low Profile residential development (*Section 6.9.2.1*). The proposed development is a combined-use building with retail commercial on the ground floor and residential uses above and is not defined as a small scale low profile residential development.

Section 3.2-Growth Concept in the City's Official Plan states that: "*Mixed use developments will be encouraged with strong pedestrian orientations and to support public transit. This concept will enable Windsor to continue its growth and foster a vibrant economy, while ensuring a safe, caring and diverse community and a sustainable, healthy environment.*" The mixed use proposal with residential above ground floor commercial/retail space is pedestrian oriented and transit supported where bike lanes are proposed and supports the following Official Plan policies:

- *Neighbourhood Housing Variety* (3.2.1.2) --the proposal encourages a range of housing types where people will have an opportunity to live in their neighbourhoods as they pass through various stages of their lives;
- *Distinctive Neighbourhood Character* (3.2.1.3) --Newly developing areas will be planned to foster their own unique neighbourhood identities with a mixture of homes, amenities and services. The subject lands is in an area of transition and the proposal provides an opportunity for limited commercial with residential uses above that will be compatible with the Sandwich Town neighbourhood;
- *Transportation System* (3.2.3.1) – the intent is to construct a mixed use development with an affordable housing choice with the target market aimed at

commuters working in Michigan and living in Windsor, as well as students attending the University of Windsor. This development helps Windsor achieve this goal of a more sustainable transportation system where businesses and services can be closer to home and all modes of transportation can play a more balanced role by providing opportunities for walking, cycling and transit.

The proposed development helps Council achieve the following land use goals because the proposal provides for an affordable residential infill opportunity on land that is in transition and has sat vacant for over fifteen (15) years and pedestrian oriented:

6.1.1 Safe, caring and diverse neighbourhoods

6.1.3 Housing suited to the needs of Windsor's residents.

6.1.10 Pedestrian oriented clusters of residential, commercial, employment and institutional uses.

In keeping with the Section 6.2.1.2-General Policies in the Official Plan the proposed mixed-use development is considered a High Profile development which is eleven (11) stories in height.

The PRR highlights how the proposed site specific OPA for the 'mixed use' proposal is in keeping with the Section 6.4 –Employment in the Official plan and is compatible in within this "Industrial" designation, which is also adjacent to other commercial and residential uses. The commercial uses located on the first floor will provide some employment opportunities and services to the surrounding industrial and residential uses. Infrastructure and public services are available to accommodate the proposed uses. The site is within an area of transition and has sat vacant for more than fifteen (15) years, where no industrial uses have been proposed for this site on record. The proposed development also satisfied the following objectives:

Section 6.4.1.3-*Compatible Development*—the expansion of commercial on the ground floor is compatible with the Tim Horton commercial use to the south and can provide services to the residential uses to the east and north, as well as provide services to the industrial uses to the west.

Section 6.4.1.4-*Range of Uses*—the commercial/retail will provide local convenience to the area.

Section 6.4.1.6-*Accessible*—the proposed development provided convenient access to all modes of transportation uses such as automobile (provides parking), bicycle (bike parking and bike lanes), pedestrians (sidewalks) and is in close proximity to existing and future transit services.

Section 6.4.1.8-*Infrastructure*—the site has full access to municipal services

The existing designation is "Industrial" the applicant is requesting a Site Specific Amendment to the Official Plan to include "Mixed-Use" as an additional permitted use. As identified in PRR the proposal is consistent with the following Mixed Use policies identified in Section 6.9 of the City's Official Plan:

Section 6.9.1.1—*Multi-Functional Area*—the proposal will provide residential and commercial uses and help service the proposed and existing residential and industrial uses in the area.

Section 6.9.1.2—*Compact Form*—the proposal will provide mixed use and compact given the size of the site.

Section 6.9.2.1—*Permitted Uses*—the proposal will provide commercial with residential uses on the site.

Section 6.9.2.3 states that mixed use development shall be located where there is access to a Class II Arterial Road or a Class I Collector Road, full municipal services can be provided, public transportation can be provided and the surrounding development pattern is compatible with the proposed development.

The parcel is located at the intersection of a Class II Arterial Road (Sandwich Street) and a Local Road (Chappell Avenue). Full municipal services are available and matters such as storm and sanitary sewer capacity will be further reviewed during site plan control. The site is adjacent to three bus routes and additional bus routes are located within 500 m. The parcel is located at the corner of one major road (the Sandwich Street Corridor) and is large enough to accommodate the proposed development in a manner that is compatible with the surrounding development pattern.

Public transit is available via the Crosstown 2. The closest bus stop to the subject lands is located on Prince Road at the southeast corner of Peter Street and is within 500 metres from the subject lands. However, according to comments provided by Transit Windsor the Council approved Transit Master Plan will introduce a new secondary route along Sandwich Street in the area of the subject lands and will likely site a bus stop on Sandwich Street at Chappell Avenue, thereby providing a direct service to the proposed development. As part of the Sandwich Street Reconstruction Project new bike lanes are proposed on the west and east sides of Sandwich Street between Chappell Street and Hill Avenue. The UDB and preliminary Site Plan illustrates the location and siting of the building.

Section 6.9.2.4 lists criteria to be used in evaluating a mixed use development. The Acoustical Report and PRR submitted by the applicant conclude that with appropriate mitigation measures and the requested Official Plan Amendment, the proposed development is feasible despite being close to road and rail noise sources and a number of stationary noise sources. The subject lands are located within the 300 metre buffer area as identified in Section 7.2.8.8 (a)-*Development Adjacent to a Corridor*. However, the subject lands are beyond the 75 metre buffer area identified for development adjacent a Rail Corridor in Section 7.2.8.8 (b) of the Official Plan.

In addition, the criteria identified in Section 6.9.2.4 is feasible for the following reasons:

- Full municipal services can be provided. A functional FSR for storm and sanitary was provided by Baird/AE and because of the potential impact of the quality and quantity of run-off a Storm Water Management Plan will be required at the time of Site Plan Control.

- The applicant submitted a Traffic Impact Study (TIS). Based on Transportation Planning's review a memo clarifying information related to minor changes to the preliminary site plan is required. In addition, due to the large deficiency in parking with respect to Zoning By-law 8600 a parking study was required. The Parking Justification Report was submitted on April 22, 2022. Transportation Planning reviewed the study but found some inaccuracies related to the interpretation of the parking requirements identified in Zoning By-law 8600. An updated memo regarding the TIS and a revised Parking Justification Report was submitted on May 25<sup>th</sup>, 2022.
- The siting of the building located at the corner of Sandwich Street and Chappell Avenue provides pedestrian access and is pedestrian oriented. The UDB provides renderings highlighting the pedestrian oriented entrances and ground floor of the building. Through the Site Plan Control process, further attention will be given to the importance of pedestrian oriented entranceways to the building and site.
- Although the proposal is significantly taller than adjacent buildings, there are other tall or higher profile buildings in the Sandwich Neighbourhood and the building has been sited/oriented along the Sandwich Street corridor and setback from lower profile residential development. The UDB provides renderings that use materials (i.e. brick, steel, glass,) and colours found within the Sandwich Neighbourhood. Through the Site Plan Control process, the proposal will be further refined to be compatible with the neighbourhood and address the Sandwich Community Improvement Plan (CIP) Urban Design Guidelines.

Section 6.9.2.5 lists guidelines when evaluating a proposed design. The proposal will be further be evaluated through the Site Plan Control process to determine how the proposal addresses Section 8, Urban Design of the City's Official Plan and how the mass, scale, orientation, form, and siting of the development achieves a compact urban form and a pedestrian friendly environment. The UDB and number of residential units (150) with commercial units provides evidence of a compact urban form and pedestrian friendly environment.

Section 7.2.8.8 (a) requires the completion of a noise study for new development within 300 metres of a rail corridor. The noise study shall identify and recommend appropriate mitigation measures, if needed. Section 7.2.8.8 (b) requires the completion of a vibration study for new development within 75 metres of a rail corridor. The vibration study shall identify and recommend appropriate mitigation measures, if needed. The proposal is within 300 metres of the Essex Terminal Railway rail corridor. However, not within 75 metres of the rail corridor.

The matter of noise and vibration were discussed under PPS Policy 1.6.9.1. The Acoustical Report satisfies Section 7.2.8.8 (a) and Section 7.2.8.8 (b).

The PRR states that an Acoustical Report was prepared for the Site to address sensitive land uses (rail, road, and noise from nearby industrial uses) and recommendations were made in the report to provide mitigation measures such as warning clauses, and minimum sound transmission class (STC) for windows, doors, and walls leading to sensitive living areas. The Planning Department concurs with the

mitigation measures identified in the Acoustical Report and PRR. However, the municipality will also requests a Class 4 area designation pursuant to Publication NPC-300 (MOECP Environmental Noise Guideline – Stationary and Transportation Sources – Approval and Planning) to ensure that existing Industrial uses existing and future facility needs are not impacted by the proposed mixed-use development (residential/commercial)

As previously stated, the applicant submitted an UDB with renderings that use materials (i.e. brick, steel, glass,) and colours found within the Sandwich Neighbourhood. Through the Site Plan Control process, the proposal will be further refined to be compatible with the neighbourhood, address the Sandwich CIP Urban Design Guidelines, and Section 8-Urban Design of the City's Official Plan.

When Official Plan Amendment 152 is approved, the requested zoning amendment will conform to the Zoning By-law Amendment Policies identified in Section 11.6.3.1—*Amendment Must Conform* and 11.6.3.3—*Evaluation Criteria*, of the Official Plan and conform to the general direction of the Official Plan.

### **Zoning By-Law:**

Relevant excerpts from Zoning By-law 8600 are attached as Appendix G.

The applicant requested the following amendments:

- From Manufacturing District 1.4 (MD1.4) to Commercial District 2.1 (CD2.1) zoning category for the property municipal known as 0 Sandwich Street;
- From a Development Reserve Residential District 1.1 (DRD 1.1) to Commercial District 2.1 (CD2.1) zoning category for Lot 28 East Side on Registered Plan 40 (PIN 012580190) known municipally as 3885 Sandwich Street situated north of the northeast corner of Sandwich Street; and
- A Site Specific Amendments to the Commercial District 2.1 (CD2.1) zoning category for all subject lands concerning a decrease in the minimum parking spaces required from 216 spaces to 156 spaces.
- A Site Specific Amendment resulting in an increase in the maximum building height from 14.0 metres to a total of 37 metres.

Regarding the Building Height there are some concerns based on renderings and the UDB related to the height of the ground floor based on the height of the residential storeys. Typically, the ground floor commercial storey is taller then residential storeys to give the building a sense of presence and to provide for a more pedestrian environment. This detail can be worked out through the Site Plan Control process.

The mitigation measures identified in the Noise Study and the various requirements of municipal departments and external agencies will be implemented and/or incorporated through the site plan review process.

### **Risk Analysis:**

N/A

## **Climate Change Risks**

### **Climate Change Mitigation:**

The redevelopment of the subject site contributes to the revitalization of the Sandwich Town Neighbourhood. The new development is compact and provides multifamily residential units, promotes walking and other alternative modes of transportation, thereby contributing to a complete community. The construction of the new building will utilize modern building methods, which will conform to the Ontario Building Code concerning safety and energy efficiency.

Utilizing an existing site in a built-up area of the City also promotes efficiency on the existing infrastructure network by not promoting development on greenfield land.

### **Climate Change Adaptation:**

As temperatures increase and when considering the Urban Heat Island effect for the City of Windsor, the property does not appear to be located within a Heat Vulnerability area. However, the rehabilitation of the existing site and construction of the new building will utilize modern building methods, which will conform to the Ontario Building Code concerning energy efficiency.

### **Financial Matters:**

Once the development is complete and the subject property reassessed the development will increase the tax assessment on the property. The applicant has also applied for grants through the Sandwich CIP. This Report will be sent to the Committee and Council on a future agenda.

### **Consultations:**

Comments received from municipal departments and external agencies are attached as Appendix H Existing and Surrounding Photos are attached as Appendix A. The Planning Department noted some minor gaps in the Planning Rational Report (PRR) submitted November 30<sup>th</sup>, 2002, as a result of changes to the Preliminary Site Plan. Pillon Abbs Inc. submitted a revised PRR dated on May 25<sup>th</sup>, 2022 that satisfies the requirements of the Planning Department.

BairdAE and Transportation Planning have discussed providing a memo to clarify information in the Traffic Impact Study (TIS) and a revised Parking Justification Report (PJR) will be submitted for review by municipal staff. An updated memo regarding the TIS and a revised Parking Justification Report was submitted on May 25<sup>th</sup>, 2022. Measures identified in the TIS will be considered during Site Plan Review.

The various requirements of municipal departments and external agencies will be considered and/or incorporated during the Site Plan Review.

On behalf of the owners, Tunio Development, Pillon Abbs Inc. hosted a virtual Public Open House held on Thursday December 16<sup>th</sup>, 2021, from 6:00PM to 7:00PM. This public open house was held in addition to the statutory public meeting required under the Ontario Planning Act. Surrounding landowners, which included owners of industrial, commercial, and residential uses, were sent the public notice via Canada Post. The Sandwich Town BIA and Ward Councillor were also sent the notice of the

virtual Public Open House. A total of 50 properties were provided notice, which represents 120 m radius of the Site.

In addition to City of Windsor Staff, Ward 1 Councillor, Planning Consultant, BIA Representative, Developer, and Architect a total of one (1) person from the public attended. The open house provided members of the public with opportunities to review and comment on the proposed development. Overall, the proposed development was supported. The following topics were discussed:

Fencing- there is existing fencing along the residential properties that front onto Peter Street. The owner of the adjacent property would like it to remain due to concerns from vandalism that occurred in the past. Through the Site Plan Control process the developer will work with the City on the final design, style, and material used for new fencing.

Garbage-concerns regarding garbage being left on the subject property. Through the Site Plan Control process, the location for garbage enclosures will be provided on site.

Commercial Uses—The BIA enquired about the size of commercial and residential space and their willingness to help the developer identify commercial uses. The BIA suggested a grocery store as a use for the commercial space. The staff Planner will review the CD2.1 zoning to limit commercial uses that may negatively impact the BIA area and Sandwich Town Main Street.

Road Improvements—The developer discussed future road improvements, bike lanes and parking spaces being included on the concept plan.

Public Notice: The Statutory notice was advertised in the Windsor Star (a local daily newspaper) on Friday, May 13th, 2022. A courtesy notice was mailed to property owners and residents within 120m of the subject lands.

### **Planner's Opinion:**

The *Planning Act* requires that a decision of Council in respect of the exercise of any authority that affects a planning matter, “*shall be consistent with*” Provincial Policy Statement 2020. The requested official plan and zoning amendments have been evaluated for consistency with the Provincial Policy Statement 2020 and conformity with the policies of the City of Windsor Official Plan.

Based on the information presented in this report, it is my opinion that the requested amendment to the City of Windsor Official Plan, to permit uses permitted under Section 6.9.2.1 as additional permitted uses such as retail, service commercial, offices, cultural, recreation and entertainment uses, and institutional, open space and residential uses, exclusive of small scale Low Profile residential development, within 300 m of a rail yard, is consistent with the PPS 2020 and is in conformity with the City of Windsor Official Plan when the mitigation measures identified in the Acoustical Report are implemented during site plan review.

The requested amendment to Zoning By-law 8600 is consistent with PPS 2020 and will be in conformity with the City of Windsor Official Plan when OPA 152 is approved.



A *Multiple Dwelling* and *Dwelling Units in a Combined Use Building* is compatible with existing and permitted uses on the subject parcel and with the surrounding neighbourhood. The mitigation measures identified in the Acoustical Report shall be implemented during the site plan review process.

**Conclusion:**

It is recommended that Official Plan Amendment 152 adding a site specific policy to the City of Windsor Official Plan and that an amendment to Zoning By-law 8600 changing the zoning of the subject lands from MD1.4, DRD1.1, and CD2.1, to CD2.1 with site specific provisions to allow the construction of a *Combined Use Building*, be approved.

**Planning Act Matters:**

Kevin Alexander, MCIP, RPP

Senior Planner – Special Projects

I concur with the above comments and opinion of the Registered Professional Planner.

Neil Robertson, MCIP, RPP

Thom Hunt, MCIP, RPP

Manager of Urban Design

City Planner

I am not a registered Planner and have reviewed as a Corporate Team Leader

JP                SAH

**Approvals:**

Name	Title
Neil Robertson	Manager of Urban Design / Deputy City Planner
Thom Hunt	City Planner / Executive Director, Planning & Development Services
Dana Paladino	Acting, Commissioner, Legal & Legislative Services
Jelena Payne	Commissioner, Economic Development & Innovation
Shelby Askin Hager	Chief Administrative Officer (A)

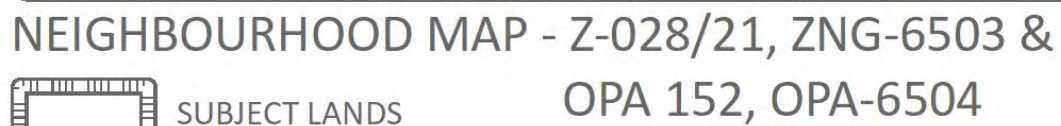
**Notifications:**

<b>Name</b>	<b>Address</b>	<b>Email</b>
Denis Demoulin		<a href="mailto:dumoulinden1@gmail.com">dumoulinden1@gmail.com</a>
Tunio Development		<a href="mailto:Khurramtunio@yahoo.com">Khurramtunio@yahoo.com</a>
Pillon Abbs Inc.		<a href="mailto:tpillonabbs@gmail.com">tpillonabbs@gmail.com</a>
Adam Meloche BAIRD/AE		<a href="mailto:AMeloche@bairdae.ca">AMeloche@bairdae.ca</a>
Shurjeel Tunio BAIRD/AE		<a href="mailto:stunio@bairdae.ca">stunio@bairdae.ca</a>
Thomas Coke, Coordinator, Sandwich Town BIA		<a href="mailto:tc_35@hotmail.com">tc_35@hotmail.com</a>
Mary Ann Cuderman, Chair, Sandwich Town BIA		<a href="mailto:macuderman@hotmail.com">macuderman@hotmail.com</a>
Councillor Costante		<a href="mailto:fcostante@citywindsor.ca">fcostante@citywindsor.ca</a>
Property owners and residents within 120 m of the subject lands		

**Appendices:**

- 1 Appendix A - Existing and Surrounding Land Uses
- 2 Appendix B - Preliminary Site Plan
- 3 Appendix C - 3D Renderings
- 4 Appendix D - Planning Rational Report
- 5 Appendix E - Excerpts from the PPS
- 6 Appendix F - Excerpts from the Official Plan
- 7 Appendix G - Excerpts from Zoning By-law 8600
- 8 Appendix H - Results of Department and Agencies Circulation
- 9 Appendix I - Draft By-Law Amendment
- 10 Appendix J - Acoustical Report
- 11 Appendix K - Functional Servicing Report
- 12 Appendix L - Urban Design Brief
- 13 Appendix M - Geotechnical Investigation
- 14 Appendix N – Parking Justification (**REVISED**)
- 15 Appendix O – Mixed Use Development (**REVISED**)

## EXISTING & SURROUNDING LAND USES



Consolidated  
ADDRESS: 0 & 3885 SANDWICH STREET



## APPENDIX 'A'

### EXISTING & SURROUNDING LAND USES



Subject Lands: Looking East at 3885 and 0 SANDWICH STREET



Subject Lands: Looking North along Chappell Avenue

## APPENDIX 'A'

### EXISTING & SURROUNDING LAND USES



SURROUNDING USES: Looking North



SURROUNDING USES: Residential uses Looking Southeast



## APPENDIX 'A'

### EXISTING & SURROUNDING LAND USES



SURROUNDING USE: Commercial use Looking South



SURROUNDING USE: Looking West

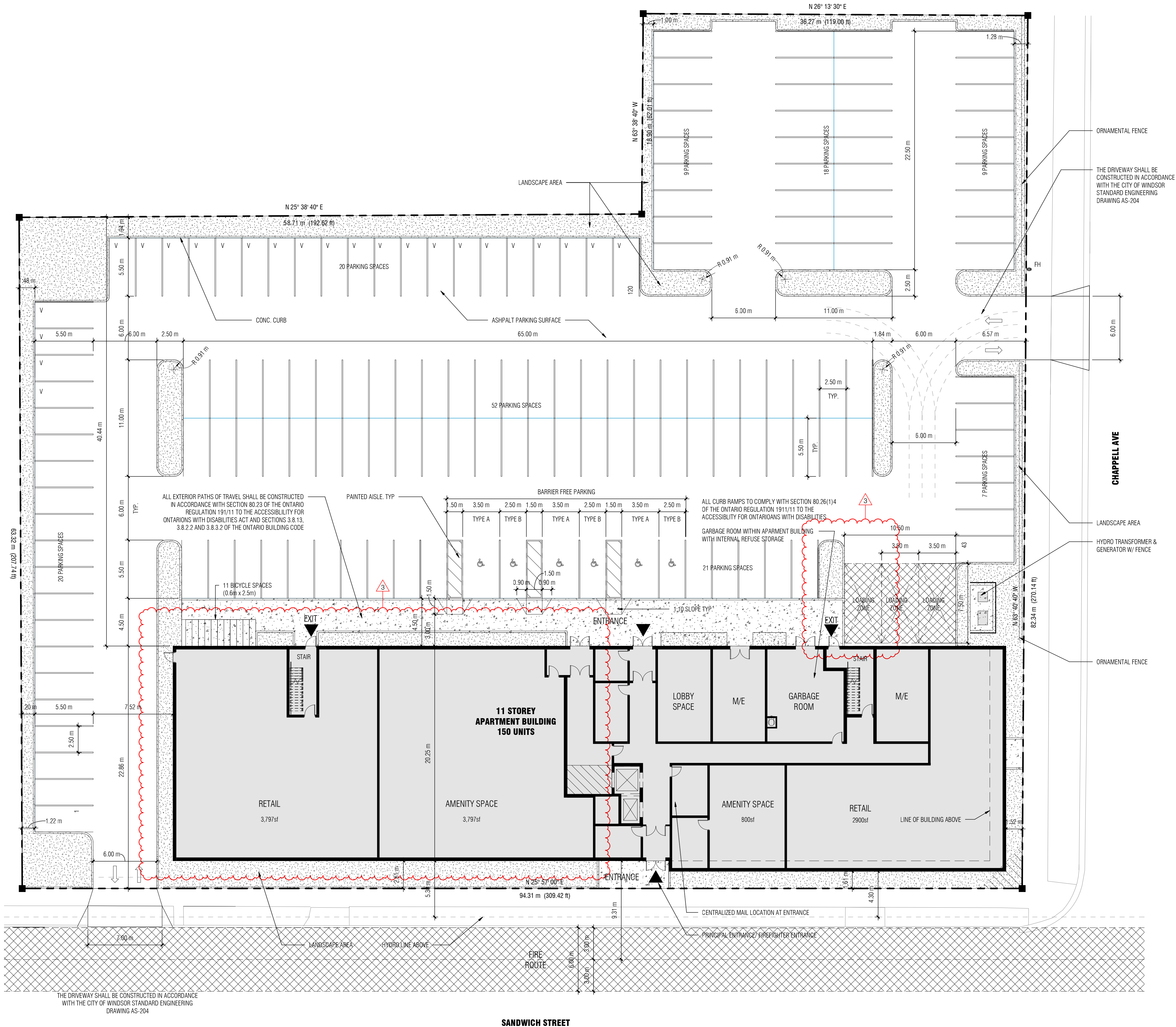
## APPENDIX 'A'

### EXISTING & SURROUNDING LAND USES



SURROUNDING USE: Looking Northwest





SITE DATA MATRIX			OBC REFERENCE
PROJECT DESCRIPTION: 11 STOREY MULTI UNIT RESIDENTIAL BUILDING 150 RESIDENTIAL UNITS			<input checked="" type="checkbox"/> NEW <input type="checkbox"/> ADDITION <input type="checkbox"/> ALTERATION <input type="checkbox"/> CHANGE OF USE
ZONING DESIGNATION: CD2.1			<input checked="" type="checkbox"/> PART 3 <input type="checkbox"/> PART 9 <input type="checkbox"/> PART 11
MAJOR OCCUPANCY: RESIDENTIAL			
BUILDING CLASSIFICATION: GROUP C			
SITE AREA		BUILDING AREA	GROSS AREA
EXISTING:	6,694m <sup>2</sup>	EXISTING:	N/A
PROPOSED:	6,694m <sup>2</sup>	PROPOSED:	1,622m <sup>2</sup>
TOTAL:	6,694m <sup>2</sup>	TOTAL:	19,122.04m <sup>2</sup>
LOT COVERAGE		MINIMUM LOT WIDTH	BUILDING HEIGHT
MAXIMUM:	N/A	REQUIRED:	N/A
PROVIDED:	1,623m <sup>2</sup>	PROVIDED:	94.8m
MINIMUM FRONT YARD DEPTH		MINIMUM REAR YARD DEPTH	MINIMUM SIDE YARD DEPTH
MAXIMUM:	N/A	REQUIRED:	N/A
PROVIDED:	1.61m	PROVIDED:	40.44m
PARKING		BICYCLE SPACES	LOADING SPACES
USE CLASSIFICATION		EXISTING:	0
EXISTING:	0	PROPOSED:	11
PROPOSED:	156 (6 BF, 3 TYPE A & 3 TYPE B) 1.04 RATIO	REQUIRED:	11
REQUIRED:	188 (6 BF, 3 TYPE A & 3 TYPE B) 1.25 RATIO	TOTAL:	11
TOTAL:	156 (6 BF, 3 TYPE A & 3 TYPE B) 1.04 RATIO		
LANDSCAPE AREA		AMENITY SPACE	SCREENING FENCE LENGTH
EXISTING:	N/A	REQUIRED:	900m <sup>2</sup>
PROPOSED:	868m <sup>2</sup>	PROPOSED:	1294m <sup>2</sup>
TOTAL:	868.56m <sup>2</sup>	TOTAL:	N/A
NOTE: 1. ALL EXTERIOR PATHS OF TRAVEL SHALL BE CONSTRUCTED IN ACCORDANCE WITH SECTION 80.23 OF ONTARIO REGULATION 191/11 TO THE ACCESSIBILITY FOR ONTARIANS WITH DISABILITIES ACT AND SECTIONS 3.8.1.3 AND 3.8.3.2 OF THE ONTARIO BUILDING CODE. 2. ALL CURB RAMP SHALL BE CONSTRUCTED IN ACCORDANCE WITH SECTION 80.26(1) OF THE ONTARIO REGULATION 191/11 TO THE ACCESSIBILITY FOR ONTARIANS WITH DISABILITIES ACT AND SECTION 3.8.3.1.8 OF THE ONTARIO BUILDING CODE.			

102 - 27 Princess Street  
Leamington ON N8H 2X8

BAIRDIAE

architecture + engineering

1000 - 267 Pelissier Street  
Windsor ON N9A 4K4

A DIVISION OF MATTHEW J. BAIRD CONSULTING SERVICES INC.

3 SITE REVISIONS 2 22.05.25

2 SITE REVISIONS 21.06.06

1 ISSUED FOR OPA/ZBA/SPC 20.11.09

No Description Date

Revision Schedule

SEAL

ONTARIO ASSOCIATION  
OF  
ARCHITECTS  
PAC WEDL  
LICENCE  
7289

MIXED-USE DEVELOPMENT

SANDWICH ST. WINDSOR ON

SHEET TITLE

SITE PLAN

20-028

SP-1

2022-05-25 3:23:07 PM

GENERAL NOTES:  
1. This drawing is NOT to be scaled.  
2. This drawing, as an instrument of service, is provided by and is the property of Baird AE.  
3. The contractor must verify and accept responsibility for all dimensions and conditions on site and must notify Baird AE of any variation from the supplied information.  
4. This disclaimer is not responsible for the accuracy of survey, and the other disciplines information shown on this drawing. Refer to the appropriate consultant's drawings before proceeding with the work.  
5. Construction must conform to all applicable codes and requirements of authorities having jurisdiction.  
6. The contractor working from drawings not specifically marked For Construction must assume full responsibility and bear costs for any corrections or damages resulting from his work.

Consolidated Council Agenda - June 13, 2022  
Page 324 of 1025



## APPENDIX 'C'

### 3D RENDERINGS





## APPENDIX 'C'

### 3D RENDERINGS





## APPENDIX 'C'

### 3D RENDERINGS





## APPENDIX 'C'

### 3D RENDERINGS





## APPENDIX 'C'

### 3D RENDERINGS





## APPENDIX 'C'

### 3D RENDERINGS





## APPENDIX 'C'

### 3D RENDERINGS



# PLANNING RATIONALE REPORT

## OFFICIAL PLAN AND ZONING BY-LAW AMENDMENT PROPOSED MIXED-USE DEVELOPMENT

**0 and 3885 Sandwich Street  
City of Windsor, Ontario**

**May 25, 2022  
AS REVISED**

**Prepared by:**



Tracey Pillon-Abbs, RPP  
Principal Planner  
23669 Prince Albert Road  
Chatham, ON N7M 5J7  
226-340-1232  
tpillonabbs@gmail.com  
www.tpillonabbs.ca

# Table of Content

1.0	INTRODUCTION	3
2.0	SITE AND SURROUNDING LAND USES	4
2.1	Legal Description and Ownership	4
2.2	Physical Features of the Site	5
2.2.1	Size and Site Dimension	5
2.2.2	Existing Structures	5
2.2.3	Vegetation and Soil	6
2.2.4	Topography	6
2.2.5	Other Physical Features	6
2.2.6	Municipal Services	6
2.2.7	Nearby Amenities	7
2.3	Surrounding Land Uses	8
3.0	DEVELOPMENT PROPOSAL	11
3.1	Proposal	11
3.2	Public Consultation Strategy	14
4.0	PROPOSED APPLICATION & AMENDMENT	15
4.1	Official Plan Amendment	15
4.2	Zoning By-Law Amendment	15
4.3	Other Application	15
4.4	Supporting Studies	16
4.4.1	Urban Design	16
4.4.2	Environmental Site Assessment	16
4.4.3	Noise and Vibration	17
4.4.4	Geo-Technical Study	17
4.4.5	Functional Servicing Report	18
4.4.6	Transportation Impact Study	18
	0 & 3885 Sandwich Street, Windsor, Ontario	1

4.4.7 Archaeological Assessment	19
4.4.8 Parking	20
5.0 PLANNING ANALYSIS	21
5.1 Policy and Regulatory Overview	21
5.1.1 Provincial Policy Statement (PPS), 2020	21
5.1.2 Official Plan (OP)	33
5.1.3 Zoning By-law (ZBL)	48
6.0 SUMMARY AND CONCLUSION	54
6.1 Context and Site Suitability Summary	54
6.1.1 Site Suitability	54
6.1.2 Compatibility of Design	54
6.1.3 Good Planning	54
6.1.4 Natural Environment Impacts	55
6.1.5 Municipal Services Impacts	55
6.1.6 Social and/or Economic Conditions	55
6.2 Conclusion	55

# 1.0 INTRODUCTION

I have been retained by the owner/applicant, Khurram Tunio (Tunio Development Inc.), to provide a land use Planning Rationale Report (PRR) in support of a proposed mixed-use development located at 0 Sandwich Street and 3885 Sandwich Street (herein the “Site”) in the City of Windsor, Ontario.

The purpose of this report is to review the relevant land use documents, including Provincial Policy Statement (PPS) 2020, the City of Windsor Official Plan (OP) and the City of Windsor Zoning By-law (ZBL).

The Site is currently vacant and is in an area of transition whereby fewer industrial activities are occurring, which is creating an attractive area for a small commercial node with residential uses.

It is proposed to use the Site for mixed use with commercial on the main floor and residential above.

The proposed commercial will provide for employment opportunities.

The residential uses will offer a new housing choice in the area, which will be constructed to be affordable with a target market for international commuters working in Michigan and living in Windsor as well as students attending the University of Windsor.

It is proposed to construct an eleven (11) storey combined use building with one-hundred and fifty (150) affordable residential units above in the tenure form of rentals and condominiums. The total commercial space proposed is 622.17 m<sup>2</sup>. Parking for 156 spaces is proposed to be located on-site to serve both the commercial and residential uses. The development is expected to be completed in 2022.

The proposal adheres to the design direction of the Old Sandwich Town Community Improvement Plan (CIP).

A site specific Official Plan Amendment (OPA) and site specific Zoning By-law Amendment (ZBA) is required in support of the proposed development.

Exemption from the provisions of Interim Control By-law 103/2020 is also requested.

Once the OPA and ZBA have been approved, the applicant will proceed with a Site Plan Control (SPC) Application and a Plan of Condominium Application.

Pre-submission was completed by the applicant (City File #PS-031/20) in addition to a meeting with City Administration on March 23, 2021. Comments were received and have been incorporated into this PRR.

This PRR will show that the proposed development is suitable intensification of affordable residential with commercial use, is consistent with the PPS, conforms to the intent and purpose of the City of Windsor OP and represents good planning.

## 2.0 SITE AND SURROUNDING LAND USES

### 2.1 Legal Description and Ownership

The Site is owned by Khurram Tunio (Tunio Development Inc.) and made up of two (2) parcels located on a corner, on the north side of Chappell Avenue and the east side of Sandwich Street (see Figures 1a – Site Location, Street View 1a – Sandwich Street and Street View 1b – Chappell Street).



Figure 1a – Site Location



Street View 1a – Sandwich Street

0 & 3885 Sandwich Street, Windsor, Ontario





Street View 1b – Chappell Street

The 2 properties are legally described as Plan 40 PT Lot 28 East Side; & Pt Lot 28 West Side; Corner (ARN 050-170-09700-0000).

The Site located at 3885 Sandwich Street was first developed in approximately 1903 for use as a hotel and restaurant. The building was used as a restaurant, tavern, and hotel for the entirety of its 103-year lifespan, suffering one major fire in 1977 and another major fire in 2006. The building was demolished in 2006.

A residential house was identified at 0 Sandwich Street since (at least) 1924; the house was demolished by 1987.

## **2.2 Physical Features of the Site**

### **2.2.1 Size and Site Dimension**

The Site consists of a total area of approximately 6,694 m<sup>2</sup> with 82.34 m of frontage on the north side of Chappell Avenue and a depth of 94.31 m on east side of Sandwich Street.

The Site is an irregularly shaped corner lot.

### **2.2.2 Existing Structures**

The Site is currently vacant. There is an electric sign on the Site, which will be removed.



There are no mature trees on the Site other than some existing hedgerows along the fence line. Soil type is Burford Loam (Bg).

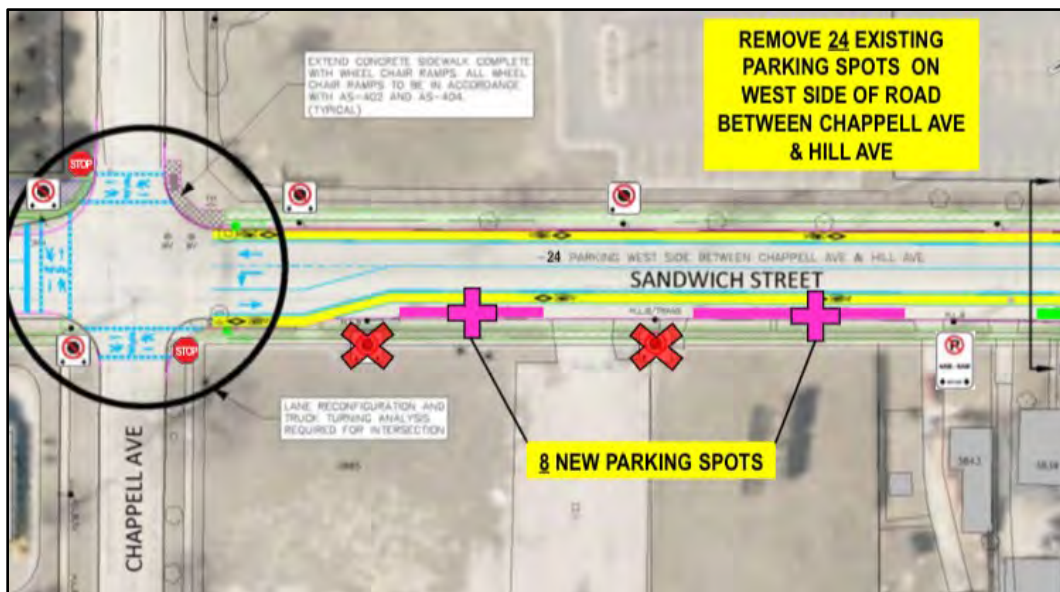
The Site is level and is outside the regulated area of the Essex Region Conservation Authority (ERCA).

The property currently has three driveways, sidewalks and a parking area left from the previous uses. Abutting land uses are buffered with existing fencing.

The property has access to municipal water, storm and sanitary services.

There are existing on-street parking spaces on Chappell Street.

The City of Windsor is proposing new bike lanes and parking spaces on the west side of Sandwich Street between Chappell Street and Hill Avenue as part of the Sandwich Street Reconstruction Project (see Figure 1b – Proposed Improvements).



### Figure 1b – Proposed Improvements

Sandwich Street is a two-lane urban north-south arterial roadway with posted speed limit of 50 km/h at the close proximity to the development. The road turns into Ojibway parkway 500m west of the development with a speed limit of 70km/h.

Chappell Street is an east-west two lane local roadway extending from Peter Street to Russell Street. It has a posted 50 km/h speed limit, with on-street parking permitted on both sides. It is stop controlled on its approach to the intersection with Sandwich Street.

The Site is in close proximity to major roadways, including Highway 3 to the northeast and Highway 401 to the south.

The Site has access to transit with the closest bus stop on Prince Road at Peter Street (Route 2).

The Site is located approximately 600 m from the Lou Romano Water Reclamation Plant.

### **2.2.7 Nearby Amenities**

There are several schools nearby, including General Brock Public School and Marlborough Public School.

There are many parks and recreation opportunities in close proximity to the Site, including Mic Mac Park, Novelletto Rosati Sports and Recreation Complex and Black Oak Heritage Park.

There is nearby shopping in the form of plazas and malls as well as employment, places of worship and local/regional amenities.

## 2.3 Surrounding Land Uses

Overall, the Site is located within an existing mixed-use area.

**North** – The lands directly north of the Site are used for low density residential (see Photo 1 - North). Those dwellings are located along Sandwich Street.



Photo 1 - North

**South** – The lands directly south of the Site are used for low density residential (see Photo 2a and 2b - South). Those dwellings are located along Chappell Avenue. Tim Hortons is located to the west, at the corner.



Photo 2a - South





Photo 2b - South

**East** – The lands east of the Site are used for low density residential (see Photo 3 - East). Those dwellings back onto the Site and are located on Peter Street.



Photo 3 - East

**West** – The lands west of the Site are used for commercial/industrial (see Photo 4 - West). Those properties are located along Sandwich Street and across from the Site.



Photo 4 - West

## **3.0 DEVELOPMENT PROPOSAL**

### **3.1 Proposal**

The applicant proposes to develop the Site for mixed-use purposes.

The Site is in an area of transition whereby fewer industrial activities are occurring in the area, which is creating an attractive area for a small commercial node with residential uses.

Through the use of materials that play homage to the sandwich heritage conservation district, the proposal aims to build the link between the southern gateway and the established commercial core. Attention was given to the residential and commercial entrances to establish a pedestrian scale through architectural features and landscape interventions.

The intent is to construct a combined use building with commercial on the main floor and residential above. The proposed commercial will provide for employment opportunities.

The proposed residential use will provide an affordable housing choice with a target market for international commuters working in Michigan and living in Windsor as well as students attending the University of Windsor.

A Concept Plan has been prepared by BairdAE architect and engineering, dated May 25, 2022 (see Figure 2a – Concept Plan).

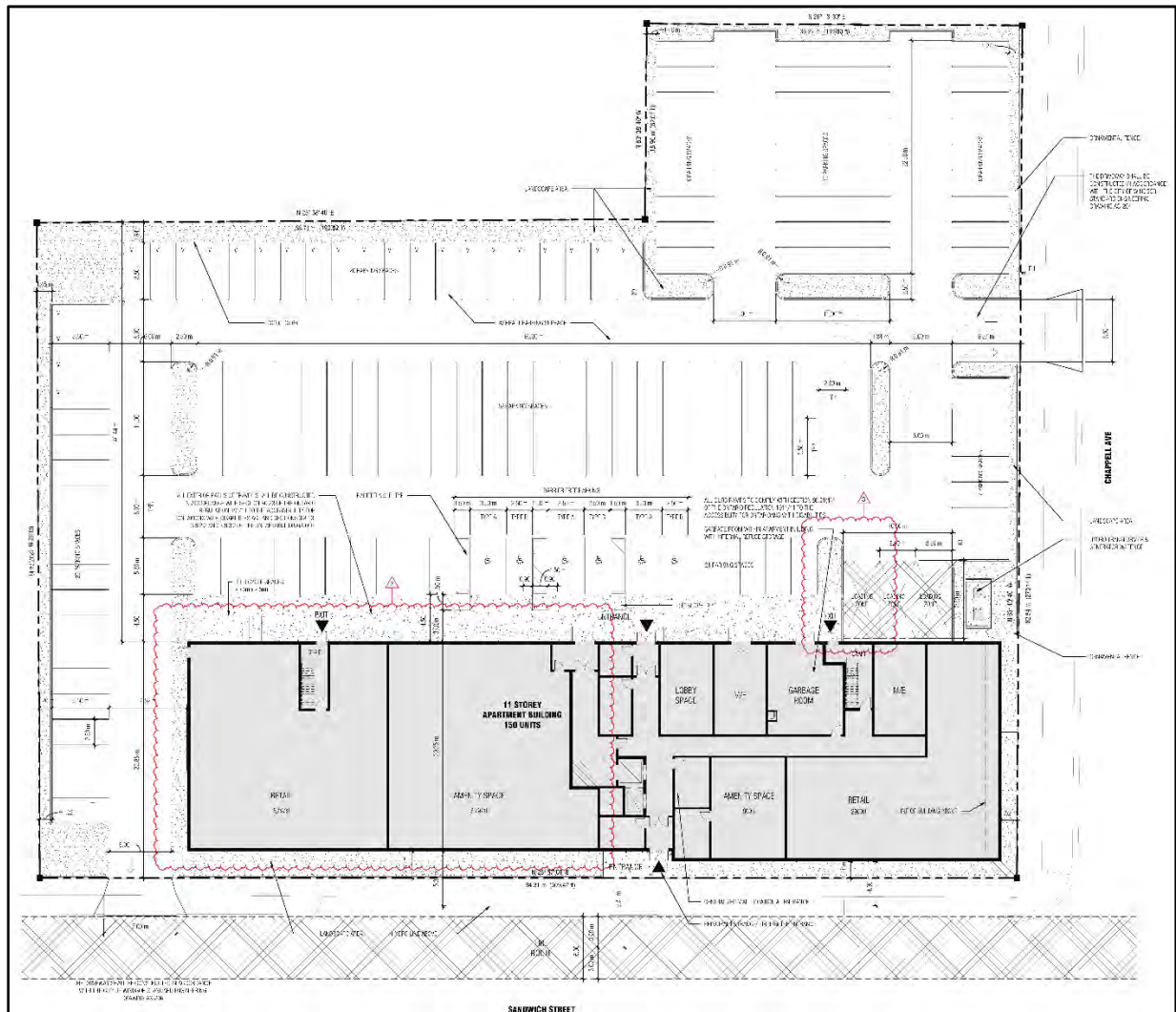


Figure 2a – Concept Plan

The proposed building will provide for 150 residential units with 70 single bedroom units and 80 double bedroom units on floors 2 to 11. There will be 15 units located on each floor. Unit sizes will range from 54.16 m<sup>2</sup> to 98.1 m<sup>2</sup>.

Two retail spaces are provided for a total area of 622.17 m<sup>2</sup>. One space is proposed to be 352.75 m<sup>2</sup>, and the second is proposed to be 269.42 m<sup>2</sup>.

The total building area is proposed to be 1,622 m<sup>2</sup> which will result in a proposed lot coverage of 24.23%. The proposed total net density is 224.21 units/ha. The proposed setback from Sandwich Street is 4.30 m and from Chappell Avenue is 1.52 m.



The building will be 11 storeys and 37 m in height and will face Sandwich Street (see Figure 2b – Elevation).



Figure 2b - Elevation

Entrances to the residential units will be from the east and west of the proposed building into a common hallway.

Both commercial spaces will have pedestrian entrances from the lobby in addition to access from Chappell Ave and the east side of the building and will be visible from the street level.

The lobby space, centralized mail, elevator, garbage room and the mechanical room will be located on the main floor.

Amenity space for the residential dwellings includes outdoor seating and a 1,294 m<sup>2</sup> main floor common area. Private balconies will also be provided.

The Site will be landscaped with key features such as trees along the side of the building and buffering around the parking area.

A total of 3 loading spaces are proposed to be located on the southeast corner of the proposed building.

Two new accesses will be provided to the Site. The first access is located about 95 m north of the intersection of Sandwich Street West and Chappell Avenue and the second access is located 64m east of the intersection. The Chappell Avenue access will be 7.08 m wide and the Sandwich Street access will be 7 m wide.

Parking will be provided on-site, back from the street, with a total of 156 spaces to serve both the commercial and residential uses.

A total of 15 % of the parking spaces will be marked for visitor parking.

A total of 11 bicycle spaces are provided along the northeast corner of the proposed building.

## **3.2 Public Consultation Strategy**

The Planning Act requires that the applicant submit a proposed strategy for public consultation with respect to an application as part of the complete application requirements.

As part of a public consultation strategy, the applicant proposes a virtual open house in addition to the required public meeting.

A summary of the open house has been provided as an addendum to this PRR.

## **4.0 PROPOSED APPLICATION & AMENDMENT**

The proposed development requires an application for Official Plan Amendment (OPA) and an application for Zoning By-law Amendment (ZBA). The following explains the amendment and application.

### **4.1 Official Plan Amendment**

A site specific Official Plan Amendment (OPA) is required in support of the proposed mixed-use development.

The OPA will change the land use designation from “Industrial” to site specific “Mixed Use” which is located on Schedule D: Land Use to permit a combined use building.

The OPA is detailed, and the justification is set out in Section 5.1.2 of this PRR.

### **4.2 Zoning By-Law Amendment**

A site specific Zoning By-law Amendment (ZBA) is required in support of the proposed mixed-use development.

The zoning for the Site is proposed to be changed from Commercial District 2.1 (CD2.1), Manufacturing District 1.4 (MD1.4) and Development Reserve District 1.1 (DRD1.1) category to a site specific Commercial District 2.1 (CD2.1 - S.20(1)(XXX)) category as shown on Map 4 of the City of Windsor Zoning By-law (ZBL) to permit a combined use building.

Relief is also requested for certain provisions set out in the CD2.1 zone.

The ZBA is detailed, and the justification is set out in Section 5.1.3 of this PRR.

### **4.3 Other Application**

Exemption from the provisions of Interim Control By-law 103/2020 is also requested.

Once the OPA and ZBA have been approved, the applicant will proceed with a Site Plan Control (SPC) Application which will set out the lighting, buffering, landscaping, signage, etc. The proposed development will be subject to a Development Agreement, which will include any required fees or securities if required.

An application for Plan of Condominium will be applied for after SPC approval in order to have rented and owned units. A building permit will be the final application.

## 4.4 Supporting Studies

The following supporting studies have been prepared to support the applications.

### 4.4.1 Urban Design

An Urban Design Brief (UDB) was prepared by BairdAE architecture and engineering, dated May 27, 2021.

The UDB is a tool to address policies set out in the OP and reinforce the ZBA through recommendations and include both 0 and 3885 Sandwich Street.

The UDB included an illustration of the building façade, building form and landscaping area.

It was concluded that;

- The proposal adheres to the design direction of Old Sandwich Town CIP and the OP.
- The proposal addressed site design and orientation, built form, public realm, landscape design, and architectural design and will be of high quality to meet the City of Windsor Standards.
- This project will be a missing link for Sandwich Town, linking and marking the arrival and exit of the neighbourhood.

A revised report was prepared, dated September 2, 2021, to address additional illustrations. Recommendations have been made, including scale, landscape features, setbacks, building orientation, design and the inclusion of retail.

### 4.4.2 Environmental Site Assessment

A Phase One Environmental Site Assessment (ESA) was completed by Wood Environment & Infrastructure Solutions, dated April 3, 2020.

The report evaluated known and possible environmental issues for properties located at 0 and 3885 Sandwich Street. The Phase One Assessment includes the 2 parcels of land.

Only 1 parcel, 3885 Sandwich Street, requires a Record of Site Condition (RSC) filing due to the change in use from its former commercial use to proposed residential use.

Based on the report, areas of potential environmental concern (APECs) were identified on 3885 Sandwich Street resulting from potentially contaminating activities (PCAs) associated with the possible infilling on the property with fill of unknown quality, the historic fire, and the various off-site current and historic industrial operations.

The report recommended that a Phase Two ESA on 3885 Sandwich Street is required to address these APECs and support an RSC filing.

The Phase Two ESA was completed on January 28, 2021.

Ontario Ministry of The Environment (MOE) has provided written acknowledgement that Record of Site Condition #228986 has been filed.

#### **4.4.3 Noise and Vibration**

An Acoustical Report was prepared by BairdAE architecture and engineering, dated June 21, 2021.

The study area included both 0 and 3885 Sandwich Street.

The study was based on an initial investigation; the primary noise affecting the development is from nearby industries as it pertains to rail noise and roadway traffic noise. The air traffic noise was not considered, as the development is located outside the zone of influence of local airports.

The report concluded that mitigation measures are required to bring residential units within the development into compliance with MOECC criteria. With the inclusion of these measures, MOECC noise criteria will be satisfied.

Recommendations included the following:

- The dwellings shall include warning clauses.
- All windows leading to sensitive living areas are to have a minimum sound transmission class (STC).
- All doors leading to sensitive living areas are to have a minimum sound transmission class (STC).
- All walls leading to sensitive living areas are to have a minimum sound transmission class (STC).
- Acoustic privacy between units in a multi-tenant building, the inter-unit wall, should meet or exceed STC-50. Wall separation between noisy spaces, such as refuse chutes or elevator shafts, and suites should meet or exceed STC-55.
- Warning clause for all units "Purchasers/tenants are advised that due to close proximity of the adjacent industries, noise from said industries may at times be audible."
- Prior to the issuance of building permits, it is recommended that an acoustical consultant review the sound transmission class (STC) for the proposed development's walls, windows and doors to ensure they conform to the recommendations outlined in this report.

It was concluded that the proposed development could, with the implementation of the recommendations, be designed to address impacts from surrounding noise sources.

The report was further updated, dated October 1, 2021, to include additional information regarding the MOECC D6 guidelines.

#### **4.4.4 Geo-Technical Study**

A Geo-Technical Study has been prepared by CT Soils and Materials Engineering Inc, Consulting Engineers, dated May 21, 2021.

The study area included both 0 and 3885 Sandwich Street.

The study is required due to the location of the nearby industrial and active salt solution mining operations.

The study provided recommendations for construction.

#### **4.4.5 Functional Servicing Report**

A Functional Servicing Report (FSR) was prepared by BairdAE architecture and engineering, dated October 8, 2020.

The study area included both 0 and 3885 Sandwich Street.

The report provided a review and identified servicing requirements for the proposed development.

The report concluded and recommended the following:

**Sanitary** – a new 200mm diameter sanitary service will be provided to the development from the existing municipal sewer from Chappell Avenue.

**Watermain** – one new 150mm diameter water service will be provided to the development from the existing 200mm watermain on Chappell Avenue. The water line will split into two at the eastern façade of the building for 150mm fire and 100mm diameter domestic service.

**Storm** – the post-development peak flows from all events from the Site will be controlled to the peak flow from target pre-development conditions. Whereas, during 100-year storm event, the maximum water depth is less than 300mm.

Stormwater quality and quantity are addressed using Armtec Defender water quality unit (FD5HC) and 150mm orifice pipe at MH 4.

**Erosion and Sediment Control** – control measures are to be implemented during construction, and detail will be provided in the tender documents.

#### **4.4.6 Transportation Impact Study**

A Traffic Impact Study (TIS) was prepared by BairdAE architecture and engineering.

The study area included both 0 and 3885 Sandwich Street.

A transportation analysis was completed to determine the existing and future operating conditions of intersection and individual turning movements.

The evaluation included the following:

- The proposed 11-storey high rise apartment building will have 150 units which will generate 1113 daily; 94 inbound traffic and 100 outbound traffic.
- It is assumed that the development will be completed by 2022.
- The background growth rate is considered in the analysis as it represents the worst-case scenario i.e. 3%.
- Under existing and future background conditions, the study area intersections operate at an acceptable level of service during morning and evening peaks. However, under 2032 existing conditions, the westbound turning lanes level of service is D. This delay is due to stop control and higher volumes on Sandwich Street. However, there is sufficient capacity available for this movement ( $v/c = 0.06$ ), indicating sufficient gaps are available; hence no mitigation measures are required.
- Under the 2022 future post-development condition, the intersections are expected to operate at an acceptable level of service during peak hours.
- Under the 2027 and 2032 future post-development conditions, the intersection of Sandwich Street and Chappell Avenue are expected to operate at an acceptable level of service during peak hours. However, the westbound turning movement at the intersection is forecast to have longer delays i.e. LOS D in 2027 and LOS E in 2032. However, sufficient gaps are available to accommodate this movement. Hence no improvements are required.
- All other intersection operates at an acceptable level of service in 2022, 2027 and 2032 post-development conditions.
- The warrant for signalization is not required at the intersection of Sandwich Street and Chappell Avenue for the 2032 post-development condition. It is expected that the City will continue to monitor traffic at this location.
- An adequate sight line distance is provided for a safe departure from the development.

It is concluded that no mitigation measures or improvements are required.

#### **4.4.7 Archaeological Assessment**

A stage 1 and 2 archaeological assessment was prepared by Earthworks Archaeological Services Inc., dated April 15, 2020.

The study area included both 0 and 3885 Sandwich Street.

The location of the study area within 200 metres of historically mapped marshlands attached to the Detroit River suggests there is potential to locate Pre and Post Contact Indigenous archaeological resources.

The report concluded that the archaeological survey did not yield any evidence of archaeological material. As a result, no additional archaeological assessments are required.

The report, which has been submitted to this ministry as a condition of licensing in accordance with Part VI of the *Ontario Heritage Act*, R.S.O. 1990, c 0.18, has been entered into the Ontario Public Register of Archaeological Reports.



#### **4.4.8 Parking**

A Parking Justification Report was prepared by BairdAE architect and engineering.

The study was prepared to determine the adequacy of the parking supply to meet the requirements of the proposed development.

It was concluded that the available existing and provided parking spaces are satisfactory to meet the City's by-law.

No further changes to parking spaces will be required.

## 5.0 PLANNING ANALYSIS

### 5.1 Policy and Regulatory Overview

#### 5.1.1 Provincial Policy Statement (PPS), 2020

The Provincial Policy Statement (PPS) provides policy direction on matters of provincial interest related to land use planning and development providing for appropriate development while protecting resources of provincial interest, public health and safety, and the quality of the natural and built environments.

The PPS is issued under Section 3 of the Planning Act and came into effect on May 1, 2020. It applies to all land use planning matters considered after this date.

The PPS supports improved land use planning and management, which contributes to a more effective and efficient land use planning system.

The following provides a summary of the key policy considerations of the PPS as it relates to the proposed development.

PPS Policy #	Policy	Response
1.0	.....Ontario's long-term prosperity, environmental health and social well-being depend on wisely managing change and promoting efficient land use and development patterns.....	Windsor has directed growth where the Site is located which will contribute positively to promoting efficient land use and development patterns.
1.1.1	Healthy, liveable and safe communities are sustained by:  a) promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term;  b) accommodating an appropriate affordable and market-based range and mix	The proposed development is consistent with the policy to build strong, healthy and livable communities. It provides for employment opportunities and a new affordable housing choice.  There are no environmental or public health and safety concerns as the area is established.

PPS Policy #	Policy	Response
	<p>of residential types, employment, institutional, recreation, park and open space, and other uses to meet long-term needs;</p> <p>c) avoiding development and land use patterns which may cause environmental or public health and safety concerns;</p> <p>d) avoiding development and land use patterns that would prevent the efficient expansion of settlement areas in those areas which are adjacent or close to settlement areas;</p> <p>e) promoting.....cost-effective development patterns and standards to minimize land consumption and servicing costs;</p> <p>f) improving accessibility for persons with disabilities and older persons by addressing land use barriers which restrict their full participation in society;</p> <p>h) promoting development and land use patterns that conserve biodiversity.</p>	<p>The development pattern does not require expansion of the settlement area as it is considered infilling.</p> <p>The Site has access to full municipal services and is close to existing local parks, places of worship, and schools.</p> <p>Accessibility of units will be addressed at the time of the building permit.</p> <p>Public service facilities are available, such as local schools and transit.</p> <p>The development pattern is proposed to be an efficient use of the vacant property.</p>
1.1.2	Sufficient land shall be made available to accommodate an appropriate range and mix of land uses to meet projected needs for a time horizon of up to 25 years.	The proposed development will help Windsor meet the full range of current and future residential and commercial needs through intensification.

PPS Policy #	Policy	Response
	Within settlement areas, sufficient land shall be made available through intensification and redevelopment and, if necessary, designated growth areas.	The Site will provide for residential infilling within an existing settlement area in the form of a new affordable housing choice and employment opportunities.
1.1.3.1	Settlement areas shall be the focus of growth and development.	The proposal enhances the vitality of the municipality, as the proposal is within an existing settlement area.
1.1.3.2	<p>Land use patterns within settlement areas shall be based on densities and a mix of land uses which:</p> <ul style="list-style-type: none"> <li>a) efficiently use land and resources;</li> <li>b) are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion;</li> <li>c) minimize negative impacts to air quality and climate change, and promote energy efficiency;</li> <li>d) prepare for the impacts of a changing climate;</li> <li>e) support active transportation;</li> </ul>	<p>The total density of the proposed development is considered appropriate as most of the existing area is a mix of uses.</p> <p>The Site offers an opportunity for intensification by creating a new mixed-use building using the vacant property.</p> <p>The design and style of building will blend well with the scale and massing of the existing surrounding area and indicated by the UDB illustration.</p> <p>Residents will have immediate access to shopping, employment, trails, active transportation, recreational areas and institutional uses.</p> <p>Transit is available for the area.</p>

PPS Policy #	Policy	Response
	<p>f) are transit-supportive, where transit is planned, exists or may be developed; and</p> <p>g) are freight-supportive.</p>	<p>The Site is located close to Highways 3 and 401.</p>
1.1.3.3	<p>Planning authorities shall identify appropriate locations and promote opportunities for transit-supportive development, accommodating a significant supply and range of housing options through intensification and redevelopment where this can be accommodated taking into account existing building stock or areas, including brownfield sites, and the availability of suitable existing or planned infrastructure and public service facilities required to accommodate projected needs.</p>	<p>The development is a Site that is physically suitable as it pertains to size and location.</p> <p>The Site is 6,694 m<sup>2</sup> in area and is located on a corner lot. The Site is an irregular shaped lot.</p> <p>The intensification can be accommodated for the proposed mixed-use development as it is an appropriate use of a vacant parcel of land.</p> <p>The Site is level which is conducive to easy vehicular movements.</p> <p>Parking will be provided on-site, including space designated for visitors. Bicycle parking is also provided.</p> <p>Parking will be located back from the street, screened by an ornamental fence.</p> <p>There are existing on-street parking spaces on Chappell Street. The City of Windsor is proposing new bike lanes and parking spaces on the west side of Sandwich Street between Chappell Street and Hill Avenue.</p>

PPS Policy #	Policy	Response
1.1.3.4	Appropriate development standards should be promoted which facilitate intensification, redevelopment and compact form, while avoiding or mitigating risks to public health and safety.	<p>The proposed mixed-use building will be built with a high standard of construction, allowing seamless integration with the existing area.</p> <p>The building will face Sandwich Street with a view of the Detroit River.</p> <p>There will be no risks to the public as identified in the ESA and TIS.</p>
1.1.3.5	Planning authorities shall establish and implement minimum targets for intensification and redevelopment within built-up areas, based on local conditions.	The City has established targets for intensification and redevelopment. The proposed development will assist in meeting those targets as the Site is located in an existing built-up area.
1.1.3.6	New development taking place in designated growth areas should occur adjacent to the existing built-up area and should have a compact form, mix of uses and densities that allow for the efficient use of land, infrastructure and public service facilities.	<p>The proposed development does have a compact built form with commercial on the main floor and residential above.</p> <p>Parking will be located on-site.</p> <p>The proposed building size will allow for the efficient use of land, pedestrian and vehicle access, infrastructure and public services.</p>
1.3.1 a) - Employment	Planning authorities shall promote economic development and competitiveness.....	The proposed development offers 1,244.90 m2 of commercial retail space, which will help provide for employment opportunities.



PPS Policy #	Policy	Response
		The Site is in close proximity to nearby commercial uses, such as Tim Hortons, located to the south.
1.3.2.3	Within employment areas planned for industrial or manufacturing uses, planning authorities shall prohibit residential uses and prohibit or limit other sensitive land uses that are not ancillary to the primary employment uses in order to maintain land use compatibility.	<p>An OPA and ZBA is proposed.</p> <p>The existing industrial uses are located on the west side of Sandwich Street.</p> <p>Sandwich Street creates an appropriate buffer.</p> <p>A Noise Study has been completed.</p> <p>The Site was previously used for commercial.</p> <p>The area is in transition.</p> <p>There is no longer a need for industrial uses in the area.</p> <p>The proposed development will be designed to blend well with the surroundings.</p>
1.3.2.4 – Employment Land Conversion, Comprehensive Review	Planning authorities may permit conversion of lands within employment areas to non-employment uses through a comprehensive review, only where it has been demonstrated that the land is not required for employment purposes over the long term and that there is a need for the conversion.	<p>The employment land conversion is not being considered as part of a comprehensive review.</p> <p>The employment land conversion is a privately initiated transition.</p>
1.3.2.5 – Employment Land Conversion, Privately-Initiated	Notwithstanding policy 1.3.2.4, and until the official plan review or update in policy	It is proposed to convert the total area of the Site from industrial to mixed use with commercial and residential.

PPS Policy #	Policy	Response
	<p>1.3.2.4 is undertaken and completed, lands within existing employment areas may be converted to a designation that permits non-employment uses provided the area has not been identified as provincially significant through a provincial plan exercise or as regionally significant by a regional economic development corporation working together with affected upper and single-tier municipalities and subject to the following:</p>	<p>Recent analysis indicates that there are vacant and viable employment lands throughout the City in addition to an oversupply of employment lands in the region.</p> <p>Also, there is a need for residential, as noted in the recent provincial legislation changes which support new housing choices.</p>
	<p>a) there is an identified need for the conversion and the land is not required for employment purposes over the long term;</p>	<p>The proposed redevelopment would not impact the supply of employment lands.</p> <p>The Site is no longer appropriate and desirable for industrial uses.</p> <p>Further, the proposed redevelopment will enhance the area.</p> <p>By keeping commercial on the main floor, no employment land opportunity is completely lost.</p> <p>The Site is not required or suitable for industrial uses.</p> <p>The Site is a small lot and in close proximity to nearby residential uses.</p> <p>There is an identified need for housing, based on market</p>

PPS Policy #	Policy	Response
		<p>potential and recent trends observed in the City.</p> <p>The ZBA proposed to change the zoning of the Site to a site specific commercial district, which will be constant with the OP.</p> <p>Commercial uses will be limited to what is appropriate for the main floor of the proposed combined use building.</p> <p>Section 1.1.4 of the Windsor OP does address Land Supply. Future Employment Area lands transferred from the Town of Tecumseh should be sufficient to accommodate growth through the 20-year planning period.</p> <p>The Site is in an area of transition.</p>
	<p>b) the proposed uses would not adversely affect the overall viability of the employment area; and</p>	<p>The proposed use as residential will not adversely affect the overall viability of the employment area.</p> <p>The Site has had a long-standing vacancy, and underutilization demonstrates that the lands are no longer appropriate and desirable for industrial uses.</p> <p>Any new employment lands would occur in nearby industrial areas, and the</p>

PPS Policy #	Policy	Response
		<p>commercial would occur in the nodes.</p> <p>Residential will provide for additional units required to meet the 25 year PPS land needs.</p> <p>The employment land conversion will have a minor impact on the overall vacant land inventory.</p>
	<p>c) existing or planned infrastructure and public service facilities are available to accommodate the proposed uses.</p>	<p>Infrastructure and public service facilities are available to accommodate the proposed uses.</p> <p>The proposed development has access to municipal services, which have been identified in the required support studies.</p>
1.4.1	<p>To provide for an appropriate range and mix of housing options and densities required to meet projected requirements of current and future residents of the regional market area, planning authorities shall:</p> <p>a) maintain at all times the ability to accommodate residential growth for a minimum of 15 years through residential intensification and redevelopment and, if necessary, lands which are designated and available for residential development; and</p> <p>b) maintain at all times where new development is to occur,</p>	<p>The proposed development will provide for a mixed-use opportunity in the existing built-up area.</p> <p>Municipal services are available, as set out in the servicing studies.</p>

PPS Policy #	Policy	Response
	land with servicing capacity sufficient to provide at least a three-year supply of residential units available through lands suitably zoned to facilitate residential intensification and redevelopment, and land in draft approved and registered plans.	
1.4.3	<p>Planning authorities shall provide for an appropriate range and mix of housing options and densities to meet projected market-based and affordable housing needs of current and future residents of the regional market area.</p>	<p>The proposed density of 150 residential units is compatible with the surrounding area and will provide intensification and infilling through the efficient use of a vacant Site.</p> <p>The UDB has illustrated nearby similar construction.</p> <p>The proposed density will have a positive impact on the area as it will blend well with the existing built form.</p> <p>The Site is close to nearby community amenities.</p> <p>There is suitable infrastructure.</p>
1.6.1	<p>Infrastructure and public service facilities shall be provided in an efficient manner that prepares for the impacts of a changing climate while accommodating projected needs.</p>	<p>The development can proceed on full municipal services.</p> <p>Electrical distribution will be determined through detailed design.</p> <p>Access to public transit is available.</p>

PPS Policy #	Policy	Response
1.6.6.2	Municipal sewage services and municipal water services are the preferred form of servicing for settlement areas to support protection of the environment and minimize potential risks to human health and safety. Within settlement areas with existing municipal sewage services and municipal water services, intensification and redevelopment shall be promoted wherever feasible to optimize the use of the services.	The proposed development will be serviced by municipal sewer, water and storm, which is the preferred form of servicing for settlement areas.
1.6.6.7	<p>Planning for stormwater management shall:</p> <ul style="list-style-type: none"> <li>a) be integrated with planning for sewage and water services and ensure that systems are optimized, feasible and financially viable over the long term;</li> <li>b) minimize, or, where possible, prevent increases in contaminant loads;</li> <li>c) minimize erosion and changes in water balance, and prepare for the impacts of a changing climate through the effective management of stormwater, including the use of green infrastructure;</li> <li>d) mitigate risks to human health, safety, property and the environment;</li> </ul>	<p>A FSR has been completed. There will be no negative impacts on the municipal system and will not add to the capacity in a significant way.</p> <p>The Site provides for drainage.</p> <p>An ESA has been completed. There will be no risk to health and safety.</p>

PPS Policy #	Policy	Response
	<p>e) maximize the extent and function of vegetative and pervious surfaces; and</p> <p>f) promote stormwater management best practices, including stormwater attenuation and re-use, water conservation and efficiency, and low impact development.</p>	
1.6.7.1	Transportation systems should be provided which are safe, energy efficient, facilitate the movement of people and goods, and are appropriate to address projected needs.	The subject property is in close proximity to major roadways.
1.6.7.2	Efficient use should be made of existing and planned infrastructure, including through the use of transportation demand management strategies, where feasible.	<p>The proposed development contributes to the City's requirements for development within a built-up area.</p> <p>The area is serviced by transit.</p>
1.6.7.4	A land use pattern, density and mix of uses should be promoted that minimize the length and number of vehicle trips and support current and future use of transit and active transportation.	<p>The proposed development contributes to the Municipality's requirement for infilling within a built-up area.</p> <p>The proposed density, scale, and building height will blend with the existing land use pattern.</p>
2.1.1	Natural features and areas shall be protected for the long term.	There are no natural features that apply to this Site.



PPS Policy #	Policy	Response
2.2.1	Planning authorities shall protect, improve or restore the quality and quantity of water.	A FSR report has been prepared in support of the proposed development.
2.6.1	Significant built heritage resources and significant cultural heritage landscapes shall be conserved.	An Archaeological Assessment has been completed. There are no heritage resources that apply to this Site.
3.0	Development shall be directed away from areas of natural or human-made hazards where there is an unacceptable risk to public health or safety or of property damage, and not create new or aggravate existing hazards.	An Acoustical Report and ESA have been completed. There are no natural or human-made hazards.

Therefore, the proposed development is consistent with the PPS and the Province's vision for long-term prosperity and social well-being.

### 5.1.2 Official Plan (OP)

The City of Windsor Official Plan (OP) was adopted by Council on October 25, 1999, approved in part by the Ministry of Municipal Affairs and Housing (MMAH) on March 28, 2000, and the remainder approved by the Ontario Municipal Board (OMB) on November 1, 2002. Office consolidation version is dated September 7, 2012.

The OP implements the PPS and establishes a policy framework to guide land use planning decisions related to development and the provision of infrastructure and community services throughout the City.

The lands are designated "Industrial" according to Schedule "D" Land Use attached to the OP for the City of Windsor (see Figure 3 – City of Windsor OP, Schedule "D").

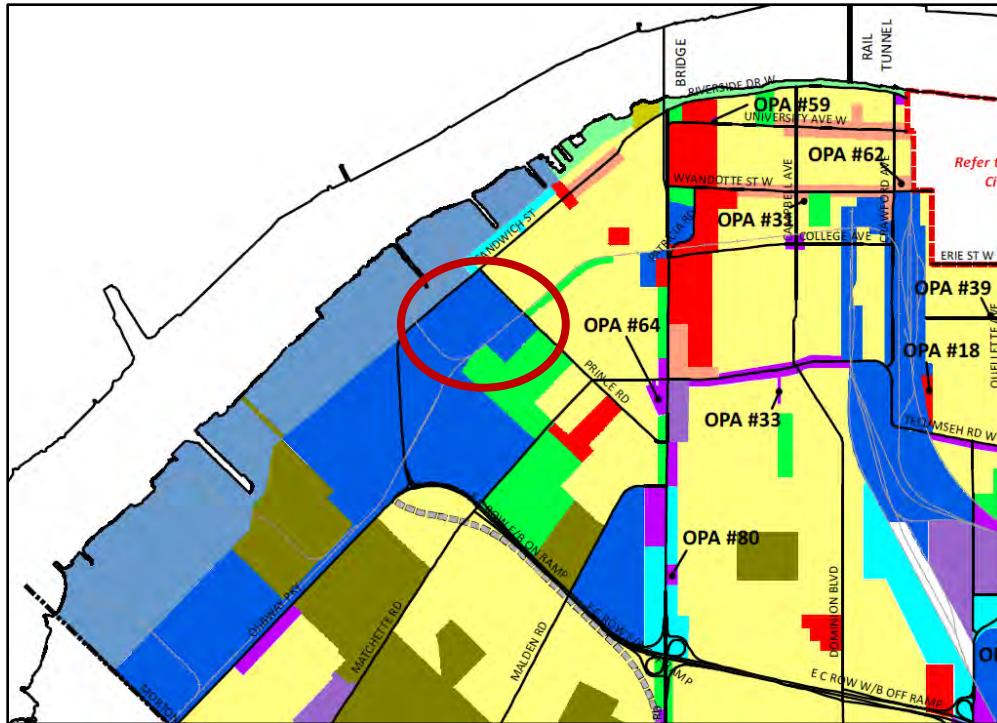


Figure 3 – City of Windsor OP, Schedule “D”

It is proposed to designate the Site to a site specific “Mixed Use” in order to permit a combined use building with commercial on the main floor and residential above.

The following provides a summary of the key policy considerations of the OP as it relates to the proposed development.

OP Policy #	Policy	Response
3.2 – Growth Concept	Mixed use developments will be encouraged with strong pedestrian orientations and to support public transit. This concept will enable Windsor to continue its growth and foster a vibrant economy, while ensuring a safe, caring and diverse community and a sustainable, healthy environment.	The proposed use is a mixed-use development with commercial on the main floor and residential above.  Commercial uses will be limited to the type of uses that are appropriate for the main floor of the proposed building.  The Site is located on a corner lot and will be oriented to provide for parking and pedestrian access.
3.2.1.2	Encouraging a range of housing types will ensure that	The proposed development supports one of the City’s

OP Policy #	Policy	Response
	people have an opportunity to live in their neighbourhoods as they pass through the various stages of their lives.	overall development strategies of providing for a range of housing choices.
3.2.1.3	Newly developing areas will be planned to foster their own unique neighbourhood identities with a mixture of homes, amenities and services.	The Site is in an area of transition whereby fewer industrial activities are occurring in the area, which is creating an attractive area for a small commercial node with residential uses.
3.2.3.1	Windsor will work toward achieving a sustainable transportation system where all modes of transportation can play a more balanced role. The creation of mixed use and employment centres will allow businesses and services to be closer to homes and allow greater opportunities for walking, cycling and transit.	The intent is to construct an affordable housing choice with a target market for international commuters working in Michigan and living in Windsor as well as students attending the University of Windsor.
4.0	The implementing healthy community policies are interwoven throughout the remainder of the Plan, particularly within the Environment, Land Use, Infrastructure and Urban Design chapters, to ensure their consideration and application as a part of the planning process.	<p>The proposed development will support the City's goal of promoting a healthy community (live, work and play).</p> <p>The proposed development is close to nearby transit, employment, shopping, local/regional amenities and parks/trails.</p>
6.0 - Preamble	A healthy and livable city is one in which people can enjoy a vibrant economy and a sustainable healthy environment in safe, caring and diverse neighbourhoods. In order to ensure that Windsor is such a city, Council will manage development through an approach which balances environmental,	The proposed development supports the policy set out in the OP as it is suited for the residential and commercial needs of the City.

OP Policy #	Policy	Response
	social and economic considerations.	
6.1 - Goals	<p>In keeping with the Strategic Directions, Council's land use goals are to achieve:</p> <p>6.1.1 Safe, caring and diverse neighbourhoods.</p> <p>6.1.3 Housing suited to the needs of Windsor's residents.</p> <p>6.1.10 Pedestrian oriented clusters of residential, commercial, employment and institutional uses.</p>	<p>The proposed development supports the goals set out in the OP as it provides for infilling of affordable residential in an area of transition.</p> <p>The employment component will complement the creation of a new commercial node.</p> <p>The proposed development allows pedestrian level access.</p>
6.2.1.2 – General Policies	<p>For the purpose of this Plan, Development Profile refers to the height of a building or structure. Accordingly, the following Development Profiles apply to all land use designations on Schedule D: Land Use unless specifically provided elsewhere in this Plan:</p> <p>(a) Low Profile developments are buildings or structures generally no greater than three (3) storeys in height;</p> <p>(b) Medium Profile developments are buildings or structures generally no greater than six (6) storeys in height; and</p> <p>(c) High Profile developments are buildings or structures generally no greater than fourteen (14) storeys in height.</p>	<p>The proposed development is considered a high profile mixed use development as it is proposed to have 11 storeys constructed on the Site.</p>
6.4 - Employment	Employment lands provide the main locations for business and industrial activities. In	The OPA will change the land use designation from "Industrial" to site specific

OP Policy #	Policy	Response
	<p>order to strengthen Windsor's economy, meet the land and infrastructure needs of employment activities and address concerns over compatibility, employment land uses are provided under two designations on Schedule D as either Industrial or Business Park.</p>	<p>"Mixed Use" which is located on Schedule D: Land Use.</p> <p>By keeping commercial on the main floor, no employment land opportunity is lost.</p> <p>Commercial uses will be limited to the type of uses that are appropriate for the main floor of the proposed building.</p> <p>There is a need for residential in the area.</p> <p>The Site is not required or suitable for industrial.</p> <p>The ZBA proposed to change the zoning of the Site to a site specific commercial district, which will be constant with the OP.</p> <p>Section 1.1.4 of the Windsor OP does address Land Supply. Future Employment Area lands transferred from the Town of Tecumseh should be sufficient to accommodate growth through the 20-year planning period.</p> <p>Infrastructure and public service facilities are available to accommodate the proposed uses.</p>
6.4.1.3 - Objectives	<p>To ensure that employment uses are developed in a manner which are compatible with other land uses.</p>	<p>Commercial on the main floor will provide an expansion of the node at the corner of Sandwich South and Chappell Avenue.</p> <p>There is a Tim Hortons located on the southeast side.</p>

OP Policy #	Policy	Response
		<p>Residential is located on the north and east side of the Site.</p> <p>Industrial is located to the west of the Site, on the opposite side of Sandwich Street.</p>
6.4.1.4	To accommodate a full range of employment activities in Windsor	<p>The proposed development will provide for employment in the form of a 1,244.90 m2 retail space.</p> <p>The retail space will provide local convenience to the surrounding area.</p>
6.4.1.6	To locate employment activities in areas which have sufficient and convenient access to all modes of transportation.	The proposed development will have on-site parking, bicycle parking, pedestrian friendly and close to transit.
6.4.1.8	To ensure that adequate infrastructure services are provided to employment areas.	The Site has access to full municipal services.
6.9 – Mixed Use Policies	The lands designated as “Mixed Use” on Schedule D: Land Use provide the main locations for compact clusters of commercial, office, institutional, open space and residential uses. These areas are intended to serve as the focal point for the surrounding neighbourhoods, community or region. As such, they will be designed with a pedestrian orientation and foster a distinctive and attractive area identity.	<p>The existing designation is “Industrial”.</p> <p>It is proposed to change the land use designation to “Mixed Use” to permit a combined use building.</p> <p>This is a unique area as it is in transition whereby fewer industrial activities are occurring in the area, which is creating an attractive area for a small commercial node with residential uses.</p> <p>There is a Tim Hortons located on the southeast side. Residential is located on the north and east side of the Site.</p>

OP Policy #	Policy	Response
		<p>Industrial is located to the west of the Site, on the opposite side of Sandwich Street.</p> <p>The result will be the creation of a new node.</p>
6.9.1.1	To encourage multi-functional areas which integrate compatible commercial, institutional, open space and residential uses.	<p>The proposed development will provide for residential and commercial uses. It will provide for convenience opportunities to the commercial, industrial and residential uses in the area.</p> <p>Commercial uses will be limited to the type of uses that are appropriate for the main floor of the proposed building.</p>
6.9.1.2	To encourage a compact form of mixed use development.	It is proposed to add a combined use building for the site specific OPA.
6.9.2.1	Uses permitted in the Mixed Use land use designation include retail and service commercial establishments, offices, cultural, recreation and entertainment uses, and institutional, open space and residential uses, exclusive of small scale Low Profile residential development.	The proposed development will provide for residential and commercial uses.
6.9.2.3	<p>Mixed Use development shall be located where:</p> <p>(a) there is access to a Controlled Access Highway, Class I or Class II Arterial Roads or Class I Collector Road;</p> <p>(b) full municipal physical services can be provided;</p> <p>(c) public transportation service can be provided; and</p> <p>(d) the surrounding development pattern is</p>	The Site is located along Sandwich Street, has access to full municipal services and transit and is compatible with the area.



OP Policy #	Policy	Response
	compatible with Mixed Use development.	
6.9.2.5	<p>The following guidelines shall be considered when evaluating the proposed design of a Mixed Use development:</p> <p>(a) the ability to achieve the associated policies as outlined in the Urban Design chapter of this Plan;</p> <p>(b) the mass, scale, orientation, form, and siting of the development achieves a compact urban form and a pedestrian friendly environment;</p> <p>(c) at least one building wall should be located on an exterior lot line and oriented to the street to afford direct sidewalk pedestrian access from the public right-of-way;</p> <p>(d) permanent loading, service and parking areas should be located so as not to significantly interrupt the pedestrian circulation or traffic flow on the public right-of-way or within a Mixed Use area;</p> <p>(e) mid-block vehicular access to properties is generally discouraged and is encouraged via a rear yard service road or alley;</p> <p>(f) parking areas shall be encouraged at the rear of buildings;</p> <p>(g) safe and convenient pedestrian access between buildings and public transportation stops, parking areas and other buildings and facilities should be provided;</p>	<p>An Urban Design Brief has been provided.</p> <p>The proposed combined use building has been designed to blend well with its surrounding.</p> <p>The building is proposed to be located close to the corner of the irregularly shaped lot. This will allow the parking to be located to the rear.</p> <p>The proposed development is compact and pedestrian-friendly.</p> <p>The Site is flat, making it conducive to vehicle access and maneuvering.</p> <p>The development will assist in creating a new identity for an area that is in transition.</p>

OP Policy #	Policy	Response
	(h) the development is designed to foster distinctive and attractive area identity; (i) the public rights-of-way are designed to foster distinctive and attractive area identity and to provide for vehicle use, regular public transportation service as well as pedestrian and cycling travel; and (j) integration of the development with the surrounding uses to contribute to the unique character of the area.	
6.9.2.7	Council may establish off street parking standards to reflect public transportation supportive designs or shared parking arrangements in Mixed Use developments.	A reduction in parking is being requested.  A Parking Study has been provided.
7.0	The provision of proper infrastructure provides a safe, healthy and efficient living environment. In order to accommodate transportation and physical service needs in Windsor, Council is committed to ensuring that infrastructure is provided in a sustainable, orderly and coordinated fashion.	The proposed development is close to nearby transit, off a major roadway and has access to full municipal services.
7.2.8.8	Council shall evaluate a proposed development adjacent to a Rail Corridor, in accordance with the following:  (a) All proponents of a new development within 300 metres of a rail corridor, may be required to complete a noise study to support the proposal, and if the need for mitigation measures is determined by such study,	An Acoustical Report was prepared for this Site to address sensitive land uses. Recommendations were made to provide mitigation measures.

OP Policy #	Policy	Response
	shall identify and recommend appropriate mitigation measures, in accordance with the Procedures chapter of this Plan.	
8.3.2.2 – Design for People	Council will encourage buildings and spaces that establish a pedestrian scale by promoting: (a) the placement of continuous horizontal features on the first two storeys adjacent to the road; (b) the repetition of landscaping elements, such as trees, shrubs or paving modules; and (c) the use of familiar sized architectural elements such as doorways and windows.	An UDB has addressed these policies. Recommendations have been made, including scale, landscape features and the inclusion of retail.  The proposed development allows pedestrian level access for both the commercial and residential uses.
8.5.2.5 - Landscaping	Council will encourage the use of landscaping to: (a) promote a human scale; (b) promote defined public spaces; (c) accentuate or screen adjacent building forms; (d) frame desired views or focal objects; (e) visually reinforce a location; (f) direct pedestrian movement; (g) demarcate various functions within a development; (h) provide seasonal variation in form, colour, texture and representation; (i) assist in energy conservation; and (j) mitigate the effects of inclement weather	An UDB has addressed these policies. Recommendations have been made, including scale, landscape features and the inclusion of retail.  The landscaping area will visually reinforce the proposed development.
8.6.2.1 – Protection from Elements	Council may encourage design measures such as	An UDB has addressed these policies. A continuous canopy

OP Policy #	Policy	Response
	awnings, canopies, arcades, or recessed ground floor facades to offer pedestrian protection from inclement weather.	is proposed along Sandwich St. and Chappell Ave.
8.7.2.1 – Built Form (New Development)	<p>Council will ensure that the design of new development:</p> <ul style="list-style-type: none"> <li>• is complementary to adjacent development in terms of its overall massing, orientation, setback and exterior design, particularly character, scale and appearance;</li> <li>• Provides links with pedestrian, cycle, public transportation and road networks; and</li> <li>• Maintains and enhances valued heritage resources and natural area features and functions.</li> <li>• Encourages the creation of attractive residential streetscapes through architectural design that reduces the visual dominance of front drive garages, consideration of rear lanes where appropriate, planting of street trees and incorporation of pedestrian scale amenities.</li> </ul>	An UDB has addressed these policies. Recommendations have been made including scale, landscape features, setbacks, building orientation, design and the inclusion of retail.
8.7.2.2 (Redevelopment Areas)	<p>Council will ensure that the design of extensive areas of redevelopment achieves the following:</p> <p>(a) provides a development pattern that support a range of uses and profiles;</p> <p>(b) defines the perimeter of such an area by a distinct edge which may be formed by roads, elements of the</p>	An UDB has addressed these policies. Recommendations have been made, including scale, landscape features, setbacks, building orientation, design and the inclusion of retail.

OP Policy #	Policy	Response
	<p>Greenway System or other linear elements;</p> <p>(c) contains activity centres or nodes which are designed to serve the area and which may be identified by one or more landmarks;</p> <p>(d) provides transportation links to adjacent areas; and</p> <p>(e) maintains and enhances valued historic development patterns or heritage resources.</p> <p>(f) is complementary to adjacent development in terms of overall massing, orientation, setback and exterior design, particularly character, scale and appearance.</p>	
8.7.2.3	<p>Council will ensure that proposed development within an established neighbourhood is designed to function as an integral and complementary part of that area's existing development pattern by having regard for:</p> <p>(a) massing;</p> <p>(b) building height;</p> <p>(c) architectural proportion;</p> <p>(d) volumes of defined space;</p> <p>(e) lot size;</p> <p>(f) position relative to the road;</p> <p>(g) building area to site area ratios;</p> <p>(h) the pattern, scale and character of existing development; and</p> <p>(i) exterior building appearance.</p>	<p>An UDB has addressed these policies.</p> <p>The proposed development will be a natural integration of the established neighbourhood to the east in addition to the commercial and industrial areas.</p> <p>Massing – the proposed building will be limited to 11 storeys which will blend well with the scale and massing of the existing surrounding area. This is accomplished through orientation, setback and design.</p> <p>Building height – the proposed building height is appropriate for this corner lot.</p> <p>Architectural proportion – the proposed visual effect of the</p>

OP Policy #	Policy	Response
		<p>relationship of the proposed development will blend well with the buildings in the immediate area as there are low profile homes. The proposed building will be located as far away from the existing homes as possible.</p> <p>Volume of defined space – the proposed design and layout of the development includes appropriate setbacks and lot coverage. The parking area will be constructed in according to City standards providing appropriate space.</p> <p>Lot size – the existing parcel is appropriate for the development. It allows for on-site parking and landscaping.</p> <p>Building area – appropriate lot coverage is proposed. The proposed building will not negatively impact the private use and enjoyment of area residents.</p> <p>Pattern, scale and character – the style of development will blend well with the scale and massing of the existing surrounding area.</p> <p>Exterior building appearance – the mixed-use building, will be designed professionally and aesthetically pleasing. The building will be located close to the intersection and will face Sandwich Street. Professional landscaping will</p>

OP Policy #	Policy	Response
		screen the parking and frame the building.
8.7.2.4 – Transition in Building Heights	Council will ensure a transition among Very High, High, Medium and Low Profile developments through the application of such urban design measures as incremental changes in building height, massing, space separation or landscape buffer.	Recommendations in the UDB have been made, including scale, landscape features, setbacks, building orientation, design and the inclusion of retail.
8.7.2.6 – Street Facades	Council will encourage the buildings facades to be visually interesting through extensive use of street level entrances and windows. Functions which do not directly serve the public, such as loading bays and blank walls, should not be located directly facing the street.	An UDB has addressed these policies.  The main pedestrian entrance faces Sandwich Street.
8.7.2.7 – Façade Setbacks	Council shall encourage all Medium, High and Very High Profile developments to setback additional storeys above the third (3) storey away from the road frontage to provide sunlight access, manage wind conditions and enhance the pedestrian scale.	An UDB has addressed these policies.
8.7.2.8 – Street Oriented Entrances	Council will ensure that main entrances to buildings are street oriented and clearly visible from principal pedestrian approaches.	An UDB has addressed these policies.
9.3.1.1 - Archaeological	For the purpose of this Plan, heritage resources include built heritage resources and cultural heritage landscapes that Council has identified as being important to the community.	An archaeological assessment was completed for this Site. No recommendations were made.



OP Policy #	Policy	Response
11.8 – Community Improvement	<p>The Community Improvement provisions of the Planning Act allow municipalities to prepare community improvement plans for designated community improvement project areas that require community improvement as the result of age, dilapidation, overcrowding, faulty arrangement, unsuitability of buildings or for any other environmental, social or community economic development reason. Once a community improvement plan has been adopted by a municipality and has come into effect, the municipality may offer grants and loans in conformity with the community improvement plan, to registered owners, assessed owners and tenants of lands and buildings within the community improvement project area, and to any person to whom such an owner or tenant has assigned the right to receive a grant or loan, to pay for the whole or any part of the eligible costs of the community improvement plan. The municipality may also undertake a wide range of actions for the purpose of carrying out the community improvement plan. Community improvement plans may be used to revitalize existing planning districts, neighbourhoods, corridors or any other area identified as being in need of community</p>	<p>The proposal adheres to the design direction of Old Sandwich Town CIP.</p> <p>An UDB has been provided to illustrate how criteria have been met.</p>

OP Policy #	Policy	Response
	improvement due to physical, environmental, economic or social conditions.	

Therefore, the proposed development conforms to the City of Windsor OP with the proposed site specific amendment.

### 5.1.3 Zoning By-law (ZBL)

The City of Windsor Zoning By-Law (ZBL) #8600 was passed by Council on July 8, 2002, and then a further Ontario Municipal Board (OMB) decision was issued on January 14, 2003.

A ZBL implements the PPS and the City OP by regulating the specific use of the property and provide for its day-to-day administration.

According to Map 4 attached to the ZBL the Site is currently zoned Commercial District 2.1 (CD2.1), Manufacturing District 1.4 (MD1.4) and Development Reserve District 1.1 (DRD1.1) category (see Figures 4 – City of Windsor Zoning Map 4).

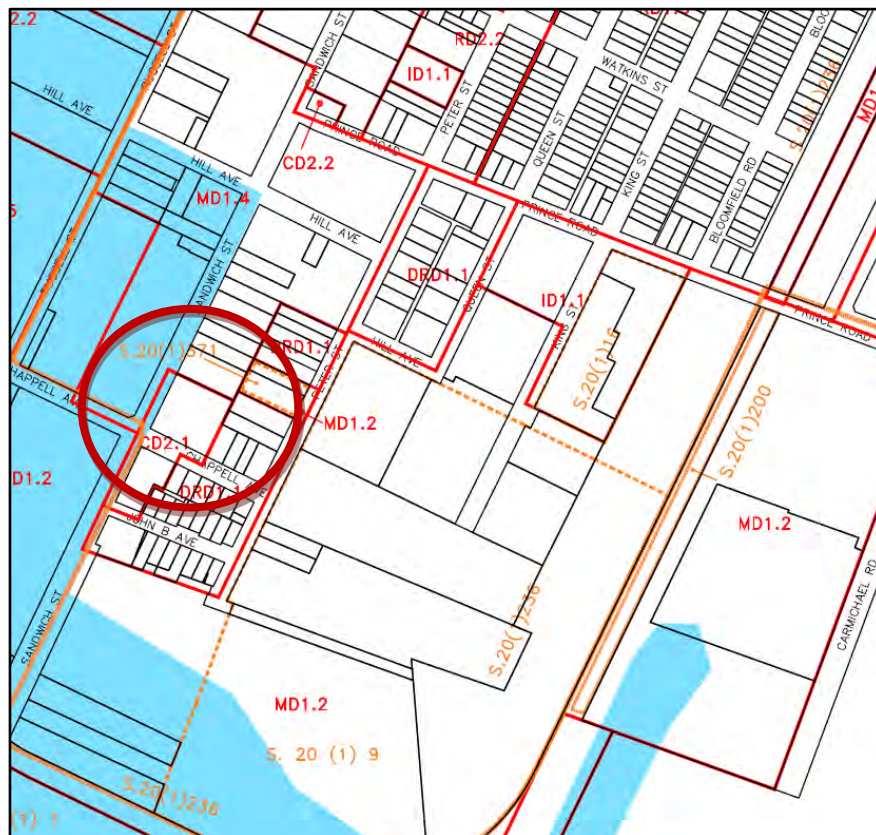


Figure 4 – City of Windsor Zoning Map 4

A site specific ZBA is required for the proposed development.

Permitted uses in the CD2.1 includes commercial uses.

The zoning for the Site is proposed to be changed to a site specific Commercial District 2.1 (CD2.1 - S.20(1)(XXX)) category as shown on Map 4 of the City ZBL to permit a combined use building.

*“COMBINED USE BUILDING means a building having, as main uses, at least one dwelling unit and at least one non-residential use. If a Combined use Building is occupied in part by a Minor Commercial Centre or a Major Commercial Centre, the total required number of parking spaces is the sum of the required number of parking spaces for each Dwelling Unit and for the Minor Commercial Centre of a Major Commercial Centre.”*

A review of the CD2.1 zone provisions, as set out in Section 15.1.5 of the ZBL is as follows:

Zone Regulations	Required CD2.1 Zone	Proposed	Compliance and/or Relief Requested with Justification
Permitted Uses	Bakery Business Office Child Care Centre Commercial School Confectionery Food Outlet - Drive-Through Food Outlet - Take-Out Funeral Establishment Garden Centre Gas Bar Medical Office Micro-Brewery Parking Garage Personal Service Shop Place of Entertainment and Recreation Place of Worship Professional Studio Public Hall Public Parking Area Repair Shop – Light Restaurant Restaurant with Drive-Through Retail Store Temporary Outdoor Vendor's Site Tourist Home Veterinary Office Wholesale Store	Dwelling Units in a Combined Use Building Business Office Child Care Centre Commercial School Food Outlet - Take-Out Medical Office Personal Service Shop Place of Entertainment and Recreation Restaurant Retail Store Any use accessory to any of the preceding uses	Complies, subject to the ZBA request.  Commercial uses can be limited to those uses listed that are appropriate for the main level of the proposed building.  Residential units to be located above the commercial uses.

	Existing Automobile Repair Garage Existing Service Station Any use accessory to any of the preceding uses. An Outdoor Storage Yard is prohibited, save and except, in combination with the following main uses: Garden Centre, Temporary Outdoor Vendor's Site, Existing Automobile Repair Garage.		
Maximum Building Height	14.0 m	37 m	Relief required.  The proposed height is appropriate for the Site.  The Site is suitable for the scale and massing of the proposed building.  The propose relief does not impact any of the abuttin properties.
Gross Floor Area – maximum	Bakery or Confectionary - 550.0 m2	352.75 m2 269.42 m2	Complies
Other	A Temporary Outdoor Vendor's Site is prohibited in a Business Improvement Area	N/A	N/A
Parking Spaces Required	<b>Combined Use Building – Dwelling Units, 1.25 for each dwelling unit:</b>  187.5 parking spaces required (rounded down = 187 parking spaces)  <b>Retail:</b>	156 total parking spaces provided, for both the commercial and residential	Relief required.  A decrease in the minimum parking space is required for the proposed combined use building for the residential and commercial uses.

	<p>1 for each 22.5 m<sup>2</sup> GFA:</p> <p>29.42 parking spaces required/based on 662.17 m<sup>2</sup> (rounded down = 29 parking spaces)</p> <p>TOTAL 187 + 29 = 216</p>		<p>A total relief of 60 parking spaces (216 - 156 = 60) for both the commercial and residential uses.</p> <p>There are existing on-street parking spaces on Chappell Street. The City of Windsor is proposing new bike lanes and parking spaces on the west side of Sandwich Street between Chappell Street and Hill Avenue.</p> <p>Retail will comply. It is uncertain at this time what uses will include in the retail space.</p> <p>Parking will be located back from the street, screened by an ornamental fence.</p> <p>The Site has access to transit and is bike and pedestrian friendly.</p> <p>A Parking Study has been completed.</p>
Visitor Parking (24.22.1)	<p>15 percent of parking spaces marked</p> <p>15% of 156 = 24 spaces</p>	24	Complies
Bicycle Parking (24.30.1)	<p>2 for the first 19 spaces plus 1 for each additional 20 parking spaces:</p>	11 spaces provided	Complies

	2 + 6.85 = 8.85 spaces required (8 rounded down)		
Accessible Parking Spaces Required (24.24.1)	For 101 to 200 total number of Parking Spaces  Type A - 1.5 percent of parking spaces  Type B - 0.5 space plus 1.5 percent of parking spaces:  $156 \times 0.5\% = 0.78 = 157 \times 1.5\% = 2.355$  Total 6 required (3 Type A and 3 Type B)	6 spaces provided (3 Type A and 3 Type B)	Complies
Loading (24.40.1.5)	For a Combined Use Building with 9 or more dwelling units, the required number of loading spaces for the dwelling units shall be calculated using the gross floor area of that part of the building occupied by all the dwelling units  3 required	3 provided	Complies
Parking Area Separation (25.5.20)	Any other street - 3.00 m	TBD	Complies
	An interior lot line or alley - 0.90 m	TBD	Complies
	A building wall in which is located a main pedestrian entrance facing the parking area – 2.00 m	TBD	Complies
	A building wall containing a habitable room window or containing both a main pedestrian entrance and a habitable room window facing the parking area where the building is located on the same lot as the parking area – 4.50 m	TBD	Complies

Therefore, the proposed development will comply with all zone provisions set out in the CD2.1 Zone except for the following, which requires relief:

1. *Increase the required height from 14.0 m to 37 m.*
2. *Decrease the minimum parking space required from 216 spaces to 156 spaces.*



## **6.0 SUMMARY AND CONCLUSION**

### **6.1 Context and Site Suitability Summary**

#### **6.1.1 Site Suitability**

The Site is ideally suited for mixed-use development for the following reasons:

- The land area is sufficient to accommodate the proposed development with adequate buffering from abutting land uses,
- The Site is level which is conducive to easy vehicular movements,
- The Site will be able to accommodate municipal water, storm and sewer systems,
- The Site provides for drainage,
- There are no anticipated traffic concerns,
- There are no environmental concerns,
- There are no hazards, and
- The location of the proposed development is appropriate in that it will blend well with the surrounding area.

#### **6.1.2 Compatibility of Design**

The proposed development will be strategically located to provide efficient ease of the proposed new access into the parking areas.

The proposed development will be limited to an 11 storey, high profile neighbourhood development, which is a compatible density with the surrounding area.

The Site is capable of accommodating the proposed development in terms of scale, massing, height and siting. On-site parking and landscaping will be provided.

The proposal adheres to the design direction of Old Sandwich Town CIP.

#### **6.1.3 Good Planning**

The proposal represents good planning as it addresses the need for the City to provide infilling development, which contributes to affordability and intensification requirements set out in the PPS and the OP.

Mixed-use on the Site represents an efficient development pattern that optimizes the use of land. The Site currently has a vacant building and is underutilized.

The fact that the proposal is supported by provincial and municipal planning policy and the Site is suitable for the intended use on a number of criteria attests that the proposal represents good planning.

#### **6.1.4 Natural Environment Impacts**

The proposal does not have any negative natural environment impacts, as there are no natural heritage features on the Site.

#### **6.1.5 Municipal Services Impacts**

There will be no negative impacts on the municipal system as the mixed-use development will not add to the capacity in a significant way.

The topography, soil, and environmental characteristics of the Site are able to accommodate an appropriate development that will minimize adverse environmental impacts.

#### **6.1.6 Social and/or Economic Conditions**

The proposed development does not negatively affect the social environment as the Site is in close proximity to major transportation corridors, transit, open space and community amenities.

Infilling in an existing established neighbourhood contributes toward the goal of 'live, work and play' where citizens share a strong sense of belonging and a collective pride of place.

The proposed development promotes efficient development and land use pattern, which sustains the financial well-being of the municipality.

The proposal does not cause any public health and safety concerns. The proposal represents a cost effective development pattern that minimizes land consumption and servicing costs.

Based on the Site area, the proposed development will result in a total net density, which is appropriate for the neighbourhood.

There will be no urban sprawl as the proposed development is within the existing settlement area and is an ideal infilling opportunity.

### **6.2 Conclusion**

The proposal to use the Site for mixed-use residential and commercial is appropriate and should be approved by the City of Windsor as it:

- is a site that is physically suitable;
- will not negatively impact the private use and enjoyment of area residents;
- will not have any negative natural environment impacts;
- will not have any negative archaeological impacts;
- is not anticipated to create any traffic issues;
- will not have any negative impacts on municipal services,
- will not have any negative social, environmental or economic impacts; and
- will have a positive impact on the City of Windsor.

In summary, for the above reasons, it would be appropriate for the City of Windsor to approve the OPA and ZBA to permit the proposed mixed-use development on the Site as it is appropriate for infilling and will offer an affordable housing choice and commercial space in an area of transition.

This PRR has shown that the proposed development is consistent with the PPS, conforms with the intent and purpose of the City of Windsor OP and represents good planning.


The report components for this PRR have set out the following, as required under the City of Windsor OP:

*10.2.13.2 Where a Planning Rationale Report is required, such a study should:*

- (a) Include a description of the proposal and the approvals required;*
- (b) Describe the Site's previous development approval history;*
- (c) Describe major physical features or attributes of the Site including current land uses(s) and surrounding land uses, built form and contextual considerations;*
- (d) Describe whether the proposal is consistent with the provincial policy statements issued under the Planning Act;*
- (e) Describe the way in which relevant Official Plan policies will be addressed, including both general policies and site-specific land use designations and policies;*
- (f) Describe whether the proposal addresses the Community Strategic Plan;*
- (g) Describe the suitability of the Site and indicate reasons why the proposal is appropriate for this Site and will function well to meet the needs of the intended future users;*
- (h) Provide an analysis of the compatibility of the design and massing of the proposed developments and land use designations;*
- (i) Provide an analysis and opinion as to why the proposal represents good planning, including the details of any methods that are used to mitigate potential negative impacts;*
- (j) Describe the impact on the natural environment;*
- (k) Describe the impact on municipal services;*
- (l) Describe how the proposal will affect the social and/or economic conditions using demographic information and current trends; and,*
- (m) Describe areas of compliance and non-compliance with the Zoning By-law.*

**Planner's Certificate:**

I hereby certify that this report was prepared by Tracey Pillon-Abbs, a Registered Professional Planner, within the meaning of the Ontario Professional Planners Institute Act, 1994.



**Tracey Pillon-Abbs, RPP  
Principal Planner**



## 1.1 Managing and Directing Land Use to Achieve Efficient and Resilient Development and Land Use Patterns

1.1.1 Healthy, liveable and safe communities are sustained by:

a) promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term;

b) accommodating an appropriate affordable and market-based range and mix of residential types (including single-detached, additional residential units, multi-unit housing, affordable housing and housing for older persons), employment (including industrial and commercial), institutional (including places of worship, cemeteries and long-term care homes), recreation, park and open space, and other uses to meet long-term needs;

c) avoiding development and land use patterns which may cause environmental or public health and safety concerns;

e) promoting the integration of land use planning, growth management, *transit-supportive* development, *intensification* and *infrastructure* planning to achieve cost-effective development patterns, optimization of transit investments, and standards to minimize land consumption and servicing costs;

1.1.2 Sufficient land shall be made available to accommodate an appropriate range and mix of land uses to meet projected needs for a time horizon of up to 25 years, informed by provincial guidelines. However, where an alternate time period has been established for specific areas of the Province as a result of a provincial planning exercise or a *provincial plan*, that time frame may be used for municipalities within the area.

### 1.1.3. Settlement Areas

Settlement areas are urban areas and rural settlement areas, and include cities, towns, villages and hamlets. Ontario's settlement areas vary significantly in terms of size, density, population, economic activity, diversity and intensity of land uses, service levels, and types of infrastructure available.

The vitality and regeneration of settlement areas is critical to the long-term economic prosperity of our communities. Development pressures and land use change will vary across Ontario. It is in the interest of all communities to use land and resources wisely, to promote efficient development patterns, protect resources, promote green spaces, ensure effective use of infrastructure and public service facilities and minimize unnecessary public expenditures.

1.1.3.1 *Settlement areas* shall be the focus of growth and development.

1.1.3.2 Land use patterns within *settlement areas* shall be based on densities and a mix of land uses which:

- a) efficiently use land and resources;
- b) are appropriate for, and efficiently use, the *infrastructure* and *public service facilities* which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion;
- c) minimize negative impacts to air quality and climate change, and promote energy efficiency;
- d) prepare for the *impacts of a changing climate*;
- e) support *active transportation*;
- f) are *transit-supportive*, where transit is planned, exists or may be developed; and

1.1.3.3 Planning authorities shall identify appropriate locations and promote opportunities for *transit-supportive* development, accommodating a significant supply and range of *housing options* through *intensification* and *redevelopment* where this can be accommodated taking into account existing building stock or areas, including *brownfield sites*, and the availability of suitable existing or planned *infrastructure* and *public service facilities* required to accommodate projected needs.

1.1.3.4 Appropriate development standards should be promoted which facilitate *intensification*, *redevelopment* and compact form, while avoiding or mitigating risks to public health and safety.

1.1.3.5 Planning authorities shall establish and implement minimum targets for *intensification* and *redevelopment* within built-up areas, based on local conditions. However, where provincial targets are established through *provincial plans*, the provincial target shall represent the minimum target for affected areas.

1.1.3.6 New development taking place in *designated growth areas* should occur adjacent to the existing built-up area and should have a compact form, mix of uses and densities that allow for the efficient use of land, *infrastructure* and *public service facilities*.

## **1.3.2 Employment Areas**

1.3.2.3 Within *employment areas* planned for industrial or manufacturing uses, planning authorities shall prohibit residential uses and prohibit or limit other *sensitive land uses* that are not ancillary to the primary employment uses in order to maintain land use compatibility. *Employment areas* planned for industrial or manufacturing uses should include an appropriate transition to adjacent non-*employment areas*.

1.3.2.4 Planning authorities may permit conversion of lands within *employment areas* to non-employment uses through a *comprehensive review*, only where it has been demonstrated that the land is not required for employment purposes over the long term and that there is a need for the conversion.

1.3.2.5 Notwithstanding policy 1.3.2.4, and until the official plan review or update in policy 1.3.2.4 is undertaken and completed, lands within existing *employment areas* may be converted to a designation that permits non-employment uses provided the area has not been identified as provincially significant through a provincial plan exercise or as regionally significant by a regional economic development corporation working together with affected upper and single-tier municipalities and subject to the following:

- a) there is an identified need for the conversion and the land is not required for employment purposes over the long term;
- b) the proposed uses would not adversely affect the overall viability of the *employment area*; and
- c) existing or planned *infrastructure* and *public service facilities* are available to accommodate the proposed uses.

## 1.4 Housing

1.4.1 To provide for an appropriate range and mix of *housing options* and densities required to meet projected requirements of current and future residents of the *regional market area*, planning authorities shall:

- a) maintain at all times the ability to accommodate residential growth for a minimum of 15 years through *residential intensification* and *redevelopment* and, if necessary, lands which are *designated and available* for residential development; and
- b) maintain at all times where new development is to occur, land with servicing capacity sufficient to provide at least a three-year supply of residential units available through lands suitably zoned to facilitate *residential intensification* and *redevelopment*, and land in draft approved and registered plans.

1.4.3 Planning authorities shall provide for an appropriate range and mix of *housing options* and densities to meet projected market-based and affordable housing needs of current and future residents of the *regional market area* by:

b) permitting and facilitating:

- 1. all *housing options* required to meet the social, health, economic and well-being requirements of current and future residents, including *special needs* requirements and needs arising from demographic changes and employment opportunities; and
- 2. all types of *residential intensification*, including additional residential units, and *redevelopment* in accordance with policy 1.1.3.3;



- c) directing the development of new housing towards locations where appropriate levels of *infrastructure* and *public service facilities* are or will be available to support current and projected needs;
- d) promoting densities for new housing which efficiently use land, resources, *infrastructure* and *public service facilities*, and support the use of *active transportation* and transit in areas where it exists or is to be developed;

## 1.6 Infrastructure and Public Service Facilities

1.6.1 *Infrastructure* and *public service facilities* shall be provided in an efficient manner that prepares for the *impacts of a changing climate* while accommodating projected needs.

### 1.6.6 Sewage, Water and Stormwater

1.6.6.2 *Municipal sewage services* and *municipal water services* are the preferred form of servicing for *settlement areas* to support protection of the environment and minimize potential risks to human health and safety. Within *settlement areas* with existing *municipal sewage services* and *municipal water services*, *intensification* and *redevelopment* shall be promoted wherever feasible to optimize the use of the services.

1.6.6.7 Planning for stormwater management shall:

- a) be integrated with planning for *sewage and water services* and ensure that systems are optimized, feasible and financially viable over the long term;
  - b) minimize, or, where possible, prevent increases in contaminant loads;
  - c) minimize erosion and changes in water balance, and prepare for the *impacts of a changing climate* through the effective management of stormwater, including the use of *green infrastructure*;
  - d) mitigate risks to human health, safety, property and the environment;
  - e) maximize the extent and function of vegetative and pervious surfaces; and
  - f) promote stormwater management best practices, including stormwater attenuation and re-use, water conservation and efficiency, and low impact development.
- 1.6.7.1 *Transportation systems* should be provided which are safe, energy efficient, facilitate the movement of people and goods, and are appropriate to address projected needs.

1.6.7.2 Efficient use should be made of existing and planned *infrastructure*, including through the use of *transportation demand management* strategies, where feasible.

1.6.7.4 A land use pattern, density and mix of uses should be promoted that minimize the length and number of vehicle trips and support current and future use of transit and *active transportation*.

### **3.0 Protecting Public Health and Safety**

Ontario's long-term prosperity, environmental health and social well-being depend on reducing the potential for public cost or risk to Ontario's residents from natural or human-made hazards.

Development shall be directed away from areas of natural or human-made hazards where there is an unacceptable risk to public health or safety or of property damage, and not create new or aggravate existing hazards.

Mitigating potential risk to public health or safety or of property damage from natural hazards, including the risks that may be associated with the impacts of a changing climate, will require the Province, planning authorities, and conservation authorities to work together.

## **Appendix F -Excerpts from the City of Windsor Official Plan**

### **3.2 Growth Concept**

The 1996 Census population of Windsor was 197,695. Windsor's population is expected to increase by between 11,980 to 23,280 from 1996 to 2016. This growth and corresponding demographic changes, is projected to result in the need for an additional 10,950 dwelling units and create between 13,900 and 29,600 new jobs. This, in turn, will result in the projected development of between 390 to 476 hectares of residential lands and 243 to 514 hectares of employment and commercial lands.

The policies of this Plan are directed toward accommodating the projected growth through practical and efficient land use management strategies that promote a compact pattern of development and balanced transportation system. Compatible residential, commercial and employment growth will be directed to appropriate locations within existing and planned neighbourhoods to reduce development and infrastructure costs and provide opportunities to live, work and shop in City of Windsor Official Plan Volume I Development Strategy 3 - 3 close proximity.

Mixed use developments will be encouraged with strong pedestrian orientations and to support public transit. This concept will enable Windsor to continue its growth and foster a vibrant economy, while ensuring a safe, caring and diverse community and a sustainable, healthy environment.

In order to manage growth consistent with the community vision, the following key policy directions are provided for in the other chapters of this Plan.

#### **3.2.1 Safe, Caring and Diverse Community**

##### **NEIGHBOURHOOD HOUSING VARIETY**

3.2.1.2 Encouraging a range of housing types will ensure that people have an opportunity to live in their neighbourhoods as they pass through the various stages of their lives. Residents will have a voice in how this new housing fits within their neighbourhood. As the city grows, more housing opportunities will mean less sprawl onto agricultural and natural lands.

##### **DISTINCTIVE NEIGHBOURHOOD CHARACTER**

3.2.1.3 Windsor will keep much of what gives its existing neighbourhoods their character – trees and greenery, heritage structures and spaces, distinctive area identities, parks, and generally low profile development outside the City Centre. Around the neighbourhood centres, the existing character of the neighbourhood will be retained and enhanced. Newly developing

areas will be planned to foster their own unique neighbourhood identities with a mixture of homes, amenities and services.

### **3.2.3 Sustainable, Healthy Environment**

#### *TRANSPORTATION SYSTEM*

3.2.3.1 Windsor will work toward achieving a sustainable transportation system where all modes of transportation can play a more balanced role. The creation of mixed use and employment centres will allow businesses and services to be closer to homes and allow greater opportunities for walking, cycling and transit.

#### **6.1 Goals**

In keeping with the Strategic Directions, Council's land use goals are to achieve:

#### **NEIGHBOURHOODS**

6.1.1--Safe, caring and diverse neighbourhoods.

#### **RESIDENTIAL**

6.1.3--Housing suited to the needs of Windsor's residents.

#### **MIXED USE**

6.1.10--Pedestrian oriented clusters of residential, commercial, employment and institutional uses.

#### **COMPATIBLE DEVELOPMENT**

6.4.1.3--To ensure that employment uses are developed in a manner which are compatible with other land uses.

#### **RANGE OF USES**

6.4.1.4--To accommodate a full range of employment activities in Windsor.

#### **ACCESSIBLE**

6.4.1.6--To locate employment activities in areas which have sufficient and convenient access to all modes of transportation.

#### **INFRASTRUCTURE**

6.4.1.8--To ensure that adequate infrastructure services are provided to employment areas.

#### **6.4.3 Industrial Policies**

The Industrial land use designation provides for a broad range of industrial uses which, because of their physical and operational characteristics, are more appropriately clustered together and separated from sensitive land uses. This designation is also applied to certain older industrial areas of Windsor where such a separation may not have been achieved.

#### PERMITTED USES

##### 6.4.3.1

Uses permitted in the Industrial land use designation identified on Schedule D: Land Use include establishments which may exhibit any or all of the following characteristics:

- (a) large physical size of site or facilities;
- (b) outdoor storage of materials or products;
- (c) large production volumes or large product size;
- (d) frequent or continuous shipment of products and/or materials;
- (e) long hours of production and shift operations;
- (f) likelihood of nuisances, such as noise, odour, dust or vibration;
- (g) multi-modal transportation facilities;

#### 6.9 Mixed Use

The lands designated as “Mixed Use” on Schedule D: Land Use provide the main locations for compact clusters of commercial, office, institutional, open space and residential uses. These areas are intended to serve as the focal point for the surrounding neighbourhoods, community or region. As such, they will be designed with a pedestrian orientation and foster a distinctive and attractive area identity.

The following objectives and policies establish the framework for development decisions in Mixed Use areas.

##### 6.9.1 Objectives

#### MULTI-FUNCTIONAL AREAS

6.9.1.1--To encourage multi-functional areas which integrate compatible commercial, institutional, open space and residential uses.

#### COMPACT FORM

6.9.1.2--To encourage a compact form of mixed use development.

##### 6.9.2 Policies--PERMITTED USES

6.9.2.1--Uses permitted in the Mixed Use land use designation include retail and service commercial establishments, offices, cultural, recreation and entertainment uses, and institutional, open space and residential uses, exclusive of small scale Low Profile residential development.

#### LOCATIONAL CRITERIA

6.9.2.3-Mixed Use development shall be located where:

- (a)there is access to a Controlled Access Highway, Class I or Class II Arterial Roads or Class I Collector Road;
- (b)full municipal physical services can be provided;
- (c)public transportation service can be provided; and
- (d)the surrounding development pattern is compatible with Mixed Use development.

#### EVALUATION CRITERIA

6.9.2.4--At the time of submission, the proponent shall demonstrate to the satisfaction of the Municipality that a proposed Mixed Use development is:

City of Windsor Official Plan ☐ Volume I ☐ Land Use 6 - 54

(a) feasible having regard to the other provisions of this Plan, provincial legislation, policies and appropriate guidelines and support studies for uses:

- (i) within or adjacent to any area identified on Schedule C: Development Constraint Areas and described in the Environment chapter of this Plan;
- (ii) within a site of potential or known contamination;
- (iii) where traffic generation and distribution is a provincial or municipal concern; and
- (iv) adjacent to sensitive land uses and/or heritage resources.

(b) in keeping with the goals, objectives and policies of any secondary plan or guideline plan affecting the surrounding area;

(c) capable of being provided with full municipal physical services and emergency services;

(d) provided with adequate off street parking;

(e) pedestrian oriented;

(f) compatible with the surrounding area in terms of scale, massing, height, siting, orientation, setbacks, parking and landscaped areas; and

(g) acceptable in terms of the proposal's market impacts on other commercial areas (see Procedures chapter).

#### DESIGN GUIDELINES

6.9.2.5--The following guidelines shall be considered when evaluating the proposed design of a Mixed Use development:

(a) the ability to achieve the associated policies as outlined in the Urban Design chapter of this Plan;

(b) the mass, scale, orientation, form, and siting of the development achieves a compact urban form and a pedestrian friendly environment;

City of Windsor Official Plan ☐ Volume I ☐ Land Use 6 - 55

- (c) at least one building wall should be located on an exterior lot line and oriented to the street to afford direct sidewalk pedestrian access from the public right-of-way;
- (d) permanent loading, service and parking areas should be located so as not to significantly interrupt the pedestrian circulation or traffic flow on the public right-of-way or within a Mixed Use area;
- (e) mid-block vehicular access to properties is generally discouraged and is encouraged via a rear yard service road or alley;
- (f) parking areas shall be encouraged at the rear of buildings;
- (g) safe and convenient pedestrian access between buildings and public transportation stops, parking areas and other buildings and facilities should be provided;
- (h) the development is designed to foster distinctive and attractive area identity;
- (i) the public rights-of-way are designed to foster distinctive and attractive area identity and to provide for vehicle use, regular public transportation service as well as pedestrian and cycling travel; and
- (j) integration of the development with the surrounding uses to contribute to the unique character of the area.

#### DEVELOPMENT ADJACENT TO A CORRIDOR

7.2.8.8 Council shall evaluate a proposed development adjacent to a Rail Corridor, in accordance with the following:

- (a) All proponents of a new development within 300 metres of a rail corridor, may be required to complete a noise study to support the proposal, and if the need for mitigation measures is determined by such study, shall identify and recommend appropriate mitigation measures, in accordance with the Procedures chapter of this Plan;
- (b) All proponents of new development, located within 75 metres of a rail corridor, shall complete a vibration study to support the City of Windsor Official Plan <sup>2</sup> Volume I <sup>2</sup> Infrastructure 7 – 25 proposal, and if the need for mitigation measures is determined by such study, shall identify and recommend appropriate mitigation measures, in accordance with the Procedures chapter of this Plan;

#### EVALUATION CRITERIA

11.6.3.3 --When considering applications for Zoning By-law amendments, Council shall consider the policies of this Plan and will, without limiting the generality of the foregoing, consider such matters as the following:

- (a) The relevant evaluation criteria contained in the Land Use Chapter of this Plan, Volume II: Secondary Plans & Special Policy Areas and other relevant standards and guidelines;
- (b) Relevant support studies;
- (c) The comments and recommendations from municipal staff and circularized agencies;



(d) Relevant provincial legislation, policies and appropriate guidelines; and

(e) The ramifications of the decision on the use of adjacent or similar lands.

## **Volume II, Part 1-Special Policy areas**

### **1.7 Sandwich Street and Chappell Avenue (amended by OPA #4 – 05/03/2001)**

#### **SOUTHEAST CORNER**

1.7.1 The property comprising Lot 28, south side of Sandwich Street and part of Lot 28 north side of Peter Street, Registered Plan 40, located on the southeast corner of Sandwich Street and Chappell Avenue is designated on Schedule A: Planning Districts and Policy Areas in Volume I – The Primary Plan

#### **ADULT ENTERTAINMENT PARLOUR**

##### **PERMITTED**

1.7.2 Notwithstanding Section 6.4.3.2 (c) of this Plan respecting Adult Entertainment Parlours in areas designated “Industrial”, the lands identified as 3885 Sandwich Street may be used for an Adult Entertainment Parlour.

## 18.4 MANUFACTURING DISTRICT 1.4 (MD1.4)

### 18.4.1 PERMITTED USES

<i>Ambulance Service</i>	<i>Manufacturing Facility</i>
<i>Bakery</i>	<i>Medical Appliance Facility</i>
<i>Business Office</i>	<i>Medical Office</i>
<i>Commercial School</i>	<i>Micro-Brewery</i>
<i>Food Catering Service</i>	<i>Professional Studio</i>
<i>Food Packaging Facility</i>	<i>Research and Development Facility</i>

Any of the following *Ancillary Uses*:

<i>Child Care Centre</i>	<i>Health Studio</i>
<i>Club</i>	<i>Personal Service Shop</i>
<i>Convenience Store</i>	<i>Restaurant</i>
<i>Food Convenience Store</i>	<i>Restaurant with Drive-through</i>
<i>Food Outlet - Drive-through</i>	<i>Veterinary Office</i>
<i>Food Outlet - Take-out</i>	<i>Warehouse</i>
<i>Gas Bar</i>	<i>Wholesale Store</i>

Any of the following *Existing Uses*:

*Motor Vehicle Dealership*  
*Sports Facility*  
*Transport Terminal*

Any use accessory to any of the above uses, including a *Retail Store*

### 18.4.3 PROHIBITED USES

*Outdoor Storage Yard*

### 18.4.5 PROVISIONS

- |     |  |  |
|-----|--|--|
| .1  | Lot Width – minimum  | 30.0 m   |
| .4  | Building Height – maximum  | 20.0 m   |
| .5  | Front Yard Depth – minimum   | 9.0 m  |
| .6  | Rear Yard Depth – minimum  |  |
|     | From a <i>rear lot line</i> that abuts a <i>lot</i> on which a <i>dwelling</i> or <i>dwelling unit</i> is located  | 6.0 m  |
| .7  | Side Yard Width – minimum  |  |
|     | From a <i>side lot line</i> that abuts a <i>lot</i> on which a <i>dwelling</i> or <i>dwelling unit</i> is located or from a <i>side lot line</i> that abuts a <i>street</i>  | 6.0 m  |
| .8  | Landscaped Open Space Yard – minimum   | 15.0% of <i>lot area</i>                       |
| .10 | Gross Floor Area – <i>Retail Store</i> – maximum   | 20.0% of the <i>GFA</i> of the <i>main use</i> |
| 50  | All activities or uses shall take place entirely within a fully enclosed <i>building</i> . This provision does not apply to the following activity or use: <i>child care centre</i> , <i>gas bar</i> , <i>loading space</i> , <i>outdoor eating area</i> , <i>parking area</i> , <i>parking space</i> , <i>sports facility</i> , or <i>refuelling area</i> . |  |

## SECTION 15 - COMMERCIAL DISTRICTS 2. (CD2.)

(B/L 8614 Jun 23/1996; B/L 11358 Mar 1/1993; B/L 11614 Nov1/1993; B/L 11922 Sep 23/1994; B/L 12602 Jun 17/1996; B/L 39-1998 Mar 10/1998; B/L 162-1998 Jun 24/1998; B/L 33-2001 Oct 23/2001, OMB Decision/Order No. 1716 Case No. PL010233; B/L 370-2001 Nov 15/2001; B/L 363-2002 Dec 31/2002; B/L 375-2004 Dec 21/2004; B/L 46-2005 Mar 23/2005; B/L 232-2006 Jan 18/2007; B/L 164-2010 Nov 17/2010; B/L 7-2018 Feb. 23/2018 )  
[ZNG/5271]

### 15.1 COMMERCIAL DISTRICT 2.1 (CD2.1)

#### 15.1.1 PERMITTED USES

*Bakery*  
*Business Office*  
*Child Care Centre*  
*Commercial School*  
*Confectionery*  
*Food Outlet - Drive-Through*  
*Food Outlet - Take-Out*  
*Funeral Establishment*  
*Garden Centre*  
*Gas Bar*  
*Medical Office*  
*Micro-Brewery*  
*Parking Garage*  
*Personal Service Shop*  
*Place of Entertainment and Recreation*  
*Place of Worship*  
*Professional Studio*  
*Public Hall*  
*Public Parking Area*  
*Repair Shop – Light*  
*Restaurant*  
*Restaurant with Drive-Through*  
*Retail Store*  
*Temporary Outdoor Vendor's Site*  
*Tourist Home*  
*Veterinary Office*  
*Wholesale Store*  
*Existing Automobile Repair Garage*  
*Existing Service Station*

Any use accessory to any of the preceding uses. An *Outdoor Storage Yard* is prohibited, save and except, in combination with the following main uses:  
*Garden Centre, Temporary Outdoor Vendor's Site, Existing Automobile Repair Garage.*

#### 15.1.5 PROVISIONS

- |     |  |                      |
|-----|--|----------------------|
| .4  | Building Height – maximum  | 14.0 m               |
| .10 | Gross Floor Area – maximum   |                      |
|     | <i>Bakery or Confectionery</i>   | 550.0 m <sup>2</sup> |
| .26 | A <i>Temporary Outdoor Vendor's Site</i> is prohibited in a <i>Business Improvement Area</i> . |                      |

## SECTION 8 - DEVELOPMENT RESERVE DISTRICTS. 1 (DRD1.)

(ADDED by B/L 370-2001, Nov. 15/2001; AMENDED by B/L 168-2002, July 18/2002; B/L 164-2010, Nov. 17/2010; AMENDED by B/L 164-2017, Dec. 7/2017 [ZNG/5270] ; AMENDED by B/L 95-2018, Aug. 21/2018;

### 8.1 DEVELOPMENT RESERVE DISTRICT 1.1 (DRD1.1)

#### 8.1.1 PERMITTED USES

*Existing Dwelling*

*Any use accessory to an Existing Dwelling*

#### 8.1.5 PROVISIONS

.1	Lot Width – minimum	As Existing
.2	Lot Area – minimum	As Existing
.3	Lot Coverage – Total – maximum	45.0%
.4	Main Building Height – maximum	10.0 m
.5	Front Yard Depth – minimum	6.0 m
.6	Rear Yard Depth – minimum	7.50 m
.7	Side Yard Width – minimum	1.20 m

(AMENDED by B/L 95-2018, Aug. 21, 2018)

**TABLE 24.20.5.1 - REQUIRED PARKING SPACES**

<b>USE</b>	<b>PARKING RATE - MINIMUM</b>
Adult Entertainment Parlour	1 for each 7.5 m <sup>2</sup> GFA
Art Gallery	1 for each 45 m <sup>2</sup> GFA
Automatic Car Wash	0
Automobile Repair Garage	1 for each 45 m <sup>2</sup> GFA
Automobile Sales Lot	1 for each 45 m <sup>2</sup> GFA
Bake Shop	1 for each 22.5 m <sup>2</sup> GFA
Bakery	1 for each 45m <sup>2</sup> GFA for the first 2,700 m <sup>2</sup> GFA and 1 for each additional 180 m <sup>2</sup>
Billiard Hall	1 for each 22.5 m <sup>2</sup> GFA
Bingo Hall	1 for each 22.5 m <sup>2</sup> GFA
Bowling Alley	4 per alley
Building Material Recycling Centre	1 for each 45 m <sup>2</sup> GFA
Business Office	1 for each 45 m <sup>2</sup> GFA
Church (including a Church Hall)	1 for each 5.5 m <sup>2</sup> GFA uses as a church, chapel or sanctuary AND 1 for each 36 m <sup>2</sup> GFA not used as a church, chapel or sanctuary
Club	1 for each 22.5 m <sup>2</sup> GFA
Coin Operated Car Wash	0
College Student Residence	1 for each 4 beds
Collision Shop	1 for each 45 m <sup>2</sup> GFA
Combined Use Building – Dwelling Units	1.25 for each dwelling unit
Commercial School	2.5 for each classroom or teaching area AND 1 for each 22.5 m <sup>2</sup> of GFA of cafeteria, auditorium, gymnasium and other area of assembly
Confectionary	1 for each 45 m <sup>2</sup> GFA for the first 2,700 m <sup>2</sup> GFA AND 1 for each additional 180 m <sup>2</sup>
Confectioner's Shop	1 for each 22.5 m <sup>2</sup> GFA
Contractor's Office	1 for each 45 m <sup>2</sup> GFA used as a business office AND 1 for each 200 m <sup>2</sup> GFA used as a warehouse
Convenience Store	1 for each 22.5 m <sup>2</sup> GFA
Convent or Monastery	1 for each 4 beds
Correctional Facility	1 for each 2 beds
Day Nursery	1.5 for each classroom or teaching area
Double-duplex Dwelling	4
Drive-through Food Outlet	1 for each 22.5 m <sup>2</sup> GFA
Drive-through Restaurant	1 for each 7.5 m <sup>2</sup> GFA
Duplex Dwelling	2



TABLE 24.20.5.1 - REQUIRED PARKING SPACES	
USE	PARKING RATE - MINIMUM
Elementary School	1.5 for each classroom or teaching area
Entertainment Lounge	1 for each 7.5 m <sup>2</sup> GFA
Exhibition Hall	1 for each 36 m <sup>2</sup> GFA
Financial Office	1 for each 45 m <sup>2</sup> GFA
Food Convenience Store	1 for each 22.5 m <sup>2</sup> GFA
Fraternity or Sorority House	1 for each 4 beds
Funeral Home	1 for each 5.5 m <sup>2</sup> GFA used for a chapel, sanctuary or reposing room
Games Arcade	1 for each 22.5 m <sup>2</sup> GFA
Garden Centre	1 for each 22.5 m <sup>2</sup> GFA
Gas Bar	1 for each 45 m <sup>2</sup> GFA
General Salvage Operation	1 for each 45 m <sup>2</sup> GFA for the first 2,700 m <sup>2</sup> GFA AND 1 for each additional 180 m <sup>2</sup>
Group Home	1
Health Studio	1 for each 36 m <sup>2</sup> GFA
Heavy Repair Shop	1 for each 45 m <sup>2</sup> GFA for the first 2,700 m <sup>2</sup> GFA AND 1 for each additional 180 m <sup>2</sup>
Hospital	1 for each bed
Hotel	1 for each guest room AND 1 for each 22.5 m <sup>2</sup> GFA used for a restaurant, convention hall, meeting room and other places of assembly.
Library	1 for each 45 m <sup>2</sup> GFA
Light Repair Shop	1 for each 45 m <sup>2</sup> GFA
Lodging House	1 for each 6 beds
Major Commercial Centre (exclusive of a hotel or motel)	1 for each 22.5 m <sup>2</sup> GFA
Marina	0.5 for each 1 boat docking space AND 1 for each 1 boat anchorage space
Material Transfer Centre	1 for each 45 m <sup>2</sup> GFA for the first 2,700 m <sup>2</sup> GFA AND 1 for each additional 180 m <sup>2</sup>
Medical Office	1 for each 13.5 m <sup>2</sup> GFA
Micro-brewery	1 for each 45 m <sup>2</sup> GFA
Minor Commercial Centre	1 for each 22.5 m <sup>2</sup> GFA and when the combined GFA of all restaurants and entertainment lounges Exceeds 30% of the GFA of the Centre, 1 for each 7.5 m <sup>2</sup> GFA of all restaurants and entertainment lounges in excess thereof
Mobile Home	1

TABLE 24.20.5.1 - REQUIRED PARKING SPACES	
USE	PARKING RATE - MINIMUM
Motel	1 for each guest room AND 1 for each 22.5 <sup>2</sup> GFA used for a restaurant, convention hall, meeting room and other places of assembly
Motor Vehicle Dealership	1 for each 45 m <sup>2</sup> GFA
Motor Vehicle Salvage Operation	1 for each 45 m <sup>2</sup> GFA for the first 2,700 m <sup>2</sup> GFA AND 1 for each additional 180 m <sup>2</sup>
Multiple Dwelling containing a maximum of 4 Dwelling units	1 for each dwelling unit
Multiple Dwelling containing a minimum of 5 Dwelling units	1.25 for each dwelling unit
Museum	1 for each 45 m <sup>2</sup> GFA
Outdoor Market	0
Pawnshop	1 for each 22.5 m <sup>2</sup> GFA
Personal Service Shop	1 for each 22.5 m <sup>2</sup> GFA
Pharmacy	1 for each 22.5 m <sup>2</sup> GFA
Place of Entertainment and Recreation	1 for each 36 m <sup>2</sup> GFA
Power Generation Plant	1 for each 200 m <sup>2</sup> GFA
Professional Studio	1 for each 45 m <sup>2</sup> GFA
Public Hall	1 for each 7.5 m <sup>2</sup> GFA
Residential Care Facility	1 for each 4 beds
Restaurant	1 for each 7.5 m <sup>2</sup> GFA
Retail Store	1 for each 22.5 m <sup>2</sup> GFA
Secondary School	1.5 for each classroom or teaching area AND 1 For each 22.5 m <sup>2</sup> of GFA of cafeteria, auditorium, gymnasium and other area of assembly
Self-storage Facility	2
Semi-Detached Dwelling	1 for each dwelling unit
Service Station	1 for each 45 m <sup>2</sup> GFA
Shelter	1 for each 6 beds
Single -unit Dwelling	1
Stacked Dwelling Unit	1 for each dwelling unit
Take-Out Food Outlet	1 for each 22.5 m <sup>2</sup> GFA
Temporary Outdoor Vendor's Site	0
Theatre	1 for each 6 seats
Tourist Home	1 for each guest room AND 1 for each 22.5 m <sup>2</sup> GFA used for a restaurant, convention hall, meeting room and other places of assembly



**TABLE 24.20.5.1 - REQUIRED PARKING SPACES**

USE	PARKING RATE - MINIMUM
Townhome Dwelling having an attached garage or carport	1 for each dwelling unit
Townhome Dwelling without an attached garage or carport	1.25 for each dwelling unit
Transport Terminal	5 parking spaces, or 1 for each 45.0 m <sup>2</sup> GFA, whichever is greater
University Student Residence	1 for each 4 beds
Veterinary Clinic	1 for each 13.5 m <sup>2</sup> GFA
Veterinary Office	1 for each 13.5 m <sup>2</sup> GFA
Warehouse	1 for each 200 m <sup>2</sup> GFA
Wholesale Store	1 for each 45 m <sup>2</sup> GFA
Workshop	1 for each 45 m <sup>2</sup> GFA for the first 2,700 m <sup>2</sup> GFA AND 1 for each additional 180 m <sup>2</sup>
All other commercial uses not specifically listed	1 for each 36 m <sup>2</sup> GFA
All other industrial uses not specifically listed	1 for each 45 m <sup>2</sup> GFA for the first 2,700 m <sup>2</sup> GFA AND 1 for each additional 180 m <sup>2</sup> GFA

(AMENDED by B/L 144-2015, Nov. 6, 2015; B/L 169-2018, Dec. 19/2018)

#### 24.24.1 REQUIRED ACCESSIBLE PARKING SPACES

- .1 There shall be provided accessible parking spaces as shown in Table 24.24.1:

TABLE 24.24.1 – REQUIRED ACCESSIBLE PARKING SPACES		
TOTAL NUMBER OF PARKING SPACES IN PARKING AREA	REQUIRED NUMBER OF ACCESSIBLE PARKING SPACES – MINIMUM	
	TYPE A	TYPE B
1 to 25	1 space	0
26 to 100	2 percent of parking spaces	2 percent of parking spaces
101 to 200	1.5 percent of parking spaces	0.5 space plus 1.5 percent of parking spaces
201 to 1,000	0.5 space plus 1 percent of parking spaces	1 space plus 1 percent of parking spaces
1,001 or more	5 spaces plus 0.5 percent of parking spaces	5.5 spaces plus 0.5 percent of parking spaces

Page 24.9

- .2 If the calculation of the number of required Type A and Type B accessible parking spaces results in a number containing a fraction, the number shall be rounded up to the nearest whole number:

## COMMENTS

### Canada Post

This development, as described, falls within our centralized mail policy.

I will specify the condition which I request to be added for Canada Post Corporation's purposes.

- a) Canada Post's multi-unit policy, which requires that the owner/developer provide the centralized mail facility (front loading lockbox assembly or rear-loading mailroom [mandatory for 100 units or more]), at their own expense, will be in effect for buildings and complexes with a common lobby, common indoor or sheltered space.

Should the description of the project change, I would appreciate an update in order to assess the impact of the change on mail service.

If you have any questions or concerns regarding these conditions, please contact me. I appreciate the opportunity to comment on this project.

### Enbridge – Windsor Mapping

After reviewing the proposal liaison sheet and consulting our mapping system, please note that Enbridge Gas has active infrastructure along the north side of the right-of-way of Chappell Ave. A PDF drawing has been attached for reference.

Please Note:

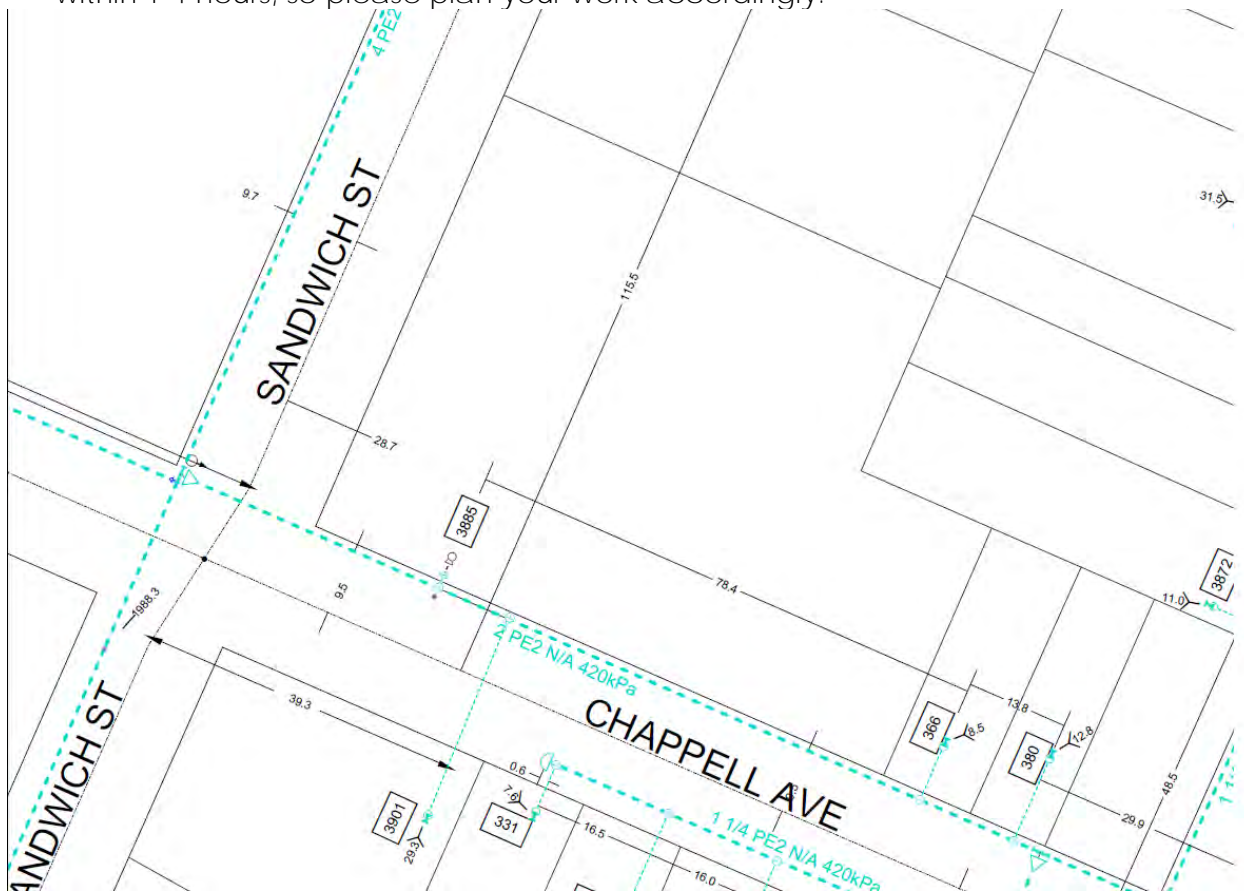
1. The shown piping locations are approximate and for information purposes only
2. The drawings are not to scale
3. This drawing does not replace field locates. Please contact Ontario One Call for onsite locates prior to excavating, digging, etc

Enbridge Gas requires a minimum separation of 0.6m horizontal and 0.3m vertical from all of our plant less than NPS 16 and a minimum separation 1.0m horizontal and 0.6m vertical between any CER-regulated and vital pipelines. For all pipelines (including vital pipelines), when drilling parallel to the pipeline, a minimum horizontal clearance measured from the edge of the pipeline to the edge of the final bore hole of 1 m (3.3 ft) is required. Please ensure that this minimum separation requirement is maintained, and that the contractor obtains locates prior to performing any work and utilizes safe excavation practices while performing any work in the vicinity.

Also, please note the following should you find any abandoned infrastructure in the area:

- Any pipe that is excavated, please assume that it is live
- If during the course of any job, any pipe is found that is not on the locate sheet and is in conflict with your work, please call our emergency number (1-877-969-0999), and one of our Union Gas representatives will respond to determine if that plant is in fact live or dead

- Please note that our Enbridge Gas representative will respond to the live or dead call within 1-4 hours, so please plan your work accordingly.



#### George Robinson – Site Plan Control

Given the site location and proximity to Windsor Salt operations, we recommend getting someone internal to review their Geotechnical Study as part of the liaison for the rezoning, rather than leaving that until SPC. Public Works or the Building Department may have capacity to do this. Recommend seeking clarification regarding the foundation type and strategy. i.e.: traditional piers/walls/footings, vs raft, vs pilings, etc.

Circulation to Windsor Salt on the OPA and ZBA should be considered.

See section 4.3.1 of the noise study report for a list of surrounding land uses that produce noise. In the context of the upcoming consultant's study on residential development and the RICBL, recommend reviewing suitability with management.

Site Plan Comments for report:

Pre-consultation review of the site plan application package is required prior to acceptance of an application for Site Plan Control, arrange consultation with a Site Plan Approval Officer. Contact George Robinson ([grobinson@citywindsor.ca](mailto:grobinson@citywindsor.ca)) or Jason Campigotto ([jcampigotto@citywindsor.ca](mailto:jcampigotto@citywindsor.ca)) with PDF copies of the plans to start

application process. Refer to Appendix 'B' of the Site Plan Approval Application for drawing information requirements.

#### Jason Scott – Transit Windsor

Transit Windsor has no objections to this development. The closest existing transit route to this property is the Crosstown 2. The closest existing bus stop to this property is located on Prince at Peter Southeast Corner. This bus stop is approximately 450 metres from this property falling outside of our 400 metre walking distance guideline to a bus stop. Our Council approved Transit Master Plan will introduce a new secondary route along Sandwich in the area of this development with a probable bus stop located on Sandwich at Chappell providing direct transit service for this development.

#### Rania Toufeili – Transportation Planning

This development is proposing a significant reduction in parking and no Parking Study was provided. I'm looking through my files and I don't think this application came through pre-submission.

Sometimes developers discuss parking in the TIS but they barely touched on it here. They should provide a Parking Study to justify the deficiency (PRR says its 85 spaces short). I'll include that a Parking Study is needed in my comments.

#### Amended Comments – April 5, 2022

The TIS is for the same proposal, and they assumed a conservative background growth so it would still be relevant. There are no additional comments for the TIS and it's acceptable.

However, as indicated before they will need parking study, the deficiency is very large.

- A parking study is required for this development for review due to the large deficiency with respect to Zoning By-Law 8600. The site plan does not indicate the correct deficiency value.
- A 4.6 meter corner cut-off is required at the corner of Sandwich Street and Chappell Avenue.
- Sandwich Street is classified as a Class II Arterial according to the Official Plan with a required right-of-way width of 20.1 meters per Schedule X. The existing right-of-way width is sufficient and therefore no conveyance is required.
- Chappell Avenue is classified as a local road according to the Official Plan with a required right-of-way width of 20 meters. The existing right-of-way width is sufficient and therefore no conveyance is required.
- A TIS was submitted for review with this application. The TIS has been reviewed and indicates that the no off-site improvements are required to accommodate this development.

- All new accesses shall conform to the TAC Geometric Design Guide for Canadian Roads and the City of Windsor Standard Engineering Drawings.
- All exterior paths of travel must meet the requirements of the Accessibility for Ontarians with Disabilities Act (AODA).

Amended Comments – May 18<sup>th</sup>, 2022

A TIS memo and updated parking study are required for the current proposal at this development.

The following can be noted to help justify the parking deficiency through the parking study:

- Ideally, the deficiency should be minimized if possible. Parts of the commercial space can be converted to amenity space to help decrease the need for parking if that option is viable.
- Zoning by-law deficiency should be correctly identified. Currently the deficiency is 85 spaces under the last proposal, if the proposal changes that might decrease.
- The developer can speak to how parking will be managed on site and the different peak parking times for the different uses (residential peak parking is in the evening and commercial is typically weekends during the day). Parking requirements will vary throughout the day for the different parts of the development.
- ITE Parking Generation Guide can be used to justify what parking requirements could be for the proposed use. A rate of 1 space per dwelling for each residential unit can be used per ITE land use code 222.
- Mitigation options should be considered (increase in bicycle parking, transit options in the area, etc.)
- On street parking availability can be used to help justify the deficiency, this was identified in the previous parking study as well.
- The specific expected use for commercial can be identified at this point if it is known, some commercial types require less parking than others such as small shops in a strip plaza versus a food store.

Jose Mejalli – Assessment Management Officer

No objection to the construction of an 11 storey Combined Use Building with 150 residential units and 12 retail spaces and respect relief from the maximum building height, minimum amenity space, and minimum parking space provisions

ERCA

The following is provided as a result of our review of Zoning By-Law Amendment Z-028-21 ZNG6503 OPA153 OPA6504. The purpose of the ZBA and OPA application is to permit the construction of an 11 storey Combined Use Building with 150 residential units and 12 retail spaces. The applicant seeks relief from maximum building height, minimum amenity space, and minimum parking space provisions.

DELEGATED RESPONSIBILITY TO REPRESENT THE PROVINCIAL INTEREST IN NATURAL HAZARDS AND REGULATORY RESPONSIBILITIES ASSOCIATED WITH THE CONSERVATION AUTHORITIES ACT

The following comments reflect our role as representing the provincial interest in natural hazards as outlined by Section 3.1 of the Provincial Policy Statement of the *Planning Act* as well as our regulatory role as defined by Section 28 of the *Conservation Authorities Act*.

We have reviewed our floodline mapping for this area and it has been determined this site is not located within a regulated area that is under the jurisdiction of the ERCA (Section 28 of the *Conservation Authorities Act*). As a result, a permit is not required from ERCA for issues related to Section 28 of the *Conservation Authorities Act*, Development, Interference with Wetlands and Alteration to Shorelines and Watercourses Regulation under the *Conservations Authorities Act*, (Ontario Regulation No. 158/06).

The applicant will require a development review from the Essex Region Conservation Authority at the time of Site Plan Control.

Significant Groundwater Recharge Area (SGRA)

The property is located within a Significant Groundwater Recharge Area (SGRA). There are no associated Significant Drinking Water Threats (SDWTS) or policies with these areas because the municipal water treatment plant does not use groundwater as its supply. However, the proponent should consider the sensitive nature of this natural feature. These areas are at a greater risk for contamination from land use activities. Any future proposed activity on these properties at minimum should not result in increased risk of contamination of the recharge area.

WATERSHED BASED RESOURCE MANAGEMENT AGENCY

The following comments are provided in an advisory capacity as a public commenting body on matters related to watershed management.

SECTION 1.6.6.7 PPS, 2020 - Stormwater Management

We are concerned with the potential impact of the quality and quantity of runoff in the downstream watercourse due to future development of this site.

We recommend that the municipality ensure through the Site Plan Control process that the release rate for any future development is controlled to the capacity available in the existing storm sewers/drains. In addition, that stormwater quality and stormwater quantity are addressed up to and including the 1:100 year storm event and be in accordance with the guidance provided by the Stormwater Management Planning and Guidance Manual, prepared by the Ministry of the Environment (MOE, March 2003) and the Windsor-Essex Region Stormwater Management Standards Manual.



If this property is subject to Site Plan Control, we request to be included in the circulation of the Site Plan Control application. We reserve to comment further on storm water management concerns, until we have had an opportunity to review the specific details of the proposal through the site plan approval stage.

#### PLANNING ADVISORY SERVICE TO PLANNING AUTHORITIES - NATURAL HERITAGE POLICIES OF THE PPS, 2020

The following comments are provided from our perspective as an advisory service provider to the Planning Authority on matters related to natural heritage and natural heritage systems as outlined in Section 2.1 of the Provincial Policy Statement of the *Planning Act*. The comments in this section do not necessarily represent the provincial position and are advisory in nature for the consideration of the Planning Authority.

The subject property is not within or adjacent to any natural heritage feature that may meet the criteria for significance as defined by the PPS. Based on our review, we have no objection to the application with respect to the natural heritage policies of the PPS.

#### FINAL RECOMMENDATION

With the review of background information and aerial photograph, we advise that a stormwater management plan be completed to the satisfaction of the City of Windsor and the Essex Region Conservation Authority, at the time of Site Plan Control. The applicant will require a development review from the Essex Region Conservation Authority at the time of Site Plan Control.

Barbara Rusan – Building Department

The Building Code Act, Section 8.(1) requires that a building permit be issued by the Chief Building Official for construction or demolition of a building. The building permit review process occurs after a development application receives approval and once a building permit application has been submitted to the Building Department and deemed a complete application.

Due to the limited Ontario Building Code related information received, review of the proposed project for compliance to the Ontario Building Code has not yet been conducted.

It is strongly recommended that the owner and/or applicant contact the Building Department to determine building permit needs for the proposed project prior to building permit submission.

The City of Windsor Building Department can be reach by phoning 519-255-6267 or, through email at [buildingdept@citywindsor.ca](mailto:buildingdept@citywindsor.ca)

In addition to the above this development would require a Record of Site Condition (i.e. proposed change to a more sensitive land use – residential from previous commercial use).

#### Frank Garardo – Policy & Special Studies

The proposed Official Plan and Zoning by-law amendments will allow a mixed use development with commercial uses on the main floor and dwelling units as an additional permitted use above commercial use(s) at 0 Sandwich Street and 3885 Sandwich Street (Plan 40 PT Lot 28 East Side; & Pt Lot 28 West Side; Corner).

The OPA will change the land use designation from "Industrial" to site specific "Industrial" to permit additional residential uses.

The subject property is designated as "Industrial" on Schedule D: Land use of the city of Windsor Official Plan.

The subject property is located in an area identified as regional employment on Schedule "J" of the City of Windsor Official Plan.

The subject property is identified on Schedule 'C-1' of the Official Plan as having High Archaeological Potential.

The subject property is identified on Schedule G: Civic image as a "Civic Way" in the City of Windsor Official Plan.

The subject property is located within the vicinity of railyards and corridors on Schedule F: of the City of Windsor Official Plan.

The subject property is located within the vicinity of "Pollution Control Plant" on Schedule C: Development Constraints in the City of Windsor Official Plan.

The proposal is located in an industrial area with nearby industrial and active salt solution mining operations.

Recommendations from the Noise and Vibration studies and Geotechnical studies should be applied.

#### Barry Horrobin – Police Services

The Windsor Police Service has no concerns or objections with the proposed Official Plan and Zoning By-law amendment to permit a multi-storey combined use building on the subject property. The proposed redevelopment will offer an injection of positive activity generation into a long vacant space that will be beneficial to the surrounding neighbourhood. In addition, a review of the draft site plan shows a layout that will facilitate proper emergency police response capability. We will provide more detailed comments that pertain to specific safety and security requirements for this development once the application reaches the site plan review stage.

Kristina Tang – Heritage Planner

Archaeological Assessment Report Entitled "Stage 1 &2 Archaeological Assessment 3885 Sandwich Street Part of Lots 27 &28 East of Sandwich Street and Part of Lot 28 West of Peter Street Registered Plan 40 Geographic Township of Sandwich City of Windsor Essex County", Dated Apr 15, 2020, Filed with MTCS Toronto Office on N/A, MTCS Project Information Form Number P321-0110-2020, MTCS File Number 0012209, has been entered into the Ontario Public Register of Archaeological Reports. Although the report recommends that no further archaeological assessment of the property is recommended, the applicant is still to note the following archaeological precautions:

1. Should archaeological resources be found during grading, construction or soil removal activities, all work in the area must stop immediately and the City's Planning & Building Department, the City's Manager of Culture and Events, and the Ontario Ministry of Heritage, Sport, Tourism and Culture Industries must be notified and confirm satisfaction of any archaeological requirements before work can recommence.
2. In the event that human remains are encountered during grading, construction or soil removal activities, all work in that area must be stopped immediately and the site secured. The local police or coroner must be contacted to determine whether or not the skeletal remains are human, and whether the remains constitute a part of a crime scene. The Local police or coroner will then notify the Ontario Ministry of Heritage, Sport, Tourism and Culture Industries and the Registrar at the Ministry of Government and Consumer Services if needed, and notification and satisfactory confirmation be given by the Ministry of Heritage, Sport, Tourism and Culture Industries.

Contacts:

Windsor Planning & Building Department:

519-255-6543 x6179, ktang@citywindsor.ca, planningdept@citywindsor.ca

Windsor Manager of Culture and Events:

Michelle Staadegaard, (O) 519-253-2300x2726, (C) 519-816-0711, mstaadegaard@citywindsor.ca

Ontario Ministry of Heritage, Sport, Tourism and Culture Industries Archaeology Programs Unit, 1-416-212-8886, Archaeology@ontario.ca

Windsor Police: 911

Ontario Ministry of Government & Consumer Services A/Registrar of Burial Sites, War Graves, Abandoned Cemeteries and Cemetery Closures, 1-416-212-7499, Crystal.Forrest@ontario.ca

In addition, there's a question I had below about Geotechnical. Who would be reviewing the Geotechnical from the City's side, are the construction vibrations reasonable and monitored for surrounding areas? There's a heritage property in the vicinity but some houses down so may/may not be impacted.

395 Chappell Ave                      House   c1915      Regency Cottage; bellcast hipped roof   Sandwich

Anne Marie Albidone – Environmental Services

No concerns

Stefan Fediuk – Landscape Architect

Pursuant to the application for a zoning amendment (Z 028-21 [ZNG-6503] & OPA 152 [OPA-6504) to permit the construction of an 11 storey Combined Use Building with 150 residential units and 12 retail spaces on the subject, please note no objections.

The applicant is also looking for relief from maximum building height, minimum amenity space, and minimum parking space provisions. Please note the following comments:

Zoning Provisions for Parking Setback:

The proposed zoning of CD2.2 does not regulate setbacks. Section 25 Parking Area General Provisions requires setbacks for parking areas from any street to be 3.0m minimum as a landscaped yard. Please include a site-specific zoning provision in conjunction with the amendment for change of permitted use, specifying a minimum 3.0 m landscape set back for parking areas in the Exterior Yard.

The Planning Rationale Report Identifies that there are a total of 157 parking spaces yet the Site plan identifies 185 spaces. As difference of 28 spaces. However, the PRR also indicates that the required number of spaces is 242 for the proposed site-specific CD2.2 Zoning.

Landscape Open Space provided is 877.56m<sup>2</sup>. It is also proposed Section 5.1.3 Zoning Bylaw (ZBL) of the PRR as part of calculations for relief of 864.1m<sup>2</sup> for the Minimum Amenity Areas from 1,800m<sup>2</sup>. Interpretation that amenity areas include all landscape areas is incorrect. Areas of green space that are part of required landscape setbacks should not be part of this calculation. Per the Zoning Bylaw, "Amenity Area" means a landscaped open space yard or a recreational facility as an accessory use to a dwelling or a dwelling unit located on the same lot therewith. The landscape areas proposed on the site plan could not provide any recreational activity or accessory use that could be considered amenity space, therefore they cannot be considered as amenity space. In addition the PRR also suggests that private balconies attached to residential units be considered. As private spaces they are not open to the public and should not be considered as public amenity space. Based on that clarification, the requested site-specific relief is greater than proposed and should be identified as follows:

- Required: 1,800m<sup>2</sup>
- Proposed: 58.34m<sup>2</sup>
- Requested Relief: 1,741.66m<sup>2</sup>.

The PRR also suggests that the site is in close proximity to nearby parks. Per Discover Our Parks; the City of Windsor's Parks and Outdoor Recreation Master Plan, the standard of 0.8km (1.2miles) is considered a comfortable walking distance for pedestrians to public parks. The closest public park to the site is the college Avenue Bikeway at 0.826km from the site. Recent studies have concluded that aging populations, in urban areas are only willing to walk half that distance, making the proposed development more isolated from

public amenity spaces provide by the City. In addition, the parks sited by the PRR are farther yet with Mic Mac Park (including the Novelletto Complex) at 1.07km (1.53km) and Black Oak Heritage Park further yet at over 2km away.

Therefore the need for common amenity space at this site is extremely important and will need to be considered in the final development through the provision of outdoor open space, or interior or roof top common space amenity facilities for the proposed residential density.

Tree Preservation:

There is a remnant hedgerow of trees and scrub undergrowth running north-south along the back boundary line the site. For submission to Site Plan Review to assess the urban tree canopy loss and any potential for preservation a tree inventory identifying species, location, and condition of all trees on the subject lands and abutting municipal boulevards is required.

Parkland Dedication:

All requirements will be determined at the time a Site Plan application is received.

Adam Coates – Senior Urban Designer

- Encourage the applicant to increase the floor to floor height of the first floor in order to be complimentary to the traditional ground floor commercial volumes found in the adjacent Sandwich Heritage Conservation District. A minimum ground floor height of 14 feet is encouraged.
- Further exterior design opportunities will be explored during the Site Plan Control process, with respect to exterior materials, and demarcations.

Robert Perissinotti – Engineering & ROW

The subject lands are located at 0 Sandwich St and 3885 Sandwich St. The applicant is proposing to construct an 11 storey Combined use building with 150 residential units and 12 retail spaces. The lands are Industrial in the City of Windsor Official Plan, zoned Commercial District 2.1 (CD2.1), Development Reserve District 1.1 (DRD1.1), and Manufacturing District 1.4 (MD1.4) in Zoning By-law 8600. The applicant is requesting relief from maximum building height, minimum amenity spaces and minimum parking space provisions.

It should be noted that the listed property owner of 0 Sandwich Street and 3885 Sandwich Street currently differ. This area of Sandwich Street has planned future reconstruction project which will involve relining most of the sewers in the area, new curb and gutter, sidewalks, grade adjustments and a proposed new curb layout to accommodate parking along Sandwich St along the development site. The reconstruction timeline has yet to be solidified. Further impacts and requirements for the development as a result of the Sandwich St redevelopment will be addressed during the SPC process.

SEWERS – There is a 450mm vitrified clay combined sewer located in the Sandwich St right-of-way and a 250mm vitrified clay combined sewer located in the Chappel Avenue right-of-way. A sewer servicing study will be required for sanitary and storm. The proposed development includes a new parking area therefore storm water management and a site-grading plan will be required restricting storm water flow to pre-development conditions.

RIGHT-OF-WAY – Sandwich Street as Class 2 Arterial Road requiring a right-of-way width of 20.1m, the current right-of-way is sufficient and no conveyance is required at this time. Chappell Avenue as Local Road requiring a right-of-way width of 20.1m, the current right-of-way is sufficient and no conveyance is required at this time. The right-of-way along Sandwich Street is located within the ERCA regulated lands and consultation with Essex Region Conservation Authority will be required to determine requirements.

A 4.6 x 4.6 meter corner cut-off is required at the north-west corner of Sandwich Street and Chappell Avenue.

Driveways are to be constructed as per AS-204, complete with straight flares and no raised curbs within the right-of-way. Redundant curb cuts and sidewalks shall be removed and restored in accordance with City Standards to the satisfaction of the City Engineer. A Street Opening permit will be required for any work in the right-of-way.

In summary we have no objection to the proposed rezoning, subject to the following requirements (requirements will be enforced at the time of Site Plan Control):

Site Plan Control Agreement - The applicant enters into an agreement with the City of Windsor for all requirements under the General Provisions of the Site Plan Control Agreement for the Engineering Department.

Corner Cut-off – The owner agrees prior to the issuance of a construction permit, to **gratuitously convey a 4.6m x 4.6m (15'x15') corner cut-off** at the north-east intersection of Sandwich Street and Chappell Avenue in accordance with City of Windsor Standard Drawing AS-230.

Redundant Curb Cuts – The owner agrees to remove and reinstate any redundant curb cuts with full height curb and properly restore the area to the satisfaction of the City Engineer.

ERCA Requirements – The owner(s) further agrees to follow all drainage and flood proofing recommendations of the Essex Region Conservation Authority (ERCA) may have with respect to the subject land, based on final approval by the City Engineer. If applicable, the Owner will obtain all necessary permits from ERCA with respect to the drainage works on the subject lands.

Servicing Study – The owner agrees, at its own expense, to retain a Consulting Engineer to provide a detailed servicing study report on the impact of the increased flow to the existing municipal sewer systems, satisfactory in content to the City Engineer and prior to

the issuance of a construction permit. The study shall review the proposed impact and recommend mitigating measures and implementation of those measures.

Sanitary Sampling Manhole – The owner agrees for all non-residential uses, to install a sanitary sampling manhole accessible at the property line of the subject lands to the City Engineer at all times. The determination of the requirement or interpretation if a sampling manhole exists or exceptions to such, will be to the satisfaction of the City Engineer.

#### Greg Atkinson – Economic Development

The subject site is located within an Employment Area. The Planning Rationale Report does not address section 3.1.2 of the PPS, which applies to Employment Areas.

The proposed site specific Industrial OPA to permit a mixed use building is not supportable as it is not consistent with PPS policy 1.3.2.3:

*“1.3.2.3 Within employment areas planned for industrial or manufacturing uses, planning authorities shall prohibit residential uses and prohibit or limit other sensitive land uses that are not ancillary to the primary employment uses in order to maintain land use compatibility.”*

As required in the pre-submission letter (attached) any OPA to permit residential use is required to justify a conversion to non-employment use consistent with PPS policy 1.3.2.5:

*“1.3.2.5 Notwithstanding policy 1.3.2.4, and until the official plan review or update in policy 1.3.2.4 is undertaken and completed, lands within existing employment areas may be converted to a designation that permits non-employment uses provided the area has not been identified as provincially significant through a provincial plan exercise or as regionally significant by a regional economic development corporation working together with affected upper and single-tier municipalities and subject to the following:*

- a) there is an identified need for the conversion and the land is not required for employment purposes over the long term;*
- b) the proposed uses would not adversely affect the overall viability of the employment area; and*
- c) existing or planned infrastructure and public service facilities are available to accommodate the proposed uses.”*

Section 5.1.2 of the Planning Rationale Report provides analysis with respect to section 6.4 of the Official Plan (Employment). The analysis states that: ***“The proposed commercial space is a permitted use in the ‘Industrial’ designation. It is requested to allow a combined use building as a site specific permitted use to Section 6.4.3.2 – Ancillary Uses in the Industrial designation.”***

The full range of commercial uses permitted in the CD2.2 District would not typically be permitted within an Industrial land use designation (e.g. business office is not a permitted use). The recommended zoning should be specific about what type and size of



commercial uses are permitted in conformity with the permitted uses in section 6.4.3.2 of the Official Plan. For example, Policy 6.4.3.2 b) permits convenience stores and restaurants provided that by their size the uses are designed to serve the employees in the Industrial area. 1,244.90 m<sup>2</sup> (13,399.99 sq. ft.) of commercial space is proposed. The typically size of a convenience retail store is approximately 3,000 sq. ft.

If the proposal includes all commercial uses within the CD2.2 zoning district with no maximum size—the appropriate commercial or mixed use land use designation should be applied.

#### Enwin

**Hydro Engineering:** No Objection provided adequate clearances are achieved and maintained, ENWIN has an existing overhead pole line along the west limit of the site with a 27600-volt primary high voltage, 600/347 volt – 3 phase secondary line and 120240-volt single phase secondary hydro distribution.

Prior to working in these areas, we would suggest notifying you contractor and referring to the Occupational Health and Safety Act and Regulations for Construction Projects to confirm clearance requirements during construction. Also, we suggest referring to the Ontario Building Code for permanent required clearances for New Building Construction.

**Water Engineering:** Water Engineering has no objections.

#### Zoning Coordinator

Most of the issues will be resolved with the rezoning. I've listed a few minor concerns below.

- Provide 4 type “B” accessible parking spaces
- Provide parking area separation dimensions
  - o Parking area separation from an interior lot line should be 0.9m
- Provide 27 visitor parking spaces
- Confirm parking spaces adjacent to walls will be 3.5m in width
- Provide 3 loadings spaces

Let me know if you need anything else.

Regards,

Connor Cowan | Zoning Coordinator  
Planning Department

## APPENDIX I - Draft By-law Amendment

BY-LAW NUMBER -2022

A BY-LAW TO FURTHER AMEND BY-LAW NUMBER 8600  
CITED AS THE "CITY OF WINDSOR ZONING BY-LAW"

Passed the \_\_\_\_\_ day of \_\_\_\_\_, 2020.

**WHEREAS** it is deemed expedient to further amend By-law Number 8600 of the Council of The Corporation of the City of Windsor, cited as the "City of Windsor Zoning By-law" passed the 31st day of March, 1986, as heretofore amended:

**THEREFORE** the Council of The Corporation of the City of Windsor enacts as follows:

1. By-law Number 8600 is further amended by changing the Zoning District Maps or parts thereof referred to in Section 1, of the by-law and made part thereof, so that the zoning district symbol of the lands described in Column 3 shall be changed from that shown in Column 5 to that shown in Column 6:

1. Item Number	2. Zoning District Map Part	3. Lands Affected	4. Official Plan Amendment Number	5. Zoning Symbol	6. New Zoning Symbol
1	4	Part Lot 27, Registered Plan 40S (PIN 012580193), Lot 28 East Side, Registered Plan 40 (PIN 012580190), and Lot 28 West Side, Registered Plan 40, situated at the northeast corner of Sandwich Street and Chappell Avenue (known municipally as 0 and 3885 Sandwich Street; Roll # 050-170-09700 and 050-170-09800)	152	DRD1.1 CD2.1 MD1.4	CD2.1

2. That subsection 1 of Section 20, of said by-law, is amended by adding the following paragraph:

445. **NORTHEAST SIDE OF SANDWICH STREET AND CHAPPELL AVENUE**

For the lands comprising Part Lot 27, Registered Plan 40S; Lot 28 East Side on Registered Plan 40; and Lot 28 West Side, Registered Plan 40:

1. The following uses are prohibited:
  - Bakery
  - Food Outlet-Drive-Through
  - Gas Bar
  - Place of Entertainment and Recreation
  - Public Hall
  - Restaurant with Drive-through
2. *A Multiple Dwelling and Dwelling Units in a Combined Use Building* shall be additional permitted main uses and shall be subject to the following provisions:
  - a) Main Building Height – maximum 37.0 m
  - b) Amenity Area – Per Dwelling Unit – minimum 5.0 m<sup>2</sup>
  - c) For a *Combined Use Building*, all *dwelling units*, not including entrances thereto, are located above the non-residential uses
  - d) Exposed flat concrete block walls or exposed flat concrete wall, whether painted or unpainted, are prohibited
  - e) Required Number of Parking Spaces – minimum - 156

[ZDM 4; ZNG/6503]

3. The said by-law is further amended by changing the Zoning District Maps or parts thereof referred to in Section 1, of said by-law and made part thereof, so that the lands described in Column 3 are delineated by a broken line and further identified by the zoning symbol shown in Column 5:

1. Item Number	2. Zoning District Map Part	3. Lands Affected	4. Official Plan Amendment Number	5. Zoning Symbol
2	4	Part Lot 27, Registered Plan 40S (PIN 012580193), Lot 28 East Side, Registered Plan 40 (PIN 012580190), and Lot 28 West Side, Registered Plan 40, situated at the northeast corner of Sandwich Street and Chappell Avenue (known municipally as 0 and 3885 Sandwich Street; Roll # 050-170-09700 and 050-170-09800)	152	S.20(1)445

DREW DILKENS, MAYOR

CLERK

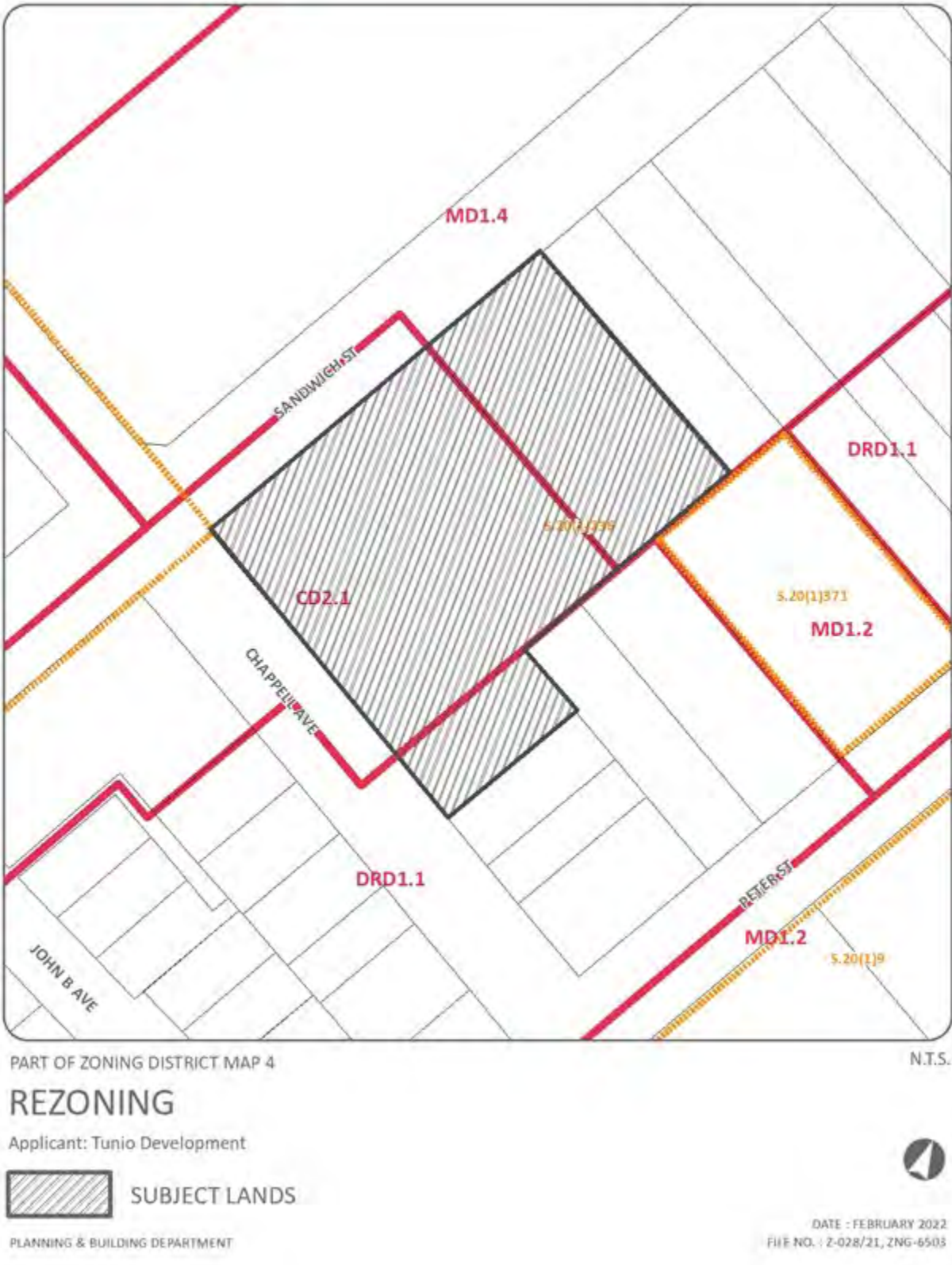
First Reading - , 2022  
Second Reading - , 2022  
Third Reading - , 2022

SCHEDULE 2

1. By-law \_\_\_\_\_ has the following purpose and effect:

To amend the zoning of Part Lot 27, Registered Plan 40S (PIN 012580193), Lot 28 East Side, Registered Plan 40, and Lot 28 West Side, Registered Plan 40 (PIN 012580190), situated at the northeast corner of Sandwich Street and Chappell Avenue (known municipally as 0 and 3885 Sandwich Street; Roll # 050-170-09700 and 050-170-09800) from Development Reserve District 1.1 (DRD1.1), Commercial District 2.1 (CD2.1) and Manufacturing District 1.4 (MD1.4) to Commercial District 2.1 (CD2.1) and by adding the following site specific provision to allow a multiple dwelling and dwelling Units in a Combined Use Building as an additional permitted use subject to additional provisions.

2. Key map showing the location of the lands to which By-law \_\_\_\_\_ applies.  
(See map following page.)



APPENDIX 'J'  
ACOUSTICAL REPORT



**ACOUSTICAL REPORT  
MIXED USED DEVELOPMENT  
3885 SANDWICH STREET  
WINDSOR, ONTARIO**

PROJECT NO. 20-028

DATED: OCTOBER 1, 2021



27 Princess St., Unit 102  
Leamington, ON N8H 2X8  
519.326.6161 T F 1.844.842.9188  
[bairdAE.ca](http://bairdAE.ca)

## TABLE OF CONTENT

1.	INTRODUCTION .....	1
2.	NOISE CRITERIA .....	1
3.	EVALUATION OF SURROUNDING LAND USES .....	3
4.	NOISE ENVIRONMENT.....	4
4.1	Noise Monitoring .....	4
4.2	Transportation Source.....	5
4.2.1	Mitigation Measures .....	7
4.3	Stationary Sources.....	8
4.3.1	Surrounding land Uses.....	8
4.3.2	Receiver Locations .....	9
4.3.3	Stationary Noise Level Results .....	10
5.	MITIGATION MEASURES.....	12
6.	RECOMMENDATIONS.....	13
7.	SUMMARY .....	15

### TABLES

Table 1 – MOECC Noise Level Criteria – Road and Rail.....	2
Table 1 – MOECC Noise Level Criteria – (Road).....	2
Table 3 – MOECC Noise Level Criteria – Stationary (Class 4).....	3
Table 4 – Background Traffic Volumes (2020).....	5
Table 5 – Noise Results Comparison .....	6
Table 6 – Receiver Locations .....	10
Table 7 – Post Development Predicted Noise Levels .....	11
Table 8 – Required STC Values (Bedroom/Living) .....	12

## **APPENDICES**

Appendix A Noise Information Plan and Background Information

Appendix B Stamson Output

Appendix C INoise Output

Appendix D Building Component

Appendix E Noise Monitoring



## 1. INTRODUCTION

Baird AE has been retained to conduct an acoustical study to examine the impacts of noise created by industries and road traffic on the new development in the City of Windsor. This report will recommend mitigation measures based on criteria set by Ministry of Environment and Climate Change (MOECC).

The subject land is approximately 0.669 hectares in size and are bounded by Sandwich Street to the west, Chappell Avenue to south, and existing residential development to the east and west. The site has driveway access from Sandwich Street which will be replaced with two new accesses, one from Sandwich Street and Chappell Avenue.

The development is mixed use building comprised of 150 residential units, retail space of 2900ft<sup>2</sup>, parking lot and landscaping area. The building is 11 storey of 35m high with a total floor area of 1480.6m<sup>2</sup>. As per City's mapping, the development area zone as residential and commercial. Refer to Appendix A for the development location, zone and site layout.

## 2. NOISE CRITERIA

The MOECC publication NPC-300 "Environmental Noise Guideline Stationery and Transportation Sources – Approval and Planning" was used in this noise study. Based on initial investigation, the primarily noise affecting the development is from nearby industries rail noise and roadway traffic noise. The air traffic noise was not considered, as the development is located outside the zone of influence of local airports.

MOECC publication NPC 300 provides noise levels resulting from transportation sources for both outdoor and indoor areas. Table 1 summarizes these limits.

**Table 1 – MOECC Noise Level Criteria – Road and Rail**

Type of Space	Type of Space	Sound Exposure Level	
		Road	Rail
Outdoor Living Area	0700-2300	55 dBA	
Living/Dining Room	0700 - 2300	45 dBA	40 dBA
	2300 - 0700	45 dBA	40 dBA
Sleeping Quarters	0700 - 2300	45 dBA	40 dBA
	2300 - 0700	40 dBA	35 dBA

MOECC Guidelines NPC-300 Section C3.2.3 – August 2013

It is noted that acoustical impacts from the railways include an additional 5.0 dBA to road noise. It has been identified that railways are located within the vicinity of the proposed development.

Following are the MOECC criteria set for transportation sources which include warning clauses and physical mitigation.

**Table 2 – MOECC Noise Level Criteria – (Road)**

Location	Time Period	Noise Levels Requirement
Outdoor - Living Areas	07:00 - 23:00	<55dBA – No Control
		55 to 60dBA – Physical Control or Type A Clause
		> 60dBA – Physical Control and Type B Clause
Outside - Living/dining Plane of Window	07:00 - 23:00	<55dBA – No Control
		55 to 65dBA – Forced Air and Type C Clause
		> 65dBA – Air Conditioner, Type D Clause and Building Components
Outside - Living/dining Plane of Window	23:00 to 07:00	<50dBA – No Control
		50 to 60dBA – Forced Air and Type C Clause
		> 60dBA – Air Conditioner, Type D Clause and Building Components

MOECC Guidelines NPC-300 Section C7.1.1 and C7.1.2 – August 2013



For this development, a "Class 4" area designation endorsed by City engineer department for stationary sources.

For Outdoor point of reception, Class 4 area has a 5dBA more relaxed criteria than a Class 1 area. For plane of windows, Class 4 area has a 10 dBA more relaxed criteria. Table 3 summarizes these limits.

**Table 3 – MOECC Noise Level Criteria – Stationary (Class 4)**

Time Period	Outdoor Points of Reception	Plane of Window
0700 - 1900	55 dBA	60 dBA
1900 - 2300	55 dBA	60 dBA
2300 - 0700	55 dBA	55 dBA

Source: MOECC publication NPC-300 "Sound level limits for Stationary Sources in Class 1 & 2 Areas (Rural)", Tables B-1, B-2; August 2013.

Table 2 describes indoor sound level limits that are used to calculate the Sound Transmission Class (STC) should outdoor sound level limits be triggered for building components.

As the site is relative to stationery and transportation noise, the mitigation measures shall be incorporated into the development through the use of acoustical barriers to protect the outdoor living areas using stationary MOECC noise level criteria (table 3), and improved building components such as door, windows and walls to protect the indoor living spaces using the MOECC rail noise criteria (table 2). These indoor and outdoor criteria represent the worst-case scenario.

### **3. EVALUATION OF SURROUNDING LAND USES**

The surrounding industrial and commercial facilities are within close proximity of the development site and having the potential for air and noise impacts. In accordance with the "Compatibility between Industrial Facilities and Sensitive Land Uses", published by the Ontario Ministry of the Environment and Climate Change (MOECC) as Guideline D-6, the three classes according to their size, volume of operations, and nature of their emissions and defines sensitive land uses. These classes are provided below and definitions to illustrate three Industrial classes are provided in Appendix A.

<u>Industrial Classification</u>	<u>Separation Distance (m)</u>
Class I – Light Industrial	20
Class II – Medium Industrial	70
Class III – Heavy Industrial	300

Based on aerial mapping and site inspection, the nearby industrial and commercial facilities fall into all three industries classes. In order to mitigate noise and air potential, the following clause should be added to the lease agreement or deed.

*"Purchasers/tenants are advised that sound levels due to the adjacent industry (facility) (utility) are required to comply with sound level limits that are protective of indoor areas and are based on the assumption that windows and exterior doors are closed. This dwelling unit has been supplied with a ventilation/air conditioning system which will allow windows and exterior doors to remain closed."*

The proposed site is residential development; hence the development will not potentially impact the surrounding users. Additionally, rows of trees will be installed along the Sandwich Street to make it a buffer in order to meet the MOECC noise and dust criteria. Detail noise mitigation are provided in Section 4.2.1.

## **4. NOISE ENVIRONMENT**

### **4.1 Noise Monitoring**

The on-site noise source measurement was carried out in accordance with the MOECC publication NPC-103 Noise Measurements Procedures. Sound levels measurement were conducted at three locations (M1, M2 and M3 as shown in Figure 1) on Friday, September 2, 2020 for 1 hours at two locations (M1 and M2) and 20 mins at M3 location. The weather condition consisted of partially cloudy, temperature ranging between 12°C to 13°C, low winds (<15km/h), and relative humidity of 66%. Measurements were taken using a Extech Noise Meter model 407780A noise monitoring device. The sound levels were measured at a height of 1.5m above the ground. The noise equipment equipped with 1/1 octave and 1/3 octave band filters. The laboratory Certification of Calibration for the noise meters are provided in Appendix E. Tonal and impulsive noise characteristics were observed during the measurements.

The equivalent sound pressure level (Leq) at three monitoring locations were 70dBA at M1, 70dBA at M2 and 60dBA at M3. The maximum sound level at these locations were 80.5dBA at M1, 80.0dBA at M2 and 68dBA at M3. Monitoring locations are shown in Appendix E, Figure 1.

The monitored noise level is higher than 60dBA during daytime. Hence, mitigation measures and building component are required.

## 4.2 Transportation Source

Traffic counts at intersections of Sandwich Street and Chappell Avenue were taken on July 21, 2020 by sub-consultant Pyramid. The traffic counts and other relevant data are included in Appendix A.

Sound level prediction software STAMSON 5.04, based on MOECC ORNAMENT (Ontario Road Noise Analysis Method for Environmental and Transportation, 1989) was used for estimate noise level from roadway traffic and rail. Further, MOECC requires that all traffic data be projected ten (10) years into the future from the date of construction such that the proposed mitigation will be relevant for future volumes. Traffic volumes along with other relevant traffic data utilized by STAMSON are summarized in Table 4.

**Table 4 – Background Traffic Volumes (2020)**

Item	Sandwich Street
Annual Average Daily Traffic	4,794 vpd
% Medium Trucks	2.5%
% Heavy Trucks	2.5%
Road Grade	< 2.0%
Speed Limit	50 km/h
Day/Night Percent Split	90% / 20%

One Canadian Pacific rail track is within vicinity of the development. This rail provides services to the nearby industries. The rail track revolves around development with 150m to 450m radii distance. The grade of rail track is level with the ground. There are two public grade crossings within study area.

Based on aerial mapping, the following information was assumed for modelling:



Number of freight trains between 0700 & 2300:	9
Number of freight trains between 2300 & 0700:	4
Average number of cars per train:	78
Maximum cars per train freight:	168
Number of Locomotives per train:	2 (4 max)
Maximum permissible train speed:	40mph for mainline, 25mph on lead

Using the above data in Stamson software, the daytime and nighttime free field limits were established. The noise level limits are shown in Figure 1 – Noise Information Plan and detail results are provided in Appendix A.

Based on free field analysis, the traffic noise from Sandwich Street and rail noise will have impact on the western facades of building as it is higher than 60dBA noise limits which will trigger for noise attenuation method. Further, mitigation measure such warning clause such as air conditioning and forced air heating are also required. Further analysis is required to compare predicted noise level with monitored noise level (see Section 3.1 for monitoring reading). Stamson output results are described in table below and details are provided in Appendix E.

**Table 5 – Noise Results Comparison**

Receiver Location	Monitoring Level Daytime (dBA)	Stamson Traffic Predicted Level (dBA)	
		Daytime	Nighttime
Monitoring 1	70.5	62.1	58.8
Monitoring 2	70	62.1	58.8
Monitoring 3	60	45.6	45.6

Based on Table 5, the results from Stamson shows that the predicted noise level has difference of 12-15dBA at monitoring locations. This difference indicates noise levels are dominated by stationary sources. Stationary noise is further discussed in Section 3.3.

#### 4.2.1 Mitigation Measures

Further, the predicted road noise level exceeds 60dBA during daytime at monitoring locations 1 and 2, hence, noise mitigation is required such as acoustical fence and noise clauses. Whereas, during the nighttime, the noise level exceeds 45dBA MOECC requirement, therefore, assessment of glazing requirements is necessary to meet indoor sound level for buildings at all receiver locations.

The Type C warning clause is required to be included in all agreements of purchase and sale or lease and all rental agreements for the residential units facing Sandwich Street because noise level exceed 55dBA during daytime, or 50dBA during nighttime. This include:

- All units on western facade
- All units on southern facade
- All units on northern facade

##### Type 'C'

"This dwelling unit has been designed with the provision for adding central air conditioning at the occupant's discretion. Installation of central air conditioning by the occupant in low and medium density developments will allow windows and exterior doors to remain closed, thereby ensuring that the indoor sound levels are within the sound level limits of the Municipality and the Ministry of the Environment."

The Type D warning clause is required to be included in all agreements of purchase and sale or lease and all rental agreements for the residential units because noise level exceed 55dBA during daytime, or 60dBA during nighttime. This include:

- All units on western, northern and southern facade

##### Type 'D'

"This dwelling unit has been supplied with a central air conditioning system which will allow windows and exterior doors to remain closed, thereby ensuring that the indoor sound levels are within the sound level limits of the Municipality and the Ministry of the Environment."



The balconies in the building are less than 4m in width hence there are no Outdoor Living Area in the development.

### **4.3 Stationary Sources**

#### **4.3.1 Surrounding land Uses**

A review of all surrounding users was conducted and multiple properties were identified as being potential significant noise sources, namely:

- Vollmer Inc – to northwest of subject property: this development is particular is manufacturing company that produces construction products. The operational hours are 24 hours. The noise on this property includes loading and unloading of items. Idling of truck, lifting and lowering of machinery, and repair activities. Significant noise sources associated with the Vollmer Inc include:
  - Based on aerial photo (see Figure 3), the closet rooftop HVAC units are two (2) with assumed noise level of 75dBA. All HVAC units are 10.5m above the ground.
  - In the parking lot, trucks usually idle for security clearance at the gate. The assumed truck idling noise level is 75dBA.
  - The noise level at parking lot is assumed to be 65dBA.
  - The idling of trucks at the intersection of Sandwich Street and Chappell Avenue. The assumed noise level is 78dBA.
  - From the facility, impulsive sound such as metal on metal impact or hydraulic hammering can be heard. Hence for model, the impulsive sound is assumed to be 90dBA.
- Southwestern Manufacturing Inc – to north of subject property: this development is particular is heavy equipment manufacturing. All the manufacturing happened inside the warehouse. Noise on this property includes compressors, lifting and lowering of machinery, and repair activities. The operational hours are 24 hours. In addition, rooftop HVAC units exist on some of the property. These HVAC systems are located over 120m away at 10.5 above ground level from the subject property, and are relatively away from proposed buildings. Given separation distance, the HVAC

noise will not be an impact. However, the noise sources associated with the Southwestern Inc. include:

- The noise level at for outdoor activities is assumed to be 70dBA.
- Truck idling noise level is assumed to be 75dBA.
- Tim Horton – located to the south-west corner of the property. The operational hours are 24 hours. This facility includes two (2) rooftop HVAC units, drive through lane, and outdoor gathering area. The main source of noise from facility is HVAC, loading and unloading of garbage bin which occurred once in a week. The assumed noise level of HVAC unit is 75dBA and 10.5m above the ground level.

All other surrounding industries which are more than 250m distance from the development.

- Construction of Gordie Howe Bridge which include equipment and metal noise.
- K-Scrape Plant mostly produces metal crush, clunk and bang noise, lifting and lowering of machinery, and repair activities.
- CBM and Windsor Salt produces noise of crane, heavy metal and truck noises.

#### **4.3.2 Receiver Locations**

The sample receiver locations were identified and are located at the worst-case locations (most exposed) for both day and night time noise.

For indoor living areas, the plane of window (POW) will be used to represent the worst case for both daytime and night time receivers.

The term “outdoor living area” (OLA) is used in reference to an outdoor patio, a backyard, a terrace, balconies or other area where passive recreation is expected to occur. The proposed dwelling units have balconies however balconies are less than four (4) meters deep hence as per MOECC publication, where the *depth is less than 4 meters the balcony is not considered as an OLA*.

Table 6 identifies the various receiver heights chosen as the “worst case” locations within the proposed development. These locations are shown in Appendix A, Figure 1.

**Table 6 – Receiver Locations**

Receiver Location	Height Above Grade (m)	Type	Represents
Ground Level	1.5m	Bedroom POW	North, South, East and West Façade
Level 2	4.8m		
Level 3	8.1m		
Level 5	14.7m		
Level 8	24.6m		
Level 11	34.5m		

### 4.3.3 Stationary Noise Level Results

The noise propagation analysis was completed using noise modelling program “iNoise”, produced by DGMR Software to match the monitoring noise levels at monitoring locations M1, M2 and M3 from surrounding industrial sources. The iNoise program follows International Standards Organization (ISO) standards 9613 parts 1 and 2. The model is capable of incorporating various site features such as elevations, berms, absorptive grounds and barrier to accurately predict noise levels at specific receptors, pertaining to noise emission from sources. The model is considered conservative since as it represents atmospheric condition that promote propagation of sound from the source to the receiver.

The following assumptions were used in the modelling:

Reflections: A building reflection of 0.8 was assumed to be representative of the brick façade present for the surrounding building including the proposed building.

Ground Absorption & Topography: A ground absorption coefficient of 0 was used to represent the most reflective (i.e. pave surface) The area surrounding the monitoring location is characterized by generally flat. As such, topography was not incorporated into the noise modelling.

All stationary sources noise levels are assumed as describe in Section 3.3 to approximate match the monitored noise level. Once noise level matches the monitoring location, the noise level at each subject receptor location (see Table 6) were obtained.



Overall unattenuated daytime and nighttime sound levels at the receiver locations are shown in Figure 3 and are describe in Table 7 and detail output results are provided in Appendix C.

**Table 7 – Post Development Predicted Noise Levels**

Location	Noise Level (dBA)				MOECC Criteria (dBA)		Meets MOECC
	Receiver 1	Receiver 2	Receiver 3	Receiver 4			
					Sta	Rail	
Level 1							
Daytime	72	72	67	56	60	40	No
Nighttime	72	72	67	56	55	35	No
Level 2							
Daytime	71	72	67	55	60	40	No
Nighttime	71	72	67	55	55	35	No
Level 3							
Daytime	71	72	67	55	60	40	No
Nighttime	71	72	67	55	55	35	No
Level 5							
Daytime	70	71	66	52	60	40	No
Nighttime	70	71	66	52	55	35	No
Level 8							
Daytime	68	70	66	55	60	40	No
Nighttime	68	70	66	55	55	35	No
Level 11							
Daytime	67	69	66	55	60	40	No
Nighttime	67	69	66	55	55	35	No

Based on predicted sound levels as shown in Figure 1 (Appendix A), the sensitive spaces (i.e., plane of windows) noise level is greater than stationary noise level criteria of 60dBA daytime and 55 dBA nighttime for receiver locations 1, 2 and 3. Therefore, mitigation measures are required such as special building components, warning clause and central air conditioning is required.

In order to calculate building component, the worst-case scenario i.e., MOECC criteria of rail noise is used to estimate the STC of building components.

## 5. MITIGATION MEASURES

### Building Component

To calculate the required building components, the dimensions of the rooms and their wall/window/door sizes must be known. Table 8 summarizes the building component areas, Sound Transmission Class (STC) ratings and component type and detail calculations and floor plans are provided in Appendix D.

**Table 8 – Required STC Values (Bedroom/Living)**

Receiver 1 & 2					
Side	BR/LR	COMPONENT	F AREA	W AREA	STC
West Façade	Living	Window	15.6	1.7	35
	Living	Door	15.6	2.3	36
	Living	Wall	15.6	8.96	42
	Bed	Window	17.4	1.8	38
	Bed	Wall	17.4	6.66	43

Receiver 3					
Side	BR/LR	COMPONENT	F AREA	W AREA	STC
North & South Façade	Living	Window	15.6	1.7	33
	Living	Door	15.6	2.3	34
	Living	Wall	15.6	8.96	40
	Bed	Window	17.4	1.8	36
	Bed	Wall	17.4	6.66	41

Receiver 4					
Side	BR/LR	COMPONENT	F AREA	W AREA	STC
East Facade	Living	Window	15.6	1.7	21
	Living	Door	15.6	2.3	22
	Living	Wall	15.6	8.96	28
	Bed	Window	17.4	1.8	24
	Bed	Wall	17.4	6.66	29

Further, The Ontario Building Code (OBC) section 5.9.1 specifies the minimum required sound insulation characteristics for partitions in terms of Sound Transmission Class (STC) values. For acoustic privacy in a multi-tenant building between suites, the inter-unit wall should meet or exceed STC-50. Walls separating suites from noisy spaces, such as refuse chutes or elevator shafts, should meet or exceed STC-55. In addition, it is recommended

that the separation of suites from any amenity or commercial space for building component (floor/ceiling) construction also meet or exceed STC-55.

## **6. RECOMMENDATIONS**

As demonstrated in this report, mitigation measures are required to bring residential units within the development into compliance with MOECC noise criteria, a “Class 4” area designation endorsed by City engineer department and Guidelines D-6 guidelines.

Further, the development is within the vicinity of railway line. The assessment of building components is based on the acoustical impacts from railways using “plane of windows” MOECC noise level criteria which is considered as worst-case scenario. With the inclusion of these measures, MOECC noise criteria will be satisfied.

### **Recommendation #1**

Due to the exceedance of the MOECC criteria for daytime and night time acoustical levels from CP railway and Sandwich Street, the dwellings shall include warning clauses as describe in Section 3.2.1.

### **Recommendation #2 (Building Components)**

Due to exposure to stationary noise, some units require special building components for areas of sensitive use (i.e. bedroom, living room, dining room, kitchen, etc.) and the following is required:

#### **Window Requirements:**

All windows leading to sensitive living areas are to have a minimum sound transmission class (STC) as per Table 8 in order to meet the MOECC indoor noise level criteria.

#### **Door Requirements:**

All doors leading to sensitive living areas are to have a minimum sound transmission class (STC) as per Table 8 in order to meet the MOECC indoor noise level criteria.

#### **Wall Requirements:**



All walls leading to sensitive living areas are to have a minimum sound transmission class (STC) as per Table 8. Also, acoustic privacy between units in a multi-tenant building, the inter-unit wall, should meet or exceed STC-50. Wall separation between noisy spaces, such as refuse chutes or elevator shafts, and suites should meet or exceed STC-55.

Further, these units shall include the following warning clause in purchase or lease agreements:

"Purchasers/tenants are advised that due to close proximity of the adjacent industries, noise from said industries may at times be audible."

### **Recommendation #3**

Due to the exceedance of the MOECC Guideline D-6 dust and noise criteria of surrounding users, the units shall be provided with the following warning clause:

*"Purchasers/tenants are advised that sound levels due to the adjacent industry facilities are required to comply with sound level limits that are protective of indoor areas and are based on the assumption that windows and exterior doors are closed. This building has been supplied with a ventilation/air conditioning system which will allow windows and exterior doors to remain closed."*

Additionally, rows of trees will be installed along the Sandwich Street to make it a buffer in order to meet the MOECC noise and dust criteria.

### **Recommendation #4 (All units within the development)**

Prior to the issuance of building permits it is recommended that an acoustical consultant review the sound transmission class (STC) for the proposed development's walls, windows and doors to ensure they conform to the recommendations outlined in this report.



## 7. SUMMARY

We conclude that this development with the implementation of the above-described mitigation measures meets the MOECC noise and D-6 guidelines.

If you have any questions or wish to discuss our findings, please advise us.

Yours truly,

**BAIRD AE INC.**  
**27 PRINCESS STREET, UNIT 102**  
**LEAMINGTON, ONTARIO N8H 2X8**

Shurjeel Tunio, P.Eng.  
Senior Project Manager  
**Baird AE**



## **Appendix A**

---

### **NOISE INFORMATION PLAN AND BACKGROUND INFORMATION**





**LEGEND**

- ☒ RECEIVER LOCATION
- ☐ HOUSE BOUNDARY
- ☐ NOISE CONTOUR



STC 24

SEE TABLE 8 OF ACOUSTICAL REPORT

SITE AREA

MONITORING LOCATION [M1 Leq 70.5dBA | M2 Leq 70.0dBA | M3 Leq 60.0dBA]

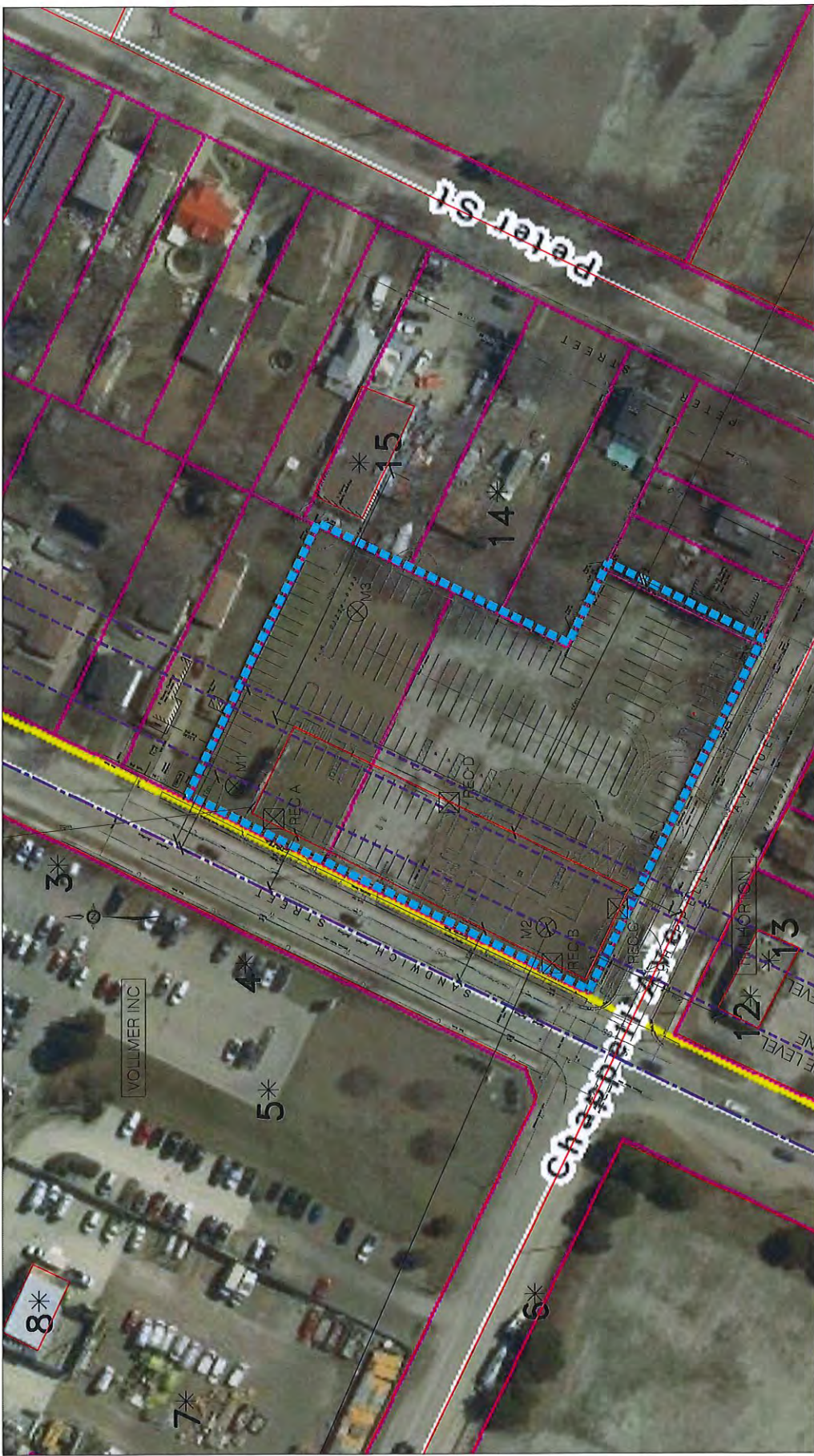
PROJECT: MIXED USED HIGH RISE DEVELOPMENT  
SHEET FILE: WINDSOR, ON

**BAIRD | AE**  
architecture + engineering  
103-17 PRINCESS STREET, 1000-10 PULVER STREET, WINDSOR, ON N9A 6A4  
TEL: 519-253-8877 FAX: 519-253-8878

DRAWN BY: S.T.  
CHECKED BY: S.T.

SCALE: NTS  
SHEET NO.: 1 OF 1  
PROJECT NO.: 20-028  
DATE: OCT 4, 2020





**LEGEND**

- X RECEIVER LOCATION
- HOUSE BOUNDARY
- NOISE CONTOUR

STC 24 SEE TABLE 8 OF ACOUSTICAL REPORT

SITE AREA



MONITORING LOCATION [M1 Leq 70.5dBA] [M2 Leq 70.0dBA] [M3 Leq 60.0dBA]

**BAIRD AE**  
architecture • engineering

180-20 PRINCETON STREET, SUITE 200, WINDSOR, ON N9M 2P7  
TEL: 519-253-8888 FAX: 519-253-8889

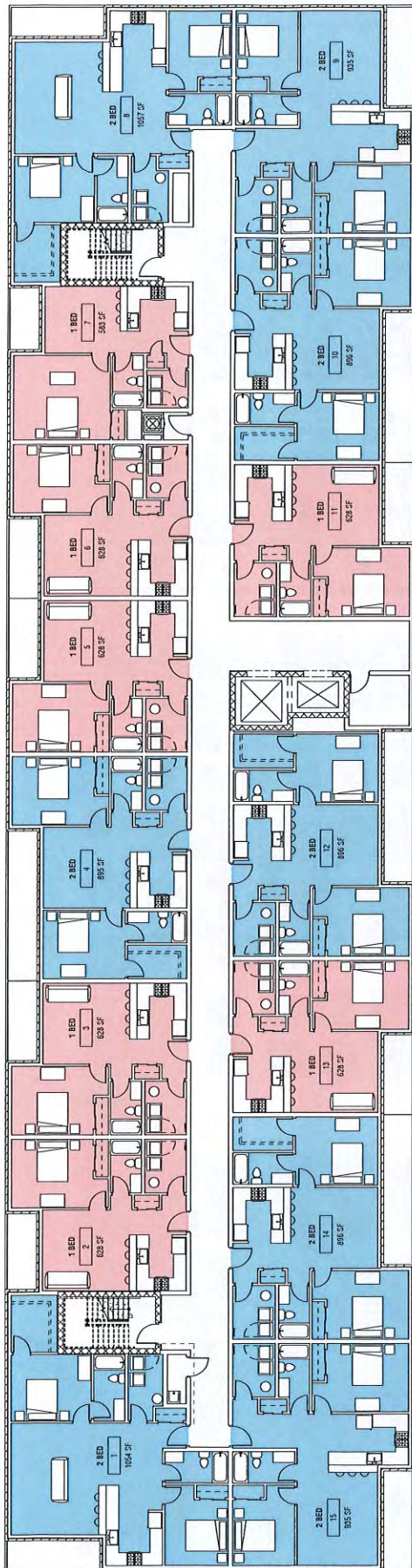
PROJECT: MIXED-USED HIGH-RISE DEVELOPMENT  
IMPACT TITLE: NOISE INFORMATION PLAN 2  
WINDSOR, ON

DRAWN BY:	SCALE:	DATE:
S.T.	NTS	OCT 4, 2020
CHECKED BY:	SHEET NO.:	PROJECT NO.:
S.T.	2 OF 2	20-028

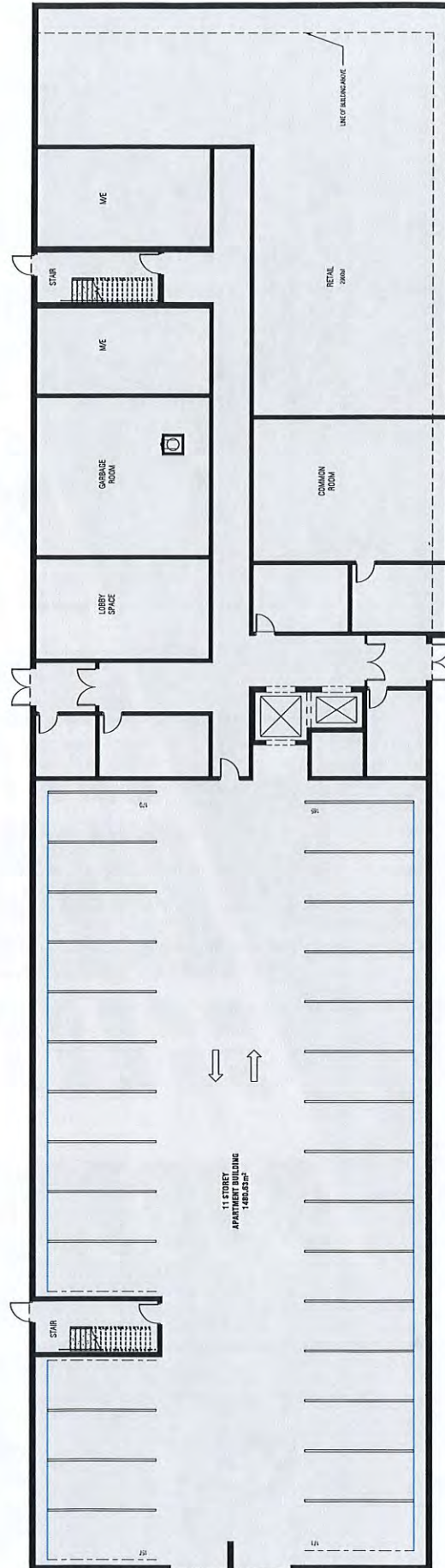




ROOM TYPES  
 1 BED  
 2 BED



2 TYPICAL FLOOR PLAN 2-11  
 1/8" = 1'-0"



1 GROUND FLOOR PLAN  
 1/8" = 1'-0"



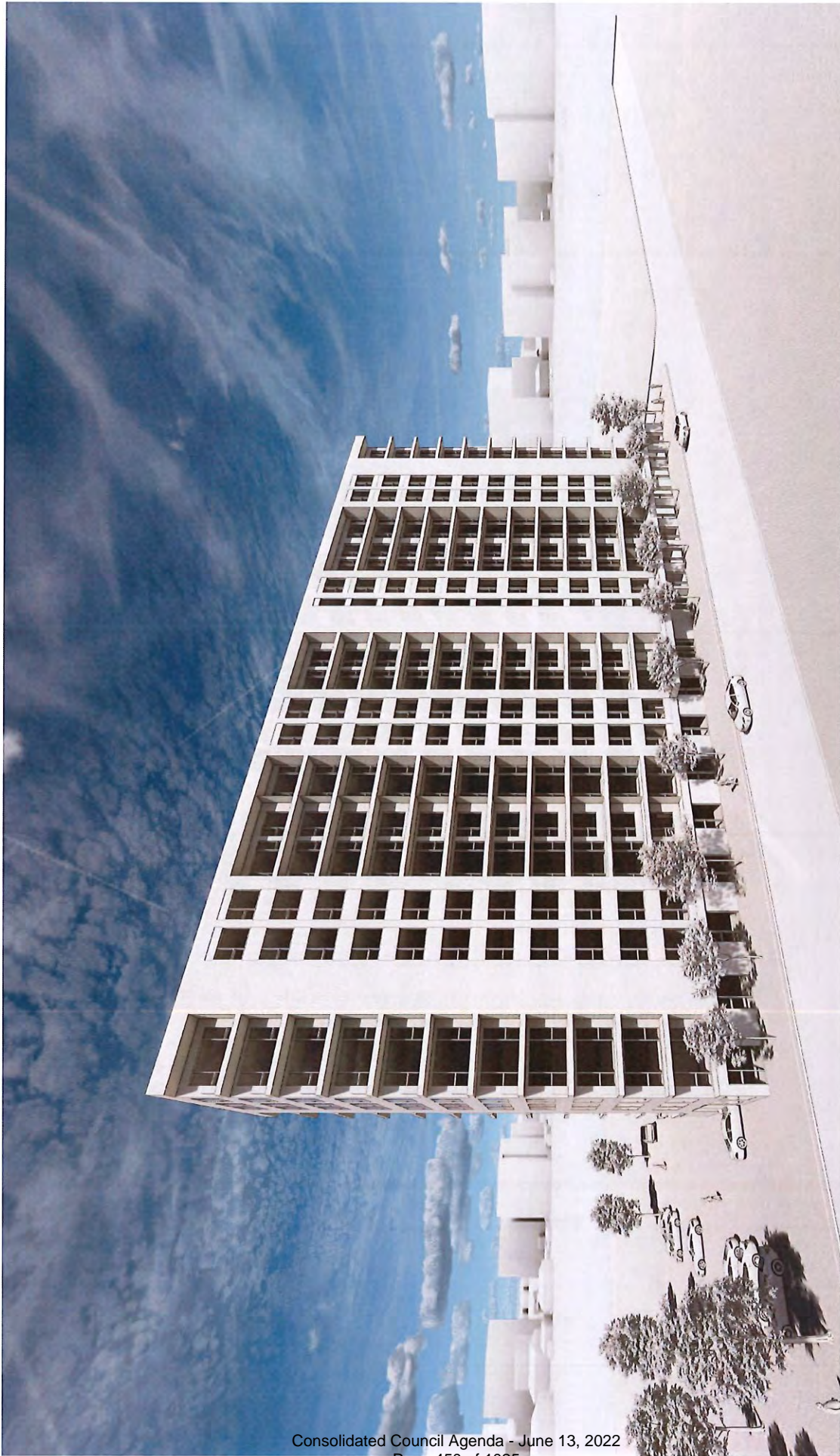


EXT RENDER / SANDWICH DEVELOPMENT  
/ JUL 19, 2020









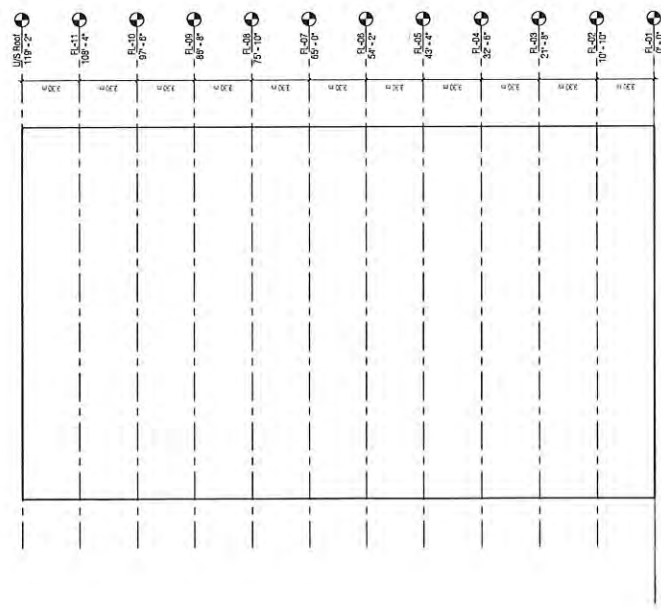
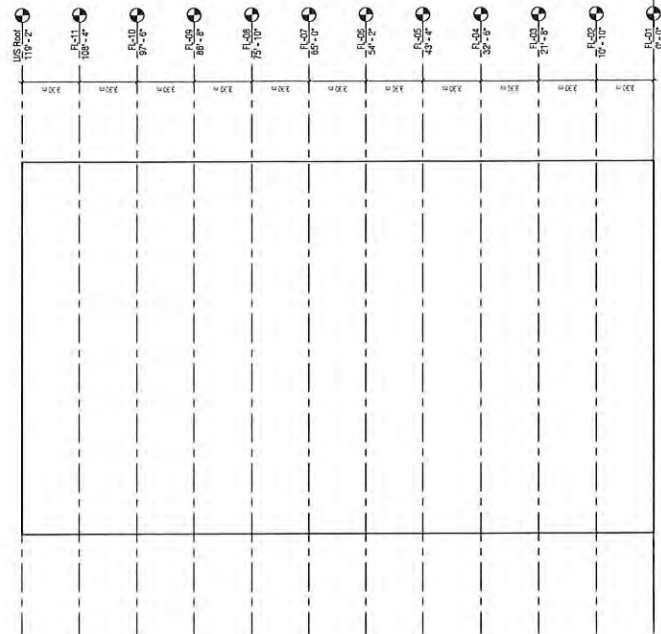
EXT RENDER / SANDWICH DEVELOPMENT  
/ JUL 19, 2020

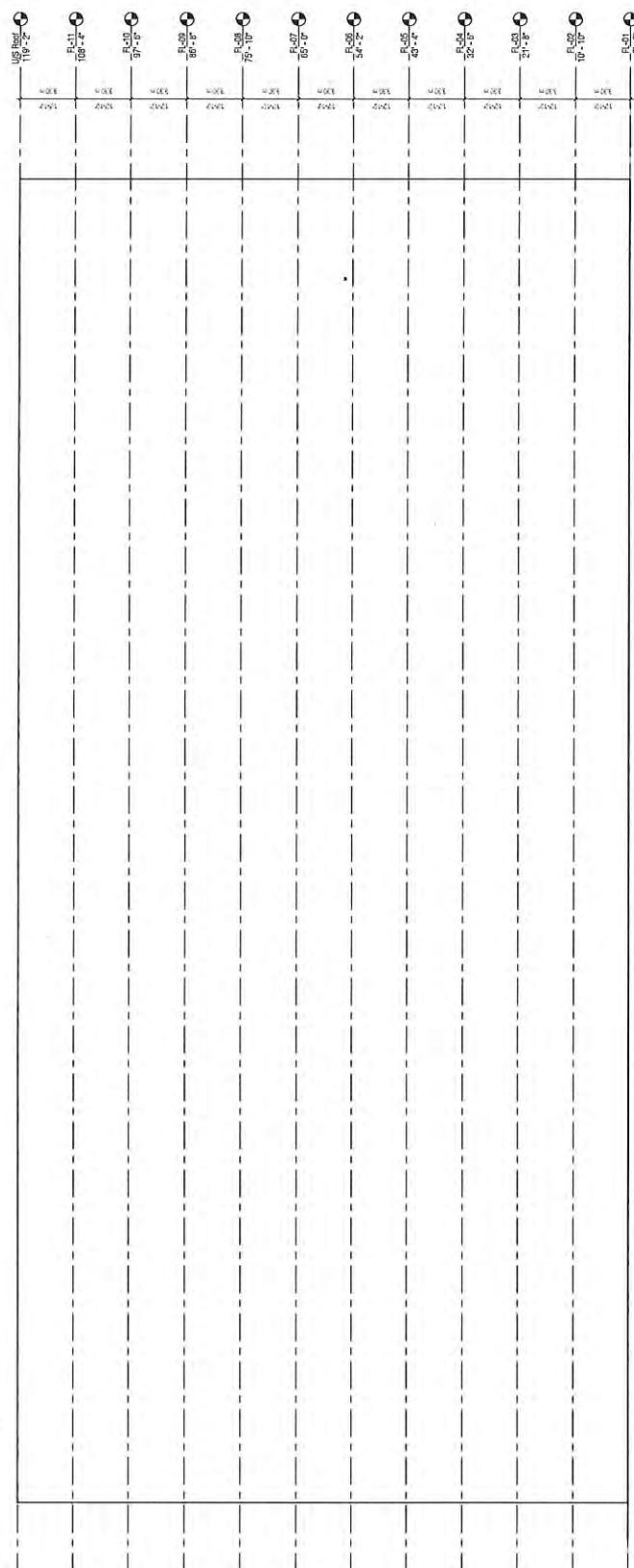
MIXED-USE DEVELOPMENT  
BUILDING ELEVATIONS

1000-2627 PATAHA STREET  
MIDWEST OHIO AKA

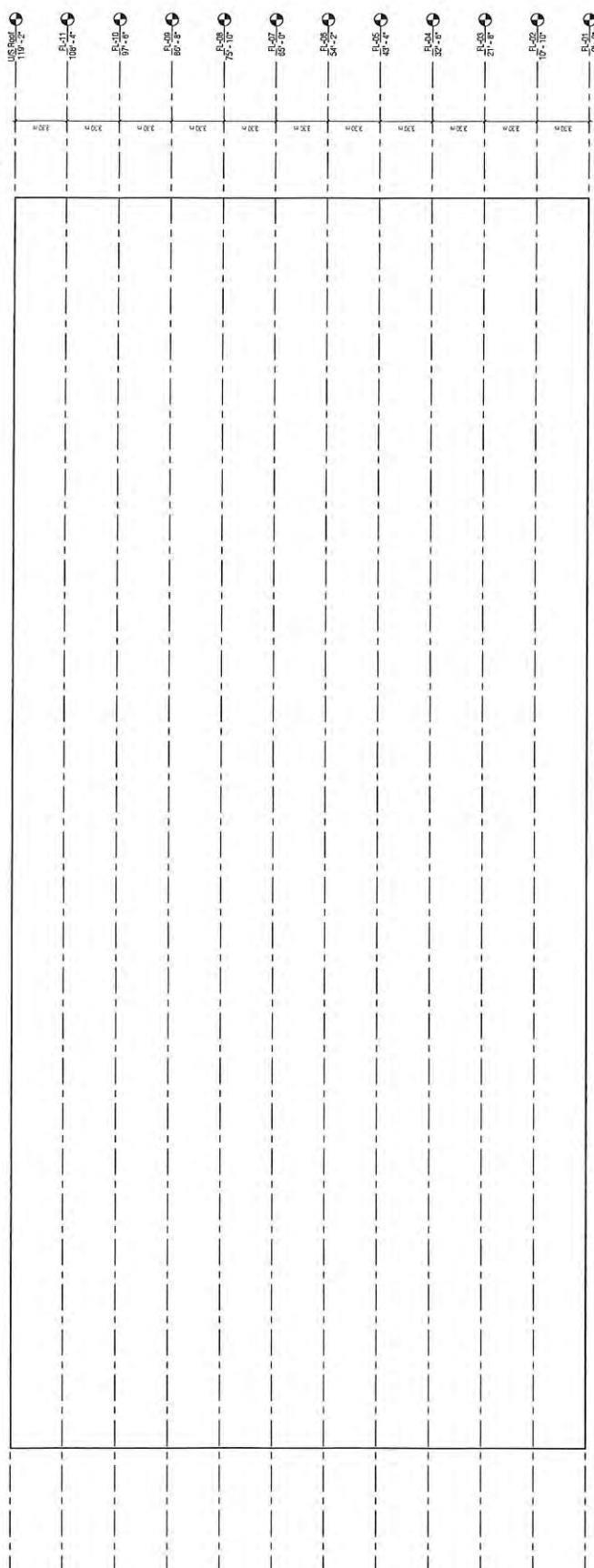


1000-2627 PATAHA STREET  
MIDWEST OHIO AKA









## DEFINITIONS OF CLASSES FROM GUIDELINE – 6

### **CLASS I INDUSTRIAL FACILITY**

A place of business for a small scale, self-contained plant or building which produces and/or stores a product which is contained in a package and has a low probability of fugitive emissions for any of the following: noise, odour, dust, and/or vibration. There are daytime operations only, with infrequent movement of products and/or heavy trucks and no outside storage.

### **CLASS II INDUSTRIAL FACILITY**

A place of business for medium scale processing and manufacturing with outdoor storage of wastes or material (i.e. it has an open process) and/or there are periodic outputs of minor annoyance. There are occasional outputs of either point source or fugitive emissions of any of the following: noise, odour, dust, and/or vibration, and low probability of fugitive emissions. Shift operations are permitted and there is frequent movement of products and/or heavy trucks during daytime hours.
















### **CLASS III INDUSTRIAL FACILITY**

A place of business for large scale manufacturing or processing, characterized by: large physical size, outside storage of raw and finished products, large production volumes and continuous movement of products and employees during daily shift operations. It has frequent outputs of major annoyance and there is a high probability of fugitive emissions.












[Home](#) > [Environment and natural resources](#) > [Weather information](#) > [Weather](#)  
 > [Local Forecasts](#) > [Ontario](#) > [Windsor](#)

## Hourly Forecast - Windsor

Date/Time (EDT)	Temp. (°C)	Weather Conditions	Likelihood of precip <sup>±</sup>	Wind (km/h)
02 October 2020				
09:00	8	 Mainly sunny	Nil	NW 15
10:00	9	 Mainly sunny	Nil	NW 15
11:00	9	 Mainly sunny	Nil	NW 15
12:00	10	 Mainly sunny	Nil	NW 15
13:00	11	 Mainly sunny	Nil	NW 15
14:00	12	 Chance of showers	Low	NW 15
15:00	12	 Chance of showers	Low	NW 15
16:00	13	 Chance of showers	Low	NW 15
17:00	13	 Chance of showers	Low	NW 15
18:00	12	 Chance of showers	Low	NW 10
19:00	10	 Chance of showers	Low	NW 10
20:00	9	 Chance of showers	Low	NW 10
21:00	8	 A few clouds	Nil	NW 10
22:00	7	 A few clouds	Nil	NW 10
23:00	6	 A few clouds	Nil	VR 5
03 October 2020				



Date/Time (EDT)	Temp. (°C)	Weather Conditions	Likelihood of precip <sup>±</sup>	Wind (km/h)
00:00	5	 A few clouds	Nil	VR 5
01:00	5	 A few clouds	Nil	VR 5
02:00	4	 Partly cloudy	Low	VR 5
03:00	3	 Partly cloudy	Low	VR 5
04:00	3	 Partly cloudy	Low	VR 5
05:00	2	 Partly cloudy	Low	VR 5
06:00	3	 Partly cloudy	Low	VR 5
07:00	3	 Mainly sunny	Low	VR 5
08:00	4	 Mainly sunny	Nil	VR 5

± Likelihood of Precipitation as described in the public forecast as a chance of measurable precipitation for a period of time.

Nil: 0%

Low: 40% or below

Medium: 60% or 70%

High: Above 70%

\* Value not significant

How to Use

**Date modified:** 2020-10-02

Government  
of CanadaGouvernement  
du Canada
[Home](#) > [Environment and natural resources](#) > [Weather information](#) > [Weather](#)
[Local forecasts](#) > [Ontario](#) > [Provincial summary](#)







## Windsor Airport, Ontario










Latitude 42.28° N | Longitude 82.96° W

Past 24 Hour Conditions

Imperial units

Graphic

Date / Time (EDT)	Conditions	Temperature (°C)	Wind (km/h)	Relative humidity (%)	Dew point (°C)	Pressure (kPa)	Visibility (km)
02 October 2020							
08:00	 Sunny	5 (5.0) ↓	WNW 13	84	3	101.9	16
07:00	 Clear	5 (5.4)	WNW 13	87	3	101.8	16
06:00	 Clear	6 (6.1)	WNW 15	87	4	101.8	16
05:00	 Partly Cloudy	7 (7.2)	NW 22	83	5	101.7	16
04:00	 Cloudy	8 (8.1)	NW 22 gust 35	86	6	101.7	16
03:00	 Partly Cloudy	9 (8.7)	NW 17 gust 30	85	6	101.7	16

Date / Time (EDT)	Conditions	Temperature (°C)	Wind (km/h)	Relative humidity (%)	Dew point (°C)	Pressure (kPa)	Visibility (km)
02:00	 Cloudy	10 (9.7)	NNW 21 gust 32	79	6	101.7	16
01:00	 Light Rain	10 (9.7)	NNW 24 gust 33	85	7	101.6	16
00:00	 Cloudy	10 (10.1)	NW 21	85	8	101.6	16
01 October 2020							
23:00	 Cloudy	10 (10.4)	WNW 21	81	7	101.6	16
22:00	 Mostly Cloudy	11 (10.6)	NW 13	78	7	101.5	16
21:00	 Mostly Cloudy	9 (8.7)	WSW 11	85	6	101.5	16
20:00	 Mostly Cloudy	9 (9.1)	W 11	81	6	101.4	16
19:00	 Mainly Sunny	12 (11.8)	WNW 8	72	7	101.4	16
18:00	 Mostly Cloudy	13 (13.0)	NNE 9	68	7	101.4	16

Date / Time (EDT)	Conditions	Temperature (°C)	Wind (km/h)	Relative humidity (%)	Dew point (°C)	Pressure (kPa)	Visibility (km)
17:00	 Mostly Cloudy	12 (12.4)	SW 11	77	9	101.3	16
16:00	 <b>Thunderstorm</b>	12 (12.3)	SW 30 gust 41	64	6	101.4	16
15:00	 Mostly Cloudy	17 (16.6) ↑	S 21 gust 32	48	6	101.2	16
14:00	 Partly Cloudy	17 (16.6) ↑	WSW 28 gust 41	51	7	101.2	16
13:00	n/a	16 (15.6)	SW 21 gust 32	60	8	101.3	16
12:00	 Mainly Sunny	15 (15.0)	W 22 gust 35	66	9	101.3	16
11:00	 Partly Cloudy	14 (13.5)	WSW 17 gust 30	74	9	101.3	16
10:00	 Mostly Cloudy	12 (11.5)	W 17	81	8	101.3	16
09:00	 Sunny	9 (9.4)	WSW 11	89	8	101.3	16
08:00	 Sunny	7 (6.7)	SW 9	97	6	101.2	16

**▼ Legend**

n/a: not available

This table displays weather elements available for this station

Highest temperature ↑

Lowest temperature ↓

Equal temperature values are all highlighted

This is an automated product, generated using preliminary data.

If you require additional historical weather information, please visit [Climate](#) website.

**Date modified:** 2020-10-02



# Sandwich St @ Chappel Ave

<b>Morning Peak Diagram</b>	<b>Specified Period</b> <b>From:</b> 7:00:00 <b>To:</b> 9:00:00	<b>One Hour Peak</b> <b>From:</b> 7:15:00 <b>To:</b> 8:15:00
<b>Municipality:</b> Windsor <b>Site #:</b> 0000000001 <b>Intersection:</b> Sandwich St & Chappel Ave <b>TFR File #:</b> 1 <b>Count date:</b> 21-Jul-2020	<b>Weather conditions:</b> Clear/Dry <b>Person(s) who counted:</b> Cam	
<b>** Non-Signalized Intersection **</b>		<b>Major Road:</b> Sandwich St runs N/S

North Leg Total: 583 North Entering: 229 North Peds: 0 Peds Cross: ∅	<table style="width: 100%; border-collapse: collapse;"> <tr><td>Heavys</td><td>0</td><td>2</td><td>0</td><td>2</td></tr> <tr><td>Trucks</td><td>1</td><td>9</td><td>0</td><td>10</td></tr> <tr><td>Cars</td><td>3</td><td>213</td><td>1</td><td>217</td></tr> <tr><td>Totals</td><td>4</td><td>224</td><td>1</td><td></td></tr> </table>	Heavys	0	2	0	2	Trucks	1	9	0	10	Cars	3	213	1	217	Totals	4	224	1		<table style="width: 100%; border-collapse: collapse;"> <tr><td>Heavys</td><td>10</td></tr> <tr><td>Trucks</td><td>9</td></tr> <tr><td>Cars</td><td>335</td></tr> <tr><td>Totals</td><td>354</td></tr> </table>	Heavys	10	Trucks	9	Cars	335	Totals	354	East Leg Total: 20 East Entering: 8 East Peds: 7 Peds Cross: ∅
Heavys	0	2	0	2																											
Trucks	1	9	0	10																											
Cars	3	213	1	217																											
Totals	4	224	1																												
Heavys	10																														
Trucks	9																														
Cars	335																														
Totals	354																														

Heavys	Trucks	Cars	Totals
17	5	16	38

Sandwich St

Chappel Ave

Cars	Trucks	Heavys	Totals
0	1	0	1
0	0	0	0
5	1	1	7
5	2	1	

Heavys	Trucks	Cars	Totals
0	3	7	10
0	0	2	2
22	2	12	36
22	5	21	

Sandwich St

Chappel Ave

Cars	Trucks	Heavys	Totals
12	0	0	12

Peds Cross: ∅ West Peds: 3 West Entering: 48 West Leg Total: 86	<table style="width: 100%; border-collapse: collapse;"> <tr><td>Cars</td><td>230</td></tr> <tr><td>Trucks</td><td>12</td></tr> <tr><td>Heavys</td><td>25</td></tr> <tr><td>Totals</td><td>267</td></tr> </table>	Cars	230	Trucks	12	Heavys	25	Totals	267	<table style="width: 100%; border-collapse: collapse;"> <tr><td>Cars</td><td>13</td><td>328</td><td>9</td><td>350</td></tr> <tr><td>Trucks</td><td>4</td><td>5</td><td>0</td><td>9</td></tr> <tr><td>Heavys</td><td>17</td><td>10</td><td>0</td><td>27</td></tr> <tr><td>Totals</td><td>34</td><td>343</td><td>9</td><td></td></tr> </table>	Cars	13	328	9	350	Trucks	4	5	0	9	Heavys	17	10	0	27	Totals	34	343	9		Peds Cross: ∅ South Peds: 4 South Entering: 386 South Leg Total: 653
Cars	230																														
Trucks	12																														
Heavys	25																														
Totals	267																														
Cars	13	328	9	350																											
Trucks	4	5	0	9																											
Heavys	17	10	0	27																											
Totals	34	343	9																												

Comments



# Sandwich St @ Chappel Ave

## Mid-day Peak Diagram

### Specified Period

**From:** 11:00:00

**To:** 14:00:00

### One Hour Peak

**From:** 12:15:00

**To:** 13:15:00

**Municipality:** Windsor

**Site #:** 0000000001

**Intersection:** Sandwich St & Chappel Ave

**TFR File #:** 1

**Count date:** 21-Jul-2020

### Weather conditions:

Clear/Dry

### Person(s) who counted:

Cam

**\*\* Non-Signalized Intersection \*\***

**Major Road:** Sandwich St runs N/S

North Leg Total: 526

North Entering: 284

North Peds: 1

Peds Cross:  $\nabla$

Heavys	0	8	0	8
Trucks	3	7	0	10
Cars	10	251	5	266
Totals	13	266	5	



Heavys 5

Trucks 8

Cars 229

Totals 242

East Leg Total: 24

East Entering: 13

East Peds: 9

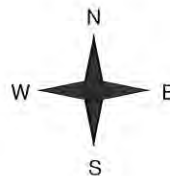
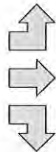
Peds Cross:  $\nabla$

Heavys	Trucks	Cars	Totals
21	6	27	54



Chappel Ave

Heavys	Trucks	Cars	Totals
0	1	3	4
0	0	1	1
26	3	20	49
26	4	24	



Sandwich St

Cars	Trucks	Heavys	Totals
4	1	0	5
2	0	0	2
6	0	0	6
12	1	0	

Chappel Ave



Cars	Trucks	Heavys	Totals
11	0	0	11

Peds Cross:  $\nabla$

West Peds: 0

West Entering: 54

West Leg Total: 108

Cars	277
Trucks	10
Heavys	34
Totals	321



Cars	15	222	5	242
Trucks	3	6	0	9
Heavys	21	5	0	26
Totals	39	233	5	

Peds Cross:  $\nabla$

South Peds: 5

South Entering: 277

South Leg Total: 598

## Comments

# Sandwich St @ Chappel Ave

<b>Afternoon Peak Diagram</b>	<b>Specified Period</b> <b>From:</b> 15:00:00 <b>To:</b> 18:00:00	<b>One Hour Peak</b> <b>From:</b> 15:15:00 <b>To:</b> 16:15:00
<b>Municipality:</b> Windsor <b>Site #:</b> 0000000001 <b>Intersection:</b> Sandwich St & Chappel Ave <b>TFR File #:</b> 1 <b>Count date:</b> 21-Jul-2020	<b>Weather conditions:</b> Clear/Dry  <b>Person(s) who counted:</b> Cam	

**\*\* Non-Signalized Intersection \*\***

**Major Road:** Sandwich St runs N/S

North Leg Total: 778	Heavys 1 4 0 5	Heavys 5	East Leg Total: 28
North Entering: 435	Trucks 1 6 1 8	Trucks 2	East Entering: 8
North Peds: 0	Cars 6 411 5 422	Cars 336	East Peds: 7
Peds Cross: ∅	Totals 8 421 6	Totals 343	Peds Cross: ∅

Heavys Trucks Cars Totals

25 2 13 40

←

Chappel Ave

←

Heavys Trucks Cars Totals

0 0 5 5

0 0 0 0

29 0 29 58

29 0 34

↑

↑

↑

Compass

N

E

S

W

Heavys Trucks Cars Totals

3 1 0 4

0 0 0 0

4 0 0 4

7 1 0

↑

↑

↑

Chappel Ave

→

Heavys Trucks Cars Totals

7 328 9 344

Trucks 1 1 4 6

Heavys 24 5 1 30

Totals 32 334 14

↑

↑

↑

Heavys Trucks Cars Totals

14 5 1 20

Peds Cross: ∅

West Peds: 0

West Entering: 63

West Leg Total: 103

Cars 444

Trucks 6

Heavys 33

Totals 483

↓

Peds Cross: ∅

South Peds: 2

South Entering: 380

South Leg Total: 863

## Comments

# Sandwich St @ Chappel Ave

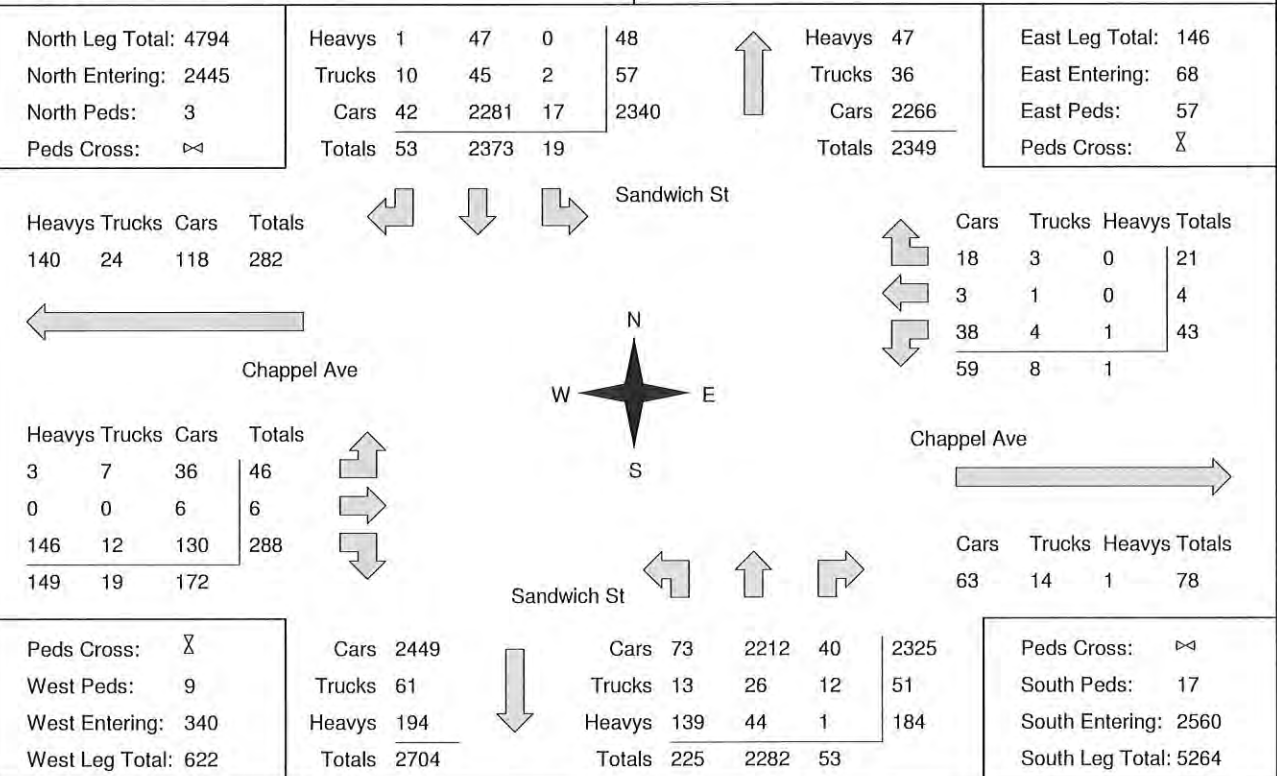
## Total Count Diagram

**Municipality:** Windsor  
**Site #:** 0000000001  
**Intersection:** Sandwich St & Chappel Ave  
**TFR File #:** 1  
**Count date:** 21-Jul-2020

**Weather conditions:**  
 Clear/Dry  
**Person(s) who counted:**  
 Cam

**\*\* Non-Signalized Intersection \*\***

**Major Road:** Sandwich St runs N/S



### Comments





## PRINCIPAL MAIN LINE REQUIREMENTS

1. Berm, or combination berm and noise attenuation fence, having extensions or returns at the ends, to be erected on adjoining property, parallel to the railway right-of-way with construction according to the following:
  - a) Minimum total height 5.5 metres above top-of-rail;
  - b) Berm minimum height 2.5 metres and side slopes not steeper than 2.5 to 1.
  - c) Fence, or wall, to be constructed without openings and of a durable material weighing not less than 20 kg. per square metre (4 lb/sq.ft.) of surface area.

No part of the berm/noise barrier is to be constructed on railway property.

A clause should be inserted in all offers of purchase and sale or lease, and be registered on title or included in the lease for each dwelling affected by any noise and vibration attenuation measures, advising that any berm, fencing, or vibration isolation features implemented are not to be tampered with or altered, and further that the owner shall have the sole responsibility for and shall maintain these features.

Dwellings must be constructed such that the interior noise levels meet the criteria of the appropriate Ministry. A noise study should be carried out by a professional noise consultant to determine what impact, if any, railway noise would have on residents of proposed subdivisions and to recommend mitigation measures, if required. The Railway may consider other measures recommended by the study.

2. Setback of dwellings from the railway right-of-way to be a minimum of 30 metres. While no dwelling should be closer to the right-of-way than the specified setback, an unoccupied building, such as a garage, may be built closer. The 2.5 metre high earth berm adjacent to the right-of-way must be provided in all instances.
3. Ground vibration transmission to be estimated through site tests. If in excess of the acceptable levels, all dwellings within 75 metres of the nearest track should be protected. The measures employed may be:
  - a) Support the building on rubber pads between the foundation and the occupied structure so that the maximum vertical natural frequency of the structure on the pads is 12 Hz;
  - b) Insulate the building from the vibration originating at the railway tracks by an intervening discontinuity or by installing adequate insulation outside the building, protected from the compaction that would reduce its effectiveness so that vibration in the building became unacceptable; or
  - c) Other suitable measures that will retain their effectiveness over time.
4. A clause should be inserted in all offers of purchase and sale or lease and in the title deed or lease of each dwelling within 300m of the railway right-of-way, warning prospective purchasers or tenants of the existence of the Railway's operating right-of-way; the possibility of alterations including the possibility that the Railway may expand its operations, which expansion may affect the living environment of the residents notwithstanding the inclusion of noise and vibration attenuating measures in the design of the subdivision and individual units, and that the Railway will not be responsible for complaints or claims arising from the use of its facilities and/or operations.
5. Any proposed alterations to the existing drainage pattern affecting railway property must receive prior concurrence from the Railway, and be substantiated by a drainage report to be reviewed by the Railway.
6. A 1.83 metre high chain link security fence be constructed and maintained along the common property line of the Railway and the development by the developer at his expense, and the developer is made aware of the necessity of including a covenant running with the lands, in all deeds, obliging the purchasers of the land to maintain the fence in a satisfactory condition at their expense.
7. Any proposed utilities under or over railway property to serve the development must be approved prior to their installation and be covered by the Railway's standard agreement.

## Appendix B

---

### STAMSON OUTPUT

STAMSON 5.0 NORMAL REPORT Date: 08-09-2005 22:37:06  
 MINISTRY OF ENVIRONMENT AND ENERGY / NOISE ASSESSMENT

Filename: sandwich.te Time Period: Day/Night 16/8 hours  
 Description: COMPASION MONITORED AND MODEL - Monitoring Location 1 and 2

Rail data, segment # 1: CP RAILWAY (day/night)

Train Type	! Trains (Left)	! Trains (Right)	! Speed (km/h)	! # loc	! # Cars	! Eng type	! Cont weld
1. Freight	0.5/0.5	0.5/0.5	20.0	2.0	168.0	Diesel	No

Data for Segment # 1: CP RAILWAY (day/night)

Angle1 Angle2 : -90.00 deg 90.00 deg  
 Wood depth : 0 (No woods.)  
 No of house rows : 0 / 0  
 Surface : 2 (Reflective ground surface)  
 Receiver source distance : 151.00 / 151.00 m  
 Receiver height : 1.50 / 4.50 m  
 Topography : 1 (Flat/gentle slope; no barrier)  
 Whistle Angle : 90 deg Track 1  
 Reference angle : 0.00

Results segment # 1: CP RAILWAY (day)

LOCOMOTIVE (0.00 + 53.23 + 0.00) = 53.23 dBA

Angle1	Angle2	Alpha	RefLeq	D.Adj	F.Adj	W.Adj	H.Adj	B.Adj	SubLeq
-90	90	0.00	63.26	-10.03	0.00	0.00	0.00	0.00	53.23

WHEEL (0.00 + 40.56 + 0.00) = 40.56 dBA

Angle1	Angle2	Alpha	RefLeq	D.Adj	F.Adj	W.Adj	H.Adj	B.Adj	SubLeq
-90	90	0.00	50.59	-10.03	0.00	0.00	0.00	0.00	40.56

LEFT WHISTLE (0.00 + 0.00 + 0.00) = 0.00 dBA

Angle1	Angle2	Alpha	RefLeq	D.Adj	F.Adj	W.Adj	H.Adj	B.Adj	SubLeq
-90	90	0.00	66.27	-10.03	0.00	0.00	0.00	0.00	56.24

Segment Leq : 53.46 dBA

Total Leq All Segments: 53.46 dBA

Results segment # 1: CP RAILWAY (night)

LOCOMOTIVE (0.00 + 56.24 + 0.00) = 56.24 dBA

Angle1	Angle2	Alpha	RefLeq	D.Adj	F.Adj	W.Adj	H.Adj	B.Adj	SubLeq
-90	90	0.00	66.27	-10.03	0.00	0.00	0.00	0.00	56.24

WHEEL (0.00 + 43.57 + 0.00) = 43.57 dBA

Angle1	Angle2	Alpha	RefLeq	D.Adj	F.Adj	W.Adj	H.Adj	B.Adj	SubLeq
-90	90	0.00	53.60	-10.03	0.00	0.00	0.00	0.00	43.57

LEFT WHISTLE (0.00 + 0.00 + 0.00) = 0.00 dBA

Angle1	Angle2	Alpha	RefLeq	D.Adj	F.Adj	W.Adj	H.Adj	B.Adj	SubLeq
-90	90	0.00	66.27	-10.03	0.00	0.00	0.00	0.00	56.24

Segment Leq : 56.47 dBA

Total Leq All Segments: 56.47 dBA



Road data, segment # 1: Sandwich (day/night)

-----  
Car traffic volume : 6315/702 veh/TimePeriod \*  
Medium truck volume : 166/18 veh/TimePeriod \*  
Heavy truck volume : 173/19 veh/TimePeriod \*  
Posted speed limit : 50 km/h  
Road gradient : 0 %  
Road pavement : 1 (Typical asphalt or concrete)

\* Refers to calculated road volumes based on the following input:

24 hr Traffic Volume (AADT or SADT): 5186  
Percentage of Annual Growth : 3.00  
Number of Years of Growth : 12.00  
Medium Truck % of Total Volume : 2.50  
Heavy Truck % of Total Volume : 2.60  
Day (16 hrs) % of Total Volume : 90.00

Data for Segment # 1: Sandwich (day/night)

-----  
Angle1 Angle2 : -90.00 deg 90.00 deg  
Wood depth : 0 (No woods.)  
No of house rows : 0 / 0  
Surface : 1 (Absorptive ground surface)  
Receiver source distance : 15.00 / 15.00 m  
Receiver height : 1.50 / 4.50 m  
Topography : 1 (Flat/gentle slope; no barrier)  
Reference angle : 0.00

Results segment # 1: Sandwich (day)

-----  
Source height = 1.27 m

ROAD (0.00 + 61.52 + 0.00) = 61.52 dBA

Angle1	Angle2	Alpha	RefLeq	P.Adj	D.Adj	F.Adj	W.Adj	H.Adj	B.Adj	SubLeq
-90	90	0.66	62.98	0.00	0.00	-1.46	0.00	0.00	0.00	61.52

-----  
Segment Leq : 61.52 dBA

Total Leq All Segments: 61.52 dBA

Results segment # 1: Sandwich (night)

-----  
Source height = 1.27 m

ROAD (0.00 + 55.09 + 0.00) = 55.09 dBA

Angle1	Angle2	Alpha	RefLeq	P.Adj	D.Adj	F.Adj	W.Adj	H.Adj	B.Adj	SubLeq
-90	90	0.58	56.41	0.00	0.00	-1.31	0.00	0.00	0.00	55.09

-----  
Segment Leq : 55.09 dBA

Total Leq All Segments: 55.09 dBA

TOTAL Leq FROM ALL SOURCES (DAY): 62.15  
(NIGHT): 58.84

STAMSON 5.0                      NORMAL REPORT                      Date: 08-09-2005 22:37:50  
 MINISTRY OF ENVIRONMENT AND ENERGY / NOISE ASSESSMENT

Filename: sandwich.te                      Time Period: Day/Night 16/8 hours  
 Description: COMPASION MONITORED AND MODEL - Monitoring Location 3

Rail data, segment # 1: CP RAILWAY (day/night)

Train Type	! Trains ! (Left)	! Trains ! (Right)	! Speed ! (km/h)	!# loc !/Train!	!# Cars !/Train!	! Eng ! type	! Cont ! weld
1. Freight	0.5/0.5	0.5/0.5	20.0	2.0	168.0	Diesel	No

Data for Segment # 1: CP RAILWAY (day/night)

Angle1 Angle2 : -90.00 deg 90.00 deg  
 Wood depth : 0 (No woods.)  
 No of house rows : 0 / 0  
 Surface : 2 (Reflective ground surface)  
 Receiver source distance : 211.00 / 211.00 m  
 Receiver height : 1.50 / 4.50 m  
 Topography : 1 (Flat/gentle slope; no barrier)  
 Whistle Angle : 90 deg Track 1  
 Reference angle : 0.00

Results segment # 1: CP RAILWAY (day)

LOCOMOTIVE (0.00 + 51.78 + 0.00) = 51.78 dBA

Angle1	Angle2	Alpha	RefLeq	D.Adj	F.Adj	W.Adj	H.Adj	B.Adj	SubLeq
-90	90	0.00	63.26	-11.48	0.00	0.00	0.00	0.00	51.78

WHEEL (0.00 + 39.11 + 0.00) = 39.11 dBA

Angle1	Angle2	Alpha	RefLeq	D.Adj	F.Adj	W.Adj	H.Adj	B.Adj	SubLeq
-90	90	0.00	50.59	-11.48	0.00	0.00	0.00	0.00	39.11

LEFT WHISTLE (0.00 + 0.00 + 0.00) = 0.00 dBA

Angle1	Angle2	Alpha	RefLeq	D.Adj	F.Adj	W.Adj	H.Adj	B.Adj	SubLeq
-90	90	0.00	50.59	-11.48	0.00	0.00	0.00	0.00	39.11

Segment Leq : 52.01 dBA

Total Leq All Segments: 52.01 dBA

Results segment # 1: CP RAILWAY (night)

LOCOMOTIVE (0.00 + 54.79 + 0.00) = 54.79 dBA

Angle1	Angle2	Alpha	RefLeq	D.Adj	F.Adj	W.Adj	H.Adj	B.Adj	SubLeq
-90	90	0.00	66.27	-11.48	0.00	0.00	0.00	0.00	54.79

WHEEL (0.00 + 42.12 + 0.00) = 42.12 dBA

Angle1	Angle2	Alpha	RefLeq	D.Adj	F.Adj	W.Adj	H.Adj	B.Adj	SubLeq
-90	90	0.00	53.60	-11.48	0.00	0.00	0.00	0.00	42.12

LEFT WHISTLE (0.00 + 0.00 + 0.00) = 0.00 dBA

Angle1	Angle2	Alpha	RefLeq	D.Adj	F.Adj	W.Adj	H.Adj	B.Adj	SubLeq
-90	90	0.00	53.60	-11.48	0.00	0.00	0.00	0.00	42.12

Segment Leq : 55.02 dBA

Total Leq All Segments: 55.02 dBA

Road data, segment # 1: Sandwich (day/night)

```
-----
Car traffic volume : 6315/702   veh/TimePeriod *
Medium truck volume : 166/18    veh/TimePeriod *
Heavy truck volume  : 173/19    veh/TimePeriod *
Posted speed limit  : 50 km/h
Road gradient       : 0 %
Road pavement       : 1 (Typical asphalt or concrete)
```

\* Refers to calculated road volumes based on the following input:

```
24 hr Traffic Volume (AADT or SADT): 5186
Percentage of Annual Growth         : 3.00
Number of Years of Growth           : 12.00
Medium Truck % of Total Volume      : 2.50
Heavy Truck % of Total Volume       : 2.60
Day (16 hrs) % of Total Volume      : 90.00
```

Data for Segment # 1: Sandwich (day/night)

```
-----
Angle1 Angle2 : -90.00 deg 90.00 deg
Wood depth : 0 (No woods.)
No of house rows : 0 / 0
Surface : 1 (Absorptive ground surface)
Receiver source distance : 60.00 / 60.00 m
Receiver height : 1.50 / 4.50 m
Topography : 1 (Flat/gentle slope; no barrier)
Reference angle : 0.00
```

Results segment # 1: Sandwich (day)

Source height = 1.27 m

ROAD (0.00 + 51.53 + 0.00) = 51.53 dBA

Angle1	Angle2	Alpha	RefLeq	P.Adj	D.Adj	F.Adj	W.Adj	H.Adj	B.Adj	SubLeq
-90	90	0.66	62.98	0.00	-9.99	-1.46	0.00	0.00	0.00	51.53

Segment Leq : 51.53 dBA

Total Leq All Segments: 51.53 dBA

Results segment # 1: Sandwich (night)

Source height = 1.27 m

ROAD (0.00 + 45.60 + 0.00) = 45.60 dBA

Angle1	Angle2	Alpha	RefLeq	P.Adj	D.Adj	F.Adj	W.Adj	H.Adj	B.Adj	SubLeq
-90	90	0.58	56.41	0.00	-9.49	-1.31	0.00	0.00	0.00	45.60

Segment Leq : 45.60 dBA

Total Leq All Segments: 45.60 dBA

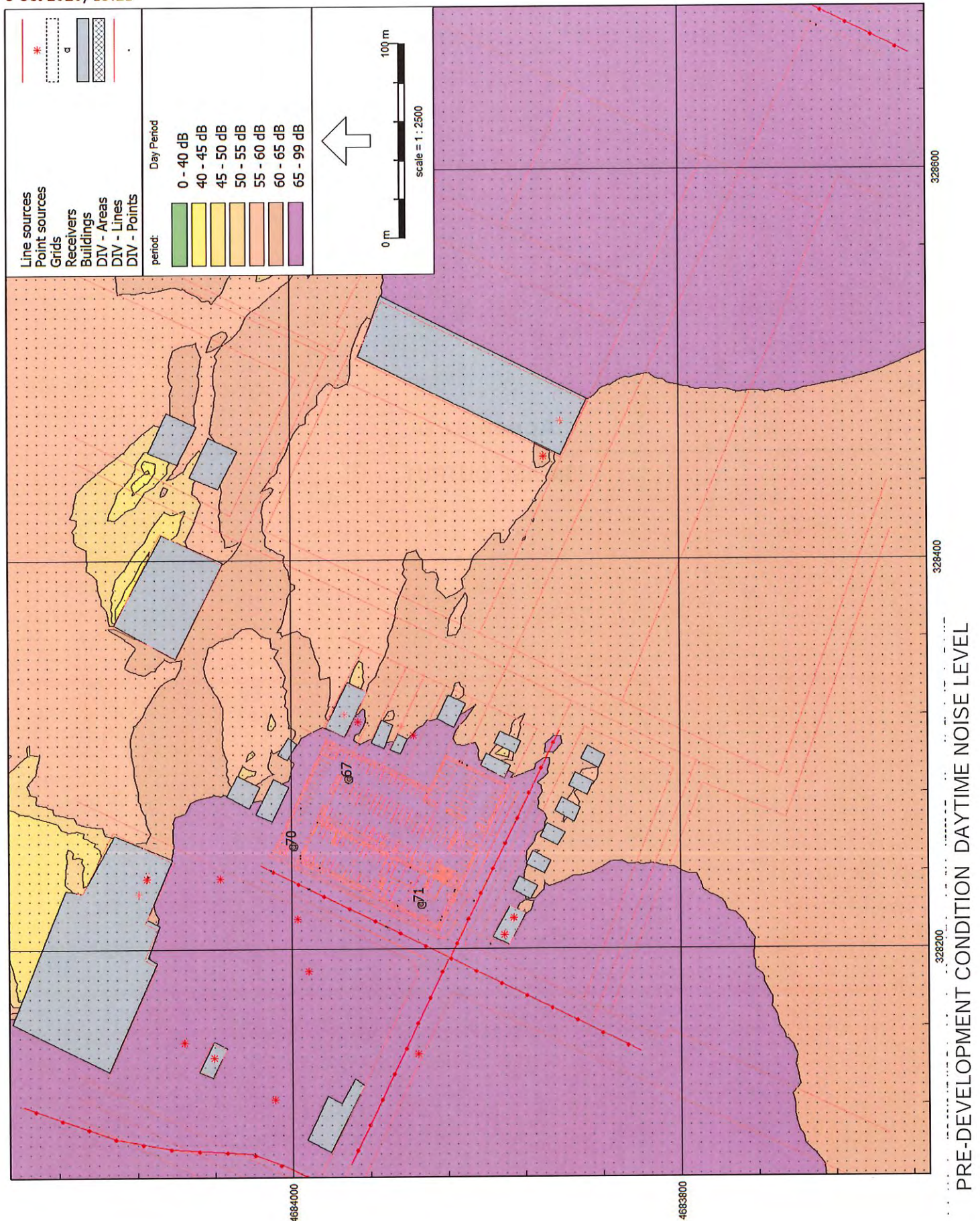
TOTAL Leq FROM ALL SOURCES (DAY): 54.79  
(NIGHT): 55.49

## Appendix C

---

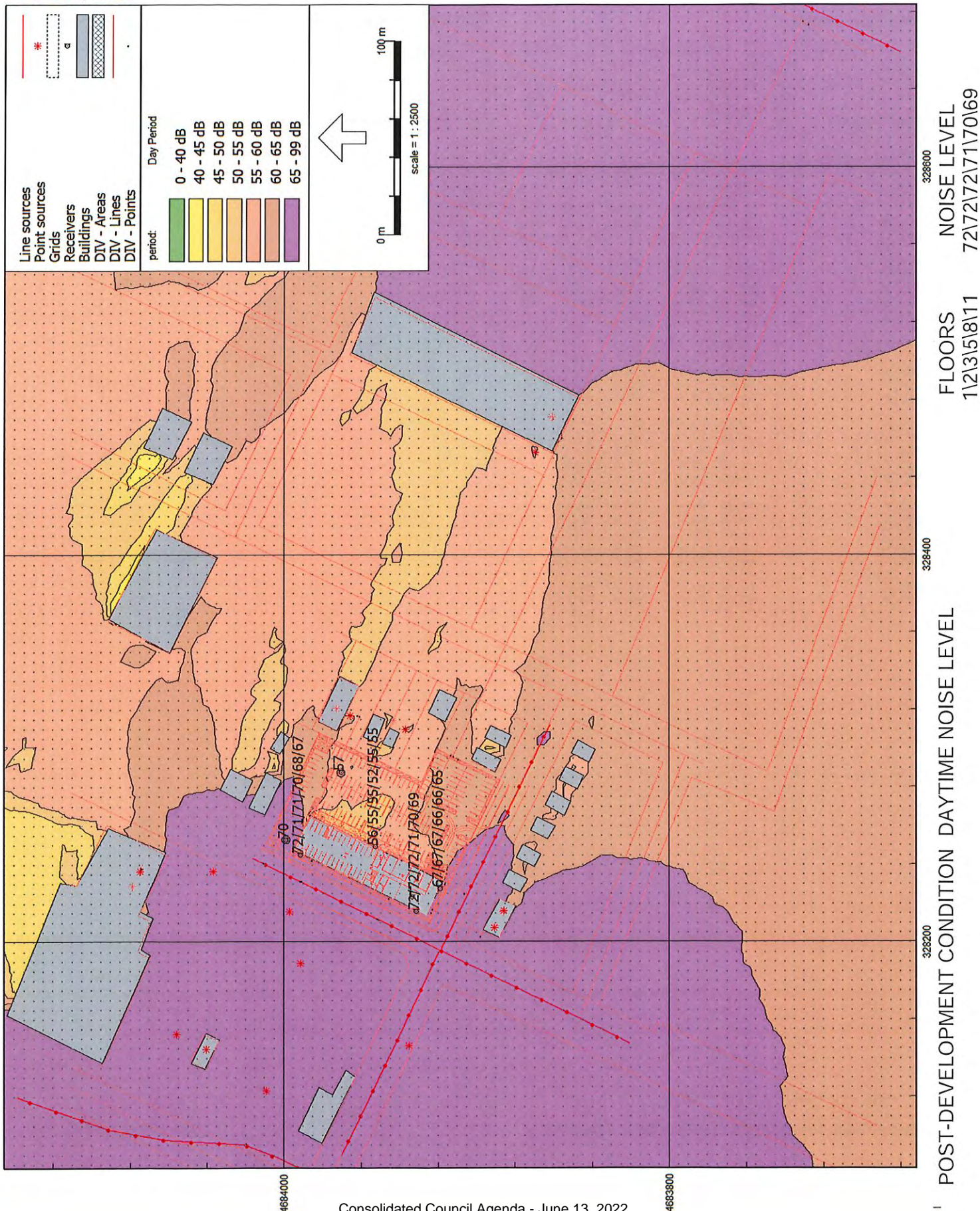
### INOISE OUTPUT







6 Oct 2020, 13:21





Report: Table of Results  
 Model: initial model  
 LAeq: total results for receivers  
 Group: (main group)  
 Group Reduction: No

Name					
Receiver	Description	Height	Day	Evening	Night
M1_A		1.50	69.7	69.7	69.7
M2_A		1.50	--	--	--
M3_A		1.50	57.0	57.0	57.0
REC1_A		1.50	71.6	71.6	71.6
REC1_B		4.80	71.1	71.1	71.1
REC1_C		8.10	70.7	70.7	70.7
REC1_D		14.70	69.6	69.6	69.6
REC1_E		24.60	68.4	68.4	68.4
REC1_F		34.50	67.4	67.4	67.4
REC2_A		1.50	72.4	72.4	72.4
REC2_B		4.80	71.9	71.9	71.9
REC2_C		8.10	71.6	71.6	71.6
REC2_D		14.70	70.7	70.7	70.7
REC2_E		24.60	69.5	69.5	69.5
REC2_F		34.50	68.6	68.6	68.6
REC3_A		1.50	67.4	67.4	67.4
REC3_B		4.80	66.7	66.7	66.7
REC3_C		8.10	66.6	66.6	66.6
REC3_D		14.70	66.4	66.4	66.4
REC3_E		24.60	65.9	65.9	65.9
REC3_F		34.50	65.4	65.4	65.4
REC4_A		1.50	55.9	55.9	55.9
REC4_B		4.80	55.2	55.2	55.2
REC4_C		8.10	54.9	54.9	54.9
REC4_D		14.70	52.1	52.1	52.1
REC4_E		24.60	54.8	54.8	54.8
REC4_F		34.50	54.8	54.8	54.8

All shown dB values are A-weighted

Model: initial model  
 version of Area - Area  
 Group: (main group)  
 Listing of: Line sources, for method Industrial noise - ISO 9613.1/2 (1/3 Octave)

Name	Desc.	ISO H	ISO Terr.	HDef.	Weighting	Ca (D)	Ca (E)	Ca (N)	Max.dist.	No refl.	No building
Train-Nor		1.50	0.00	Relative	A	0.00	0.00	0.00	15.00	No	No
Train-West		1.50	0.00	Relative	A	0.00	0.00	0.00	15.00	No	No
Sandwich		1.50	0.00	Relative	A	0.00	0.00	0.00	15.00	No	No
Chappell-N		1.50	0.00	Relative	A	0.00	0.00	0.00	15.00	No	No
Chappell-S		1.50	0.00	Relative	A	0.00	0.00	0.00	15.00	No	No

Model: initial model  
 version of Area - Area  
 Group: (main group)  
 Listing of: Line sources, for method Industrial noise - ISO 9613.1/2 (1/3 Octave)

Name	LwM 315	LwM 400	LwM 500	LwM 630	LwM 800	LwM 1k	LwM 1.25k	LwM 1.6k	LwM 2k	LwM 2.5k	LwM 3.1k	LwM 4k
Train-Nor	--	--	--	--	--	--	95.00	--	--	--	--	--
Train-West	--	--	--	--	--	--	95.00	--	--	--	--	--
Sandwich	--	--	--	--	--	--	85.00	--	--	--	--	--
Chappell-N	--	--	--	--	--	--	78.00	--	--	--	--	--
Chappell-S	--	--	--	--	--	--	65.00	--	--	--	--	--

Model: initial model  
 version of Area - Area  
 Group: (main group)  
 Listing of: Point sources, for method Industrial noise - ISO 9613.1/2 (1/3 Octave)

Name	Desc.	Height	Terrain L	HDef.	Type	DI	DI_Horz	DI_Vert	DI(0)	DI(10)	DI(20)	DI(30)
Ind-14		1.50	0.00	Relative	Normal point source	none	0	0	0.0	0.0	0.0	0.0
Ind-15		1.50	0.00	Relative	Normal point source	none	0	0	0.0	0.0	0.0	0.0
Truck-6		1.50	0.00	Relative	Normal point source	none	0	0	0.0	0.0	0.0	0.0
Park-5		1.50	0.00	Relative	Normal point source	none	0	0	0.0	0.0	0.0	0.0
Park-4		1.50	0.00	Relative	Normal point source	none	0	0	0.0	0.0	0.0	0.0
Park-3		1.50	0.00	Relative	Normal point source	none	0	0	0.0	0.0	0.0	0.0
HVAC-2		1.50	0.00	Relative	Normal point source	none	0	0	0.0	0.0	0.0	0.0
Truck-9		1.50	0.00	Relative	Normal point source	none	0	0	0.0	0.0	0.0	0.0
HVAC-8		10.00	0.00	Relative	Normal point source	none	0	0	0.0	0.0	0.0	0.0
Ind-7		1.50	0.00	Relative	Normal point source	none	0	0	0.0	0.0	0.0	0.0
Ind-11		1.50	0.00	Relative	Normal point source	none	0	0	0.0	0.0	0.0	0.0
Ind-10		1.50	0.00	Relative	Normal point source	none	0	0	0.0	0.0	0.0	0.0
HVAC-12		10.50	0.00	Relative	Normal point source	none	0	0	0.0	0.0	0.0	0.0
HVAC-13		10.50	0.00	Relative	Normal point source	none	0	0	0.0	0.0	0.0	0.0
HVAC14		1.50	0.00	Relative	Normal point source	none	0	0	0.0	0.0	0.0	0.0

Model: initial model  
 version of Area - Area  
 Group: (main group)  
 Listing of: Point sources, for method Industrial noise - ISO 9613.1/2 (1/3 Octave)

Name	DI(170)	DI(180)	Ca(D)	Ca(E)	Ca(N)	Weighting	No refl.	No building	No ind.site	Lw 25	Lw 31	Lw 40
Ind-14	0.0	0.0	0.00	0.00	0.00	A	No	No	No	--	--	--
Ind-15	0.0	0.0	0.00	0.00	0.00	A	No	No	No	--	--	--
Truck-6	0.0	0.0	0.00	0.00	0.00	A	No	No	No	--	--	--
Park-5	0.0	0.0	0.00	0.00	0.00	A	No	No	No	--	--	--
Park-4	0.0	0.0	0.00	0.00	0.00	A	No	No	No	--	--	--
Park-3	0.0	0.0	0.00	0.00	0.00	A	No	No	No	--	--	--
HVAC-2	0.0	0.0	0.00	0.00	0.00	A	No	No	No	--	--	--
Truck-9	0.0	0.0	0.00	0.00	0.00	A	No	No	No	--	--	--
HVAC-8	0.0	0.0	0.00	0.00	0.00	A	No	No	No	--	--	--
Ind-7	0.0	0.0	0.00	0.00	0.00	A	No	No	No	--	--	--
Ind-11	0.0	0.0	0.00	0.00	0.00	A	No	No	No	--	--	--
Ind-10	0.0	0.0	0.00	0.00	0.00	A	No	No	No	--	--	--
HVAC-12	0.0	0.0	0.00	0.00	0.00	A	No	No	No	--	--	--
HVAC-13	0.0	0.0	0.00	0.00	0.00	A	No	No	No	--	--	--
HVAC14	0.0	0.0	0.00	0.00	0.00	A	No	No	No	--	--	--

Model: initial model  
 version of Area - Area  
 Group: (main group)  
 Listing of: Point sources, for method Industrial noise - ISO 9613.1/2 (1/3 Octave)

Name	Lw 1.25k	Lw 1.6k	Lw 2k	Lw 2.5k	Lw 3.1k	Lw 4k	Lw 5k	Lw 6.3k	Lw 8k	Lw 10k	Red 25	Red 31	Red 40
Ind-14	70.00	--	--	--	--	--	--	--	--	--	0.00	0.00	0.00
Ind-15	70.00	--	--	--	--	--	--	--	--	--	0.00	0.00	0.00
Truck-6	75.00	--	--	--	--	--	--	--	--	--	0.00	0.00	0.00
Park-5	65.00	--	--	--	--	--	--	--	--	--	0.00	0.00	0.00
Park-4	65.00	--	--	--	--	--	--	--	--	--	0.00	0.00	0.00
Park-3	65.00	--	--	--	--	--	--	--	--	--	0.00	0.00	0.00
HVAC-2	70.00	--	--	--	--	--	--	--	--	--	0.00	0.00	0.00
Truck-9	75.00	--	--	--	--	--	--	--	--	--	0.00	0.00	0.00
HVAC-8	75.00	--	--	--	--	--	--	--	--	--	0.00	0.00	0.00
Ind-7	80.00	--	--	--	--	--	--	--	--	--	0.00	0.00	0.00
Ind-11	90.00	--	--	--	--	--	--	--	--	--	0.00	0.00	0.00
Ind-10	90.00	--	--	--	--	--	--	--	--	--	0.00	0.00	0.00
HVAC-12	70.00	--	--	--	--	--	--	--	--	--	0.00	0.00	0.00
HVAC-13	70.00	--	--	--	--	--	--	--	--	--	0.00	0.00	0.00
HVAC14	75.00	--	--	--	--	--	--	--	--	--	0.00	0.00	0.00



Model: initial model  
 version of Area - Area  
 Group: (main group)  
 Listing of: Receivers, for method Industrial noise - ISO 9613.1/2 (1/3 Octave)

Name	Desc.	Terrain L	HDef.	Height A	Height B	Height C	Height D	Height E	Height F	Façade
M2		0.00	Relative	1.50	--	--	--	--	--	Yes
M1		0.00	Relative	1.50	--	--	--	--	--	Yes
M3		0.00	Relative	1.50	--	--	--	--	--	Yes
REC2		0.00	Relative	1.50	4.80	8.10	14.70	24.60	34.50	Yes
REC1		0.00	Relative	1.50	4.80	8.10	14.70	24.60	34.50	Yes
REC3		0.00	Relative	1.50	4.80	8.10	14.70	24.60	34.50	Yes
REC4		0.00	Relative	1.50	4.80	8.10	14.70	24.60	34.50	Yes

## Appendix D

---

### BUILDING COMPONENT

## 3885 Sandwich Street Development Building Component Review

Receiver 1 & 2					
Receiver 1 & 2	BR/LR	COMPONENT	F AREA	W AREA	STC
W Façade	Living	Window	15.6	1.7	35
W Façade	Living	Door	15.6	2.3	36
W Façade	Living	Wall	15.6	8.96	42
W Façade	Bed	Window	17.4	1.8	38
W Façade	Bed	Wall	17.4	6.66	43

Receiver 3					
Receiver 3	BR/LR	COMPONENT	F AREA	W AREA	STC
NS Façade	Living	Window	15.6	1.7	33
NS Façade	Living	Door	15.6	2.3	34
NS Façade	Living	Wall	15.6	8.96	40
NS Façade	Bed	Window	17.4	1.8	36
NS Façade	Bed	Wall	17.4	6.66	41

Receiver 4					
Receiver 4	BR/LR	COMPONENT	F AREA	W AREA	STC
E Façade	Living	Window	15.6	1.7	21
E Façade	Living	Door	15.6	2.3	22
E Façade	Living	Wall	15.6	8.96	28
E Façade	Bed	Window	17.4	1.8	24
E Façade	Bed	Wall	17.4	6.66	29

\*NOTE: STC VALUES ARE BASED OFF A SOUND LEVEL OF RECEIVER LOCATIONS

## 3885 SANDWICH STREET DEVELOPMENT WALL DOOR WINDOWS COMPONENT

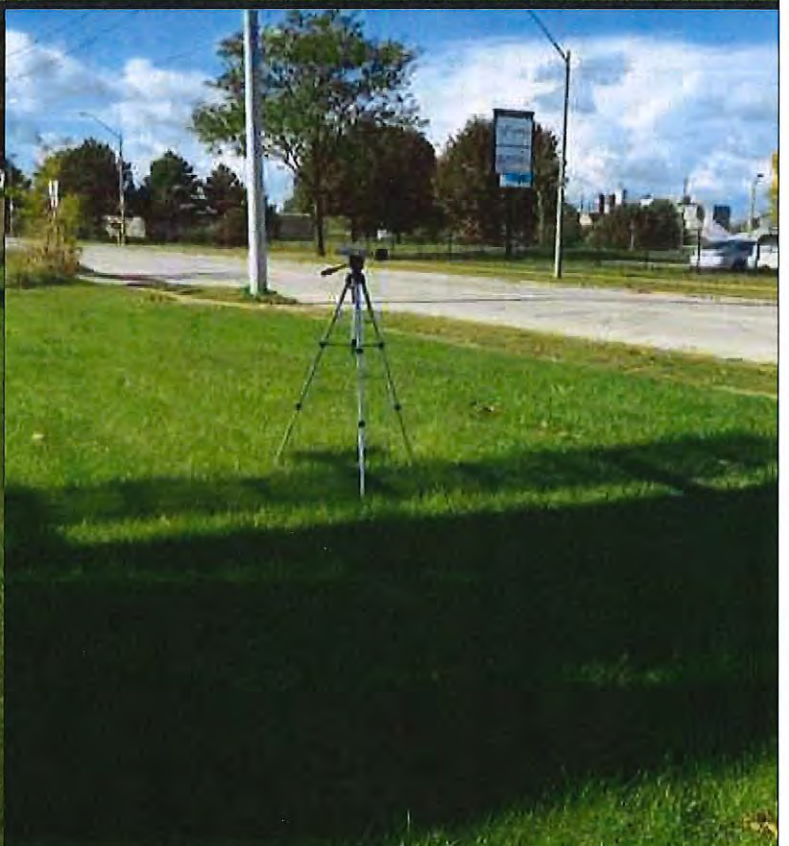
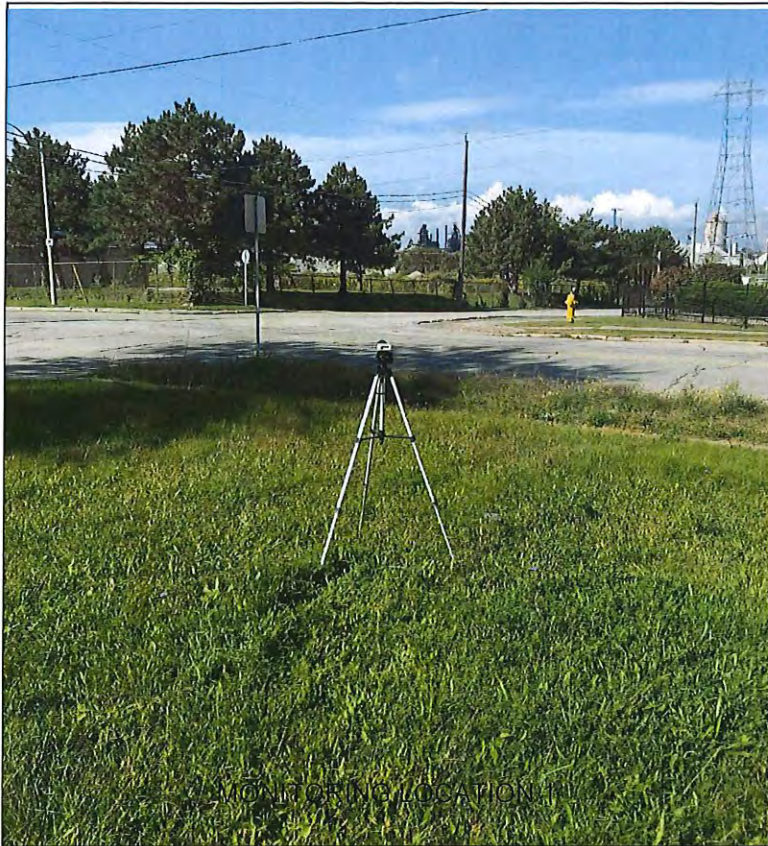
[illegible]

## Appendix E

---

### NOISE MONITORING





<p>PROJECT TITLE:</p> <p>HIGH RISE DEVELOPMENT 3885 SANDWICH STREET, WINDSOR ON</p>	<p>DATE:</p> <p>OCT 5, 2020</p>	 <p>27 PRINCESS STREET, SUITE #102 LEAMINGTON, ONTARIO N3H 2X8</p> <p>1000 - 287 PELISSIER STREET, WINDSOR, ONTARIO N9A 4K4</p>
<p>SHEET TITLE:</p> <p>PHOTOS</p>	<p>SCALE:</p> <p>NTS</p>	
	<p>PROJECT NO:</p> <p>20-028</p> <p>FIGURE NO:</p> <p>EXHIBIT 1</p>	



Freq Weight : A  
 Time Weight : SLOW  
 Level Range : 40-100  
 Max dB : 80.5 - 2020/10/02 14:45:07  
 Level Range : 40-100  
 SEL : 105.6  
 Leq : 70.0

No.s	Date Time	(dB)					
1	2020-10-02 14:22:12	72.1	72.6	72.8	72.5	72.8	
6	2020-10-02 14:22:17	72.5	71.6	71.7	71.1	69.3	
11	2020-10-02 14:22:22	67.5	65.8	63.9	61.6	59.8	
16	2020-10-02 14:22:27	58.9	58.9	58.1	57.9	57.7	
21	2020-10-02 14:22:32	57.8	57.5	56.8	57.3	59.9	
26	2020-10-02 14:22:37	62.4	65.7	66.8	65.8	65.4	
31	2020-10-02 14:22:42	66.4	67.9	70.1	69.7	68.3	
36	2020-10-02 14:22:47	68.4	69.7	72.3	73.4	72.7	
41	2020-10-02 14:22:52	71.3	70.2	69.0	67.9	66.9	
46	2020-10-02 14:22:57	65.9	66.1	66.6	68.3	68.5	
51	2020-10-02 14:23:02	67.9	66.3	64.5	62.7	62.5	
56	2020-10-02 14:23:07	64.8	66.3	65.7	63.8	61.3	
61	2020-10-02 14:23:12	59.4	57.8	57.0	56.5	55.6	
66	2020-10-02 14:23:17	55.1	55.0	55.6	55.8	55.3	
71	2020-10-02 14:23:22	54.7	55.1	55.9	56.6	58.2	
76	2020-10-02 14:23:27	59.8	61.7	66.4	67.7	67.7	
81	2020-10-02 14:23:32	66.8	65.3	66.7	68.7	69.2	
86	2020-10-02 14:23:37	68.7	67.6	65.8	65.2	66.2	
91	2020-10-02 14:23:42	66.8	66.5	69.5	70.0	68.4	
96	2020-10-02 14:23:47	66.6	65.6	65.7	66.6	67.0	
101	2020-10-02 14:23:52	65.2	63.4	63.4	67.2	67.7	
106	2020-10-02 14:23:57	65.8	63.2	61.1	59.1	62.0	
111	2020-10-02 14:24:02	61.4	62.1	66.4	66.9	66.0	
116	2020-10-02 14:24:07	64.2	62.0	60.8	59.7	58.9	
121	2020-10-02 14:24:12	59.6	60.2	64.8	67.4	66.7	
126	2020-10-02 14:24:17	65.2	64.5	62.4	61.8	61.4	
131	2020-10-02 14:24:22	62.7	64.6	66.3	68.2	68.0	
136	2020-10-02 14:24:27	67.7	66.6	65.2	63.7	63.1	
141	2020-10-02 14:24:32	61.1	61.0	63.6	67.2	69.0	
146	2020-10-02 14:24:37	67.3	66.1	67.8	70.1	69.7	
151	2020-10-02 14:24:42	67.7	65.9	63.8	65.5	62.8	
156	2020-10-02 14:24:47	60.1	59.7	61.3	66.2	67.8	
161	2020-10-02 14:24:52	66.6	64.0	62.8	65.6	67.2	
166	2020-10-02 14:24:57	67.6	65.9	63.5	62.8	61.9	
171	2020-10-02 14:25:02	59.9	58.1	57.5	58.9	63.6	
176	2020-10-02 14:25:07	67.8	68.6	67.7	65.5	64.4	
181	2020-10-02 14:25:12	63.7	63.2	63.7	66.7	67.6	
186	2020-10-02 14:25:17	66.2	64.1	62.8	64.5	67.4	
191	2020-10-02 14:25:22	69.0	68.4	66.6	64.4	62.0	
196	2020-10-02 14:25:27	60.5	60.1	59.3	58.9	58.8	
201	2020-10-02 14:25:32	60.4	61.5	65.4	67.0	66.3	
206	2020-10-02 14:25:37	64.5	62.6	62.0	62.2	64.5	
211	2020-10-02 14:25:42	68.2	69.1	67.7	65.4	63.7	
216	2020-10-02 14:25:47	64.5	66.1	67.8	68.2	67.1	
221	2020-10-02 14:25:52	66.3	68.1	71.3	72.6	72.8	
226	2020-10-02 14:25:57	71.9	70.6	69.5	68.9	67.3	
231	2020-10-02 14:26:02	67.8	68.3	68.5	67.9	67.8	
236	2020-10-02 14:26:07	69.8	69.9	68.5	66.0	63.7	
241	2020-10-02 14:26:12	62.1	60.6	60.4	61.1	63.1	
246	2020-10-02 14:26:17	67.0	69.5	69.7	68.5	67.9	
251	2020-10-02 14:26:22	70.8	71.7	71.7	70.9	69.5	
256	2020-10-02 14:26:27	68.0	66.4	64.9	66.4	67.2	
261	2020-10-02 14:26:32	66.6	64.6	62.0	59.9	58.4	
266	2020-10-02 14:26:37	58.6	57.6	56.5	56.1	58.2	
271	2020-10-02 14:26:42	60.8	65.7	67.7	66.9	65.9	
276	2020-10-02 14:26:47	67.2	66.7	65.0	62.7	61.6	
281	2020-10-02 14:26:52	63.2	65.9	68.9	70.5	70.9	
286	2020-10-02 14:26:57	71.5	70.8	70.5	69.9	69.2	
291	2020-10-02 14:27:02	70.2	70.6	70.6	70.8	71.7	
296	2020-10-02 14:27:07	72.0	71.9	71.2	69.1	66.9	
301	2020-10-02 14:27:12	65.7	63.8	61.8	59.9	58.1	
306	2020-10-02 14:27:17	56.9	56.2	55.7	55.2	54.8	
311	2020-10-02 14:27:22	54.7	56.4	55.9	56.4	59.7	
316	2020-10-02 14:27:27	64.4	68.7	70.3	70.2	71.1	
321	2020-10-02 14:27:32	71.5	71.4	71.1	69.9	67.6	
326	2020-10-02 14:27:37	65.3	63.9	63.1	65.3	66.9	
331	2020-10-02 14:27:42	72.3	75.1	74.4	71.3	69.5	
336	2020-10-02 14:27:47	68.8	67.7	68.2	68.4	69.1	
341	2020-10-02 14:27:52	69.1	69.0	68.7	68.7	68.3	
346	2020-10-02 14:27:57	66.8	65.0	64.8	66.8	68.3	
351	2020-10-02 14:28:02	68.2	66.6	65.1	63.2	61.3	
356	2020-10-02 14:28:07	62.0	60.0	59.4	60.7	63.1	
361	2020-10-02 14:28:12	66.5	68.6	69.5	68.5	66.2	
366	2020-10-02 14:28:17	63.9	64.2	67.7	70.1	69.3	
371	2020-10-02 14:28:22	67.7	66.2	66.2	69.1	69.7	
376	2020-10-02 14:28:27	67.9	67.5	68.5	68.1	65.9	
381	2020-10-02 14:28:32	63.5	61.2	59.2	59.7	60.3	
386	2020-10-02 14:28:37	61.8	61.5	59.3	60.4	59.1	
391	2020-10-02 14:28:42	59.5	60.5	60.3	59.0	58.5	
396	2020-10-02 14:28:47	61.6	67.9	69.9	68.3	65.7	
401	2020-10-02 14:28:52	66.1	68.7	69.3	69.7	69.2	
406	2020-10-02 14:28:57	69.9	71.2	70.1	68.7	67.0	
411	2020-10-02 14:29:02	65.1	64.3	64.3	62.9	62.2	
416	2020-10-02 14:29:07	66.0	64.3	64.3	62.9	62.2	

421	2020-10-02	14:29:12	67.9	67.7	68.2	67.4	66.2
426	2020-10-02	14:29:17	68.2	68.8	67.3	65.1	64.6
431	2020-10-02	14:29:22	65.0	64.3	63.2	64.5	66.4
436	2020-10-02	14:29:27	65.5	63.4	62.0	62.7	66.4
441	2020-10-02	14:29:32	69.1	69.2	68.4	68.6	69.1
446	2020-10-02	14:29:37	68.2	66.3	64.0	65.8	65.4
451	2020-10-02	14:29:42	65.8	65.1	65.3	66.3	66.2
456	2020-10-02	14:29:47	64.0	61.8	60.1	58.2	57.1
461	2020-10-02	14:29:52	57.2	56.1	55.5	55.2	55.2
466	2020-10-02	14:29:57	55.1	55.2	54.8	54.8	55.8
471	2020-10-02	14:30:02	56.0	56.5	58.1	60.5	63.7
476	2020-10-02	14:30:07	67.4	69.9	70.3	70.0	70.8
481	2020-10-02	14:30:12	70.8	69.7	68.2	67.9	69.1
486	2020-10-02	14:30:17	73.3	76.4	76.6	74.6	72.5
491	2020-10-02	14:30:22	70.3	67.8	64.8	62.2	60.3
496	2020-10-02	14:30:27	59.4	59.3	61.4	63.8	67.6
501	2020-10-02	14:30:32	68.7	69.2	69.6	70.5	70.1
506	2020-10-02	14:30:37	68.4	65.4	63.1	61.9	60.3
511	2020-10-02	14:30:42	59.1	57.6	55.9	54.9	54.3
516	2020-10-02	14:30:47	54.1	54.2	53.9	54.1	55.0
521	2020-10-02	14:30:52	56.4	58.5	61.0	63.9	65.9
526	2020-10-02	14:30:57	67.3	66.8	64.7	63.1	62.7
531	2020-10-02	14:31:02	64.9	66.3	70.1	71.4	70.7
536	2020-10-02	14:31:07	68.8	67.3	65.7	63.8	65.5
541	2020-10-02	14:31:12	65.8	66.5	65.7	66.4	66.6
546	2020-10-02	14:31:17	67.5	66.8	65.8	67.0	67.8
551	2020-10-02	14:31:22	66.1	64.7	64.5	67.1	70.0
556	2020-10-02	14:31:27	71.3	71.8	70.8	68.3	65.3
561	2020-10-02	14:31:32	62.5	60.8	61.3	63.4	64.8
566	2020-10-02	14:31:37	64.0	62.3	61.4	59.3	57.6
571	2020-10-02	14:31:42	56.8	56.5	56.7	57.3	57.8
576	2020-10-02	14:31:47	58.4	60.8	63.1	64.5	64.9
581	2020-10-02	14:31:52	64.9	64.2	62.9	60.8	59.1
586	2020-10-02	14:31:57	58.6	61.4	62.9	65.7	66.2
591	2020-10-02	14:32:02	64.7	61.9	59.7	58.0	57.4
596	2020-10-02	14:32:07	61.1	60.8	62.5	65.9	68.6
601	2020-10-02	14:32:12	68.1	65.9	63.6	63.7	64.6
606	2020-10-02	14:32:17	64.0	63.6	64.0	65.6	67.7
611	2020-10-02	14:32:22	68.5	69.0	68.9	68.6	69.0
616	2020-10-02	14:32:27	69.4	70.2	69.8	69.8	69.5
621	2020-10-02	14:32:32	68.6	68.1	66.5	64.7	64.3
626	2020-10-02	14:32:37	65.8	68.6	70.7	71.3	69.7
631	2020-10-02	14:32:42	68.7	71.3	71.8	70.5	69.0
636	2020-10-02	14:32:47	69.0	69.1	69.1	68.1	69.7
641	2020-10-02	14:32:52	69.9	69.4	68.5	68.8	68.9
646	2020-10-02	14:32:57	68.9	68.6	70.0	71.0	70.9
651	2020-10-02	14:33:02	69.6	67.5	65.3	65.4	66.0
656	2020-10-02	14:33:07	66.9	65.9	63.6	61.7	61.7
661	2020-10-02	14:33:12	64.6	66.2	66.5	65.6	64.4
666	2020-10-02	14:33:17	65.7	67.0	65.5	63.4	61.6
671	2020-10-02	14:33:22	60.1	59.8	60.0	60.6	65.6
676	2020-10-02	14:33:27	70.3	71.1	68.5	65.5	62.8
681	2020-10-02	14:33:32	60.8	60.4	62.5	67.3	70.7
686	2020-10-02	14:33:37	69.7	67.0	65.5	67.4	69.2
691	2020-10-02	14:33:42	68.5	66.3	65.8	68.2	69.3
696	2020-10-02	14:33:47	67.4	65.5	65.7	66.6	65.8
701	2020-10-02	14:33:52	63.9	62.5	61.1	59.6	59.8
706	2020-10-02	14:33:57	64.2	68.0	69.6	70.6	70.4
711	2020-10-02	14:34:02	69.9	70.3	69.5	67.7	65.7
716	2020-10-02	14:34:07	64.1	63.3	63.8	65.5	66.7
721	2020-10-02	14:34:12	65.9	63.6	60.8	58.6	57.0
726	2020-10-02	14:34:17	55.9	55.7	55.3	55.1	55.9
731	2020-10-02	14:34:22	57.1	59.1	61.8	64.8	65.3
736	2020-10-02	14:34:27	65.5	66.8	67.6	67.6	68.2
741	2020-10-02	14:34:32	69.1	68.7	67.4	66.0	65.5
746	2020-10-02	14:34:37	66.0	66.6	68.6	68.3	66.9
751	2020-10-02	14:34:42	65.4	65.0	66.4	66.5	68.2
756	2020-10-02	14:34:47	70.0	69.5	68.1	66.4	66.9
761	2020-10-02	14:34:52	68.3	68.8	68.4	66.5	64.2
766	2020-10-02	14:34:57	61.7	59.2	57.8	57.2	57.1
771	2020-10-02	14:35:02	57.6	58.5	60.8	64.2	65.2
776	2020-10-02	14:35:07	64.6	63.4	61.9	61.7	64.8
781	2020-10-02	14:35:12	66.8	65.9	63.6	62.7	63.0
786	2020-10-02	14:35:17	65.2	68.0	68.3	68.6	71.3
791	2020-10-02	14:35:22	72.2	72.3	71.9	71.8	71.3
796	2020-10-02	14:35:27	70.8	71.2	71.3	71.8	71.7
801	2020-10-02	14:35:32	70.3	68.6	66.5	66.2	64.8
806	2020-10-02	14:35:37	63.9	62.8	61.6	60.8	60.8
811	2020-10-02	14:35:42	60.6	60.4	60.6	62.4	64.5
816	2020-10-02	14:35:47	69.4	72.3	73.3	71.3	69.4
821	2020-10-02	14:35:52	67.2	64.6	63.7	62.4	61.8
826	2020-10-02	14:35:57	61.3	60.6	61.7	64.7	65.2
831	2020-10-02	14:36:02	64.8	64.1	64.2	67.4	71.2
836	2020-10-02	14:36:07	72.7	72.0	69.4	66.9	64.4
841	2020-10-02	14:36:12	63.5	62.8	62.1	61.7	60.4
846	2020-10-02	14:36:17	60.7	62.6	65.3	67.3	67.4
851	2020-10-02	14:36:22	68.0	67.5	66.3	64.1	61.7
856	2020-10-02	14:36:27	60.3	59.1	60.8	64.2	65.1
861	2020-10-02	14:36:32	64.6	63.1	61.5	59.6	57.6
866	2020-10-02	14:36:37	55.9	54.5	57.8	59.9	57.2
871	2020-10-02	14:36:42	55.4	54.1	53.3	56.9	54.8
876	2020-10-02	14:36:47	53.7	52.8	52.3	53.0	52.7
881	2020-10-02	14:36:52	53.0	52.6	52.5	62.5	63.2
886	2020-10-02	14:36:57	60.7	61.8	64.1	67.5	67.3
891	2020-10-02	14:37:02	66.4	67.3	69.0	68.1	65.4
896	2020-10-02	14:37:07	63.2	63.8	64.1	64.1	64.1
901	2020-10-02	14:37:12	70.8	70.8	71.9	71.6	71.5
906	2020-10-02	14:37:17	70.2	71.2	71.9	71.6	71.5

911	2020-10-02	14:37:22	71.3	71.3	71.6	71.5	70.7
916	2020-10-02	14:37:27	69.5	70.2	70.7	71.1	70.5
921	2020-10-02	14:37:32	70.1	69.1	67.8	66.6	66.2
926	2020-10-02	14:37:37	66.2	67.2	69.6	70.0	70.5
931	2020-10-02	14:37:42	69.5	67.7	66.4	65.2	63.2
936	2020-10-02	14:37:47	61.3	63.2	63.9	66.3	66.7
941	2020-10-02	14:37:52	64.7	63.8	64.6	65.5	68.2
946	2020-10-02	14:37:57	71.9	74.2	75.0	73.5	70.7
951	2020-10-02	14:38:02	70.6	70.9	70.3	69.2	68.2
956	2020-10-02	14:38:07	67.4	66.7	66.7	67.6	68.6
961	2020-10-02	14:38:12	68.3	68.1	67.8	67.9	68.8
966	2020-10-02	14:38:17	68.6	68.3	67.8	66.7	66.5
971	2020-10-02	14:38:22	67.0	66.3	65.3	65.8	66.5
976	2020-10-02	14:38:27	67.1	66.4	65.4	63.4	61.2
981	2020-10-02	14:38:32	60.0	59.5	58.9	60.3	60.9
986	2020-10-02	14:38:37	61.7	64.2	68.0	72.3	73.2
991	2020-10-02	14:38:42	72.0	71.0	69.9	68.9	67.5
996	2020-10-02	14:38:47	65.2	63.5	64.6	66.8	67.5
1001	2020-10-02	14:38:52	66.9	65.8	64.1	63.2	62.0
1006	2020-10-02	14:38:57	60.7	59.8	58.9	57.1	55.8
1011	2020-10-02	14:39:02	56.2	61.4	59.1	57.0	54.9
1016	2020-10-02	14:39:07	53.9	53.2	54.7	53.8	54.0
1021	2020-10-02	14:39:12	53.9	53.8	53.8	54.4	55.6
1026	2020-10-02	14:39:17	59.5	60.4	61.0	63.0	65.6
1031	2020-10-02	14:39:22	67.5	69.7	70.7	70.5	69.1
1036	2020-10-02	14:39:27	67.8	68.7	68.9	67.4	65.7
1041	2020-10-02	14:39:32	66.3	67.0	66.9	66.6	66.5
1046	2020-10-02	14:39:37	69.2	70.1	71.1	71.4	71.7
1051	2020-10-02	14:39:42	71.7	70.4	69.0	67.7	66.8
1056	2020-10-02	14:39:47	65.2	64.3	63.9	61.5	59.6
1061	2020-10-02	14:39:52	58.2	58.1	59.3	64.8	68.8
1066	2020-10-02	14:39:57	69.1	67.8	66.0	64.6	63.6
1071	2020-10-02	14:40:02	65.3	65.7	67.6	69.9	69.5
1076	2020-10-02	14:40:07	67.5	65.3	65.2	67.5	71.6
1081	2020-10-02	14:40:12	71.9	69.5	66.5	63.7	61.7
1086	2020-10-02	14:40:17	61.1	62.3	64.7	65.0	65.6
1091	2020-10-02	14:40:22	66.8	66.7	64.7	62.9	60.8
1096	2020-10-02	14:40:27	59.1	57.6	57.2	57.4	57.9
1101	2020-10-02	14:40:32	61.2	61.5	62.5	64.9	68.4
1106	2020-10-02	14:40:37	69.1	69.6	68.4	67.3	67.3
1111	2020-10-02	14:40:42	65.6	63.7	62.1	62.2	63.3
1116	2020-10-02	14:40:47	65.1	68.7	70.3	71.0	70.2
1121	2020-10-02	14:40:52	69.2	68.5	67.6	66.0	65.7
1126	2020-10-02	14:40:57	66.0	65.5	66.7	67.4	67.8
1131	2020-10-02	14:41:02	66.7	66.0	63.1	61.3	60.0
1136	2020-10-02	14:41:07	60.2	60.8	59.7	58.8	57.8
1141	2020-10-02	14:41:12	56.1	55.8	55.9	56.0	54.8
1146	2020-10-02	14:41:17	54.1	54.7	58.1	59.7	57.9
1151	2020-10-02	14:41:22	56.1	55.2	56.9	58.1	59.7
1156	2020-10-02	14:41:27	61.7	63.2	67.5	69.2	69.1
1161	2020-10-02	14:41:32	67.5	65.6	64.1	62.0	60.7
1166	2020-10-02	14:41:37	58.9	57.9	56.5	56.3	57.2
1171	2020-10-02	14:41:42	58.5	61.2	64.6	68.1	72.4
1176	2020-10-02	14:41:47	74.9	76.2	74.6	71.8	70.4
1181	2020-10-02	14:41:52	69.3	69.8	69.4	70.4	71.3
1186	2020-10-02	14:41:57	71.4	70.2	68.4	68.6	69.7
1191	2020-10-02	14:42:02	69.8	70.1	70.0	70.1	70.8
1196	2020-10-02	14:42:07	71.2	71.7	71.7	71.6	72.1
1201	2020-10-02	14:42:12	71.4	70.0	68.0	66.5	64.8
1206	2020-10-02	14:42:17	63.7	65.4	68.0	69.1	70.0
1211	2020-10-02	14:42:22	70.0	69.5	70.6	69.3	67.1
1216	2020-10-02	14:42:27	65.9	66.8	66.9	64.8	63.2
1221	2020-10-02	14:42:32	61.1	58.8	57.4	56.4	55.8
1226	2020-10-02	14:42:37	54.7	59.3	57.2	55.7	54.4
1231	2020-10-02	14:42:42	53.9	55.6	56.4	57.0	59.8
1236	2020-10-02	14:42:47	61.9	66.9	69.2	68.7	68.1
1241	2020-10-02	14:42:52	67.9	67.6	68.3	67.6	65.2
1246	2020-10-02	14:42:57	63.1	61.8	60.4	59.8	59.4
1251	2020-10-02	14:43:02	57.7	58.3	60.7	63.7	67.3
1256	2020-10-02	14:43:07	67.1	66.4	68.5	68.8	67.7
1261	2020-10-02	14:43:12	67.8	69.2	68.7	68.0	69.2
1266	2020-10-02	14:43:17	70.2	70.4	70.4	70.5	70.2
1271	2020-10-02	14:43:22	70.0	70.3	70.7	70.1	70.1
1276	2020-10-02	14:43:27	70.6	72.0	72.6	73.1	73.0
1281	2020-10-02	14:43:32	71.9	70.3	69.5	68.7	67.4
1286	2020-10-02	14:43:37	65.8	63.8	62.5	62.4	63.3
1291	2020-10-02	14:43:42	62.8	62.4	61.5	63.6	65.0
1296	2020-10-02	14:43:47	64.6	63.4	65.0	66.9	68.3
1301	2020-10-02	14:43:52	66.6	63.7	61.0	59.1	58.5
1306	2020-10-02	14:43:57	58.6	59.5	59.9	61.4	63.4
1311	2020-10-02	14:44:02	65.8	67.8	69.2	69.9	69.0
1316	2020-10-02	14:44:07	66.8	64.4	62.1	59.9	58.3
1321	2020-10-02	14:44:12	57.5	58.1	59.5	61.2	64.9
1326	2020-10-02	14:44:17	68.5	70.6	71.3	72.4	73.0
1331	2020-10-02	14:44:22	72.2	70.7	70.7	71.5	71.5
1336	2020-10-02	14:44:27	70.0	68.4	66.6	64.6	65.3
1341	2020-10-02	14:44:32	69.1	69.6	69.5	68.3	66.3
1346	2020-10-02	14:44:37	65.8	67.5	68.5	67.2	65.4
1351	2020-10-02	14:44:42	65.4	66.4	69.5	69.2	68.1
1356	2020-10-02	14:44:47	67.4	68.7	69.2	68.8	66.9
1361	2020-10-02	14:44:52	64.3	64.7	65.0	67.9	70.8
1366	2020-10-02	14:44:57	71.1	69.7	67.4	65.1	64.8
1371	2020-10-02	14:45:02	66.2	68.4	70.1	74.3	77.7
1376	2020-10-02	14:45:07	80.5	78.7	75.2	72.6	70.8
1381	2020-10-02	14:45:12	68.4	66.1	64.8	63.2	65.1
1386	2020-10-02	14:45:17	67.8	68.3	67.2	67.7	67.7
1391	2020-10-02	14:45:22	69.9	70.6	69.5	67.8	66.1
1396	2020-10-02	14:45:27	66.9	67.9	67.4	67.8	67.8

1401	2020-10-02	14:45:32	70.9	70.6	68.1	65.4	62.7
1406	2020-10-02	14:45:37	61.0	59.9	58.9	60.0	63.3
1411	2020-10-02	14:45:42	67.0	72.2	72.5	71.8	70.9
1416	2020-10-02	14:45:47	70.9	70.8	70.4	69.7	68.7
1421	2020-10-02	14:45:52	68.1	67.4	67.5	67.9	68.4
1426	2020-10-02	14:45:57	68.6	69.0	68.9	68.9	70.5
1431	2020-10-02	14:46:02	72.5	74.7	75.1	75.2	74.4
1436	2020-10-02	14:46:07	72.9	71.8	71.1	70.8	70.3
1441	2020-10-02	14:46:12	70.2	70.6	70.1	69.1	67.3
1446	2020-10-02	14:46:17	65.8	64.2	62.0	59.7	58.6
1451	2020-10-02	14:46:22	57.6	56.4	56.2	55.5	55.0
1456	2020-10-02	14:46:27	54.4	53.8	54.1	53.7	52.5
1461	2020-10-02	14:46:32	51.9	51.2	50.9	50.6	50.7
1466	2020-10-02	14:46:37	50.4	52.5	52.2	52.0	52.3
1471	2020-10-02	14:46:42	54.2	53.7	52.9	53.1	53.6
1476	2020-10-02	14:46:47	54.0	55.0	57.5	59.0	61.8
1481	2020-10-02	14:46:52	65.5	65.8	63.6	60.8	58.7
1486	2020-10-02	14:46:57	57.7	57.4	59.5	62.9	67.5
1491	2020-10-02	14:47:02	70.7	70.1	67.6	65.0	63.3
1496	2020-10-02	14:47:07	63.7	65.7	66.1	65.9	64.0
1501	2020-10-02	14:47:12	62.1	60.1	58.9	57.8	57.3
1506	2020-10-02	14:47:17	56.3	55.1	55.8	60.1	59.1
1511	2020-10-02	14:47:22	57.5	56.0	54.4	55.1	55.6
1516	2020-10-02	14:47:27	57.0	60.0	64.9	67.7	66.5
1521	2020-10-02	14:47:32	63.7	61.3	60.4	63.3	66.4
1526	2020-10-02	14:47:37	68.5	69.7	68.7	67.1	64.6
1531	2020-10-02	14:47:42	62.7	61.8	63.9	67.2	67.3
1536	2020-10-02	14:47:47	66.2	64.3	62.5	62.7	64.9
1541	2020-10-02	14:47:52	68.6	70.3	70.9	71.2	70.1
1546	2020-10-02	14:47:57	67.9	66.3	64.8	64.1	66.8
1551	2020-10-02	14:48:02	68.5	68.8	69.0	68.6	67.7
1556	2020-10-02	14:48:07	67.6	67.9	67.9	66.3	64.1
1561	2020-10-02	14:48:12	64.2	67.5	69.8	69.5	68.1
1566	2020-10-02	14:48:17	65.9	65.4	62.1	58.9	56.9
1571	2020-10-02	14:48:22	55.5	55.5	56.1	57.1	58.6
1576	2020-10-02	14:48:27	59.8	60.5	62.7	65.6	69.9
1581	2020-10-02	14:48:32	73.2	73.3	71.7	71.4	71.5
1586	2020-10-02	14:48:37	70.9	69.7	69.1	70.3	71.1
1591	2020-10-02	14:48:42	70.8	70.2	70.0	69.8	68.5
1596	2020-10-02	14:48:47	67.6	67.8	67.5	66.1	68.3
1601	2020-10-02	14:48:52	70.4	70.9	70.9	70.7	71.6
1606	2020-10-02	14:48:57	71.8	70.3	68.1	66.5	66.3
1611	2020-10-02	14:49:02	68.6	69.5	68.5	66.3	63.8
1616	2020-10-02	14:49:07	62.5	63.5	66.4	69.0	70.3
1621	2020-10-02	14:49:12	70.9	70.5	69.5	67.8	65.4
1626	2020-10-02	14:49:17	63.9	64.6	67.2	67.8	67.8
1631	2020-10-02	14:49:22	67.1	67.3	67.6	65.5	63.3
1636	2020-10-02	14:49:27	62.4	62.8	65.6	69.5	75.3
1641	2020-10-02	14:49:32	77.8	78.0	76.4	73.9	71.7
1646	2020-10-02	14:49:37	69.2	67.1	66.5	67.7	67.5
1651	2020-10-02	14:49:42	65.5	63.9	62.1	60.9	61.5
1656	2020-10-02	14:49:47	61.6	61.0	60.1	59.2	58.8
1661	2020-10-02	14:49:52	59.2	61.8	65.7	66.5	64.9
1666	2020-10-02	14:49:57	62.6	61.3	64.9	68.6	69.1
1671	2020-10-02	14:50:02	68.3	67.7	66.4	64.0	61.4
1676	2020-10-02	14:50:07	58.9	57.4	55.6	54.2	53.7
1681	2020-10-02	14:50:12	53.5	54.1	56.0	59.0	61.6
1686	2020-10-02	14:50:17	65.4	67.2	69.4	69.6	69.2
1691	2020-10-02	14:50:22	69.8	70.5	69.5	68.5	67.2
1696	2020-10-02	14:50:27	68.0	70.2	70.5	69.4	67.3
1701	2020-10-02	14:50:32	65.2	62.7	62.6	64.9	67.6
1706	2020-10-02	14:50:37	68.1	69.2	69.5	69.3	68.1
1711	2020-10-02	14:50:42	65.7	63.5	61.6	59.8	58.3
1716	2020-10-02	14:50:47	56.5	55.2	54.5	56.6	59.2
1721	2020-10-02	14:50:52	60.2	63.3	65.0	64.2	62.1
1726	2020-10-02	14:50:57	59.4	57.2	55.4	53.9	53.2
1731	2020-10-02	14:51:02	52.8	53.8	54.4	53.8	53.5
1736	2020-10-02	14:51:07	54.8	54.1	53.9	54.8	55.0
1741	2020-10-02	14:51:12	57.2	57.0	58.0	59.8	64.2
1746	2020-10-02	14:51:17	68.0	70.1	70.8	69.5	68.2
1751	2020-10-02	14:51:22	68.6	68.5	68.4	68.3	69.4
1756	2020-10-02	14:51:27	68.3	68.0	68.2	69.5	69.5
1761	2020-10-02	14:51:32	68.6	69.7	70.0	69.7	68.4
1766	2020-10-02	14:51:37	68.4	68.7	68.0	67.9	68.7
1771	2020-10-02	14:51:42	69.9	70.9	70.0	66.9	64.1
1776	2020-10-02	14:51:47	62.6	61.4	61.4	60.5	59.6
1781	2020-10-02	14:51:52	58.1	56.8	55.8	55.7	58.4
1786	2020-10-02	14:51:57	61.5	65.3	67.7	66.9	64.9
1791	2020-10-02	14:52:02	63.5	64.0	64.9	65.3	64.4
1796	2020-10-02	14:52:07	62.8	63.4	65.4	65.4	63.8
1801	2020-10-02	14:52:12	61.5	59.4	58.3	58.5	58.5
1806	2020-10-02	14:52:17	57.3	56.4	55.3	54.3	54.0
1811	2020-10-02	14:52:22	53.9	55.5	55.8	58.9	60.9
1816	2020-10-02	14:52:27	61.5	61.5	64.8	68.6	69.5
1821	2020-10-02	14:52:32	68.7	68.0	69.4	70.2	71.9
1826	2020-10-02	14:52:37	72.4	72.3	72.3	71.6	70.6
1831	2020-10-02	14:52:42	69.1	68.9	69.7	68.2	68.2
1836	2020-10-02	14:52:47	68.9	69.5	70.3	70.8	70.6
1841	2020-10-02	14:52:52	69.7	69.3	69.0	67.1	65.3
1846	2020-10-02	14:52:57	63.9	62.9	61.6	62.0	65.5
1851	2020-10-02	14:53:02	68.0	68.6	69.0	69.4	69.3
1856	2020-10-02	14:53:07	68.2	66.1	63.8	61.4	59.9
1861	2020-10-02	14:53:12	59.1	61.0	63.2	66.8	69.3
1866	2020-10-02	14:53:17	68.7	65.9	62.8	60.2	58.4
1871	2020-10-02	14:53:22	57.2	58.4	58.2	57.5	58.6
1876	2020-10-02	14:53:27	59.6	61.6	65.8	66.8	65.4
1881	2020-10-02	14:53:32	63.4	59.3	59.3	59.3	58.2
1886	2020-10-02	14:53:37	58.7	59.3	59.3	59.3	58.2



1891	2020-10-02	14:53:42	58.8	60.0	60.1	59.8	61.9
1896	2020-10-02	14:53:47	65.2	68.7	71.2	71.5	70.7
1901	2020-10-02	14:53:52	69.6	69.0	68.7	67.8	65.6
1906	2020-10-02	14:53:57	63.7	61.5	59.4	59.4	59.8
1911	2020-10-02	14:54:02	59.7	61.3	65.7	68.3	69.0
1916	2020-10-02	14:54:07	67.5	65.8	65.0	62.3	59.3
1921	2020-10-02	14:54:12	57.5	56.6	55.5	55.5	55.5
1926	2020-10-02	14:54:17	55.5	56.4	57.9	62.1	65.6
1931	2020-10-02	14:54:22	67.7	70.6	72.3	71.1	67.7
1936	2020-10-02	14:54:27	64.6	61.9	59.1	57.5	55.5
1941	2020-10-02	14:54:32	54.5	53.9	55.2	56.7	57.4
1946	2020-10-02	14:54:37	60.4	63.4	69.1	70.3	68.3
1951	2020-10-02	14:54:42	65.9	66.3	69.7	72.3	72.7
1956	2020-10-02	14:54:47	73.7	74.6	73.2	71.8	69.9
1961	2020-10-02	14:54:52	69.3	70.0	70.8	70.5	68.9
1966	2020-10-02	14:54:57	67.0	65.6	65.1	63.1	62.5
1971	2020-10-02	14:55:02	60.5	59.1	58.3	57.1	56.0
1976	2020-10-02	14:55:07	56.1	55.2	54.8	55.0	55.0
1981	2020-10-02	14:55:12	55.1	56.4	60.5	64.9	67.7
1986	2020-10-02	14:55:17	68.4	68.7	67.3	64.9	64.0
1991	2020-10-02	14:55:22	65.2	68.9	70.3	70.2	69.9
1996	2020-10-02	14:55:27	69.1	67.3	67.0	68.0	67.5
2001	2020-10-02	14:55:32	65.3	62.7	60.2	58.7	58.0
2006	2020-10-02	14:55:37	56.8	56.4	57.9	59.7	60.6
2011	2020-10-02	14:55:42	63.2	68.7	70.2	70.1	69.7
2016	2020-10-02	14:55:47	67.8	65.3	63.5	63.0	63.4
2021	2020-10-02	14:55:52	60.3	57.9	56.0	55.0	54.2
2026	2020-10-02	14:55:57	52.8	52.4	52.3	52.7	53.2
2031	2020-10-02	14:56:02	53.2	52.8	53.7	54.8	56.0
2036	2020-10-02	14:56:07	55.7	55.7	56.8	57.2	58.9
2041	2020-10-02	14:56:12	59.9	61.0	65.0	68.0	68.8
2046	2020-10-02	14:56:17	70.3	72.3	74.6	76.8	76.7
2051	2020-10-02	14:56:22	76.5	73.2	69.8	66.4	68.3
2056	2020-10-02	14:56:27	67.7	67.1	66.1	65.0	64.0
2061	2020-10-02	14:56:32	61.9	60.0	59.2	57.5	55.8
2066	2020-10-02	14:56:37	55.6	56.4	57.4	59.9	66.0
2071	2020-10-02	14:56:42	68.6	69.4	67.5	65.2	62.9
2076	2020-10-02	14:56:47	62.1	62.6	63.0	61.3	59.4
2081	2020-10-02	14:56:52	58.0	56.6	55.6	56.0	60.8
2086	2020-10-02	14:56:57	65.3	68.2	67.8	68.1	68.9
2091	2020-10-02	14:57:02	68.2	66.0	63.5	61.1	59.2
2096	2020-10-02	14:57:07	57.2	59.4	62.4	65.4	66.3
2101	2020-10-02	14:57:12	64.6	61.8	59.3	57.9	58.3
2106	2020-10-02	14:57:17	58.3	60.9	65.2	67.6	68.0
2111	2020-10-02	14:57:22	68.6	69.5	71.2	72.2	71.6
2116	2020-10-02	14:57:27	70.8	70.0	68.3	66.0	63.8
2121	2020-10-02	14:57:32	62.8	62.4	60.9	59.0	58.6
2126	2020-10-02	14:57:37	57.8	59.4	62.2	63.2	62.1
2131	2020-10-02	14:57:42	64.2	66.8	68.2	67.6	66.4
2136	2020-10-02	14:57:47	66.9	68.1	66.9	65.9	66.2
2141	2020-10-02	14:57:52	66.5	67.0	67.7	67.8	67.7
2146	2020-10-02	14:57:57	66.7	65.7	66.0	65.9	65.8
2151	2020-10-02	14:58:02	65.5	65.8	67.1	67.8	67.2
2156	2020-10-02	14:58:07	65.5	66.2	67.2	67.0	65.5
2161	2020-10-02	14:58:12	63.2	61.9	63.3	66.2	67.6
2166	2020-10-02	14:58:17	68.6	69.1	69.3	69.8	70.0
2171	2020-10-02	14:58:22	69.7	68.7	67.5	68.4	68.3
2176	2020-10-02	14:58:27	67.2	65.0	62.9	61.5	59.7
2181	2020-10-02	14:58:32	58.6	60.1	59.4	57.5	56.6
2186	2020-10-02	14:58:37	55.5	55.3	54.6	54.1	53.7
2191	2020-10-02	14:58:42	53.3	52.9	53.6	54.6	53.8
2196	2020-10-02	14:58:47	54.8	55.3	58.7	60.0	61.3
2201	2020-10-02	14:58:52	62.5	66.4	68.8	69.5	70.0
2206	2020-10-02	14:58:57	70.4	70.4	68.9	66.6	65.1
2211	2020-10-02	14:59:02	63.8	63.7	63.0	61.0	62.3
2216	2020-10-02	14:59:07	61.2	58.8	58.7	59.4	60.8
2221	2020-10-02	14:59:12	63.2	67.2	70.9	71.2	70.0
2226	2020-10-02	14:59:17	67.8	65.4	66.0	68.2	67.3
2231	2020-10-02	14:59:22	64.9	62.4	60.3	61.1	63.5
2236	2020-10-02	14:59:27	67.5	68.3	68.6	69.8	69.4
2241	2020-10-02	14:59:32	67.5	65.6	66.2	68.6	69.7
2246	2020-10-02	14:59:37	69.9	68.8	67.0	65.0	63.6
2251	2020-10-02	14:59:42	62.8	61.9	64.1	65.5	67.2
2256	2020-10-02	14:59:47	68.3	68.1	69.9	72.1	71.9
2261	2020-10-02	14:59:52	70.1	69.7	69.3	72.0	71.4
2266	2020-10-02	14:59:57	69.7	69.3	68.7	68.7	69.8
2271	2020-10-02	15:00:02	70.4	70.1	70.0	70.2	70.4
2276	2020-10-02	15:00:07	69.7	69.4	71.9	73.3	72.0
2281	2020-10-02	15:00:12	70.0	68.6	68.4	67.5	65.3
2286	2020-10-02	15:00:17	63.4	61.8	60.3	59.1	58.6
2291	2020-10-02	15:00:22	58.1	58.6	60.0	63.1	64.1
2296	2020-10-02	15:00:27	62.7	60.5	58.7	58.5	59.8
2301	2020-10-02	15:00:32	63.1	63.8	62.5	61.9	61.7
2306	2020-10-02	15:00:37	62.3	63.4	65.3	66.7	65.6
2311	2020-10-02	15:00:42	63.3	61.4	61.8	61.8	65.0
2316	2020-10-02	15:00:47	67.5	68.0	68.3	68.7	68.0
2321	2020-10-02	15:00:52	65.8	64.0	63.7	62.4	60.5
2326	2020-10-02	15:00:57	58.4	57.6	59.7	61.7	63.5
2331	2020-10-02	15:01:02	66.9	69.9	70.6	70.1	67.9
2336	2020-10-02	15:01:07	65.3	63.1	61.4	60.6	61.2
2341	2020-10-02	15:01:12	60.7	61.9	63.5	66.2	71.9
2346	2020-10-02	15:01:17	74.1	74.7	73.4	71.3	69.8
2351	2020-10-02	15:01:22	69.2	69.5	70.5	70.9	70.5
2356	2020-10-02	15:01:27	70.6	71.1	70.8	71.1	70.3
2361	2020-10-02	15:01:32	68.3	65.9	64.9	66.5	67.6
2366	2020-10-02	15:01:37	65.7	63.1	67.1	65.9	68.9
2371	2020-10-02	15:01:42	69.5	69.5	69.5	68.6	68.6
2376	2020-10-02	15:01:47	65.5	65.3	65.3	65.0	69.5

2381	2020-10-02	15:01:52	70.3	68.9	66.8	65.0	63.3
2386	2020-10-02	15:01:57	62.5	62.7	63.4	67.0	70.5
2391	2020-10-02	15:02:02	72.2	72.1	71.1	69.9	69.9
2396	2020-10-02	15:02:07	70.0	70.4	70.0	70.1	71.0
2401	2020-10-02	15:02:12	74.8	79.5	77.2	75.0	73.3
2406	2020-10-02	15:02:17	70.7	69.6	70.1	69.9	70.5
2411	2020-10-02	15:02:22	70.8	70.9	71.3	70.8	69.3
2416	2020-10-02	15:02:27	68.6	68.5	68.3	68.2	68.2
2421	2020-10-02	15:02:32	68.7	69.6	69.3	68.6	67.4
2426	2020-10-02	15:02:37	65.9	64.2	62.2	59.9	58.5
2431	2020-10-02	15:02:42	57.2	56.6	55.8	55.3	54.9
2436	2020-10-02	15:02:47	55.1	54.5	54.8	54.2	52.9
2441	2020-10-02	15:02:52	52.0	51.3	51.3	50.8	50.4
2446	2020-10-02	15:02:57	50.0	50.1	51.2	51.9	51.8
2451	2020-10-02	15:03:02	51.7	51.7	51.8	51.4	51.2
2456	2020-10-02	15:03:07	51.5	51.9	51.8	51.9	51.8
2461	2020-10-02	15:03:12	52.8	54.6	56.3	58.8	62.2
2466	2020-10-02	15:03:17	64.4	63.1	61.8	61.6	63.6
2471	2020-10-02	15:03:22	66.6	67.3	68.2	68.4	67.2
2476	2020-10-02	15:03:27	64.7	62.7	64.0	66.3	70.0
2481	2020-10-02	15:03:32	70.7	70.2	70.8	71.6	71.1
2486	2020-10-02	15:03:37	70.2	69.6	70.6	72.4	71.9
2491	2020-10-02	15:03:42	69.9	70.3	69.7	68.3	66.1
2496	2020-10-02	15:03:47	64.1	61.9	60.1	58.8	62.1
2501	2020-10-02	15:03:52	64.0	67.2	68.3	65.9	63.3
2506	2020-10-02	15:03:57	60.9	58.8	58.6	58.8	59.3
2511	2020-10-02	15:04:02	62.3	66.8	68.9	70.3	70.4
2516	2020-10-02	15:04:07	69.4	67.6	64.7	62.5	61.7
2521	2020-10-02	15:04:12	64.2	66.6	68.0	69.0	70.4
2526	2020-10-02	15:04:17	71.9	72.0	70.7	69.7	70.6
2531	2020-10-02	15:04:22	71.0	71.6	72.1	71.5	71.3
2536	2020-10-02	15:04:27	72.5	73.4	72.1	71.0	69.6
2541	2020-10-02	15:04:32	67.8	66.5	67.7	69.4	69.2
2546	2020-10-02	15:04:37	68.8	67.5	65.8	64.1	62.4
2551	2020-10-02	15:04:42	62.7	63.2	63.9	67.1	69.8
2556	2020-10-02	15:04:47	69.5	68.4	66.6	66.4	64.8
2561	2020-10-02	15:04:52	65.2	66.8	67.5	65.7	63.2
2566	2020-10-02	15:04:57	61.0	59.2	57.6	56.1	55.8
2571	2020-10-02	15:05:02	55.3	56.4	58.8	63.0	65.6
2576	2020-10-02	15:05:07	64.9	63.0	62.0	64.2	66.8
2581	2020-10-02	15:05:12	67.2	66.3	64.4	62.2	60.6
2586	2020-10-02	15:05:17	59.3	60.3	63.3	66.6	67.1
2591	2020-10-02	15:05:22	65.4	62.5	60.5	58.5	56.7
2596	2020-10-02	15:05:27	55.5	55.1	55.1	56.2	56.7
2601	2020-10-02	15:05:32	57.5	58.3	59.5	61.3	64.6
2606	2020-10-02	15:05:37	67.4	69.1	68.4	68.1	66.3
2611	2020-10-02	15:05:42	67.4	70.9	70.6	68.2	65.6
2616	2020-10-02	15:05:47	62.7	60.1	59.8	59.3	57.6
2621	2020-10-02	15:05:52	57.2	57.4	63.7	63.1	61.7
2626	2020-10-02	15:05:57	62.2	64.4	67.6	67.7	66.7
2631	2020-10-02	15:06:02	66.3	68.6	69.2	69.0	67.8
2636	2020-10-02	15:06:07	66.8	64.6	62.7	60.0	57.6
2641	2020-10-02	15:06:12	55.9	55.7	55.1	54.9	55.0
2646	2020-10-02	15:06:17	56.3	57.0	57.9	58.8	59.8
2651	2020-10-02	15:06:22	62.7	67.2	68.9	69.3	67.9
2656	2020-10-02	15:06:27	65.5	63.8	63.2	64.2	67.6
2661	2020-10-02	15:06:32	69.2	67.8	65.8	64.9	67.3
2666	2020-10-02	15:06:37	68.7	68.8	67.6	65.7	63.7
2671	2020-10-02	15:06:42	62.7	62.3	65.4	67.1	68.1
2676	2020-10-02	15:06:47	68.0	66.9	65.5	67.5	68.0
2681	2020-10-02	15:06:52	66.2	66.1	65.9	66.2	64.7
2686	2020-10-02	15:06:57	63.1	62.5	61.1	58.8	57.6
2691	2020-10-02	15:07:02	57.7	57.8	59.1	60.5	64.9
2696	2020-10-02	15:07:07	67.8	67.7	65.3	63.5	61.0
2701	2020-10-02	15:07:12	59.1	56.8	55.5	56.1	55.5
2706	2020-10-02	15:07:17	55.3	56.3	57.6	60.9	65.6
2711	2020-10-02	15:07:22	67.2	67.1	65.3	62.4	60.8
2716	2020-10-02	15:07:27	59.4	58.0	56.2	54.9	54.6
2721	2020-10-02	15:07:32	54.9	54.8	56.0	57.4	60.8
2726	2020-10-02	15:07:37	64.3	66.2	66.2	64.2	62.2
2731	2020-10-02	15:07:42	61.3	60.1	61.3	64.1	64.4
2736	2020-10-02	15:07:47	63.5	61.8	60.4	60.0	60.7
2741	2020-10-02	15:07:52	62.7	64.3	65.2	65.8	67.1
2746	2020-10-02	15:07:57	67.6	67.4	67.8	68.1	67.1
2751	2020-10-02	15:08:02	65.2	63.5	63.4	64.0	66.8
2756	2020-10-02	15:08:07	69.7	71.1	70.6	70.7	70.3
2761	2020-10-02	15:08:12	70.1	70.6	69.9	69.0	68.5
2766	2020-10-02	15:08:17	69.7	70.2	70.1	69.5	69.6
2771	2020-10-02	15:08:22	69.8	68.7	67.7	65.9	64.5
2776	2020-10-02	15:08:27	64.8	64.1	63.4	63.7	67.5
2781	2020-10-02	15:08:32	70.3	70.5	69.1	66.4	63.1
2786	2020-10-02	15:08:37	60.1	57.5	55.8	55.1	53.9
2791	2020-10-02	15:08:42	53.6	54.1	55.8	60.8	66.6
2796	2020-10-02	15:08:47	70.3	72.0	72.0	71.6	71.3
2801	2020-10-02	15:08:52	70.2	69.3	69.5	69.5	68.6
2806	2020-10-02	15:08:57	66.7	64.8	64.1	64.4	66.3
2811	2020-10-02	15:09:02	68.6	69.4	68.7	68.7	69.2
2816	2020-10-02	15:09:07	70.9	71.2	70.9	70.9	70.6
2821	2020-10-02	15:09:12	68.7	68.7	69.3	69.7	70.0
2826	2020-10-02	15:09:17	69.8	68.9	68.6	70.2	72.3
2831	2020-10-02	15:09:22	73.3	71.4	70.0	70.3	70.1
2836	2020-10-02	15:09:27	70.6	70.2	70.3	71.3	72.3
2841	2020-10-02	15:09:32	72.7	73.6	73.6	73.3	72.5
2846	2020-10-02	15:09:37	71.5	69.9	68.5	66.5	64.9
2851	2020-10-02	15:09:42	63.3	61.4	60.1	59.4	58.4
2856	2020-10-02	15:09:47	57.5	56.6	56.6	56.6	61.0
2861	2020-10-02	15:09:52	55.5	56.6	70.0	70.4	69.6
2866	2020-10-02	15:09:57	61.7	66.9	70.0	70.4	69.6



2871	2020-10-02	15:10:02	69.8	68.9	66.3	63.4	60.5
2876	2020-10-02	15:10:07	58.5	57.6	57.3	56.8	57.7
2881	2020-10-02	15:10:12	58.4	59.0	62.4	66.0	68.5
2886	2020-10-02	15:10:17	68.7	67.9	67.0	69.3	69.4
2891	2020-10-02	15:10:22	68.1	65.9	63.5	60.7	58.9
2896	2020-10-02	15:10:27	58.6	59.8	62.0	65.1	65.9
2901	2020-10-02	15:10:32	64.7	62.9	60.6	58.9	59.2
2906	2020-10-02	15:10:37	60.1	63.2	66.2	67.1	66.7
2911	2020-10-02	15:10:42	66.4	68.8	73.0	73.9	73.4
2916	2020-10-02	15:10:47	71.2	69.8	68.4	67.8	67.3
2921	2020-10-02	15:10:52	69.8	70.3	70.5	69.6	69.1
2926	2020-10-02	15:10:57	67.7	66.6	66.6	66.7	66.4
2931	2020-10-02	15:11:02	65.1	64.3	64.5	66.3	67.0
2936	2020-10-02	15:11:07	67.3	68.1	68.3	68.5	67.5
2941	2020-10-02	15:11:12	66.4	66.2	67.2	68.5	69.0
2946	2020-10-02	15:11:17	68.9	69.6	70.7	73.1	73.6
2951	2020-10-02	15:11:22	73.8	73.5	73.3	73.9	74.0
2956	2020-10-02	15:11:27	72.9	71.7	70.9	70.4	70.2
2961	2020-10-02	15:11:32	70.3	70.5	70.3	69.7	68.8
2966	2020-10-02	15:11:37	68.1	67.8	67.9	67.6	67.3
2971	2020-10-02	15:11:42	67.4	68.1	68.8	68.9	68.1
2976	2020-10-02	15:11:47	66.9	65.9	64.9	64.5	64.0
2981	2020-10-02	15:11:52	63.9	63.0	63.6	65.8	66.6
2986	2020-10-02	15:11:57	65.9	63.9	62.9	61.8	60.5
2991	2020-10-02	15:12:02	60.8	63.6	67.2	68.1	66.8
2996	2020-10-02	15:12:07	64.4	63.2	63.8	66.6	69.9
3001	2020-10-02	15:12:12	70.0	68.4	65.6	64.0	62.9
3006	2020-10-02	15:12:17	61.9	61.5	59.9	59.3	58.6
3011	2020-10-02	15:12:22	57.5	57.1	58.4	62.5	65.9
3016	2020-10-02	15:12:27	67.6	68.7	68.6	69.4	71.6
3021	2020-10-02	15:12:32	73.3	73.5	72.2	69.8	67.1
3026	2020-10-02	15:12:37	64.5	62.7	63.4	63.6	65.1
3031	2020-10-02	15:12:42	68.2	70.1	71.4	70.4	70.5
3036	2020-10-02	15:12:47	69.7	70.5	71.2	71.3	71.1
3041	2020-10-02	15:12:52	69.2	67.0	66.0	66.4	68.3
3046	2020-10-02	15:12:57	69.2	69.6	71.1	70.5	68.9
3051	2020-10-02	15:13:02	67.7	67.4	69.5	69.6	67.7
3056	2020-10-02	15:13:07	65.2	64.7	63.8	63.9	66.9
3061	2020-10-02	15:13:12	70.7	70.4	69.8	71.0	70.6
3066	2020-10-02	15:13:17	71.3	71.5	70.6	68.7	67.5
3071	2020-10-02	15:13:22	67.4	67.5	65.5	65.8	65.9
3076	2020-10-02	15:13:27	65.7	65.0	63.1	61.0	59.1
3081	2020-10-02	15:13:32	56.5	54.5	53.5	53.9	56.4
3086	2020-10-02	15:13:37	58.8	62.8	67.9	67.9	67.2
3091	2020-10-02	15:13:42	65.4	62.6	59.9	58.0	57.2
3096	2020-10-02	15:13:47	59.1	64.6	66.4	64.7	62.1
3101	2020-10-02	15:13:52	60.2	60.5	61.2	65.4	66.4
3106	2020-10-02	15:13:57	64.1	62.3	62.5	63.5	65.4
3111	2020-10-02	15:14:02	66.5	66.7	66.5	65.6	64.5
3116	2020-10-02	15:14:07	62.8	61.1	59.6	59.3	60.8
3121	2020-10-02	15:14:12	64.9	68.1	70.3	70.4	70.0
3126	2020-10-02	15:14:17	69.8	69.2	68.2	66.9	66.3
3131	2020-10-02	15:14:22	65.3	66.8	68.3	68.2	66.8
3136	2020-10-02	15:14:27	64.6	63.5	62.4	62.8	66.0
3141	2020-10-02	15:14:32	68.7	69.5	67.5	65.0	63.6
3146	2020-10-02	15:14:37	61.7	61.3	62.4	64.6	68.5
3151	2020-10-02	15:14:42	71.0	70.3	67.8	65.7	64.1
3156	2020-10-02	15:14:47	62.9	61.0	60.0	60.1	61.6
3161	2020-10-02	15:14:52	65.2	65.9	64.6	64.8	66.8
3166	2020-10-02	15:14:57	67.3	66.5	67.6	67.8	66.8
3171	2020-10-02	15:15:02	65.2	63.2	61.2	59.7	58.4
3176	2020-10-02	15:15:07	59.5	60.9	65.3	67.0	68.6
3181	2020-10-02	15:15:12	68.6	66.9	65.6	66.1	67.1
3186	2020-10-02	15:15:17	66.0	65.9	68.0	68.0	66.3
3191	2020-10-02	15:15:22	63.9	61.9	60.6	59.0	57.9
3196	2020-10-02	15:15:27	57.3	56.9	56.0	55.3	56.0
3201	2020-10-02	15:15:32	59.7	63.8	68.1	68.5	68.7
3206	2020-10-02	15:15:37	70.6	71.5	72.0	72.2	71.5
3211	2020-10-02	15:15:42	69.3	67.3	65.5	66.4	69.4
3216	2020-10-02	15:15:47	70.6	70.0	69.3	68.9	68.8
3221	2020-10-02	15:15:52	69.9	68.8	67.3	67.5	69.0
3226	2020-10-02	15:15:57	70.0	69.9	68.5	66.2	64.1
3231	2020-10-02	15:16:02	61.9	60.2	59.1	61.0	62.4
3236	2020-10-02	15:16:07	65.5	68.3	68.5	67.7	69.3
3241	2020-10-02	15:16:12	70.4	70.6	71.6	72.0	71.4
3246	2020-10-02	15:16:17	70.3	70.5	70.1	70.0	69.4
3251	2020-10-02	15:16:22	69.7	69.5	70.7	71.8	72.7
3256	2020-10-02	15:16:27	73.8	73.6	72.3	70.1	67.5
3261	2020-10-02	15:16:32	65.6	64.0	64.3	66.9	68.0
3266	2020-10-02	15:16:37	67.9	68.4	69.8	70.0	69.8
3271	2020-10-02	15:16:42	68.6	67.2	65.0	62.4	60.4
3276	2020-10-02	15:16:47	59.2	60.4	61.3	65.1	67.5
3281	2020-10-02	15:16:52	68.0	66.4	66.3	68.0	69.0
3286	2020-10-02	15:16:57	70.2	70.1	69.4	68.8	68.0
3291	2020-10-02	15:17:02	67.2	64.8	62.2	61.2	60.8
3296	2020-10-02	15:17:07	62.5	64.7	65.6	67.6	68.8
3301	2020-10-02	15:17:12	67.9	67.0	69.5	70.4	71.0
3306	2020-10-02	15:17:17	70.6	70.2	69.2	67.2	65.8
3311	2020-10-02	15:17:22	63.8	62.0	60.9	63.4	65.8
3316	2020-10-02	15:17:27	71.3	69.7	67.2	64.3	61.3
3321	2020-10-02	15:17:32	58.7	57.7	60.1	59.9	61.3
3326	2020-10-02	15:17:37	62.4	61.2	60.5	61.2	61.8
3331	2020-10-02	15:17:42	65.1	66.6	68.0	70.7	71.7
3336	2020-10-02	15:17:47	70.9	69.0	67.1	65.2	64.2
3341	2020-10-02	15:17:52	64.8	66.7	69.3	70.3	70.8
3346	2020-10-02	15:17:57	69.9	68.9	69.9	67.8	67.8
3351	2020-10-02	15:18:02	66.8	66.4	66.5	69.2	59.2
3356	2020-10-02	15:18:07	56.8	55.2	54.4	53.4	53.5

3361	2020-10-02	15:18:12	54.1	55.1	59.2	62.7	68.6
3366	2020-10-02	15:18:17	69.4	68.4	65.9	64.1	61.8
3371	2020-10-02	15:18:22	59.3	56.6	54.5	53.3	52.7
3376	2020-10-02	15:18:27	53.3	57.3	61.3	65.9	69.4
3381	2020-10-02	15:18:32	68.9	70.5	71.7	72.6	71.8
3386	2020-10-02	15:18:37	70.1	69.5	69.6	69.1	68.7
3391	2020-10-02	15:18:42	67.9	66.1	63.6	62.8	64.7
3396	2020-10-02	15:18:47	67.9	69.8	71.3	72.3	73.4
3401	2020-10-02	15:18:52	73.2	72.9	71.7	70.6	69.7
3406	2020-10-02	15:18:57	69.5	68.8	69.1	70.7	71.4
3411	2020-10-02	15:19:02	73.2	74.5	74.1	72.9	71.6
3416	2020-10-02	15:19:07	69.4	67.6	66.3	68.3	70.1
3421	2020-10-02	15:19:12	69.6	70.0	70.9	70.7	70.5
3426	2020-10-02	15:19:17	70.1	69.4	68.6	67.7	66.8
3431	2020-10-02	15:19:22	66.9	68.2	68.2	67.3	65.5
3436	2020-10-02	15:19:27	63.9	61.9	60.5	59.0	59.9
3441	2020-10-02	15:19:32	58.5	58.8	60.9	66.2	69.6
3446	2020-10-02	15:19:37	69.9	69.1	66.8	64.6	62.9
3451	2020-10-02	15:19:42	63.4	66.7	68.1	67.5	68.4
3456	2020-10-02	15:19:47	69.8	71.4	71.2	70.3	69.3
3461	2020-10-02	15:19:52	69.2	71.1	72.3	72.4	72.4
3466	2020-10-02	15:19:57	71.7	70.4	68.6	67.4	65.7
3471	2020-10-02	15:20:02	63.7	63.4	64.4	66.0	66.9
3476	2020-10-02	15:20:07	66.9	66.3	66.5	67.2	66.8
3481	2020-10-02	15:20:12	66.4	68.6	69.7	69.3	67.9
3486	2020-10-02	15:20:17	65.9	66.1	67.8	67.7	66.1
3491	2020-10-02	15:20:22	65.6	66.8	67.7	68.4	69.6
3496	2020-10-02	15:20:27	70.3	69.9	67.7	65.3	63.2
3501	2020-10-02	15:20:32	61.3	58.6	57.9	58.8	62.6
3506	2020-10-02	15:20:37	65.7	66.2	67.2	68.4	67.9
3511	2020-10-02	15:20:42	65.6	62.8	61.4	60.8	62.8
3516	2020-10-02	15:20:47	66.4	70.6	70.6	69.4	68.6
3521	2020-10-02	15:20:52	69.4	69.7	68.4	67.9	69.0
3526	2020-10-02	15:20:57	69.5	69.8	69.0	67.7	67.1
3531	2020-10-02	15:21:02	66.4	64.5	62.2	60.5	61.5
3536	2020-10-02	15:21:07	64.8	68.9	71.2	71.6	71.0
3541	2020-10-02	15:21:12	69.2	67.0	64.8	63.1	61.6
3546	2020-10-02	15:21:17	60.6	61.6	63.5	63.7	66.5
3551	2020-10-02	15:21:22	69.3	70.3	68.7	66.5	64.3
3556	2020-10-02	15:21:27	61.8	59.9	59.2	60.1	60.7
3561	2020-10-02	15:21:32	62.1	63.4	63.4	61.7	59.6
3566	2020-10-02	15:21:37	58.3	57.1	56.9	59.7	62.6
3571	2020-10-02	15:21:42	65.7	68.2	69.2	68.4	66.2
3576	2020-10-02	15:21:47	63.4	61.0	60.1	60.1	63.0
3581	2020-10-02	15:21:52	67.0	67.5	65.4	62.7	61.0
3586	2020-10-02	15:21:57	60.2	59.4	58.8	58.4	59.3
3591	2020-10-02	15:22:02	61.8	65.2	67.3	69.0	70.5
3596	2020-10-02	15:22:07	70.8	70.0	69.9	71.0	71.5
3601	2020-10-02	15:22:12	71.4	71.1	69.2	67.0	64.6
3606	2020-10-02	15:22:17	62.6	62.8	64.6	66.0	65.5
3611	2020-10-02	15:22:22	63.7	62.8	61.4	60.4	59.8
3616	2020-10-02	15:22:27	60.8	63.7	68.2	71.0	71.9
3621	2020-10-02	15:22:32	71.5	70.1	68.1	66.3	64.6
3626	2020-10-02	15:22:37	62.1	61.0	59.3	57.4	56.0
3631	2020-10-02	15:22:42	54.8	55.0	56.3	58.0*	60.3*
3636	2020-10-02	15:22:47	62.5*	66.1*	69.5*	71.2*	70.0*
3641	2020-10-02	15:22:52	68.0*	65.7*	66.1	68.1	

Freq Weight : A  
 Time Weight : SLOW  
 Level Range : 40-100  
 Max dB : 80.1 - 2020/10/02 14:11:58  
 Level Range : 40-100  
 SEL : 105.6  
 Leq : 70.0

No.s	Date Time	(dB)					
1	2020-10-02 13:19:19	65.9	65.7	68.3	74.0	73.9	
6	2020-10-02 13:19:24	74.4	72.7	71.6	69.8	69.3	
11	2020-10-02 13:19:29	68.6	67.2	66.1	65.2	63.9	
16	2020-10-02 13:19:34	62.9	62.7	62.8	62.3	60.7	
21	2020-10-02 13:19:39	59.7	59.6	62.0	62.5	63.8	
26	2020-10-02 13:19:44	65.6	69.8	68.1	66.8	66.2	
31	2020-10-02 13:19:49	66.2	64.9	65.4	64.1	62.3	
36	2020-10-02 13:19:54	61.9	62.6	60.6	58.5	58.1	
41	2020-10-02 13:19:59	58.6	58.8	58.3	57.4	56.9	
46	2020-10-02 13:20:04	55.8	55.4	55.1	56.2	56.1	
51	2020-10-02 13:20:09	55.4	55.2	55.2	55.2	55.2	
56	2020-10-02 13:20:14	55.3	55.3	56.6	57.9	58.2	
61	2020-10-02 13:20:19	62.3	65.1	68.2	68.5	68.8	
66	2020-10-02 13:20:24	70.3	70.1	69.8	70.7	71.0	
71	2020-10-02 13:20:29	70.0	69.9	67.9	65.5	63.1	
76	2020-10-02 13:20:34	60.9	59.1	58.4	57.7	58.1	
81	2020-10-02 13:20:39	59.1	59.2	60.1	62.3	65.5	
86	2020-10-02 13:20:44	69.3	69.3	68.1	65.7	63.7	
91	2020-10-02 13:20:49	62.4	61.8	60.8	62.6	67.6	
96	2020-10-02 13:20:54	68.8	70.7	68.5	67.2	67.7	
101	2020-10-02 13:20:59	69.5	68.1	66.9	67.3	65.4	
106	2020-10-02 13:21:04	64.6	64.7	65.9	67.7	69.4	
111	2020-10-02 13:21:09	69.8	69.4	67.7	66.9	66.8	
116	2020-10-02 13:21:14	66.1	66.5	66.6	65.4	65.9	
121	2020-10-02 13:21:19	64.8	64.4	66.3	68.3	68.3	
126	2020-10-02 13:21:24	67.7	67.1	69.1	69.5	69.6	
131	2020-10-02 13:21:29	69.1	70.3	69.6	69.5	69.5	
136	2020-10-02 13:21:34	68.0	66.1	64.3	64.1	64.3	
141	2020-10-02 13:21:39	66.4	68.5	66.7	67.0	67.2	
146	2020-10-02 13:21:44	67.8	66.8	66.9	67.5	67.5	
151	2020-10-02 13:21:49	65.9	63.3	61.5	60.5	59.4	
156	2020-10-02 13:21:54	60.6	59.0	58.3	57.2	57.3	
161	2020-10-02 13:21:59	61.7	63.4	61.8	58.9	63.7	
166	2020-10-02 13:22:04	60.3	58.1	58.2	61.8	64.1	
171	2020-10-02 13:22:09	69.1	68.5	66.0	62.9	60.3	
176	2020-10-02 13:22:14	57.8	55.8	55.1	58.0	56.5	
181	2020-10-02 13:22:19	55.8	54.5	57.3	56.8	59.7	
186	2020-10-02 13:22:24	62.1	64.2	66.3	63.8	61.4	
191	2020-10-02 13:22:29	59.3	58.2	56.3	55.7	54.8	
196	2020-10-02 13:22:34	55.2	55.3	55.0	54.6	54.6	
201	2020-10-02 13:22:39	54.9	55.0	56.4	56.6	57.7	
206	2020-10-02 13:22:44	57.3	57.9	59.1	59.9	62.4	
211	2020-10-02 13:22:49	67.4	68.8	68.0	66.9	65.3	
216	2020-10-02 13:22:54	66.7	67.6	69.3	69.4	68.3	
221	2020-10-02 13:22:59	70.0	70.6	73.7	72.3	70.5	
226	2020-10-02 13:23:04	73.9	72.3	70.4	67.9	67.7	
231	2020-10-02 13:23:09	70.5	70.3	69.4	69.3	70.3	
236	2020-10-02 13:23:14	68.8	67.8	67.8	70.2	69.1	
241	2020-10-02 13:23:19	67.6	66.8	69.1	68.5	66.9	
246	2020-10-02 13:23:24	66.9	66.4	64.8	63.3	62.7	
251	2020-10-02 13:23:29	62.5	63.5	64.5	63.1	62.3	
256	2020-10-02 13:23:34	62.2	62.0	65.3	65.6	65.1	
261	2020-10-02 13:23:39	66.3	66.6	65.0	64.9	67.2	
266	2020-10-02 13:23:44	67.4	68.0	68.9	68.1	66.4	
271	2020-10-02 13:23:49	64.4	62.7	63.0	65.5	68.5	
276	2020-10-02 13:23:54	68.2	66.3	63.9	61.9	60.2	
281	2020-10-02 13:23:59	59.9	60.2	61.0	63.5	66.3	
286	2020-10-02 13:24:04	66.5	64.8	63.1	61.0	59.5	
291	2020-10-02 13:24:09	59.5	59.5	62.6	65.7	66.7	
296	2020-10-02 13:24:14	68.2	66.0	63.6	61.4	60.0	
301	2020-10-02 13:24:19	60.8	64.2	63.3	65.8	67.6	
306	2020-10-02 13:24:24	67.1	65.7	62.7	60.5	59.3	
311	2020-10-02 13:24:29	61.0	63.4	67.5	68.9	69.7	
316	2020-10-02 13:24:34	68.2	66.4	64.4	62.3	60.2	
321	2020-10-02 13:24:39	58.8	58.1	57.9	60.5	62.9	
326	2020-10-02 13:24:44	65.8	65.7	64.4	62.8	60.8	
331	2020-10-02 13:24:49	58.8	57.5	56.9	56.6	57.5	
336	2020-10-02 13:24:54	59.4	62.4	67.4	70.9	73.5	
341	2020-10-02 13:24:59	75.1	73.7	70.9	69.6	69.9	
346	2020-10-02 13:25:04	69.8	69.7	69.7	69.8	69.6	
351	2020-10-02 13:25:09	70.3	69.3	69.4	69.1	68.1	
356	2020-10-02 13:25:14	65.7	64.3	63.4	64.1	66.3	
361	2020-10-02 13:25:19	66.9	68.4	66.8	64.2	62.7	
366	2020-10-02 13:25:24	62.0	62.4	61.5	62.7	64.1	
371	2020-10-02 13:25:29	65.5	65.3	62.7	59.8	57.9	
376	2020-10-02 13:25:34	56.7	55.2	54.2	53.9	56.3	
381	2020-10-02 13:25:39	55.5	54.5	54.4	54.9	55.8	
386	2020-10-02 13:25:44	58.2	60.8	63.4	68.0	68.5	
391	2020-10-02 13:25:49	67.3	65.5	65.2	67.9	68.0	
396	2020-10-02 13:25:54	67.5	68.2	68.7	67.3	65.4	
401	2020-10-02 13:25:59	65.0	66.7	70.3	71.2	70.7	
406	2020-10-02 13:26:04	70.5	68.0	69.9	71.2	70.7	
411	2020-10-02 13:26:09	66.6	65.5	69.9	71.2	70.7	
416	2020-10-02 13:26:14	63.2	61.0	59.1	58.1	56.8	

421	2020-10-02	13:26:19	56.0	55.1	54.6	54.0	53.7
426	2020-10-02	13:26:24	53.5	60.2	57.5	55.3	54.6
431	2020-10-02	13:26:29	55.0	56.4	59.2	61.2	63.2
436	2020-10-02	13:26:34	63.9	64.1	63.7	63.0	61.6
441	2020-10-02	13:26:39	60.8	60.4	59.7	60.0	62.0
446	2020-10-02	13:26:44	67.4	70.3	70.9	69.4	70.3
451	2020-10-02	13:26:49	70.3	71.0	71.1	72.4	73.0
456	2020-10-02	13:26:54	72.1	70.7	69.6	70.1	68.7
461	2020-10-02	13:26:59	68.6	70.8	70.9	69.2	66.7
466	2020-10-02	13:27:04	64.5	62.9	60.5	59.1	57.8
471	2020-10-02	13:27:09	56.5	56.0	55.4	54.9	55.9
476	2020-10-02	13:27:14	57.3	59.7	62.6	64.6	67.8
481	2020-10-02	13:27:19	68.2	69.2	67.4	65.0	63.6
486	2020-10-02	13:27:24	65.3	67.7	70.0	73.3	71.6
491	2020-10-02	13:27:29	68.3	65.0	62.2	59.5	57.8
496	2020-10-02	13:27:34	56.7	56.4	57.6	57.7	56.8
501	2020-10-02	13:27:39	55.3	55.2	57.5	66.3	62.9
506	2020-10-02	13:27:44	59.8	57.4	57.1	57.9	60.0
511	2020-10-02	13:27:49	64.2	64.3	63.7	64.7	65.0
516	2020-10-02	13:27:54	65.0	66.4	67.7	66.3	65.1
521	2020-10-02	13:27:59	66.2	66.3	65.2	65.2	66.9
526	2020-10-02	13:28:04	67.4	65.6	63.3	62.2	62.8
531	2020-10-02	13:28:09	60.6	61.4	64.3	65.4	64.9
536	2020-10-02	13:28:14	63.1	61.7	60.8	63.0	67.6
541	2020-10-02	13:28:19	69.5	69.3	69.2	70.0	67.1
546	2020-10-02	13:28:24	63.9	61.2	58.9	57.8	58.3
551	2020-10-02	13:28:29	57.5	58.2	59.9	63.1	65.1
556	2020-10-02	13:28:34	65.1	64.0	64.2	62.5	60.0
561	2020-10-02	13:28:39	57.9	56.8	56.2	55.8	57.2
566	2020-10-02	13:28:44	59.5	61.6	65.1	69.8	69.1
571	2020-10-02	13:28:49	67.4	66.6	65.5	67.5	67.1
576	2020-10-02	13:28:54	66.1	65.5	65.1	66.4	66.9
581	2020-10-02	13:28:59	68.7	68.0	68.5	69.4	68.0
586	2020-10-02	13:29:04	67.2	66.6	66.0	65.4	66.0
591	2020-10-02	13:29:09	67.4	67.1	68.4	68.4	68.3
596	2020-10-02	13:29:14	69.9	72.7	75.5	75.1	73.6
601	2020-10-02	13:29:19	71.7	69.8	69.0	66.7	65.0
606	2020-10-02	13:29:24	65.7	67.2	67.9	67.2	65.7
611	2020-10-02	13:29:29	64.5	63.8	63.2	62.4	61.6
616	2020-10-02	13:29:34	62.3	63.5	64.8	63.9	63.7
621	2020-10-02	13:29:39	66.2	73.2	75.6	74.2	71.4
626	2020-10-02	13:29:44	69.6	68.3	67.4	67.4	68.0
631	2020-10-02	13:29:49	73.5	70.7	69.1	67.4	66.0
636	2020-10-02	13:29:54	66.1	67.8	66.4	65.3	63.6
641	2020-10-02	13:29:59	63.0	61.2	59.4	59.6	59.4
646	2020-10-02	13:30:04	60.9	61.5	63.5	64.4	63.4
651	2020-10-02	13:30:09	62.3	61.2	60.4	60.8	60.7
656	2020-10-02	13:30:14	61.6	60.9	59.9	60.4	64.9
661	2020-10-02	13:30:19	69.2	70.5	71.6	71.1	70.4
666	2020-10-02	13:30:24	69.2	68.0	68.4	68.0	66.4
671	2020-10-02	13:30:29	65.0	63.7	62.3	61.1	61.8
676	2020-10-02	13:30:34	62.3	63.9	67.0	67.4	65.8
681	2020-10-02	13:30:39	63.5	61.5	63.3	64.4	65.9
686	2020-10-02	13:30:44	65.8	63.0	60.8	58.6	58.5
691	2020-10-02	13:30:49	59.5	59.9	60.7	63.6	63.6
696	2020-10-02	13:30:54	62.1	63.2	64.4	66.4	69.5
701	2020-10-02	13:30:59	69.6	68.9	66.8	67.2	66.6
706	2020-10-02	13:31:04	65.5	66.6	66.9	66.2	65.4
711	2020-10-02	13:31:09	64.1	63.6	65.4	63.0	60.6
716	2020-10-02	13:31:14	59.2	59.0	59.0	58.3	57.6
721	2020-10-02	13:31:19	57.6	57.3	57.8	59.4	62.4
726	2020-10-02	13:31:24	63.9	65.5	65.2	66.1	67.4
731	2020-10-02	13:31:29	68.7	70.0	68.0	65.8	64.3
736	2020-10-02	13:31:34	62.7	60.9	59.9	60.2	60.4
741	2020-10-02	13:31:39	60.1	62.0	64.5	67.0	68.0
746	2020-10-02	13:31:44	68.4	68.7	69.3	68.6	68.2
751	2020-10-02	13:31:49	68.1	68.0	67.5	68.7	69.9
756	2020-10-02	13:31:54	71.1	70.4	69.7	68.0	65.5
761	2020-10-02	13:31:59	63.5	62.6	62.5	61.4	59.5
766	2020-10-02	13:32:04	58.6	57.4	57.4	58.7	56.8
771	2020-10-02	13:32:09	56.3	55.7	56.2	58.4	60.6
776	2020-10-02	13:32:14	61.7	64.5	63.9	62.7	60.7
781	2020-10-02	13:32:19	60.1	59.4	60.3	60.6	62.4
786	2020-10-02	13:32:24	65.7	69.0	68.5	68.2	67.9
791	2020-10-02	13:32:29	69.3	68.4	67.3	65.4	64.0
796	2020-10-02	13:32:34	64.2	63.3	64.4	67.7	70.7
801	2020-10-02	13:32:39	74.0	74.9	73.3	72.4	71.3
806	2020-10-02	13:32:44	70.8	69.5	68.2	67.0	66.3
811	2020-10-02	13:32:49	65.9	67.4	68.3	67.3	67.4
816	2020-10-02	13:32:54	68.1	68.8	69.0	68.0	67.8
821	2020-10-02	13:32:59	68.0	70.1	71.6	71.8	71.6
826	2020-10-02	13:33:04	69.5	68.1	67.9	67.8	66.6
831	2020-10-02	13:33:09	65.4	63.8	62.8	62.5	62.6
836	2020-10-02	13:33:14	63.8	64.4	65.9	66.7	67.0
841	2020-10-02	13:33:19	65.6	63.9	63.9	65.9	67.3
846	2020-10-02	13:33:24	68.2	67.5	65.4	63.6	61.8
851	2020-10-02	13:33:29	60.3	59.4	58.2	58.0	60.1
856	2020-10-02	13:33:34	63.4	66.1	67.7	66.9	64.9
861	2020-10-02	13:33:39	62.6	60.8	60.5	59.3	59.1
866	2020-10-02	13:33:44	59.8	63.4	67.1	68.2	67.7
871	2020-10-02	13:33:49	66.5	64.9	63.5	64.3	66.7
876	2020-10-02	13:33:54	69.8	70.3	69.7	71.7	70.3
881	2020-10-02	13:33:59	70.7	70.1	69.0	68.9	68.2
886	2020-10-02	13:34:04	68.6	69.7	70.8	70.6	68.3
891	2020-10-02	13:34:09	66.5	65.3	65.1	66.7	66.1
896	2020-10-02	13:34:14	66.4	66.2	66.9	67.0	67.5
901	2020-10-02	13:34:19	68.9	65.1	65.1	65.1	65.1
906	2020-10-02	13:34:24	67.4	65.8	65.8	65.8	68.1



911	2020-10-02	13:34:29	67.8	67.0	66.8	67.3	67.4
916	2020-10-02	13:34:34	68.2	69.0	68.2	68.2	67.9
921	2020-10-02	13:34:39	66.5	64.6	62.7	61.7	60.2
926	2020-10-02	13:34:44	59.1	58.9	58.4	57.5	56.7
931	2020-10-02	13:34:49	56.3	57.9	57.2	56.7	56.1
936	2020-10-02	13:34:54	57.2	57.2	57.1	56.6	56.4
941	2020-10-02	13:34:59	57.2	57.3	56.9	57.2	57.5
946	2020-10-02	13:35:04	58.0	58.1	58.2	59.8	60.3
951	2020-10-02	13:35:09	61.1	61.8	63.6	63.1	64.2
956	2020-10-02	13:35:14	67.4	69.9	70.0	74.0	74.9
961	2020-10-02	13:35:19	75.5	78.3	78.7	75.9	72.9
966	2020-10-02	13:35:24	70.3	67.5	65.0	62.5	61.8
971	2020-10-02	13:35:29	60.6	60.6	59.7	59.2	60.8
976	2020-10-02	13:35:34	59.6	59.4	59.5	58.9	58.3
981	2020-10-02	13:35:39	57.8	57.1	57.8	59.5	59.7
986	2020-10-02	13:35:44	61.2	61.3	60.1	60.4	61.0
991	2020-10-02	13:35:49	61.5	62.4	65.1	66.8	67.5
996	2020-10-02	13:35:54	68.4	69.5	68.3	66.5	66.4
1001	2020-10-02	13:35:59	67.1	70.0	71.1	69.9	68.5
1006	2020-10-02	13:36:04	68.0	67.2	67.9	68.8	69.1
1011	2020-10-02	13:36:09	70.2	71.5	73.1	73.1	74.8
1016	2020-10-02	13:36:14	76.1	76.4	74.9	73.2	72.9
1021	2020-10-02	13:36:19	74.7	74.7	73.8	74.8	73.7
1026	2020-10-02	13:36:24	70.7	69.2	69.4	68.2	66.6
1031	2020-10-02	13:36:29	65.0	64.6	63.7	67.8	68.0
1036	2020-10-02	13:36:34	68.2	68.9	68.6	67.0	64.7
1041	2020-10-02	13:36:39	62.1	59.7	59.7	59.8	58.0
1046	2020-10-02	13:36:44	58.0	57.7	56.9	56.9	58.0
1051	2020-10-02	13:36:49	60.3	69.0	70.9	69.1	66.8
1056	2020-10-02	13:36:54	64.8	64.1	65.4	65.2	64.8
1061	2020-10-02	13:36:59	63.4	61.1	60.5	61.5	59.6
1066	2020-10-02	13:37:04	64.7	63.6	60.6	58.3	57.3
1071	2020-10-02	13:37:09	57.5	56.4	57.5	59.1	58.1
1076	2020-10-02	13:37:14	59.5	61.3	62.8	66.3	67.5
1081	2020-10-02	13:37:19	67.7	66.8	65.0	63.4	62.8
1086	2020-10-02	13:37:24	64.3	65.9	68.8	67.9	65.9
1091	2020-10-02	13:37:29	63.3	63.6	61.4	62.1	61.5
1096	2020-10-02	13:37:34	60.6	61.3	61.3	61.6	60.8
1101	2020-10-02	13:37:39	60.6	59.9	59.5	60.0	61.2
1106	2020-10-02	13:37:44	61.8	65.7	71.3	74.8	72.4
1111	2020-10-02	13:37:49	69.1	66.3	64.0	61.4	59.3
1116	2020-10-02	13:37:54	58.6	58.6	60.2	63.8	68.2
1121	2020-10-02	13:37:59	73.4	72.9	70.6	68.6	67.5
1126	2020-10-02	13:38:04	65.9	64.6	63.0	61.7	61.2
1131	2020-10-02	13:38:09	60.1	59.2	58.6	58.4	58.2
1136	2020-10-02	13:38:14	58.6	58.8	58.8	58.9	59.7
1141	2020-10-02	13:38:19	59.6	60.1	60.7	62.1	63.6
1146	2020-10-02	13:38:24	66.0	68.4	68.6	68.0	67.7
1151	2020-10-02	13:38:29	65.6	65.0	67.7	69.8	72.7
1156	2020-10-02	13:38:34	72.7	71.7	71.2	69.4	67.5
1161	2020-10-02	13:38:39	66.4	66.3	66.5	67.6	67.1
1166	2020-10-02	13:38:44	67.1	68.4	66.5	64.4	62.7
1171	2020-10-02	13:38:49	61.0	60.7	64.4	62.7	60.7
1176	2020-10-02	13:38:54	60.0	60.4	60.2	62.9	65.1
1181	2020-10-02	13:38:59	66.6	65.7	63.6	63.0	64.9
1186	2020-10-02	13:39:04	63.2	65.0	66.0	66.5	66.0
1191	2020-10-02	13:39:09	64.3	64.8	66.8	68.3	70.5
1196	2020-10-02	13:39:14	73.5	73.6	72.3	70.9	69.0
1201	2020-10-02	13:39:19	67.1	65.9	64.5	63.2	62.6
1206	2020-10-02	13:39:24	62.8	61.2	59.5	58.7	57.9
1211	2020-10-02	13:39:29	57.7	57.6	58.5	58.5	60.0
1216	2020-10-02	13:39:34	60.1	59.9	59.4	59.4	60.0
1221	2020-10-02	13:39:39	60.1	60.0	60.5	60.2	59.8
1226	2020-10-02	13:39:44	60.5	62.8	65.0	69.8	70.1
1231	2020-10-02	13:39:49	68.8	66.9	65.3	64.9	65.0
1236	2020-10-02	13:39:54	67.3	67.5	67.6	68.0	69.0
1241	2020-10-02	13:39:59	67.1	65.7	65.0	68.3	67.6
1246	2020-10-02	13:40:04	65.7	68.0	70.5	70.7	71.0
1251	2020-10-02	13:40:09	71.5	71.4	70.8	69.3	67.1
1256	2020-10-02	13:40:14	65.1	62.8	60.8	60.0	64.0
1261	2020-10-02	13:40:19	66.5	65.4	64.7	66.6	67.2
1266	2020-10-02	13:40:24	68.2	67.6	68.0	68.7	69.0
1271	2020-10-02	13:40:29	67.2	65.4	63.5	61.8	60.8
1276	2020-10-02	13:40:34	61.1	64.0	64.3	64.5	65.6
1281	2020-10-02	13:40:39	64.1	66.2	66.8	65.2	65.3
1286	2020-10-02	13:40:44	66.4	68.7	67.5	67.7	67.9
1291	2020-10-02	13:40:49	69.5	69.7	69.9	73.2	75.8
1296	2020-10-02	13:40:54	74.6	72.1	71.5	71.5	70.8
1301	2020-10-02	13:40:59	69.7	67.6	65.8	64.6	64.3
1306	2020-10-02	13:41:04	64.7	65.4	64.5	66.0	63.7
1311	2020-10-02	13:41:09	62.1	60.5	58.6	57.3	56.9
1316	2020-10-02	13:41:14	56.7	56.4	55.4	55.0	55.0
1321	2020-10-02	13:41:19	55.5	56.0	56.3	56.5	56.9
1326	2020-10-02	13:41:24	57.5	61.1	64.3	69.5	69.9
1331	2020-10-02	13:41:29	68.2	67.1	69.7	69.9	68.9
1336	2020-10-02	13:41:34	69.4	67.2	65.0	63.3	62.3
1341	2020-10-02	13:41:39	61.0	59.9	59.1	60.8	60.8
1346	2020-10-02	13:41:44	60.1	59.8	62.3	64.6	65.5
1351	2020-10-02	13:41:49	65.4	64.0	65.3	67.3	66.9
1356	2020-10-02	13:41:54	64.8	63.8	63.9	64.8	65.7
1361	2020-10-02	13:41:59	65.9	65.7	64.9	63.4	62.1
1366	2020-10-02	13:42:04	61.3	61.6	62.3	64.0	65.2
1371	2020-10-02	13:42:09	67.5	67.2	64.3	62.2	61.8
1376	2020-10-02	13:42:14	60.7	59.0	58.4	58.5	57.9
1381	2020-10-02	13:42:19	56.6	56.3	57.0	58.1	58.9
1386	2020-10-02	13:42:24	60.7	59.9	59.9	59.9	59.0
1391	2020-10-02	13:42:29	59.7	60.0	60.3	61.0	61.0
1396	2020-10-02	13:42:34	61.3	62.8	65.2	67.1	68.9

1401	2020-10-02	13:42:39	70.3	70.9	71.1	70.3	71.3
1406	2020-10-02	13:42:44	70.7	69.5	67.7	66.7	66.2
1411	2020-10-02	13:42:49	66.1	67.5	67.2	66.6	66.9
1416	2020-10-02	13:42:54	68.4	68.0	67.0	66.1	66.2
1421	2020-10-02	13:42:59	67.3	68.5	69.0	69.5	70.5
1426	2020-10-02	13:43:04	71.2	71.6	71.8	71.3	70.9
1431	2020-10-02	13:43:09	70.5	69.4	68.0	65.9	64.4
1436	2020-10-02	13:43:14	62.9	61.8	62.3	63.6	65.8
1441	2020-10-02	13:43:19	68.8	70.3	72.2	72.1	69.7
1446	2020-10-02	13:43:24	67.5	67.0	67.6	67.6	68.4
1451	2020-10-02	13:43:29	69.3	70.5	70.4	70.9	71.2
1456	2020-10-02	13:43:34	71.3	68.9	67.5	66.0	64.4
1461	2020-10-02	13:43:39	63.5	65.1	66.8	69.2	70.6
1466	2020-10-02	13:43:44	70.5	70.4	70.4	70.9	69.7
1471	2020-10-02	13:43:49	68.3	68.5	69.4	70.0	69.9
1476	2020-10-02	13:43:54	68.6	70.0	69.3	68.6	69.1
1481	2020-10-02	13:43:59	68.0	69.9	68.9	68.8	69.1
1486	2020-10-02	13:44:04	68.9	68.1	65.8	64.8	62.8
1491	2020-10-02	13:44:09	60.9	58.9	58.6	59.2	59.4
1496	2020-10-02	13:44:14	58.4	57.3	57.5	58.5	60.3
1501	2020-10-02	13:44:19	63.4	68.2	73.0	72.8	73.0
1506	2020-10-02	13:44:24	72.9	70.2	67.5	67.3	65.9
1511	2020-10-02	13:44:29	64.6	63.2	63.4	63.3	62.6
1516	2020-10-02	13:44:34	63.0	62.9	65.4	67.0	68.6
1521	2020-10-02	13:44:39	68.2	66.7	65.5	64.7	62.8
1526	2020-10-02	13:44:44	61.2	60.3	61.1	61.0	63.9
1531	2020-10-02	13:44:49	65.2	67.5	66.8	64.5	62.1
1536	2020-10-02	13:44:54	60.0	59.3	60.4	61.3	62.2
1541	2020-10-02	13:44:59	61.0	60.2	61.3	63.5	67.5
1546	2020-10-02	13:45:04	69.6	69.7	67.7	65.7	65.9
1551	2020-10-02	13:45:09	65.8	65.8	65.3	65.5	64.9
1556	2020-10-02	13:45:14	64.4	63.1	62.1	62.1	63.3
1561	2020-10-02	13:45:19	64.4	66.7	66.4	65.9	67.5
1566	2020-10-02	13:45:24	67.2	66.7	67.3	67.0	67.9
1571	2020-10-02	13:45:29	69.5	70.4	70.9	70.9	71.2
1576	2020-10-02	13:45:34	70.3	69.1	67.9	67.5	69.3
1581	2020-10-02	13:45:39	69.9	69.6	70.0	70.3	69.9
1586	2020-10-02	13:45:44	71.0	72.3	71.2	69.9	70.1
1591	2020-10-02	13:45:49	71.4	71.6	72.1	71.6	71.4
1596	2020-10-02	13:45:54	70.1	70.1	69.8	68.0	67.3
1601	2020-10-02	13:45:59	67.5	67.8	66.3	64.3	62.4
1606	2020-10-02	13:46:04	60.8	59.7	59.5	59.9	60.3
1611	2020-10-02	13:46:09	59.7	60.5	60.5	60.0	59.5
1616	2020-10-02	13:46:14	63.5	63.2	65.9	67.8	68.9
1621	2020-10-02	13:46:19	69.2	68.4	67.1	65.1	62.6
1626	2020-10-02	13:46:24	62.4	64.7	65.0	66.2	64.0
1631	2020-10-02	13:46:29	61.7	59.9	60.0	59.7	58.8
1636	2020-10-02	13:46:34	57.8	57.7	56.9	56.8	58.6
1641	2020-10-02	13:46:39	60.0	65.9	65.8	64.0	61.7
1646	2020-10-02	13:46:44	59.8	58.8	58.9	62.4	65.4
1651	2020-10-02	13:46:49	68.3	67.6	66.4	66.4	67.7
1656	2020-10-02	13:46:54	68.4	67.5	65.7	63.8	61.2
1661	2020-10-02	13:46:59	59.0	57.9	57.5	56.9	55.6
1666	2020-10-02	13:47:04	55.2	54.3	54.1	54.0	55.1
1671	2020-10-02	13:47:09	56.9	59.3	61.1	61.7	63.1
1676	2020-10-02	13:47:14	63.7	65.0	65.5	66.5	66.1
1681	2020-10-02	13:47:19	68.1	68.9	69.1	68.6	68.9
1686	2020-10-02	13:47:24	69.9	69.3	70.3	70.4	69.4
1691	2020-10-02	13:47:29	68.4	66.6	66.6	67.2	68.1
1696	2020-10-02	13:47:34	68.5	70.3	70.5	70.2	70.4
1701	2020-10-02	13:47:39	69.9	69.2	68.8	67.0	65.6
1706	2020-10-02	13:47:44	65.2	67.0	69.6	69.7	70.3
1711	2020-10-02	13:47:49	68.2	65.9	63.6	61.6	60.7
1716	2020-10-02	13:47:54	58.5	56.8	55.3	54.8	54.8
1721	2020-10-02	13:47:59	55.7	57.7	60.1	63.3	65.1
1726	2020-10-02	13:48:04	65.7	63.7	65.9	67.3	69.1
1731	2020-10-02	13:48:09	67.2	64.9	63.3	61.9	61.4
1736	2020-10-02	13:48:14	63.5	66.2	66.2	64.2	62.2
1741	2020-10-02	13:48:19	60.4	58.6	57.1	56.2	55.2
1746	2020-10-02	13:48:24	54.4	54.4	54.8	54.6	54.9
1751	2020-10-02	13:48:29	55.6	58.4	57.9	59.4	61.7
1756	2020-10-02	13:48:34	66.3	67.1	66.2	64.5	62.1
1761	2020-10-02	13:48:39	59.6	57.4	55.8	54.7	53.6
1766	2020-10-02	13:48:44	53.1	52.7	51.9	51.5	51.3
1771	2020-10-02	13:48:49	51.5	51.8	51.7	51.6	51.8
1776	2020-10-02	13:48:54	51.4	51.9	51.6	52.5	53.3
1781	2020-10-02	13:48:59	52.9	52.5	52.3	53.0	52.9
1786	2020-10-02	13:49:04	53.7	54.9	56.1	57.7	61.6
1791	2020-10-02	13:49:09	63.2	64.9	65.1	66.7	67.6
1796	2020-10-02	13:49:14	69.4	70.7	70.6	68.8	66.8
1801	2020-10-02	13:49:19	65.8	66.4	67.9	67.9	68.5
1806	2020-10-02	13:49:24	68.0	66.7	66.1	67.0	67.8
1811	2020-10-02	13:49:29	68.5	69.1	69.1	67.1	66.2
1816	2020-10-02	13:49:34	65.6	66.4	66.1	68.4	68.6
1821	2020-10-02	13:49:39	67.5	66.6	67.8	68.6	70.8
1826	2020-10-02	13:49:44	72.7	75.0	75.2	73.4	71.9
1831	2020-10-02	13:49:49	71.3	70.7	70.7	71.5	70.7
1836	2020-10-02	13:49:54	70.6	71.6	69.9	69.5	71.3
1841	2020-10-02	13:49:59	72.3	71.2	69.4	68.9	67.9
1846	2020-10-02	13:50:04	66.7	66.4	66.5	66.0	66.1
1851	2020-10-02	13:50:09	66.8	64.7	63.8	64.1	64.8
1856	2020-10-02	13:50:14	65.7	67.0	68.5	68.2	68.5
1861	2020-10-02	13:50:19	68.5	68.1	67.9	67.2	66.9
1866	2020-10-02	13:50:24	66.6	65.3	63.3	61.8	61.9
1871	2020-10-02	13:50:29	62.9	62.3	61.7	63.1	63.9
1876	2020-10-02	13:50:34	63.3	66.9	60.5	60.6	60.2
1881	2020-10-02	13:50:39	64.6	61.2	60.5	60.6	60.2
1886	2020-10-02	13:50:44	61.8	61.2	60.5	60.6	60.2



1891	2020-10-02	13:50:49	59.8	60.0	61.4	61.3	61.2
1896	2020-10-02	13:50:54	62.5	64.0	65.4	65.5	64.3
1901	2020-10-02	13:50:59	63.7	64.4	64.4	65.0	66.4
1906	2020-10-02	13:51:04	66.8	73.3	70.1	67.6	73.7
1911	2020-10-02	13:51:09	70.3	68.6	68.2	67.9	66.6
1916	2020-10-02	13:51:14	66.2	66.6	68.8	68.7	68.3
1921	2020-10-02	13:51:19	67.0	65.0	63.0	63.2	63.9
1926	2020-10-02	13:51:24	62.6	63.4	64.7	67.5	68.5
1931	2020-10-02	13:51:29	69.6	69.0	68.1	67.6	69.4
1936	2020-10-02	13:51:34	69.3	68.4	69.1	71.0	72.9
1941	2020-10-02	13:51:39	71.5	69.0	68.0	68.9	69.9
1946	2020-10-02	13:51:44	70.3	69.6	67.6	67.3	67.9
1951	2020-10-02	13:51:49	68.6	67.6	66.2	66.3	66.7
1956	2020-10-02	13:51:54	67.7	66.4	66.4	65.0	63.4
1961	2020-10-02	13:51:59	62.8	63.6	64.7	66.8	68.9
1966	2020-10-02	13:52:04	70.5	72.4	72.5	71.7	71.2
1971	2020-10-02	13:52:09	70.5	69.7	69.3	69.0	69.2
1976	2020-10-02	13:52:14	69.9	70.8	69.9	69.4	69.1
1981	2020-10-02	13:52:19	68.5	68.7	69.1	68.7	69.1
1986	2020-10-02	13:52:24	70.0	69.5	69.8	69.0	69.2
1991	2020-10-02	13:52:29	69.1	68.8	68.5	67.7	68.3
1996	2020-10-02	13:52:34	68.4	68.9	68.5	69.3	68.3
2001	2020-10-02	13:52:39	68.8	68.9	69.4	71.6	73.1
2006	2020-10-02	13:52:44	72.8	70.3	68.4	67.0	66.3
2011	2020-10-02	13:52:49	66.7	65.0	64.2	63.5	62.6
2016	2020-10-02	13:52:54	62.4	63.5	65.0	64.3	64.3
2021	2020-10-02	13:52:59	64.0	62.8	62.1	64.3	66.2
2026	2020-10-02	13:53:04	68.8	72.1	72.5	72.9	71.0
2031	2020-10-02	13:53:09	68.8	66.1	63.2	61.0	60.8
2036	2020-10-02	13:53:14	60.6	60.3	61.7	62.9	62.8
2041	2020-10-02	13:53:19	61.9	62.0	60.5	60.3	59.0
2046	2020-10-02	13:53:24	57.7	56.6	55.1	54.5	54.4
2051	2020-10-02	13:53:29	54.1	53.9	53.7	54.0	54.5
2056	2020-10-02	13:53:34	55.6	55.6	55.7	56.0	55.9
2061	2020-10-02	13:53:39	56.3	56.1	57.6	59.8	61.0
2066	2020-10-02	13:53:44	60.8	60.5	59.8	59.4	60.4
2071	2020-10-02	13:53:49	59.5	58.6	58.9	58.8	59.5
2076	2020-10-02	13:53:54	61.2	62.7	63.4	64.1	65.0
2081	2020-10-02	13:53:59	65.5	66.8	66.2	65.7	65.5
2086	2020-10-02	13:54:04	64.4	64.4	64.1	63.1	61.7
2091	2020-10-02	13:54:09	62.0	61.2	61.7	63.7	65.9
2096	2020-10-02	13:54:14	67.4	67.7	67.7	67.1	66.2
2101	2020-10-02	13:54:19	65.5	66.5	64.5	69.1	67.7
2106	2020-10-02	13:54:24	66.4	67.5	67.2	67.2	66.6
2111	2020-10-02	13:54:29	64.2	62.1	62.0	61.9	63.3
2116	2020-10-02	13:54:34	64.5	65.6	65.9	67.1	66.0
2121	2020-10-02	13:54:39	65.8	68.1	70.0	70.5	70.6
2126	2020-10-02	13:54:44	70.0	70.8	71.1	70.0	69.6
2131	2020-10-02	13:54:49	69.3	68.6	68.4	68.5	67.8
2136	2020-10-02	13:54:54	67.6	66.6	65.3	66.2	66.7
2141	2020-10-02	13:54:59	64.6	62.8	62.1	63.2	64.1
2146	2020-10-02	13:55:04	63.6	63.0	62.1	60.6	59.7
2151	2020-10-02	13:55:09	60.5	63.3	64.8	64.3	62.7
2156	2020-10-02	13:55:14	61.4	62.5	64.0	66.3	67.1
2161	2020-10-02	13:55:19	69.1	69.3	69.8	70.2	69.5
2166	2020-10-02	13:55:24	69.4	70.2	69.6	68.4	66.4
2171	2020-10-02	13:55:29	65.2	64.3	63.8	65.2	65.8
2176	2020-10-02	13:55:34	64.4	63.4	62.8	64.1	64.0
2181	2020-10-02	13:55:39	63.0	62.5	62.8	62.7	62.0
2186	2020-10-02	13:55:44	61.4	64.5	66.0	68.5	67.0
2191	2020-10-02	13:55:49	65.8	63.4	65.4	63.1	59.9
2196	2020-10-02	13:55:54	58.5	58.6	61.3	62.9	63.0
2201	2020-10-02	13:55:59	63.9	63.5	61.5	60.0	58.2
2206	2020-10-02	13:56:04	58.2	58.7	59.6	59.2	58.1
2211	2020-10-02	13:56:09	58.1	59.8	63.5	67.8	67.8
2216	2020-10-02	13:56:14	66.4	64.5	62.3	60.8	60.5
2221	2020-10-02	13:56:19	59.8	60.5	64.0	67.7	69.3
2226	2020-10-02	13:56:24	69.7	71.5	71.5	70.8	70.1
2231	2020-10-02	13:56:29	68.8	69.2	67.9	65.6	65.1
2236	2020-10-02	13:56:34	64.5	65.9	69.2	67.8	66.4
2241	2020-10-02	13:56:39	64.4	62.4	61.1	61.6	63.6
2246	2020-10-02	13:56:44	66.7	66.7	65.1	63.1	61.2
2251	2020-10-02	13:56:49	59.7	59.0	58.4	57.0	55.9
2256	2020-10-02	13:56:54	55.6	55.6	56.9	58.4	60.1
2261	2020-10-02	13:56:59	67.4	67.8	68.6	66.2	64.1
2266	2020-10-02	13:57:04	62.8	63.5	66.4	68.6	70.5
2271	2020-10-02	13:57:09	70.6	69.0	66.3	63.8	62.3
2276	2020-10-02	13:57:14	60.6	59.3	58.2	58.4	60.3
2281	2020-10-02	13:57:19	62.3	67.3	68.5	69.5	66.6
2286	2020-10-02	13:57:24	64.2	62.2	60.4	58.9	58.9
2291	2020-10-02	13:57:29	58.5	59.2	60.6	62.8	67.7
2296	2020-10-02	13:57:34	69.2	69.7	68.8	67.6	65.7
2301	2020-10-02	13:57:39	63.8	62.1	60.5	59.0	57.7
2306	2020-10-02	13:57:44	56.6	56.1	55.5	55.4	55.7
2311	2020-10-02	13:57:49	56.0	55.5	55.5	55.2	55.5
2316	2020-10-02	13:57:54	56.3	56.9	57.1	58.7	61.4
2321	2020-10-02	13:57:59	65.5	65.3	66.0	68.3	68.8
2326	2020-10-02	13:58:04	67.6	66.3	66.9	69.2	70.1
2331	2020-10-02	13:58:09	69.6	67.0	64.7	62.9	61.2
2336	2020-10-02	13:58:14	60.8	59.7	59.2	59.3	59.9
2341	2020-10-02	13:58:19	61.8	64.6	67.0	67.3	65.6
2346	2020-10-02	13:58:24	63.6	62.0	60.5	59.9	59.3
2351	2020-10-02	13:58:29	58.7	58.1	57.6	59.1	57.8
2356	2020-10-02	13:58:34	57.6	58.7	60.0	60.7	60.8
2361	2020-10-02	13:58:39	60.5	60.4	60.8	61.9	62.3
2366	2020-10-02	13:58:44	66.5	66.5	65.8	66.4	64.8
2371	2020-10-02	13:58:49	69.0	67.7	67.7	67.4	67.4
2376	2020-10-02	13:58:54	67.8	66.5	65.8	66.4	64.8

2381	2020-10-02	13:58:59	62.3	60.7	59.3	58.9	59.8
2386	2020-10-02	13:59:04	61.3	63.7	65.5	66.8	65.8
2391	2020-10-02	13:59:09	64.6	63.0	63.8	66.2	68.9
2396	2020-10-02	13:59:14	68.6	66.3	67.3	67.2	64.6
2401	2020-10-02	13:59:19	62.2	61.1	61.5	63.9	67.4
2406	2020-10-02	13:59:24	69.2	67.2	64.2	61.3	59.0
2411	2020-10-02	13:59:29	59.0	61.6	63.1	65.5	64.3
2416	2020-10-02	13:59:34	62.5	61.9	64.1	67.7	69.3
2421	2020-10-02	13:59:39	69.5	67.0	65.6	66.5	66.7
2426	2020-10-02	13:59:44	67.7	67.0	68.6	69.5	70.7
2431	2020-10-02	13:59:49	71.5	72.8	72.2	71.5	70.2
2436	2020-10-02	13:59:54	69.5	70.1	69.5	70.7	71.9
2441	2020-10-02	13:59:59	72.1	72.6	73.1	71.7	69.5
2446	2020-10-02	14:00:04	67.4	66.8	65.8	66.4	68.4
2451	2020-10-02	14:00:09	68.5	67.6	66.4	64.2	62.1
2456	2020-10-02	14:00:14	60.4	59.4	59.3	60.2	61.5
2461	2020-10-02	14:00:19	61.6	62.2	63.0	63.0	62.3
2466	2020-10-02	14:00:24	63.8	65.1	65.4	67.0	69.3
2471	2020-10-02	14:00:29	68.4	66.7	65.3	65.3	65.8
2476	2020-10-02	14:00:34	68.0	69.5	68.3	66.0	63.6
2481	2020-10-02	14:00:39	61.3	59.7	58.4	59.5	61.5
2486	2020-10-02	14:00:44	63.1	66.3	66.6	68.4	66.8
2491	2020-10-02	14:00:49	64.7	65.1	67.5	69.4	69.0
2496	2020-10-02	14:00:54	67.8	65.6	63.4	62.2	61.4
2501	2020-10-02	14:00:59	59.2	58.2	57.3	57.8	58.4
2506	2020-10-02	14:01:04	57.9	57.9	60.1	63.1	66.2
2511	2020-10-02	14:01:09	66.8	65.9	64.2	63.9	67.9
2516	2020-10-02	14:01:14	69.8	68.6	68.4	68.6	67.5
2521	2020-10-02	14:01:19	65.0	63.7	62.3	62.0	63.0
2526	2020-10-02	14:01:24	64.2	68.2	66.9	63.5	60.6
2531	2020-10-02	14:01:29	58.1	56.1	53.9	54.0	59.1
2536	2020-10-02	14:01:34	62.8	66.7	70.9	72.6	76.4
2541	2020-10-02	14:01:39	74.5	70.9	67.7	64.9	63.0
2546	2020-10-02	14:01:44	63.6	62.8	64.3	67.4	71.0
2551	2020-10-02	14:01:49	73.0	71.8	70.6	70.7	69.5
2556	2020-10-02	14:01:54	68.9	66.9	65.7	63.9	62.0
2561	2020-10-02	14:01:59	59.9	59.3	61.0	61.9	66.2
2566	2020-10-02	14:02:04	66.8	66.8	66.9	67.2	67.6
2571	2020-10-02	14:02:09	66.8	65.0	64.5	64.9	66.1
2576	2020-10-02	14:02:14	69.1	72.0	72.4	71.1	68.6
2581	2020-10-02	14:02:19	66.6	64.1	61.8	60.1	60.1
2586	2020-10-02	14:02:24	60.4	60.5	59.3	59.0	59.7
2591	2020-10-02	14:02:29	60.9	62.3	65.2	65.6	66.0
2596	2020-10-02	14:02:34	64.3	62.0	59.3	57.3	56.8
2601	2020-10-02	14:02:39	56.7	58.0	59.7	62.0	65.9
2606	2020-10-02	14:02:44	66.0	65.2	63.6	61.9	60.4
2611	2020-10-02	14:02:49	60.7	60.7	62.7	65.8	65.9
2616	2020-10-02	14:02:54	64.1	62.9	63.3	66.0	66.5
2621	2020-10-02	14:02:59	67.2	68.1	67.7	66.1	65.8
2626	2020-10-02	14:03:04	67.3	68.5	69.9	68.4	68.1
2631	2020-10-02	14:03:09	69.3	69.8	69.1	68.0	65.9
2636	2020-10-02	14:03:14	63.9	62.7	61.1	59.9	60.5
2641	2020-10-02	14:03:19	60.0	59.5	58.4	57.4	58.4
2646	2020-10-02	14:03:24	59.9	62.7	64.7	64.4	65.6
2651	2020-10-02	14:03:29	66.6	65.5	63.3	62.3	62.9
2656	2020-10-02	14:03:34	62.1	60.9	59.9	60.3	61.9
2661	2020-10-02	14:03:39	66.1	67.7	69.6	67.0	64.1
2666	2020-10-02	14:03:44	61.6	60.5	60.7	62.3	64.5
2671	2020-10-02	14:03:49	65.8	66.6	65.5	65.0	66.2
2676	2020-10-02	14:03:54	67.3	68.9	68.5	66.8	65.0
2681	2020-10-02	14:03:59	64.0	63.2	63.5	64.4	66.6
2686	2020-10-02	14:04:04	67.3	68.5	72.6	69.5	67.3
2691	2020-10-02	14:04:09	65.5	68.7	71.6	70.5	70.9
2696	2020-10-02	14:04:14	74.0	72.8	71.6	72.3	72.2
2701	2020-10-02	14:04:19	71.3	72.2	71.1	72.6	72.5
2706	2020-10-02	14:04:24	71.4	70.3	69.4	70.2	69.9
2711	2020-10-02	14:04:29	70.3	69.8	70.1	71.2	70.1
2716	2020-10-02	14:04:34	70.9	72.6	72.8	72.6	72.8
2721	2020-10-02	14:04:39	72.3	72.3	70.7	68.9	68.2
2726	2020-10-02	14:04:44	68.9	67.0	64.8	62.8	61.7
2731	2020-10-02	14:04:49	61.6	63.0	63.1	64.1	64.5
2736	2020-10-02	14:04:54	62.6	61.3	62.0	62.4	61.1
2741	2020-10-02	14:04:59	60.7	61.3	61.5	61.8	62.1
2746	2020-10-02	14:05:04	61.3	61.7	62.0	62.0	63.3
2751	2020-10-02	14:05:09	64.5	66.0	67.5	68.2	67.0
2756	2020-10-02	14:05:14	66.7	70.0	69.5	67.4	65.8
2761	2020-10-02	14:05:19	67.4	66.2	67.6	68.3	69.5
2766	2020-10-02	14:05:24	69.1	69.1	69.2	68.7	67.0
2771	2020-10-02	14:05:29	64.8	63.4	62.4	62.9	63.7
2776	2020-10-02	14:05:34	64.8	63.9	63.6	64.3	66.8
2781	2020-10-02	14:05:39	68.1	67.1	65.1	63.3	62.6
2786	2020-10-02	14:05:44	62.5	61.4	62.9	64.9	66.5
2791	2020-10-02	14:05:49	69.7	69.3	67.4	65.3	63.3
2796	2020-10-02	14:05:54	62.3	62.3	61.5	60.9	62.3
2801	2020-10-02	14:05:59	63.9	67.5	68.0	69.7	69.8
2806	2020-10-02	14:06:04	70.8	69.6	67.9	67.6	67.1
2811	2020-10-02	14:06:09	67.1	69.2	68.7	66.9	64.6
2816	2020-10-02	14:06:14	62.5	61.5	61.7	63.3	65.8
2821	2020-10-02	14:06:19	67.8	67.4	66.2	66.0	65.5
2826	2020-10-02	14:06:24	65.3	65.8	67.0	68.2	68.6
2831	2020-10-02	14:06:29	67.8	68.7	68.0	68.4	69.4
2836	2020-10-02	14:06:34	70.3	71.1	71.3	71.3	70.7
2841	2020-10-02	14:06:39	70.3	70.7	70.5	70.2	69.5
2846	2020-10-02	14:06:44	70.0	68.8	69.7	68.9	68.7
2851	2020-10-02	14:06:49	67.4	66.4	65.5	66.1	68.1
2856	2020-10-02	14:06:54	71.4	71.1	71.1	71.1	71.1
2861	2020-10-02	14:06:59	69.2	67.5	68.2	69.3	65.3
2866	2020-10-02	14:07:04	65.1	65.8	68.2	69.3	69.9

2871	2020-10-02	14:07:09	69.5	68.3	67.3	67.4	67.4
2876	2020-10-02	14:07:14	67.7	67.3	65.4	64.3	63.7
2881	2020-10-02	14:07:19	64.9	66.1	69.1	69.3	68.8
2886	2020-10-02	14:07:24	69.0	69.2	69.8	68.8	67.0
2891	2020-10-02	14:07:29	66.2	66.8	69.5	74.2	73.9
2896	2020-10-02	14:07:34	72.3	70.0	68.3	67.5	67.8
2901	2020-10-02	14:07:39	67.8	69.3	71.1	71.7	71.8
2906	2020-10-02	14:07:44	69.4	68.3	67.5	67.1	65.6
2911	2020-10-02	14:07:49	63.1	61.8	61.6	61.0	61.8
2916	2020-10-02	14:07:54	64.3	62.6	62.0	62.5	63.5
2921	2020-10-02	14:07:59	66.7	66.8	66.9	67.4	68.3
2926	2020-10-02	14:08:04	67.9	66.2	64.7	62.7	60.5
2931	2020-10-02	14:08:09	58.7	60.5	60.1	59.0	58.0
2936	2020-10-02	14:08:14	57.6	59.1	59.4	61.4	62.4
2941	2020-10-02	14:08:19	64.9	69.1	69.1	67.5	66.2
2946	2020-10-02	14:08:24	66.0	67.3	67.4	66.3	65.6
2951	2020-10-02	14:08:29	65.7	65.7	65.0	63.7	63.2
2956	2020-10-02	14:08:34	63.3	65.1	67.2	70.6	72.1
2961	2020-10-02	14:08:39	71.9	71.6	72.2	71.9	71.0
2966	2020-10-02	14:08:44	70.7	70.9	70.9	71.2	71.7
2971	2020-10-02	14:08:49	72.3	72.1	71.9	71.6	70.5
2976	2020-10-02	14:08:54	70.0	68.6	67.6	67.9	68.6
2981	2020-10-02	14:08:59	69.2	70.6	69.5	70.2	70.5
2986	2020-10-02	14:09:04	70.7	70.0	68.0	66.2	64.7
2991	2020-10-02	14:09:09	63.1	61.8	62.2	61.6	60.1
2996	2020-10-02	14:09:14	59.3	58.9	58.5	58.5	57.6
3001	2020-10-02	14:09:19	58.1	58.8	58.9	59.4	61.9
3006	2020-10-02	14:09:24	64.2	66.3	65.6	64.7	64.1
3011	2020-10-02	14:09:29	65.0	67.4	66.5	65.4	63.8
3016	2020-10-02	14:09:34	64.2	65.0	66.4	65.7	64.2
3021	2020-10-02	14:09:39	62.2	60.1	58.5	58.6	57.8
3026	2020-10-02	14:09:44	56.8	55.9	55.6	56.9	57.1
3031	2020-10-02	14:09:49	56.2	55.9	55.5	55.1	55.1
3036	2020-10-02	14:09:54	55.5	55.6	55.5	55.6	56.2
3041	2020-10-02	14:09:59	57.0	59.0	59.8	61.0	62.1
3046	2020-10-02	14:10:04	60.6	60.2	60.5	63.7	66.0
3051	2020-10-02	14:10:09	69.0	68.4	66.5	64.2	62.8
3056	2020-10-02	14:10:14	61.1	60.4	59.4	59.5	60.5
3061	2020-10-02	14:10:19	61.1	62.1	63.7	67.5	69.3
3066	2020-10-02	14:10:24	71.1	73.7	76.0	78.6	79.2
3071	2020-10-02	14:10:29	77.8	75.4	72.6	69.9	67.5
3076	2020-10-02	14:10:34	65.2	63.0	61.4	60.2	61.2
3081	2020-10-02	14:10:39	63.0	64.2	66.2	69.6	69.9
3086	2020-10-02	14:10:44	71.6	71.3	70.3	69.7	70.3
3091	2020-10-02	14:10:49	72.2	72.9	73.0	72.7	72.5
3096	2020-10-02	14:10:54	71.8	72.0	74.7	74.8	74.6
3101	2020-10-02	14:10:59	72.1	69.1	68.5	68.2	68.9
3106	2020-10-02	14:11:04	68.7	69.0	70.1	70.9	71.2
3111	2020-10-02	14:11:09	69.4	68.5	68.7	70.3	71.1
3116	2020-10-02	14:11:14	72.2	71.7	70.6	69.9	68.3
3121	2020-10-02	14:11:19	68.0	68.1	68.5	66.7	65.6
3126	2020-10-02	14:11:24	63.8	61.6	60.8	60.1	59.8
3131	2020-10-02	14:11:29	59.9	63.3	62.9	62.8	62.9
3136	2020-10-02	14:11:34	62.9	64.1	63.1	61.7	61.3
3141	2020-10-02	14:11:39	61.5	61.6	61.4	61.6	61.1
3146	2020-10-02	14:11:44	60.8	61.1	60.4	60.1	61.0
3151	2020-10-02	14:11:49	66.4	66.6	67.9	70.2	69.1
3156	2020-10-02	14:11:54	69.3	71.3	74.3	76.1	79.6
3161	2020-10-02	14:11:59	76.8	74.0	71.9	69.5	67.4
3166	2020-10-02	14:12:04	65.8	65.2	65.6	66.8	67.2
3171	2020-10-02	14:12:09	66.7	66.1	65.9	64.5	64.7
3176	2020-10-02	14:12:14	63.6	63.3	63.6	63.5	63.8
3181	2020-10-02	14:12:19	64.1	65.3	66.2	67.8	67.7
3186	2020-10-02	14:12:24	67.2	67.8	68.7	68.0	68.0
3191	2020-10-02	14:12:29	69.2	69.2	70.0	71.0	71.3
3196	2020-10-02	14:12:34	69.6	69.5	73.4	75.3	72.8
3201	2020-10-02	14:12:39	70.4	69.1	69.5	68.7	67.3
3206	2020-10-02	14:12:44	67.5	67.9	68.3	69.0	69.0
3211	2020-10-02	14:12:49	69.3	68.6	67.7	67.1	66.5
3216	2020-10-02	14:12:54	67.7	67.9	69.2	69.5	68.3
3221	2020-10-02	14:12:59	69.4	68.7	68.1	70.1	69.7
3226	2020-10-02	14:13:04	69.7	69.8	71.3	72.1	70.4
3231	2020-10-02	14:13:09	68.1	66.3	64.6	63.7	65.8
3236	2020-10-02	14:13:14	66.1	68.3	69.2	69.4	69.5
3241	2020-10-02	14:13:19	68.1	66.8	65.5	64.9	64.2
3246	2020-10-02	14:13:24	64.8	65.5	68.3	66.8	65.5
3251	2020-10-02	14:13:29	65.4	64.8	64.8	65.0	65.7
3256	2020-10-02	14:13:34	65.9	68.1	69.3	69.5	68.5
3261	2020-10-02	14:13:39	66.7	65.3	65.4	66.1	69.0
3266	2020-10-02	14:13:44	70.4	70.1	69.8	69.7	68.0
3271	2020-10-02	14:13:49	68.7	68.2	66.8	66.9	66.9
3276	2020-10-02	14:13:54	67.8	68.9	70.2	70.1	68.9
3281	2020-10-02	14:13:59	68.2	68.9	68.7	69.2	69.5
3286	2020-10-02	14:14:04	68.2	67.9	68.1	68.7	69.3
3291	2020-10-02	14:14:09	70.3	72.2	75.6	74.7	73.1
3296	2020-10-02	14:14:14	71.7	70.3	68.5	66.8	66.8
3301	2020-10-02	14:14:19	66.2	66.5	64.7	63.5	62.2
3306	2020-10-02	14:14:24	60.8	59.3	58.1	57.0	63.7
3311	2020-10-02	14:14:29	60.7	59.0	57.3	55.9	55.6
3316	2020-10-02	14:14:34	56.0	58.0	57.5	57.1	56.8
3321	2020-10-02	14:14:39	59.9	59.5	57.3	55.8	55.0
3326	2020-10-02	14:14:44	55.4	57.2	60.2	62.8	65.2
3331	2020-10-02	14:14:49	65.6	67.2	66.5	65.1	63.4
3336	2020-10-02	14:14:54	63.1	64.9	64.3	62.9	61.4
3341	2020-10-02	14:14:59	60.4	58.9	58.5	58.0	57.1
3346	2020-10-02	14:15:04	56.9	58.1	58.5	62.1	66.3
3351	2020-10-02	14:15:09	59.0	58.1	58.5	62.1	66.3
3356	2020-10-02	14:15:14	57.0	57.9	58.5	62.1	66.3

3361	2020-10-02	14:15:19	69.0	69.2	69.9	68.6	67.1
3366	2020-10-02	14:15:24	65.6	63.6	62.2	61.9	63.5
3371	2020-10-02	14:15:29	65.5	69.7	69.9	71.1	70.6
3376	2020-10-02	14:15:34	71.9	71.8	72.5	73.1	72.6
3381	2020-10-02	14:15:39	71.0	69.7	69.3	67.4	67.0
3386	2020-10-02	14:15:44	68.0	69.8	70.0	70.9	71.4
3391	2020-10-02	14:15:49	72.0	71.2	70.6	70.7	69.2
3396	2020-10-02	14:15:54	69.1	69.7	67.5	66.5	64.6
3401	2020-10-02	14:15:59	62.5	60.8	59.4	58.2	59.5
3406	2020-10-02	14:16:04	59.4	62.1	66.6	66.3	64.6
3411	2020-10-02	14:16:09	62.9	61.5	60.2	58.1	56.9
3416	2020-10-02	14:16:14	56.3	56.6	57.5	58.9	62.7
3421	2020-10-02	14:16:19	64.6	67.8	69.6	69.5	68.5
3426	2020-10-02	14:16:24	66.9	67.3	67.8	66.7	65.9
3431	2020-10-02	14:16:29	65.0	66.5	67.2	67.6	66.7
3436	2020-10-02	14:16:34	66.8	67.5	66.7	65.0	63.1
3441	2020-10-02	14:16:39	60.8	59.4	59.1	60.5	60.5
3446	2020-10-02	14:16:44	63.7	67.3	69.3	71.2	71.6
3451	2020-10-02	14:16:49	71.6	68.9	66.1	63.6	62.7
3456	2020-10-02	14:16:54	63.8	66.1	68.0	66.6	66.3
3461	2020-10-02	14:16:59	67.8	69.5	68.3	66.7	64.7
3466	2020-10-02	14:17:04	63.7	63.5	66.0	68.1	67.2
3471	2020-10-02	14:17:09	66.3	66.9	67.1	70.0	69.3
3476	2020-10-02	14:17:14	67.8	67.5	68.4	67.9	69.2
3481	2020-10-02	14:17:19	67.6	66.5	65.5	63.6	63.6
3486	2020-10-02	14:17:24	64.2	64.0	64.1	64.0	63.1
3491	2020-10-02	14:17:29	64.1	66.1	67.7	68.3	67.2
3496	2020-10-02	14:17:34	65.5	65.7	66.4	65.9	66.7
3501	2020-10-02	14:17:39	66.6	67.5	67.9	68.7	70.2
3506	2020-10-02	14:17:44	72.6	71.8	71.8	69.9	69.6
3511	2020-10-02	14:17:49	67.8	65.5	64.4	64.6	66.0
3516	2020-10-02	14:17:54	67.0	68.9	70.0	69.5	67.2
3521	2020-10-02	14:17:59	65.1	63.6	63.1	62.5	61.6
3526	2020-10-02	14:18:04	61.3	61.3	60.9	62.0	64.3
3531	2020-10-02	14:18:09	65.7	67.8	68.2	69.0	70.3
3536	2020-10-02	14:18:14	70.6	68.9	68.2	68.4	69.6
3541	2020-10-02	14:18:19	69.5	67.2	65.7	65.4	67.9
3546	2020-10-02	14:18:24	71.9	71.5	69.7	67.7	65.6
3551	2020-10-02	14:18:29	64.4	62.9	62.6	63.3	62.8
3556	2020-10-02	14:18:34	63.0	66.3	67.3	67.1	66.2
3561	2020-10-02	14:18:39	64.0	62.7	62.2	64.1	66.1
3566	2020-10-02	14:18:44	67.2	66.6	66.8	66.9	66.5
3571	2020-10-02	14:18:49	66.3	67.5	67.9	69.3	70.0
3576	2020-10-02	14:18:54	69.2	70.1	70.1	69.2	68.5
3581	2020-10-02	14:18:59	67.8	67.7	66.3	64.9	64.7
3586	2020-10-02	14:19:04	66.7	67.5	68.7	68.7	65.9
3591	2020-10-02	14:19:09	66.7	66.6	65.3	64.3	65.0
3596	2020-10-02	14:19:14	64.6	64.7	67.4	67.2	66.8
3601	2020-10-02	14:19:19	66.0	64.1	64.7	66.0	67.2
3606	2020-10-02	14:19:24	67.7	67.7	68.1	66.0	64.0
3611	2020-10-02	14:19:29	64.2	64.4	63.7	63.3	63.1
3616	2020-10-02	14:19:34	65.3	67.1	66.2	65.4	63.4
3621	2020-10-02	14:19:39	61.9	61.6	64.1	66.0	66.8
3626	2020-10-02	14:19:44	68.4	69.2	69.0	67.7	68.1
3631	2020-10-02	14:19:49	68.6	70.4	71.0	71.6	70.8
3636	2020-10-02	14:19:54	70.2	68.6	70.2	71.6	72.6
3641	2020-10-02	14:19:59	71.5	71.6	71.7	70.9	70.9
3646	2020-10-02	14:20:04	70.9	70.6	71.6	71.0	71.2
3651	2020-10-02	14:20:09	69.9	71.1	69.8	69.2	68.9
3656	2020-10-02	14:20:14	68.2	69.0	69.2	70.2	70.9
3661	2020-10-02	14:20:19	71.4	71.0	70.2	69.2	68.0
3666	2020-10-02	14:20:24	66.3	65.0	63.7	62.3	61.4
3671	2020-10-02	14:20:29	59.8	58.7	59.0	58.8	60.6
3676	2020-10-02	14:20:34	62.5	64.7	66.1*	65.5*	64.4*
3681	2020-10-02	14:20:39	62.2*	59.7*	58.0*	57.7*	57.2
3686	2020-10-02	14:20:44	57.3				



Freq Weight : A  
 Time Weight : SLOW  
 Level Range : 40-100  
 Max dB : 68.5 - 2020/10/02 15:43:01  
 Level Range : 40-100  
 SEL : 89.0  
 Leq : 58.4

No.s	Date Time	(dB)				
1	2020-10-02 15:24:35	58.3	58.5	57.5	57.1	58.4
6	2020-10-02 15:24:40	59.0	58.4	58.7	60.2	62.2
11	2020-10-02 15:24:45	61.5	60.5	60.6	60.8	61.0
16	2020-10-02 15:24:50	61.0	60.8	60.7	60.5	60.6
21	2020-10-02 15:24:55	60.5	60.3	59.5	58.9	58.1
26	2020-10-02 15:25:00	57.0	56.5	57.2	57.4	57.2
31	2020-10-02 15:25:05	57.4	57.8	58.5	59.7	60.7
36	2020-10-02 15:25:10	63.1	62.5	61.2	60.1	58.9
41	2020-10-02 15:25:15	58.4	57.9	58.0	57.1	56.3
46	2020-10-02 15:25:20	55.2	54.4	54.1	53.8	53.5
51	2020-10-02 15:25:25	54.6	54.2	54.5	55.0	55.2
56	2020-10-02 15:25:30	56.6	56.5	56.2	57.1	57.1
61	2020-10-02 15:25:35	58.0	58.4	58.6	58.8	59.1
66	2020-10-02 15:25:40	59.8	59.1	59.0	59.9	58.7
71	2020-10-02 15:25:45	59.1	57.6	56.7	56.0	57.9
76	2020-10-02 15:25:50	60.5	58.0	56.3	55.9	55.2
81	2020-10-02 15:25:55	53.9	53.7	55.1	55.1	55.5
86	2020-10-02 15:26:00	56.2	54.7	53.9	53.4	53.0
91	2020-10-02 15:26:05	53.0	54.2	54.9	55.2	56.0
96	2020-10-02 15:26:10	56.9	57.3	57.9	58.0	57.6
101	2020-10-02 15:26:15	57.9	57.8	57.6	55.9	54.4
106	2020-10-02 15:26:20	53.9	54.2	56.1	56.5	56.3
111	2020-10-02 15:26:25	56.2	55.8	55.2	54.4	53.5
116	2020-10-02 15:26:30	52.7	52.5	52.6	54.4	53.0
121	2020-10-02 15:26:35	51.7	51.8	51.9	51.2	50.8
126	2020-10-02 15:26:40	52.8	53.7	53.7	53.9	55.3
131	2020-10-02 15:26:45	57.4	57.4	56.2	57.3	58.3
136	2020-10-02 15:26:50	58.3	57.4	56.0	55.0	54.1
141	2020-10-02 15:26:55	53.6	53.1	53.4	53.7	53.9
146	2020-10-02 15:27:00	53.9	55.0	54.9	55.8	56.2
151	2020-10-02 15:27:05	56.9	56.5	57.3	59.3	57.8
156	2020-10-02 15:27:10	57.4	57.2	56.7	58.4	59.8
161	2020-10-02 15:27:15	57.9	56.7	56.4	55.7	55.0
166	2020-10-02 15:27:20	54.9	54.8	55.4	55.3	56.3
171	2020-10-02 15:27:25	54.9	57.1	57.0	55.4	53.7
176	2020-10-02 15:27:30	52.7	52.5	52.5	53.4	55.0
181	2020-10-02 15:27:35	57.7	58.9	59.7	60.4	61.4
186	2020-10-02 15:27:40	61.1	61.0	61.4	61.4	61.2
191	2020-10-02 15:27:45	61.1	60.9	60.1	60.7	60.8
196	2020-10-02 15:27:50	61.8	62.5	62.2	62.0	60.4
201	2020-10-02 15:27:55	59.3	58.5	57.3	56.9	56.8
206	2020-10-02 15:28:00	56.3	56.0	55.7	56.4	57.1
211	2020-10-02 15:28:05	56.7	56.1	56.9	57.8	57.5
216	2020-10-02 15:28:10	57.4	57.7	57.6	56.7	55.9
221	2020-10-02 15:28:15	55.0	55.5	56.3	57.3	57.0
226	2020-10-02 15:28:20	58.1	59.5	60.6	61.8	63.1
231	2020-10-02 15:28:25	64.0	63.3	61.6	60.2	59.3
236	2020-10-02 15:28:30	59.0	59.0	58.1	57.4	57.4
241	2020-10-02 15:28:35	58.3	59.0	59.3	59.8	61.2
246	2020-10-02 15:28:40	63.2	64.4	66.4	67.5	67.9
251	2020-10-02 15:28:45	67.4	67.4	65.3	64.0	63.7
256	2020-10-02 15:28:50	62.6	62.6	61.6	61.3	60.9
261	2020-10-02 15:28:55	60.4	59.6	59.3	58.8	57.9
266	2020-10-02 15:29:00	57.0	56.5	56.2	56.4	56.8
271	2020-10-02 15:29:05	56.9	57.1	56.4	55.2	55.2
276	2020-10-02 15:29:10	57.5	58.1	56.8	54.9	53.8
281	2020-10-02 15:29:15	53.2	52.6	52.7	53.3	55.0
286	2020-10-02 15:29:20	56.3	56.8	57.2	56.9	58.0
291	2020-10-02 15:29:25	58.9	58.4	56.9	55.9	55.3
296	2020-10-02 15:29:30	55.7	57.0	57.3	55.7	53.8
301	2020-10-02 15:29:35	52.7	53.4	55.7	54.8	54.0
306	2020-10-02 15:29:40	53.3	53.1	53.2	53.4	54.4
311	2020-10-02 15:29:45	56.3	57.3	59.4	58.7	59.0
316	2020-10-02 15:29:50	60.8	60.8	60.4	60.1	60.1
321	2020-10-02 15:29:55	60.6	60.2	60.4	60.8	61.2
326	2020-10-02 15:30:00	61.3	60.6	60.5	60.8	60.4
331	2020-10-02 15:30:05	59.7	59.1	59.0	58.4	57.3
336	2020-10-02 15:30:10	56.2	55.4	55.2	56.3	57.0
341	2020-10-02 15:30:15	58.4	59.6	60.3	59.2	57.9
346	2020-10-02 15:30:20	57.2	58.1	57.9	58.4	57.7
351	2020-10-02 15:30:25	57.9	57.9	56.8	55.4	54.7
356	2020-10-02 15:30:30	54.3	54.0	53.2	52.1	52.0
361	2020-10-02 15:30:35	52.0	52.6	52.9	53.2	54.8
366	2020-10-02 15:30:40	55.5	55.0	55.9	56.7	57.5
371	2020-10-02 15:30:45	59.0	59.9	60.0	59.1	59.1
376	2020-10-02 15:30:50	59.2	59.5	59.0	60.5	61.8
381	2020-10-02 15:30:55	62.1	61.7	61.4	60.9	60.1
386	2020-10-02 15:31:00	59.1	58.3	58.2	57.9	57.5
391	2020-10-02 15:31:05	57.2	57.7	58.3	59.3	58.5
396	2020-10-02 15:31:10	58.2	57.4	56.9	56.8	56.3
401	2020-10-02 15:31:15	55.9	55.9	55.5	56.1	60.1
406	2020-10-02 15:31:20	58.7	58.7	58.7	58.7	58.7
411	2020-10-02 15:31:25	56.4	56.5	56.5	56.5	57.9
416	2020-10-02 15:31:30	57.3	57.4	57.0	55.9	54.9

421	2020-10-02	15:31:35	55.2	54.6	54.7	54.9	57.9
426	2020-10-02	15:31:40	56.4	55.8	55.8	55.5	56.5
431	2020-10-02	15:31:45	56.9	58.9	58.9	58.9	61.0
436	2020-10-02	15:31:50	62.0	61.2	60.7	61.3	61.0
441	2020-10-02	15:31:55	59.9	60.0	60.1	59.0	58.2
446	2020-10-02	15:32:00	58.4	59.5	59.3	59.7	59.8
451	2020-10-02	15:32:05	60.1	59.9	60.6	61.5	61.8
456	2020-10-02	15:32:10	61.8	61.1	60.6	60.5	60.3
461	2020-10-02	15:32:15	60.3	60.0	60.6	60.4	59.5
466	2020-10-02	15:32:20	59.8	59.5	59.3	59.2	59.4
471	2020-10-02	15:32:25	59.2	58.6	58.0	57.5	57.2
476	2020-10-02	15:32:30	56.7	56.6	56.6	55.1	54.1
481	2020-10-02	15:32:35	54.3	56.3	55.9	55.9	55.9
486	2020-10-02	15:32:40	55.2	55.8	56.1	55.5	55.1
491	2020-10-02	15:32:45	55.9	57.3	56.8	57.0	56.2
496	2020-10-02	15:32:50	55.4	55.9	56.8	56.4	57.8
501	2020-10-02	15:32:55	56.6	56.2	56.8	55.4	53.8
506	2020-10-02	15:33:00	53.0	52.9	52.4	51.9	51.9
511	2020-10-02	15:33:05	52.8	53.5	54.5	54.4	54.9
516	2020-10-02	15:33:10	55.5	54.6	54.0	54.3	53.6
521	2020-10-02	15:33:15	53.5	53.9	55.1	55.6	54.9
526	2020-10-02	15:33:20	54.3	53.6	54.8	55.1	55.9
531	2020-10-02	15:33:25	57.0	58.8	60.0	58.1	57.4
536	2020-10-02	15:33:30	56.4	55.7	56.4	56.7	56.4
541	2020-10-02	15:33:35	56.0	55.5	55.2	56.2	55.1
546	2020-10-02	15:33:40	54.7	54.8	56.3	57.0	57.8
551	2020-10-02	15:33:45	58.7	58.2	57.5	56.9	55.5
556	2020-10-02	15:33:50	54.2	53.7	53.9	54.3	54.1
561	2020-10-02	15:33:55	54.4	54.2	55.3	54.8	54.5
566	2020-10-02	15:34:00	54.6	56.6	58.2	57.2	56.7
571	2020-10-02	15:34:05	56.6	56.2	56.7	57.6	58.4
576	2020-10-02	15:34:10	59.0	59.8	60.2	59.7	58.8
581	2020-10-02	15:34:15	57.2	56.2	55.3	55.8	57.2
586	2020-10-02	15:34:20	56.5	56.4	57.1	56.5	56.9
591	2020-10-02	15:34:25	55.8	54.9	54.0	53.5	53.7
596	2020-10-02	15:34:30	55.9	57.0	57.8	59.9	59.5
601	2020-10-02	15:34:35	58.5	58.4	59.6	61.5	61.3
606	2020-10-02	15:34:40	62.9	63.0	62.3	62.3	62.6
611	2020-10-02	15:34:45	62.7	62.5	62.3	62.7	63.3
616	2020-10-02	15:34:50	61.8	60.7	61.5	60.2	59.2
621	2020-10-02	15:34:55	58.8	58.6	58.6	59.3	59.2
626	2020-10-02	15:35:00	59.5	59.2	58.8	58.6	58.0
631	2020-10-02	15:35:05	57.1	56.8	56.8	56.5	57.1
636	2020-10-02	15:35:10	56.9	56.9	56.8	57.0	56.8
641	2020-10-02	15:35:15	56.8	56.0	56.1	55.2	55.5
646	2020-10-02	15:35:20	56.9	56.5	56.7	57.4	58.3
651	2020-10-02	15:35:25	59.7	60.0	59.9	60.9	60.8
656	2020-10-02	15:35:30	60.5	60.6	63.0	63.0	62.9
661	2020-10-02	15:35:35	62.7	62.0	61.3	60.1	60.2
666	2020-10-02	15:35:40	59.7	58.4	57.2	57.0	56.2
671	2020-10-02	15:35:45	55.7	55.4	55.5	54.6	54.1
676	2020-10-02	15:35:50	54.5	56.1	56.7	57.3	57.2
681	2020-10-02	15:35:55	58.2	59.0	58.2	58.2	58.6
686	2020-10-02	15:36:00	58.9	59.1	59.2	57.6	56.5
691	2020-10-02	15:36:05	57.6	59.0	59.2	59.7	60.1
696	2020-10-02	15:36:10	59.3	58.2	56.5	55.1	54.5
701	2020-10-02	15:36:15	54.8	55.4	55.3	55.8	55.5
706	2020-10-02	15:36:20	55.0	54.9	56.1	56.5	57.6
711	2020-10-02	15:36:25	57.6	58.4	59.1	59.9	60.3
716	2020-10-02	15:36:30	60.6	60.9	60.6	60.7	61.5
721	2020-10-02	15:36:35	61.1	61.1	60.8	61.4	61.6
726	2020-10-02	15:36:40	61.7	61.1	60.7	60.2	59.5
731	2020-10-02	15:36:45	59.4	59.2	58.5	59.0	59.5
736	2020-10-02	15:36:50	60.0	59.6	59.6	60.3	60.8
741	2020-10-02	15:36:55	60.4	60.0	59.0	58.4	59.3
746	2020-10-02	15:37:00	59.4	59.3	59.1	59.6	60.3
751	2020-10-02	15:37:05	59.3	58.3	57.6	57.3	56.0
756	2020-10-02	15:37:10	55.2	55.4	55.4	56.8	57.5
761	2020-10-02	15:37:15	57.4	57.5	58.1	58.1	57.4
766	2020-10-02	15:37:20	55.9	54.7	53.6	53.4	53.7
771	2020-10-02	15:37:25	52.6	52.1	51.5	51.0	52.1
776	2020-10-02	15:37:30	52.0	51.9	52.7	53.1	54.0
781	2020-10-02	15:37:35	55.1	55.1	56.7	55.8	54.0
786	2020-10-02	15:37:40	53.2	53.2	53.7	55.7	56.7
791	2020-10-02	15:37:45	59.2	58.7	58.8	58.4	58.3
796	2020-10-02	15:37:50	58.4	57.7	57.3	58.1	57.8
801	2020-10-02	15:37:55	58.1	57.8	57.3	57.3	57.8
806	2020-10-02	15:38:00	57.0	56.2	55.5	56.7	57.3
811	2020-10-02	15:38:05	55.6	54.0	53.0	52.2	52.7
816	2020-10-02	15:38:10	55.5	57.3	56.2	54.7	53.7
821	2020-10-02	15:38:15	52.8	52.2	51.9	51.7	51.7
826	2020-10-02	15:38:20	51.9	52.4	56.0	58.0	56.7
831	2020-10-02	15:38:25	56.3	55.4	56.0	55.4	55.5
836	2020-10-02	15:38:30	56.5	56.1	55.0	54.6	53.8
841	2020-10-02	15:38:35	54.6	56.1	56.8	58.3	59.3
846	2020-10-02	15:38:40	60.1	60.5	60.0	60.0	60.7
851	2020-10-02	15:38:45	62.1	62.1	61.5	60.4	59.9
856	2020-10-02	15:38:50	58.5	56.8	56.0	56.0	56.4
861	2020-10-02	15:38:55	57.9	58.3	57.8	58.0	57.8
866	2020-10-02	15:39:00	58.4	57.5	56.5	55.4	55.7
871	2020-10-02	15:39:05	57.5	56.9	57.0	56.9	56.1
876	2020-10-02	15:39:10	55.0	54.1	54.3	53.9	54.5
881	2020-10-02	15:39:15	54.6	54.9	56.2	55.9	55.9
886	2020-10-02	15:39:20	54.8	54.5	54.0	54.0	58.6
891	2020-10-02	15:39:25	59.0	58.8	58.0	57.6	57.0
896	2020-10-02	15:39:30	57.8	57.9	57.9	57.9	57.9
901	2020-10-02	15:39:35	57.1	57.2	57.2	57.2	57.2
906	2020-10-02	15:39:40	55.6	55.5	55.4	57.0	58.6



911	2020-10-02	15:39:45	59.3	59.0	59.0	59.2	58.5
916	2020-10-02	15:39:50	57.5	56.8	55.9	55.5	55.4
921	2020-10-02	15:39:55	57.7	58.5	58.3	58.9	59.7
926	2020-10-02	15:40:00	59.3	58.0	57.8	57.8	58.0
931	2020-10-02	15:40:05	59.1	58.6	58.3	58.1	58.1
936	2020-10-02	15:40:10	58.3	60.3	62.5	61.5	59.0
941	2020-10-02	15:40:15	57.1	55.8	54.9	54.3	54.4
946	2020-10-02	15:40:20	55.3	56.5	57.0	56.6	55.9
951	2020-10-02	15:40:25	56.1	55.7	55.5	56.5	57.5
956	2020-10-02	15:40:30	58.9	59.1	61.0	60.5	59.8
961	2020-10-02	15:40:35	60.1	59.8	59.3	59.4	59.6
966	2020-10-02	15:40:40	59.5	58.6	58.4	58.1	57.9
971	2020-10-02	15:40:45	59.3	60.1	60.2	59.8	60.5
976	2020-10-02	15:40:50	61.5	60.4	59.9	60.6	61.1
981	2020-10-02	15:40:55	59.3	58.1	56.7	56.2	55.2
986	2020-10-02	15:41:00	54.8	58.7	58.9	62.1	62.4
991	2020-10-02	15:41:05	62.0	61.1	58.7	57.3	56.3
996	2020-10-02	15:41:10	55.3	54.5	55.7	55.9	56.1
1001	2020-10-02	15:41:15	55.4	56.0	56.7	56.4	55.9
1006	2020-10-02	15:41:20	57.2	57.9	57.0	57.1	56.5
1011	2020-10-02	15:41:25	58.1	58.9	58.2	57.5	58.2
1016	2020-10-02	15:41:30	59.2	58.0	56.9	58.2	57.2
1021	2020-10-02	15:41:35	55.9	57.7	56.1	56.3	54.1
1026	2020-10-02	15:41:40	52.7	52.5	52.9	52.6	51.9
1031	2020-10-02	15:41:45	55.0	53.3	52.6	52.6	53.8
1036	2020-10-02	15:41:50	53.8	53.1	52.2	51.5	51.8
1041	2020-10-02	15:41:55	53.1	55.3	56.6	55.3	54.2
1046	2020-10-02	15:42:00	53.3	53.3	53.3	53.3	54.8
1051	2020-10-02	15:42:05	58.3	58.5	57.7	57.9	59.4
1056	2020-10-02	15:42:10	58.7	57.6	58.2	58.1	58.5
1061	2020-10-02	15:42:15	58.9	59.9	59.6	59.0	58.3
1066	2020-10-02	15:42:20	58.2	59.4	60.4	59.2	60.0
1071	2020-10-02	15:42:25	60.8	60.9	60.0	59.7	60.5
1076	2020-10-02	15:42:30	60.0	60.1	60.7	61.7	61.8
1081	2020-10-02	15:42:35	61.1	60.8	61.5	60.5	59.6
1086	2020-10-02	15:42:40	59.8	59.7	59.1	59.3	58.0
1091	2020-10-02	15:42:45	57.9	55.8	54.9	54.2	54.6
1096	2020-10-02	15:42:50	55.1	57.0	57.8	57.7	58.6
1101	2020-10-02	15:42:55	60.2	62.9	65.8	68.3	68.3
1106	2020-10-02	15:43:00	68.4	68.5	67.6	66.2	64.2
1111	2020-10-02	15:43:05	61.9	59.8	58.7	58.0	57.6
1116	2020-10-02	15:43:10	57.6	58.2	58.6	57.6	56.4
1121	2020-10-02	15:43:15	55.9	55.9	55.0	56.5	56.8
1126	2020-10-02	15:43:20	56.3	55.9	56.0	55.6	55.1
1131	2020-10-02	15:43:25	54.9	56.0	57.3	56.6	56.0
1136	2020-10-02	15:43:30	55.9	55.4	56.5	57.8	57.8
1141	2020-10-02	15:43:35	60.5	60.6	58.1	56.2	54.8
1146	2020-10-02	15:43:40	53.4	52.9	53.7	53.3	55.5
1151	2020-10-02	15:43:45	58.8	57.4	56.0	55.3	54.3
1156	2020-10-02	15:43:50	53.1	52.6	52.9	53.7	52.9
1161	2020-10-02	15:43:55	52.6	52.8	54.3	55.5	56.5
1166	2020-10-02	15:44:00	57.9	60.2*	59.8*	59.6*	59.4*
1171	2020-10-02	15:44:05	58.8*	58.8*	59.2*	59.4*	59.2*
1176	2020-10-02	15:44:10	60.8*	61.7*	61.0	60.8	

APPENDIX 'K'  
FUNCTIONAL SERVICING REPORT



**FUNCTIONAL SERVICING REPORT  
MIXED USED DEVELOPMENT  
3885 SANDWICH STREET  
WINDSOR, ONTARIO**

PROJECT NO. 20-028

DATED: OCTOBER 8, 2020



27 Princess St., Unit 102  
Leamington, ON N8H 2X8  
519.326.6161 TF 1.844.842.9188  
[bairdAE.ca](http://bairdAE.ca)

# TABLE OF CONTENT

1.	INTRODUCTION .....	1
2.	EXISTING CONDITION .....	1
3.	PROPOSED CONDITION .....	2
4.	STORMWATER MANAGEMENT .....	3
4.1	Storm Quantity Control.....	3
4.2	Storm Quality Control .....	5
5.	EROSION AND SEDIMENT CONTROL .....	5
6.	CONCLUSION .....	5

## TABLES

Table 1: Rainfall Intensity .....	4
Table 2: Hydrograph Modelling Variables.....	4
Table 3: Peak Discharges .....	4
Table 4: Ponding Characterstic .....	5

## APPENDICES

Appendix A Background Information  
Appendix B Storm and Sanitary Design Sheet  
Appendix C HYDRAGRAPH Model Output  
Appendix D Stormwater Quality Unit & Grading Plan

## **1. INTRODUCTION**

BairdAE was retained to prepare functional servicing report to review and identified servicing requirement for the proposed condominium development in 3885 Sandwich Street, Windsor. This report will be supporting document for the required site plan application.

The proposed development will consist of upto 150 residential units with a density of 3.5 people per dwelling. The density will have equivalent population of 525 people. The proposed development concepts are provided in Appendix A.

## **2. EXISTING CONDITIONS**

### ***2.1 Description of Study Area***

The subject land is approximately 0.669 hectares in size and are bounded by Sandwich Street to the west, Chappell Avenue to south, and existing residential development to the east and west. The site has driveway access from Sandwich Street which will be replaced with two new accesses, one from Sandwich Street and Chappell Avenue.

### ***2.2 Topography and Drainage***

Referring to Figure 1, the property is undeveloped and the flow from site sheet drains to the west and north, and then routed to Crawford Avenue storm sewer.

Based on the geotechnical report prepared by Soil and material Engineering Inc. dated November 2, 2018, the soil is silty clay with groundwater table 4±m from existing grade and finished floor elevation is set at 185.4m.

The soil maps provided by ERCA suggests that the soil in the site area is Brookston Clay Loam. In terms of hydrological soil groups, Brookston Clay is considered to be in the 'D' group. A CN value of 84 was used for stormwater management modelling of clay soil.

### ***2.3 Infrastructure***

#### ***Storm and Sanitary Sewer***

- The current use of land is vacant land hence the pre-development population density was not analyzed for sanitary.
- An existing 450mm combine storm and sanitary sewer is located within the Sandwich Street centerline. And an existing 250mm combine storm and sanitary sewer system within Chappell Avenue centerline.
- A 2400 trunk sewer pipe is located on the Chappell Avenue right-of-way.
- The municipal 450mm combine sewer having 0.5% slope and have capacity of approximately 201 liters per second.

## ***Watermain***

The existing watermain runs along Wyandotte Street and Crawford Avenue. The following are the findings:

- An existing 200mm watermain is located within right-of-way of Sandwich Street. Also, a 150mm watermain runs eastern side of Chappell Avenue.
- A 50mm service line is provided to property from the Sandwich Street.
- Two existing hydrants are located approximately 6m south and west of the development property line.
- Given the large diameter (200mm) of the municipal watermain, capacity should not be of the issue.

## **3. PROPOSED CONDITIONS**

The future development will consist of new 6-storey high-rise building, asphalt area, grass area and parking area. The proposed building will have 69 1-bed and 78 2-bed units stacked at the maximum height of 18m.

### ***4.4 Infrastructure***

#### ***Storm Sewer***

The storm servicing was analyzed as per criteria set by the City. Findings are summarized below and detail calculations are provided in Appendix B:

- The proposed minor storm outlet for the development of 150mm pipe is connected to the combined storm and sanitary sewer system on Sandwich Street.
- The post development peak flow for all events from the development will be controlled to 2-year pre-development conditions. Detailed calculations are provided in Stormwater Management section.
- Major storm flow will be contained in parking lot and grass area. Hence, the existing municipal sewers will not be adversely affected.

#### ***Sanitary Sewer***

The sanitary servicing will be provided from Chappell Avenue. Design and analyzed was completed as per criteria set by the City. Findings are summarized below and detail calculations are provided in Appendix B:

- The projected post-development equivalent population based on type of development flow shall be 525 people in accordance with the City of Windsor Design Manual.
- Based on population density, infiltration allowance, the proposed peak sanitary discharge flow is estimated as 0.010m/s (10.11l/s).



- To facilitate development of the condominium, a new 200mm diameter sanitary servicing connected will be provided from southwest corner of proposed building to the existing combined storm and sanitary sewer system located on the allowance of Sandwich Street. The designed sanitary servicing scheme has been illustrated in Appendix D.
- The proposed sewer will have 0.41% slope and have capacity of approximately 21 liters per second.

### **Watermain**

In order to serve the proposed development for domestic and fire water supply requirements, this development proposes to provide 150mm watermain from Chappell Avenue.

- The proposed watermain will split into two at the western side of building to service 150mm fire and 100mm domestic.
- The static pressure on the nearest hydrant on Sandwich Street to the development assumed to be 50-60 psi. Pressure test in progress and detail will be provided.
- An existing 50mm service line will be abandoned.

## **4. STORMWATER MANAGEMENT**

The stormwater management criteria for this development are in compliance with the requirements of the City of Windsor and the Essex Region Stormwater Management Standards Manual:

- Stormwater quantity controls are required for the site to control the proposed conditions peak flows from the site to the existing condition levels for 2, 5 and 100 year storm events which include the stress test flow.
- Water quality control are to be provided for the site to a "Normal Protection level" as per MOE (2003) guidelines.

### **4.5 Storm Quantity Control**

Referring to Figure 2, the study area will have 11-storey high-rise building, landscape area and new asphalt parking lot. Water quantity is addressed through parking lot storage and quality control is provided using Goss Gully Traps.

For areas 201, the 100-year rainfall flow will be storage in parking lot and allowable flow will be directed to Sandwich Street storm sewer through orifice pipe.

The 24-hour storm duration derived from Environment Canada website were used for the stormwater management modelling. The total depth of rainfall for the modelled storms are indicated in Table 1.



**Table 1: Rainfall Intensity**

Return Period	Storm Duration	Rainfall Depth
2 Year	24 hour	53.4 mm
5 Year	24 hour	68.0 mm
100 year	24 Hour	107.90 mm
100 year (Stress Test)	24 Hour	150.00 mm

The existing and proposed conditions have been modeled using Hydraflow Hydrographs Extension for Civil 3D. The program computes runoff hydrograph using the SCS (Soil Conservation Service) 24-hour storms.

Based on soil condition, the Hydrologic soil group fall into category 'C'. For the modelling, the CN value of 98 is used for impervious area, paved roads and roof area. The modelling variable are summarized in Table 2.

**Table 2: Hydrograph Modelling Variables**

Catchment ID	Description	Area (ha)	Gradient	Coefficient /Curve No
<b>PRE-DEVELOPMENT CONDITION</b>				
101	Drain to Sandwich St	0.669ha	0.3	C 0.5
<b>POST DEVELOPMENT CONDITION</b>				
201	Building/ Driveway/grass areas	0.669ha	0.3	CN 95

For area 201, the storage will be provided in the parking lot, and control will be provided through orifice pipes. The existing and proposed peak flows from the site are summarized on Table 3, and the modelling output is included in Appendix C.

**Table 3: Peak Discharges**

Return Event	Pre-Condition Flow (m <sup>3</sup> /s)	Total Post Condition Flow (m <sup>3</sup> /s)
2 Year	0.057	0.058
5 Year	<b>0.065* (0.075-0.01)</b>	0.058
100 Year	0.122	0.065
100 Year (Stress Test)	0.122 (100-yr)	0.066

Note: \* Allowable Release Rate = 5-yr release rate – sanitary release rate = 0.075-0.01 = 0.065m<sup>3</sup>/s

As indicted in Table 3, the proposed design results are no increase in the 5-year peak flow.

The parking lot ponding characteristic is provided in Table 4.

**Table 4: Ponding Characteristic**

Return Period	Area 201	Depth (m)	Elevation (m)*	Storage Volume (m <sup>3</sup> )
5 Year	Area 201	2.06	180.72	69.8
100 Year	Area 201	2.13	180.79	164
100 Year (Stress Test)	Area 201	2.17	180.83	276

Note: Maximum pond elevation 180.85; Bottom of Pond 178.66; Maximum capacity 309m<sup>3</sup>

All minor and major flow will be controlled to the 5-year predevelopment condition (0.06m<sup>3</sup>/s) using the proposed 148mm diameter orifice plate. The 100-year water level set at 180.79m throughout the development, hence, maximum ponding within development is 190mm which is lower than allowable ponding i.e. 300mm.

The building floor elevation is 181.133m therefore no surface water will enter into ground parking under the building.

#### **4.6 Storm Quality Control**

The site will have Armtec Defender water quality unit (FD5HC) which is sufficient enough to remove long term suspended solids removal requirement. Based on results, the unit will have removal efficiency of 74.4% of the suspended. This exceed Normal long-term suspended solids removal requirement (70 percent TSS removal efficient for 85 percent of annual flow). Details of unit are provided in Appendix D.

### **5. Erosion and Sediment Control**

The erosion and sediment control measures for the site will be indicated on the tender documents, and include the following:

- Silt fence is to be erected before grading begins on the property to protect downstream areas from migration of sediment in overland flow;
- Filter fabric will be placed over the drainage grates; and
- All disturbed areas will be stabilized by restoration of vegetative ground cover as soon as possible.

Additional erosion and sediment control notes will be provided on the tender documents.

### **6. CONCLUSION**

This functional servicing report is to be read in conjunction with the submission material. The report presents municipal servicing details, proposed servicing and stormwater management plan for the proposed High-Rise Condominium development in the City of Windsor.



Furthermore, the report demonstrates that approximate stormwater management measure will be provided to satisfy water quality treatment and quantity attenuation criteria. The sanitary service and water supply for the proposed development through existing infrastructure along Sandwich Street and Chappell Avenue.

Based on our investigation, we conclude and recommend the following:

**Sanitary** – a new 200mm diameter sanitary service will be provided to the development from the existing municipal sewer from Chappell Avenue.

**Watermain** – one new 150mm diameter water service will be provided to the development from the existing 200mm watermain on Chappell Avenue. The water line will split into two at the eastern façade of building for 150mm fire and 100mm diameter domestic service.

**Storm** – the post-development peak flows from all events from the site will be controlled to the peak flow from target pre-development conditions. Whereas, during 100-year storm event the maximum water depth is less than 300mm.

Stormwater quality and quantity is addressed using Armtec Defender water quality unit (FD5HC) and 150mm orifice pipe at MH 4.

**Erosion and Sediment Control** – control measures are to be implemented during construction and detail will be provided in the tender documents.

We trust the foregoing is satisfactory, and will allow you to review and approve the functional servicing report and engineering drawings for this development. If you have any questions or require additional information please do not hesitate to contact the undersigned at your convenience.

All of which is respectfully submitted.

**BAIRD AE INC.**  
**27 PRINCESS STREET, UNIT 102**  
**LEAMINGTON, ONTARIO N8H 2X8**

Shurjeel Tunio, P.Eng.  
Senior Project Manager  
**Baird AE**



## Appendix A

---

### BACKGROUND INFORMATION









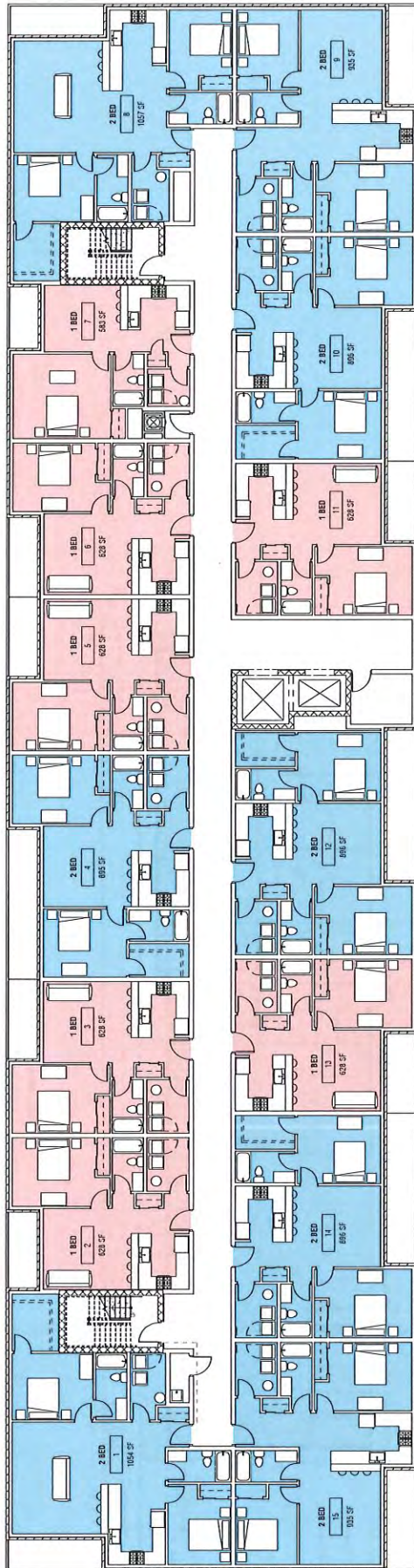




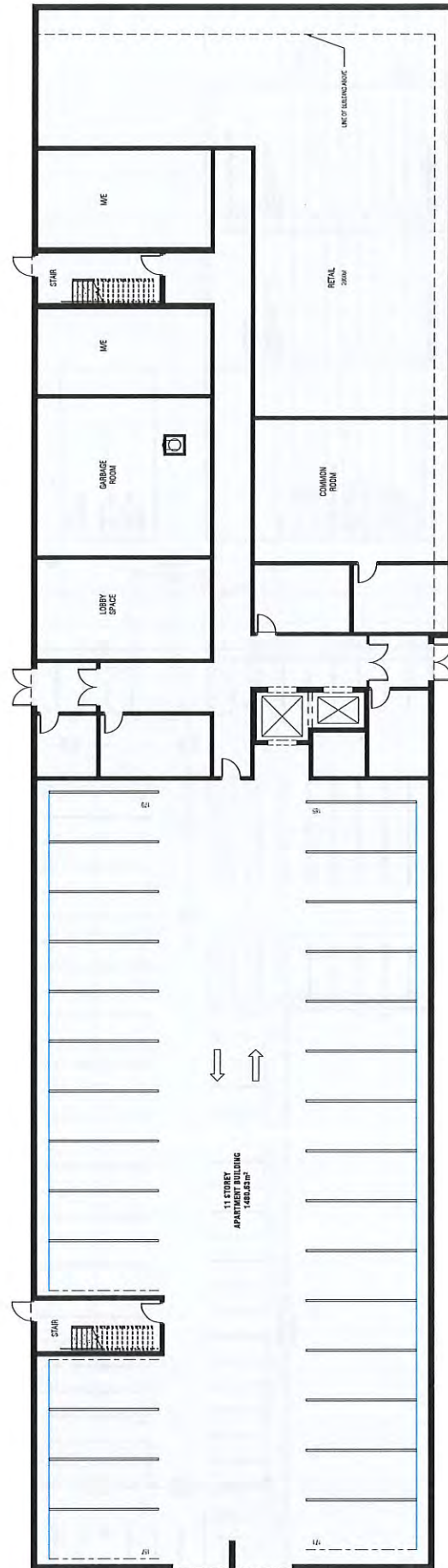




ROOM TYPES  
 1 BD  
 2 BD



2 TYPICAL FLOOR PLAN 2-11  
 1/8" = 1'-0"



1 GROUND FLOOR PLAN  
 1/8" = 1'-0"



	idf_v2-3_2014_12_21_613_ON_6139525_WINDSOR_A								
1967	13.7	20.1	21.8	24.9	26.4	26.4	41.1	62.2	72.6
1968	10.2	15.2	17.5	25.9	36.8	38.1	60.2	77.2	78.2
1969	11.2	19.0	20.8	27.7	27.7	36.1	57.1	57.1	57.1
1970	9.4	12.2	16.5	17.0	22.6	29.5	29.7	36.6	39.6
1971	9.4	17.8	25.7	35.1	35.1	35.1	43.9	43.9	43.9
1972	9.9	12.2	14.5	19.6	25.9	28.7	31.0	31.2	39.1
1973	12.7	18.0	22.6	27.9	30.0	30.2	33.5	37.8	40.4
1974	15.0	26.2	39.4	41.1	45.0	49.5	49.8	49.8	49.8
1975	8.4	14.2	21.1	24.9	25.1	31.7	40.6	44.7	52.8
1976	9.1	13.2	14.0	16.5	22.9	27.4	34.3	35.3	40.4
1977	7.1	10.9	14.0	16.5	25.4	29.2	29.2	31.7	41.4
1978	8.8	10.5	11.8	20.4	21.1	21.1	22.5	28.6	31.0
1979	10.4	16.6	24.9	32.0	48.3	52.6	55.3	60.8	61.2
1980	14.1	17.2	25.0	35.6	45.3	45.6	46.3	79.8	80.0
1981	-99.9	16.7	23.3	26.0	32.0	45.6	77.3	81.7	92.3
1982	7.7	10.7	13.2	18.9	27.3	28.3	28.3	40.4	49.9
1983	15.0	16.5	22.1	32.3	38.7	45.4	62.1	62.1	82.0
1984	6.0	8.8	11.3	17.5	17.7	21.0	32.6	34.9	37.2
1985	11.2	13.3	13.9	18.7	24.6	39.6	58.4	59.2	59.2
1986	8.0	12.7	18.0	19.4	20.7	32.0	37.7	47.7	88.6
1987	11.9	16.5	19.8	24.5	29.9	36.1	39.1	41.6	52.8
1988	7.0	8.8	12.5	12.7	12.9	14.4	28.0	32.3	33.0
1989	7.4	11.9	17.5	21.2	27.0	36.3	48.3	61.7	71.8
1990	11.4	16.0	18.8	20.5	22.4	26.4	41.9	52.2	70.6
1991	5.6	9.6	12.9	25.7	37.2	40.5	40.5	40.7	43.2
1992	6.5	9.8	12.0	16.9	25.7	29.8	34.4	34.4	45.8
1993	7.0	9.6	10.5	11.2	17.2	23.9	28.7	30.6	44.7
1994	8.3	11.3	14.6	23.8	30.0	43.2	51.3	51.5	80.7
1995	9.7	17.2	24.3	40.5	56.7	58.9	63.0	63.0	63.6
1996	13.5	15.4	16.8	18.7	18.7	19.1	40.2	40.4	46.3
1997	7.9	11.5	15.6	17.5	21.8	30.6	38.2	39.9	41.7
1998	7.3	12.7	13.9	15.7	16.4	26.8	31.4	36.2	57.4
1999	9.3	13.3	16.5	20.8	21.0	22.2	23.4	24.8	29.8
2000	7.6	11.2	13.1	20.4	26.4	31.0	51.8	89.0	94.6
2001	6.1	10.2	12.2	12.8	14.3	17.2	24.1	38.1	48.4
2002	6.9	9.1	10.8	14.4	17.2	17.4	29.6	31.7	43.2
2003	7.2	10.0	12.2	14.4	14.8	14.8	22.7	33.5	34.6
2004	13.3	15.7	18.6	20.4	22.1	33.2	35.8	37.3	53.7
2005	10.5	16.9	24.0	25.8	26.0	26.0	29.8	30.6	41.2
2006	10.6	18.3	23.6	26.6	35.7	51.3	53.1	53.3	66.9
2007	8.0	15.1	18.7	30.9	48.6	48.8	50.4	55.8	57.6
<hr/>									
# Yrs.	60	61	61	61	61	61	61	61	61
Années									
Mean	9.9	14.4	17.9	23.5	29.2	34.7	42.8	48.8	56.1
Moyenne									
Std. Dev.	2.9	4.0	5.6	8.1	10.6	11.4	13.8	15.7	16.5
Écart-type									



idf\_v2-3\_2014\_12\_21\_613\_ON\_6139525\_WINDSOR\_A

Skew.	0.96	0.47	0.99	0.81	0.85	0.42	1.23	1.07	0.74
Dissymétrie									
Kurtosis	4.27	2.90	4.84	3.21	3.19	2.88	5.84	4.28	3.13

\*-99.9 Indicates Missing Data/Données manquantes

Warning: annual maximum amount greater than 100-yr return period amount

Avertissement : la quantité maximale annuelle excède la quantité  
pour une période de retour de 100 ans

Year/Année	Duration/Durée	Data/Données	100-yr/ans
1953	5 min	20.1	18.9
1957	6 h	96.8	86.2
1957	12 h	100.3	98.0
1974	15 min	39.4	35.6

\*\*\*\*\*

Table 2a : Return Period Rainfall Amounts (mm)  
Quantité de pluie (mm) par période de retour

\*\*\*\*\*

Duration/Durée	2	5	10	25	50	100	#Years
	yr/ans	yr/ans	yr/ans	yr/ans	yr/ans	yr/ans	Années
5 min	9.4	11.9	13.6	15.8	17.4	18.9	60
10 min	13.7	17.2	19.5	22.5	24.6	26.8	61
15 min	17.0	22.0	25.3	29.5	32.5	35.6	61
30 min	22.2	29.4	34.1	40.1	44.6	49.0	61
1 h	27.5	36.9	43.1	50.9	56.7	62.5	61
2 h	32.8	42.9	49.6	58.1	64.3	70.6	61
6 h	40.5	52.8	60.8	71.1	78.6	86.2	61
12 h	46.2	60.1	69.2	80.8	89.4	98.0	61
24 h	53.4	68.0	77.6	89.8	98.9	107.9	61

\*\*\*\*\*

Table 2b :

Return Period Rainfall Rates (mm/h) - 95% Confidence limits

Intensité de la pluie (mm/h) par période de retour - Limites de confiance de 95%

\*\*\*\*\*

Duration/Durée	2	5	10	25	50	100	#Years
	yr/ans	yr/ans	yr/ans	yr/ans	yr/ans	yr/ans	Années
5 min	112.5	143.2	163.5	189.2	208.2	227.1	60
	+/- 8.1	+/- 13.6	+/- 18.3	+/- 24.7	+/- 29.6	+/- 34.5	60
10 min	82.3	103.3	117.3	134.8	147.9	160.8	61



idf_v2-3_2014_12_21_613_ON_6139525_WINDSOR_A							
	+/-	5.5	+/-	9.2	+/-	12.5	+/- 16.8 +/- 20.1 +/- 23.4 61
15 min		68.0		87.9		101.1	117.8 130.2 142.5 61
	+/-	5.2	+/-	8.8	+/-	11.8	+/- 16.0 +/- 19.1 +/- 22.2 61
30 min		44.4		58.7		68.2	80.2 89.1 98.0 61
	+/-	3.7	+/-	6.3	+/-	8.5	+/- 11.5 +/- 13.7 +/- 16.0 61
1 h		27.5		36.9		43.1	50.9 56.7 62.5 61
	+/-	2.4	+/-	4.1	+/-	5.6	+/- 7.5 +/- 9.0 +/- 10.4 61
2 h		16.4		21.5		24.8	29.0 32.2 35.3 61
	+/-	1.3	+/-	2.2	+/-	3.0	+/- 4.0 +/- 4.8 +/- 5.6 61
6 h		6.8		8.8		10.1	11.8 13.1 14.4 61
	+/-	0.5	+/-	0.9	+/-	1.2	+/- 1.6 +/- 1.9 +/- 2.3 61
12 h		3.9		5.0		5.8	6.7 7.5 8.2 61
	+/-	0.3	+/-	0.5	+/-	0.7	+/- 0.9 +/- 1.1 +/- 1.3 61
24 h		2.2		2.8		3.2	3.7 4.1 4.5 61
	+/-	0.2	+/-	0.3	+/-	0.4	+/- 0.5 +/- 0.6 +/- 0.7 61

\*\*\*\*\*

Table 3 : Interpolation Equation / Équation d'interpolation:  $R = A \cdot T^B$

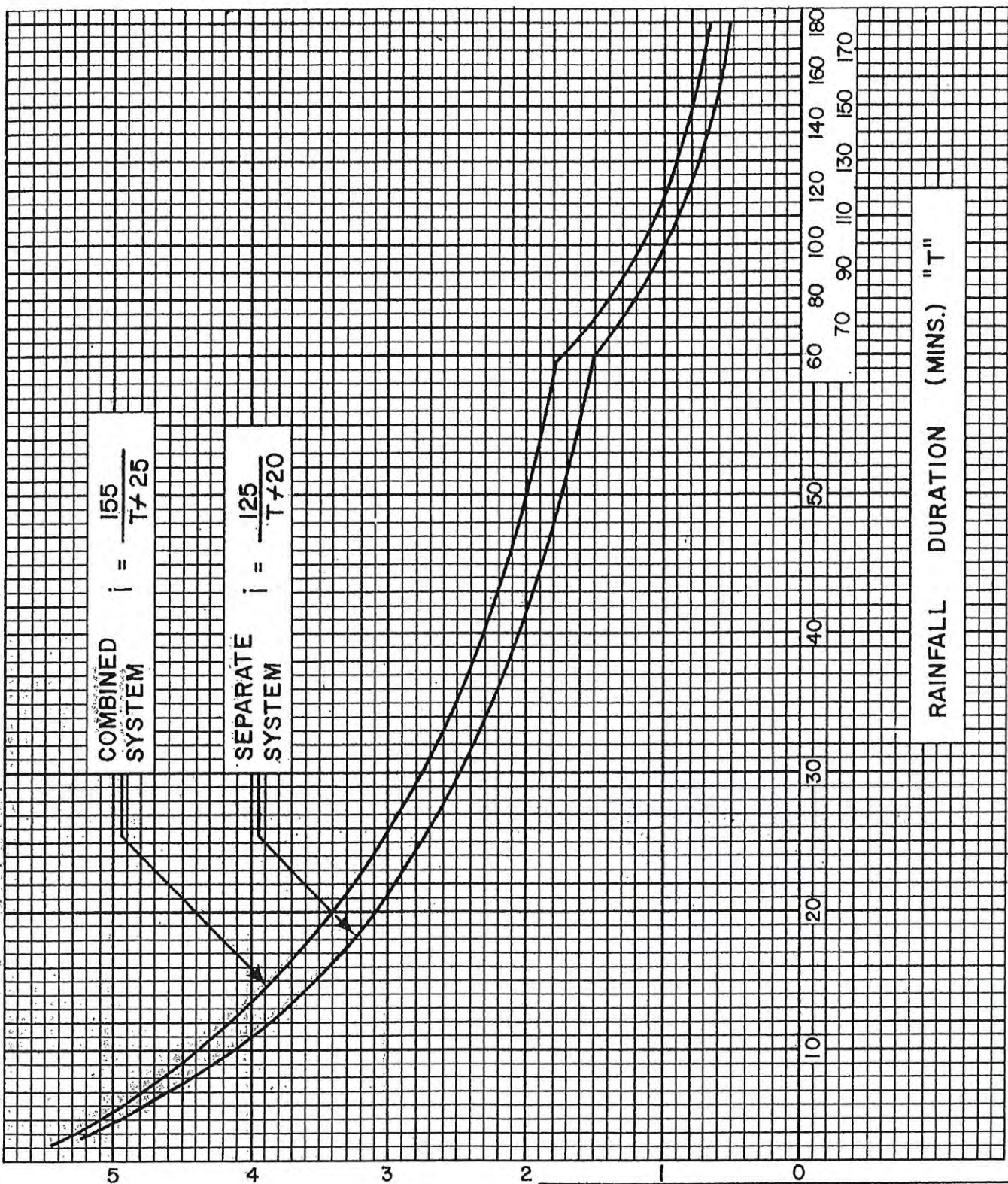
R = Interpolated Rainfall rate (mm/h)/Intensité interpolée de la pluie (mm/h)

RR = Rainfall rate (mm/h) / Intensité de la pluie (mm/h)

T = Rainfall duration (h) / Durée de la pluie (h)

\*\*\*\*\*

Statistics/Statistiques	2	5	10	25	50	100
	yr/ans	yr/ans	yr/ans	yr/ans	yr/ans	yr/ans
Mean of RR/Moyenne de RR	40.4	52.0	59.7	69.4	76.6	83.7
Std. Dev. /Écart-type (RR)	39.4	49.9	56.9	65.8	72.3	78.9
Std. Error/Erreur-type	10.8	15.0	17.8	21.3	24.0	26.6
Coefficient (A)	24.0	31.0	35.7	41.7	46.0	50.4
Exponent/Exposant (B)	-0.710	-0.709	-0.708	-0.707	-0.707	-0.706
Mean % Error/% erreur moyenne	9.9	11.4	12.0	12.6	13.0	13.3



RAINFALL INTENSITY  
 (INS./HR.) "I"

CITY OF WINDSOR DEPARTMENT OF PUBLIC WORKS	
RAINFALL INTENSITY CURVE	
DR'N BY : E. STERGIOU	DATE : JUNE 11 / 1975
REVISION :	CH'KD BY : <i>SA</i>
CH'KD BY : <i>PM</i>	PASSED BY :
<i>H. S. 2022</i> COMMISSIONER OF WORKS	
AS-105	

## Appendix B

---

### STORM AND SANITARY DESIGN SHEET



# Hydraflow Table of Contents

Hydrograph 10082020.gpw

Hydraflow Hydrographs Extension for Autodesk® Civil 3D® by Autodesk, Inc. v2020

Thursday, 10 / 8 / 2020

<b>Watershed Model Schematic.....</b>	<b>1</b>
<b>2 - Year</b>	
<b>Summary Report.....</b>	<b>2</b>
<b>Hydrograph Reports.....</b>	<b>3</b>
Hydrograph No. 1, Rational, Pre-Development 101.....	3
TR-55 Tc Worksheet.....	4
Hydrograph No. 2, SCS Runoff, Post-Development-201.....	5
Hydrograph No. 3, Reservoir, Parking Lot.....	6
Pond Report - Parking Lot.....	7
<b>5 - Year</b>	
<b>Summary Report.....</b>	<b>8</b>
<b>Hydrograph Reports.....</b>	<b>9</b>
Hydrograph No. 1, Rational, Pre-Development 101.....	9
Hydrograph No. 2, SCS Runoff, Post-Development-201.....	10
Hydrograph No. 3, Reservoir, Parking Lot.....	11
<b>100 - Year</b>	
<b>Summary Report.....</b>	<b>12</b>
<b>Hydrograph Reports.....</b>	<b>13</b>
Hydrograph No. 1, Rational, Pre-Development 101.....	13
Hydrograph No. 2, SCS Runoff, Post-Development-201.....	14
Hydrograph No. 3, Reservoir, Parking Lot.....	15
<b>IDF Report.....</b>	<b>16</b>

# Hydraflow Table of Contents

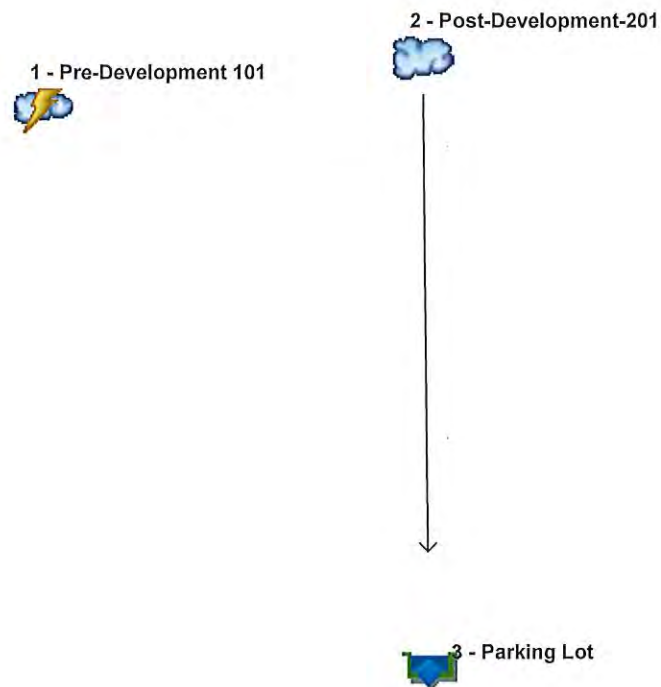
Hydrograph 10082020.gpw

Hydraflow Hydrographs Extension for Autodesk® Civil 3D® by Autodesk, Inc. v2020

Thursday, 10 / 8 / 2020

<b>Watershed Model Schematic.....</b>	<b>1</b>
<b>2 - Year</b>	
<b>Summary Report.....</b>	<b>2</b>
<b>Hydrograph Reports.....</b>	<b>3</b>
Hydrograph No. 1, Rational, Pre-Development 101.....	3
TR-55 Tc Worksheet.....	4
Hydrograph No. 2, SCS Runoff, Post-Development-201.....	5
Hydrograph No. 3, Reservoir, Parking Lot.....	6
Pond Report - Parking Lot.....	7
<b>5 - Year</b>	
<b>Summary Report.....</b>	<b>8</b>
<b>Hydrograph Reports.....</b>	<b>9</b>
Hydrograph No. 1, Rational, Pre-Development 101.....	9
Hydrograph No. 2, SCS Runoff, Post-Development-201.....	10
Hydrograph No. 3, Reservoir, Parking Lot.....	11
<b>100 - Year</b>	
<b>Summary Report.....</b>	<b>12</b>
<b>Hydrograph Reports.....</b>	<b>13</b>
Hydrograph No. 1, Rational, Pre-Development 101.....	13
Hydrograph No. 2, SCS Runoff, Post-Development-201.....	14
Hydrograph No. 3, Reservoir, Parking Lot.....	15
<b>IDF Report.....</b>	<b>16</b>

## Hydraflow Hydrographs Extension for Autodesk® Civil 3D® by Autodesk, Inc. v2020







# Hydrograph Report

Hydraflow Hydrographs Extension for Autodesk® Civil 3D® by Autodesk, Inc. v2020

Thursday, 10 / 8 / 2020

## Hyd. No. 1

Pre-Development 101

Hydrograph type	= Rational	Peak discharge	= 0.057 cms
Storm frequency	= 2 yrs	Time to peak	= 17 min
Time interval	= 1 min	Hyd. volume	= 58.5 cum
Drainage area	= 0.670 hectare	Runoff coeff.	= 0.49*
Intensity	= 63.452 mm/hr	Tc by TR55	= 17.00 min
IDF Curve	= Windsor A 2007.IDF	Asc/Rec limb fact	= 1/1

\* Composite (Area/C) =  $[(0.320 \times 0.70) + (0.350 \times 0.30)] / 0.670$



# TR55 Tc Worksheet

Hydraflow Hydrographs Extension for Autodesk® Civil 3D® by Autodesk, Inc. v2020

## Hyd. No. 1

Pre-Development 101

<u>Description</u>	<u>A</u>	<u>B</u>	<u>C</u>	<u>Totals</u>
<b>Sheet Flow</b>				
Manning's n-value	= 0.026	0.011	0.011	
Flow length (m)	= 91.0	0.0	0.0	
Two-year 24-hr precip. (mm)	= 53.40	0.00	0.00	
Land slope (%)	= 0.30	0.00	0.00	
<b>Travel Time (min)</b>	<b>= 15.24</b>	<b>+</b> <b>0.00</b>	<b>+</b> <b>0.00</b>	<b>= 15.24</b>
<b>Shallow Concentrated Flow</b>				
Flow length (m)	= 34.00	0.00	0.00	
Watercourse slope (%)	= 0.30	0.00	0.00	
Surface description	= Paved	Paved	Paved	
Average velocity (m/s)	=0.34	0.00	0.00	
<b>Travel Time (min)</b>	<b>= 1.67</b>	<b>+</b> <b>0.00</b>	<b>+</b> <b>0.00</b>	<b>= 1.67</b>
<b>Channel Flow</b>				
X sectional flow area (sqm)	= 0.00	0.00	0.00	
Wetted perimeter (m)	= 0.00	0.00	0.00	
Channel slope (%)	= 0.00	0.00	0.00	
Manning's n-value	= 0.015	0.015	0.015	
Velocity (m/s)	=0.00	0.00	0.00	
Flow length (m)	{0}0.0	0.0	0.0	
<b>Travel Time (min)</b>	<b>= 0.00</b>	<b>+</b> <b>0.00</b>	<b>+</b> <b>0.00</b>	<b>= 0.00</b>
<b>Total Travel Time, Tc .....</b>				<b>17.00 min</b>

# Hydrograph Report

Hydraflow Hydrographs Extension for Autodesk® Civil 3D® by Autodesk, Inc. v2020

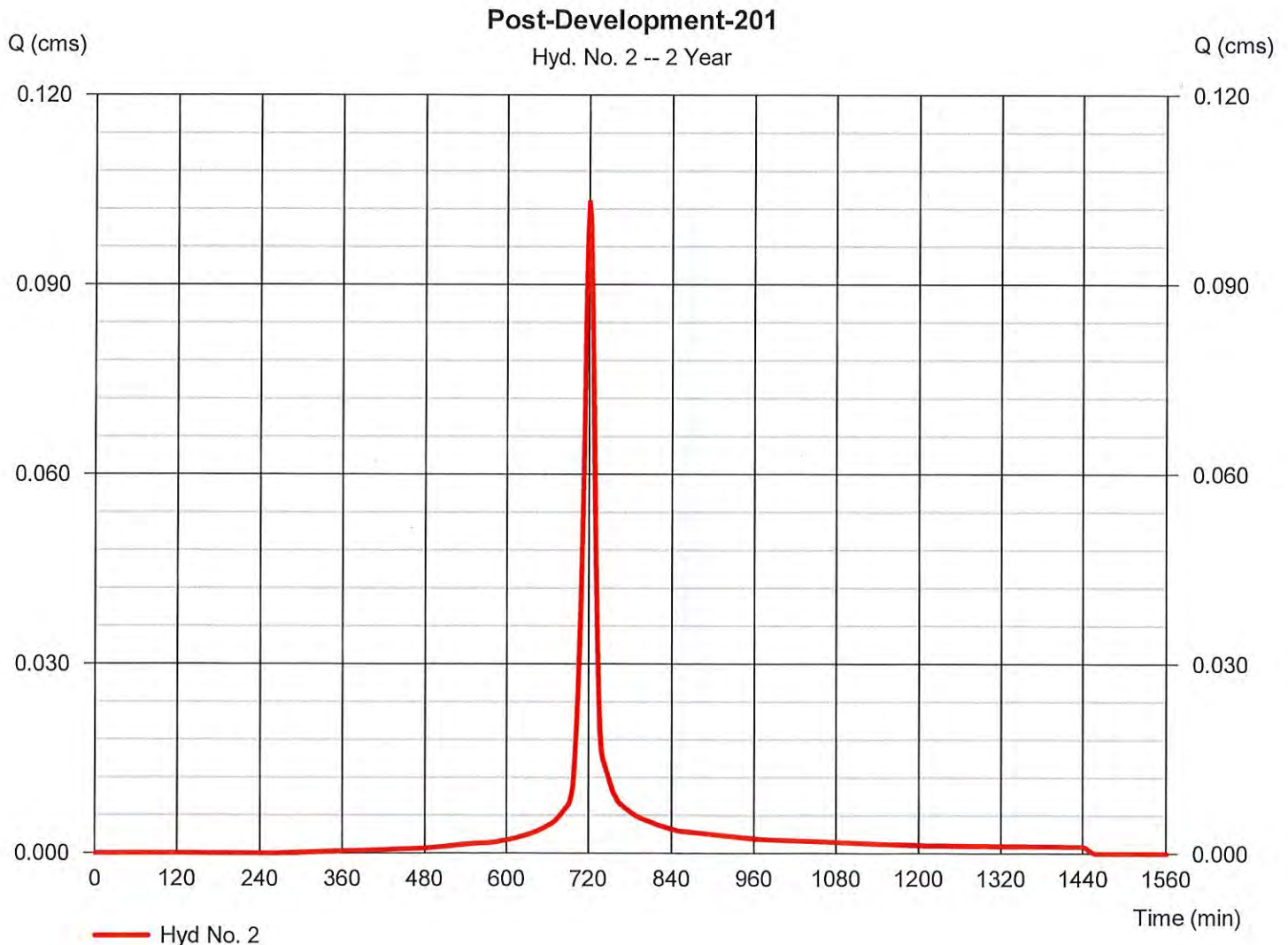
Thursday, 10 / 8 / 2020

## Hyd. No. 2

Post-Development-201

Hydrograph type	= SCS Runoff	Peak discharge	= 0.103 cms
Storm frequency	= 2 yrs	Time to peak	= 720 min
Time interval	= 2 min	Hyd. volume	= 277.0 cum
Drainage area	= 0.669 hectare	Curve number	= 95*
Basin Slope	= 0.0 %	Hydraulic length	= 0 m
Tc method	= User	Time of conc. (Tc)	= 11.62 min
Total precip.	= 53.40 mm	Distribution	= Type II
Storm duration	= 24 hrs	Shape factor	= 484

\* Composite (Area/CN) =  $[(0.120 \times 80) + (0.507 \times 98)] / 0.669$



# Hydrograph Report

Hydraflow Hydrographs Extension for Autodesk® Civil 3D® by Autodesk, Inc. v2020

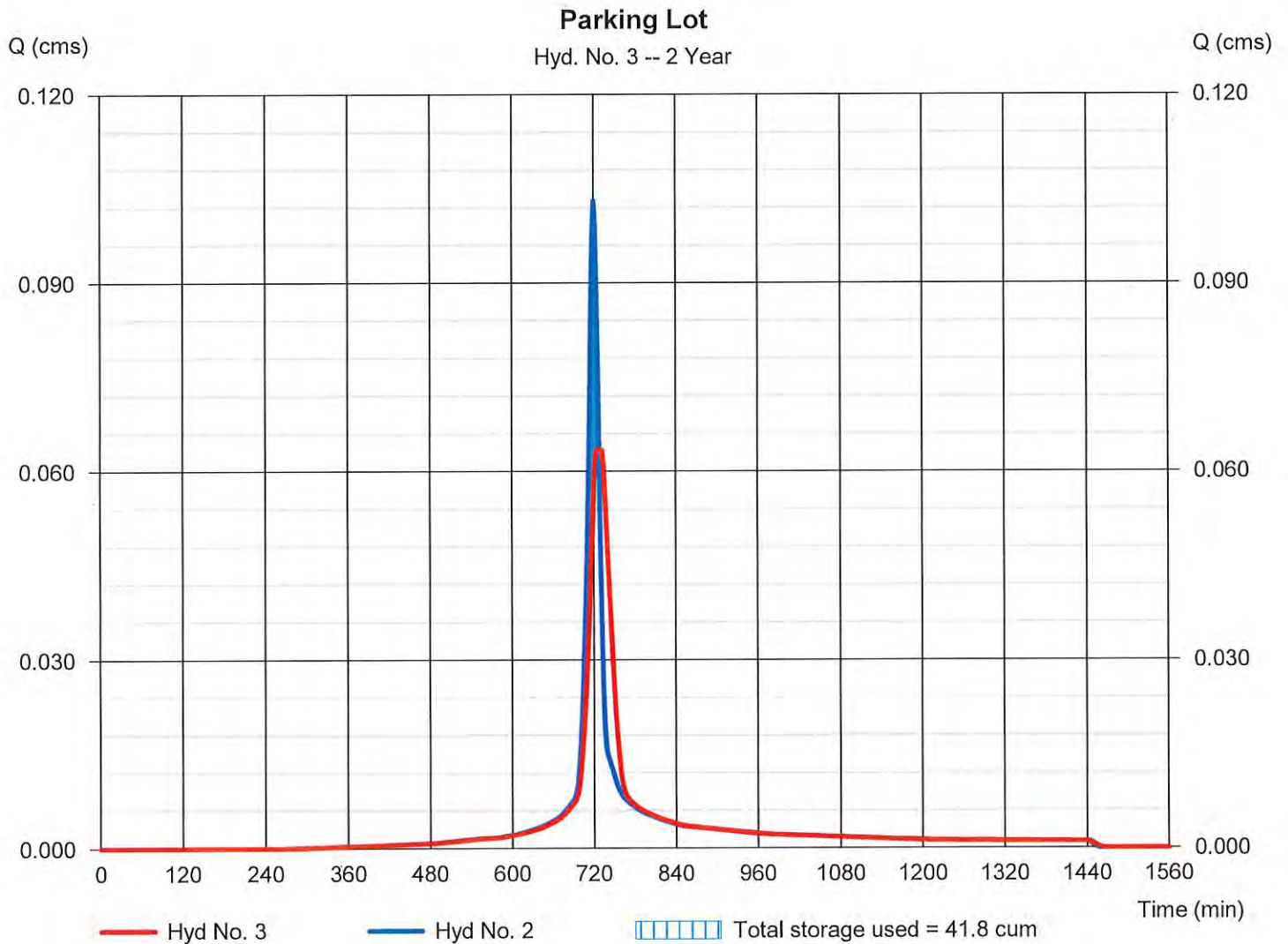
Thursday, 10 / 8 / 2020

## Hyd. No. 3

### Parking Lot

Hydrograph type	= Reservoir	Peak discharge	= 0.064 cms
Storm frequency	= 2 yrs	Time to peak	= 728 min
Time interval	= 2 min	Hyd. volume	= 277.0 cum
Inflow hyd. No.	= 2 - Post-Development-201	Max. Elevation	= 180.66 m
Reservoir name	= Parking Lot	Max. Storage	= 41.8 cum

Storage Indication method used.





# Pond Report

7

Hydraflow Hydrographs Extension for Autodesk® Civil 3D® by Autodesk, Inc. v2020

Thursday, 10 / 8 / 2020

## Pond No. 1 - Parking Lot

### Pond Data

Pond storage is based on user-defined values.

### Stage / Storage Table

Stage (m)	Elevation (m)	Contour area (sqm)	Incr. Storage (cum)	Total storage (cum)
0.00	178.66	n/a	0.0	0.0
1.94	180.60	n/a	35.4	35.4
1.99	180.65	n/a	1.6	37.0
2.04	180.70	n/a	17.7	54.7
2.09	180.75	n/a	45.8	100.6
2.14	180.80	n/a	89.2	189.7
2.19	180.85	n/a	153.0	342.8

### Culvert / Orifice Structures

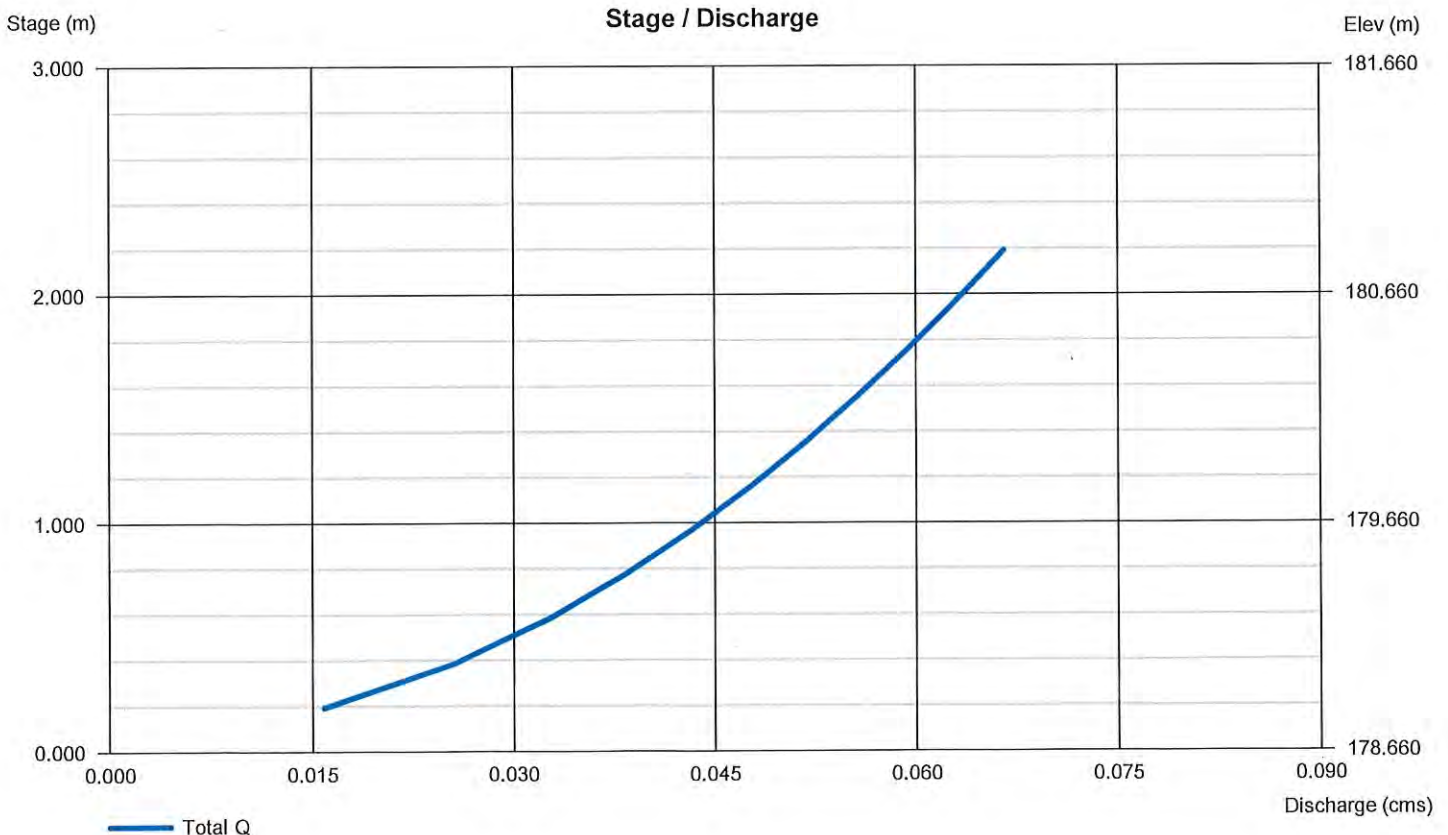
	[A]	[B]	[C]	[PrfRsr]
Rise (mm)	= 148.00	0.00	0.00	0.00
Span (mm)	= 148.00	0.00	0.00	0.00
No. Barrels	= 1	0	0	0
Invert El. (m)	= 178.660	0.000	0.000	0.000
Length (m)	= 0.000	0.000	0.000	0.000
Slope (%)	= 1.00	0.00	0.00	n/a
N-Value	= .013	.013	.013	n/a
Orifice Coeff.	= 0.60	0.60	0.60	0.60
Multi-Stage	= n/a	No	No	No

(by Wet area)

### Weir Structures

	[A]	[B]	[C]	[D]
Crest Len (m)	= 0.000	0.000	0.000	0.000
Crest El. (m)	= 0.000	0.000	0.000	0.000
Weir Coeff.	= 3.33	3.33	3.33	3.33
Weir Type	= ---	---	---	---
Multi-Stage	= No	No	No	No
Exfil.(cm/hr)	= 0.000			
TW Elev. (m)	= 0.00			

Note: Culvert/Orifice outflows are analyzed under inlet (ic) and outlet (oc) control. Weir risers checked for orifice conditions (ic) and submergence (s).







# Hydrograph Report

Hydraflow Hydrographs Extension for Autodesk® Civil 3D® by Autodesk, Inc. v2020

Thursday, 10 / 8 / 2020

## Hyd. No. 1

Pre-Development 101

Hydrograph type	= Rational	Peak discharge	= 0.075 cms
Storm frequency	= 5 yrs	Time to peak	= 17 min
Time interval	= 1 min	Hyd. volume	= 76.2 cum
Drainage area	= 0.670 hectare	Runoff coeff.	= 0.49*
Intensity	= 82.621 mm/hr	Tc by TR55	= 17.00 min
IDF Curve	= Windsor A 2007.IDF	Asc/Rec limb fact	= 1/1

\* Composite (Area/C) =  $[(0.320 \times 0.70) + (0.350 \times 0.30)] / 0.670$



# Hydrograph Report

Hydraflow Hydrographs Extension for Autodesk® Civil 3D® by Autodesk, Inc. v2020

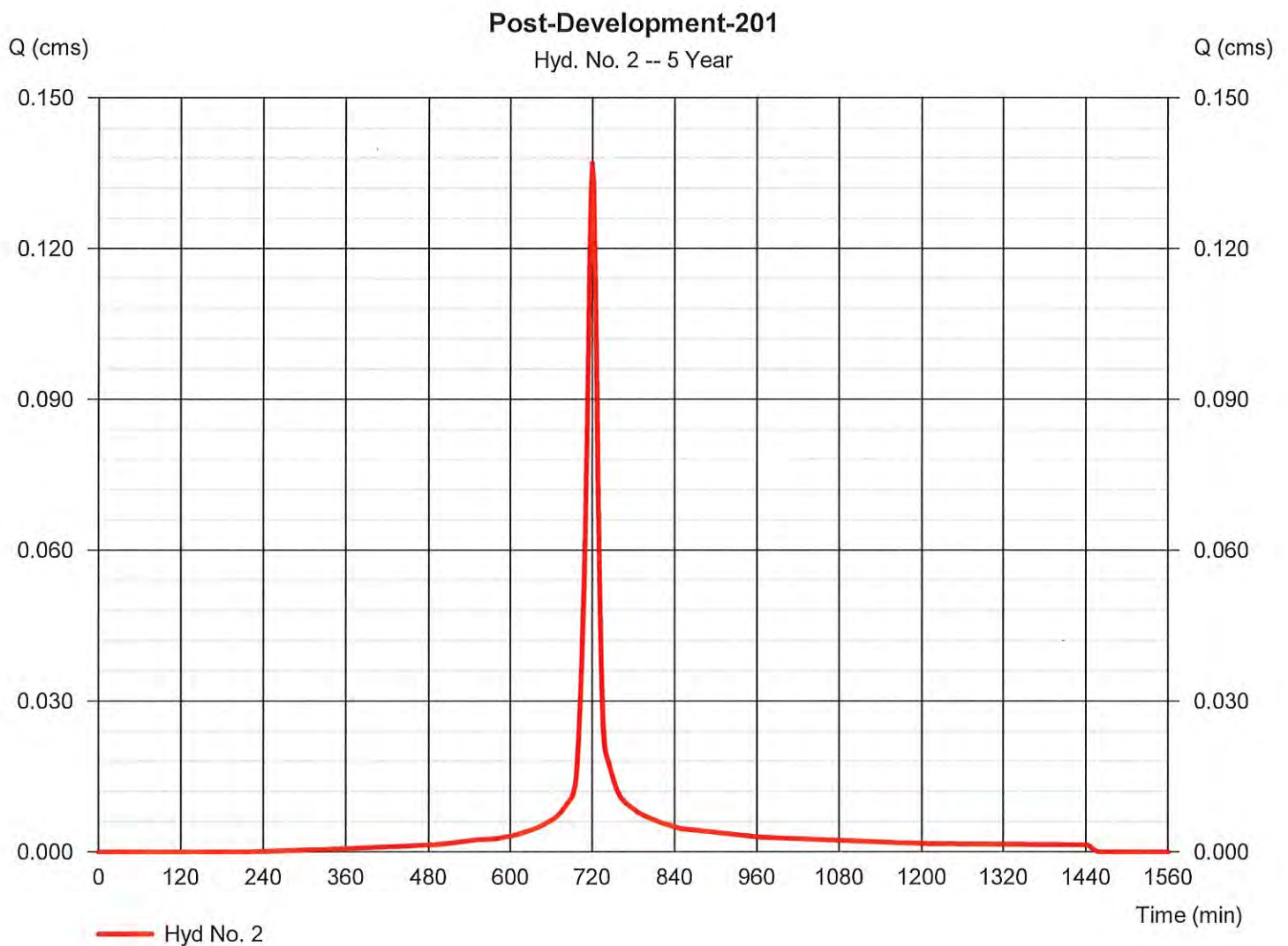
Thursday, 10 / 8 / 2020

## Hyd. No. 2

Post-Development-201

Hydrograph type	= SCS Runoff	Peak discharge	= 0.137 cms
Storm frequency	= 5 yrs	Time to peak	= 720 min
Time interval	= 2 min	Hyd. volume	= 374.1 cum
Drainage area	= 0.669 hectare	Curve number	= 95*
Basin Slope	= 0.0 %	Hydraulic length	= 0 m
Tc method	= User	Time of conc. (Tc)	= 11.62 min
Total precip.	= 68.00 mm	Distribution	= Type II
Storm duration	= 24 hrs	Shape factor	= 484

\* Composite (Area/CN) =  $[(0.120 \times 80) + (0.507 \times 98)] / 0.669$



# Hydrograph Report

Hydraflow Hydrographs Extension for Autodesk® Civil 3D® by Autodesk, Inc. v2020

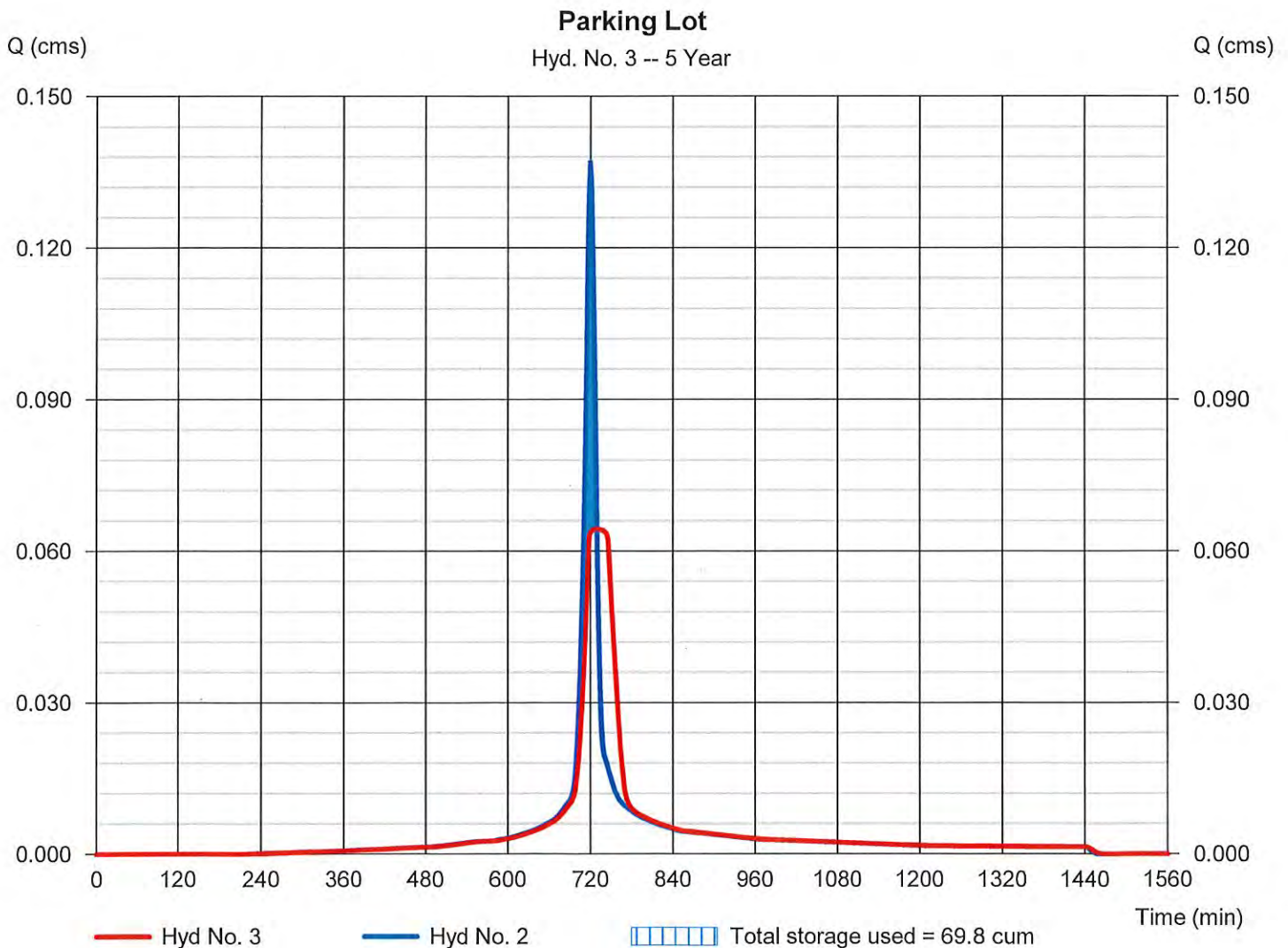
Thursday, 10 / 8 / 2020

## Hyd. No. 3

Parking Lot

Hydrograph type	= Reservoir	Peak discharge	= 0.064 cms
Storm frequency	= 5 yrs	Time to peak	= 730 min
Time interval	= 2 min	Hyd. volume	= 374.1 cum
Inflow hyd. No.	= 2 - Post-Development-201	Max. Elevation	= 180.72 m
Reservoir name	= Parking Lot	Max. Storage	= 69.8 cum

Storage Indication method used.



# Hydrograph Summary Report

Hydraflow Hydrographs Extension for Autodesk® Civil 3D® by Autodesk, Inc. v2020

Hyd. No.	Hydrograph type (origin)	Peak flow (cms)	Time interval (min)	Time to Peak (min)	Hyd. volume (cum)	Inflow hyd(s)	Maximum elevation (m)	Total strge used (cum)	Hydrograph Description
1	Rational	0.122	1	17	124.3	-----	-----	-----	Pre-Development 101
2	SCS Runoff	0.229	2	720	644.1	-----	-----	-----	Post-Development-201
3	Reservoir	0.065	2	732	644.1	2	180.79	5,801	Parking Lot
Hydrograph 10082020.gpw					Return Period: 100 Year			Thursday, 10 / 8 / 2020	



# Hydrograph Report

Hydraflow Hydrographs Extension for Autodesk® Civil 3D® by Autodesk, Inc. v2020

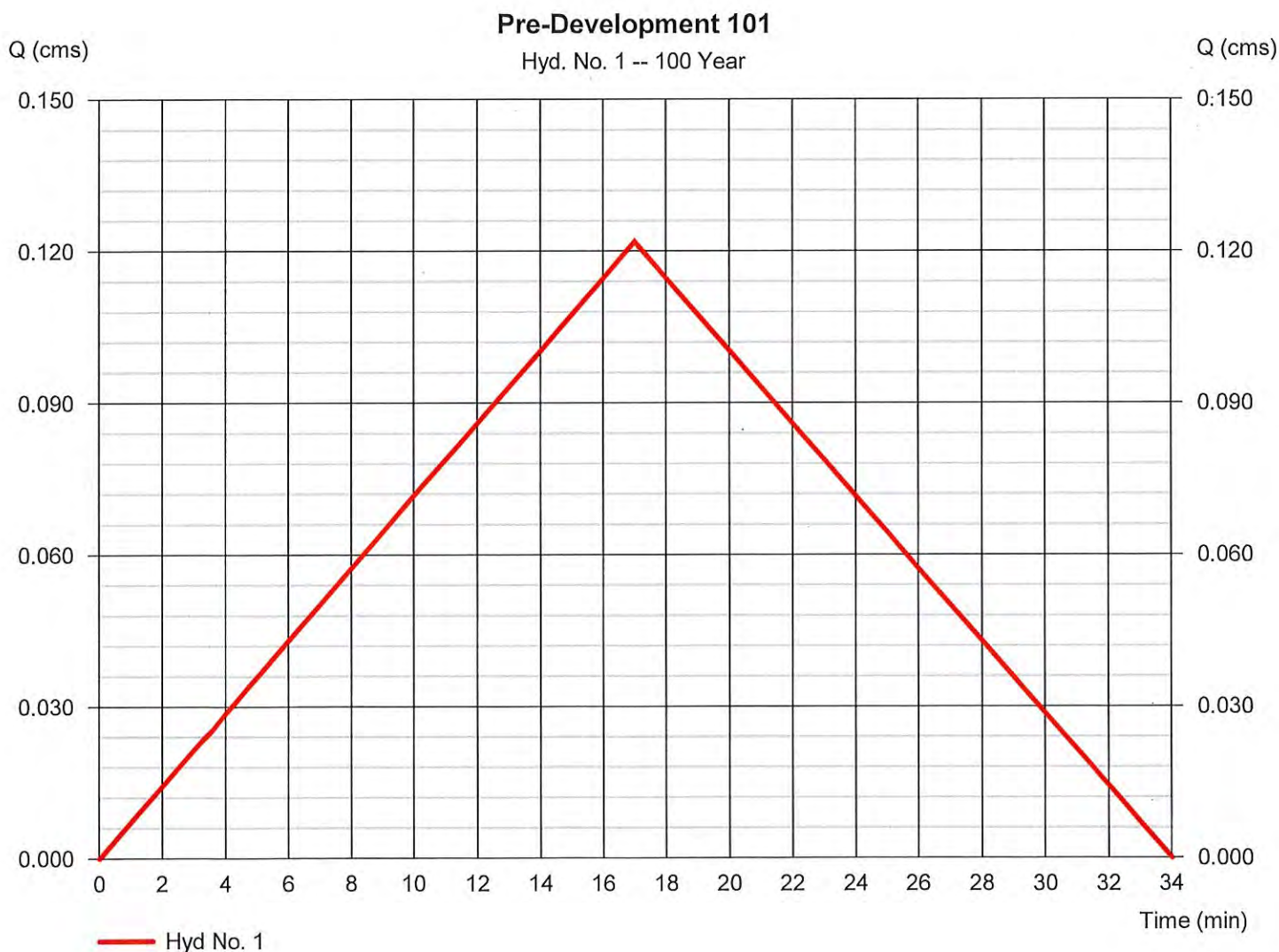
Thursday, 10 / 8 / 2020

## Hyd. No. 1

Pre-Development 101

Hydrograph type	= Rational	Peak discharge	= 0.122 cms
Storm frequency	= 100 yrs	Time to peak	= 17 min
Time interval	= 1 min	Hyd. volume	= 124.3 cum
Drainage area	= 0.670 hectare	Runoff coeff.	= 0.49*
Intensity	= 134.791 mm/hr	Tc by TR55	= 17.00 min
IDF Curve	= Windsor A 2007.IDF	Asc/Rec limb fact	= 1/1

\* Composite (Area/C) =  $[(0.320 \times 0.70) + (0.350 \times 0.30)] / 0.670$





# Hydrograph Report

Hydraflow Hydrographs Extension for Autodesk® Civil 3D® by Autodesk, Inc. v2020

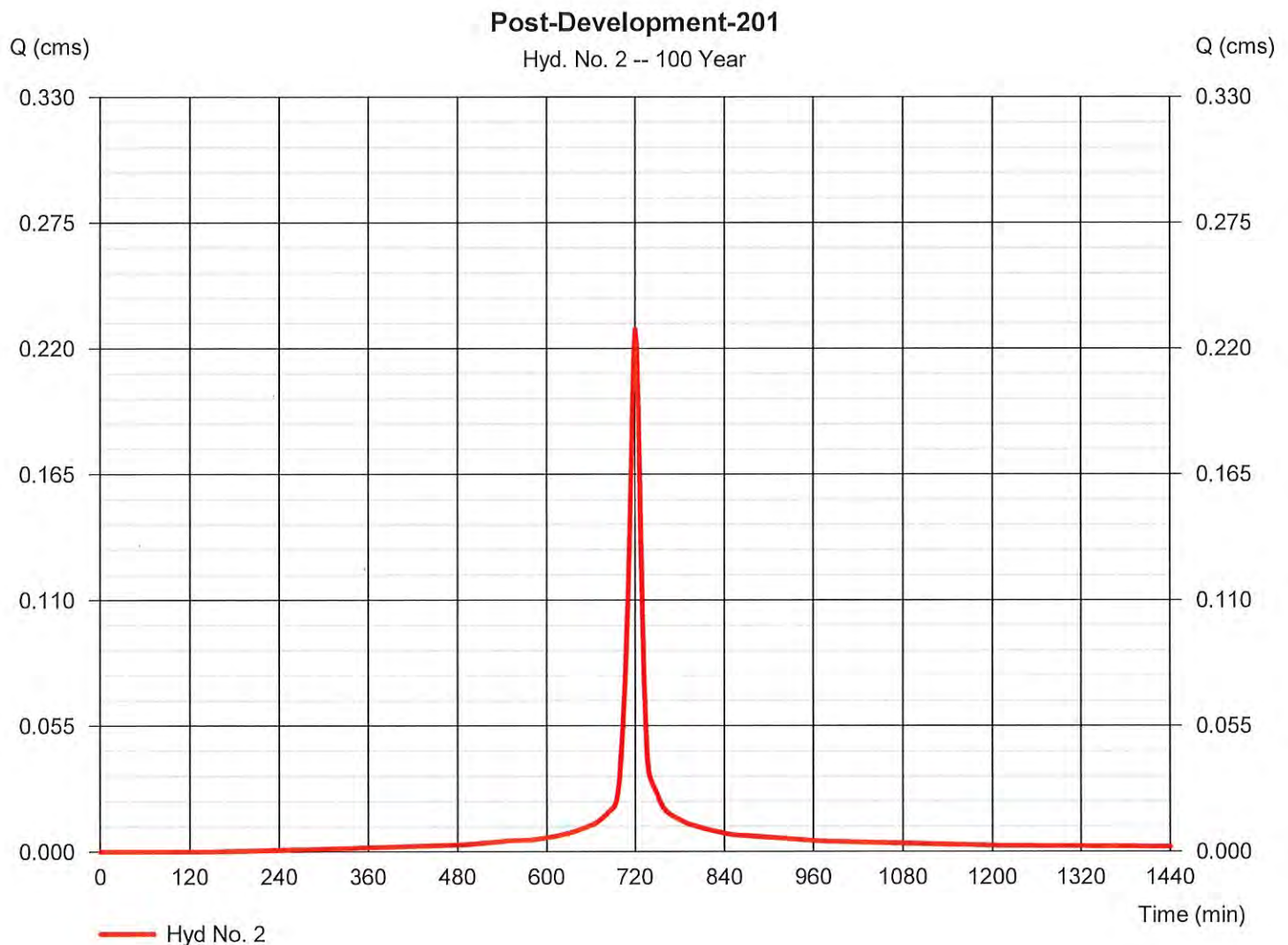
Thursday, 10 / 8 / 2020

## Hyd. No. 2

Post-Development-201

Hydrograph type	= SCS Runoff	Peak discharge	= 0.229 cms
Storm frequency	= 100 yrs	Time to peak	= 720 min
Time interval	= 2 min	Hyd. volume	= 644.1 cum
Drainage area	= 0.669 hectare	Curve number	= 95*
Basin Slope	= 0.0 %	Hydraulic length	= 0 m
Tc method	= User	Time of conc. (Tc)	= 11.62 min
Total precip.	= 107.90 mm	Distribution	= Type II
Storm duration	= 24 hrs	Shape factor	= 484

\* Composite (Area/CN) =  $[(0.120 \times 80) + (0.507 \times 98)] / 0.669$



# Hydrograph Report

Hydraflow Hydrographs Extension for Autodesk® Civil 3D® by Autodesk, Inc. v2020

Thursday, 10 / 8 / 2020

## Hyd. No. 3

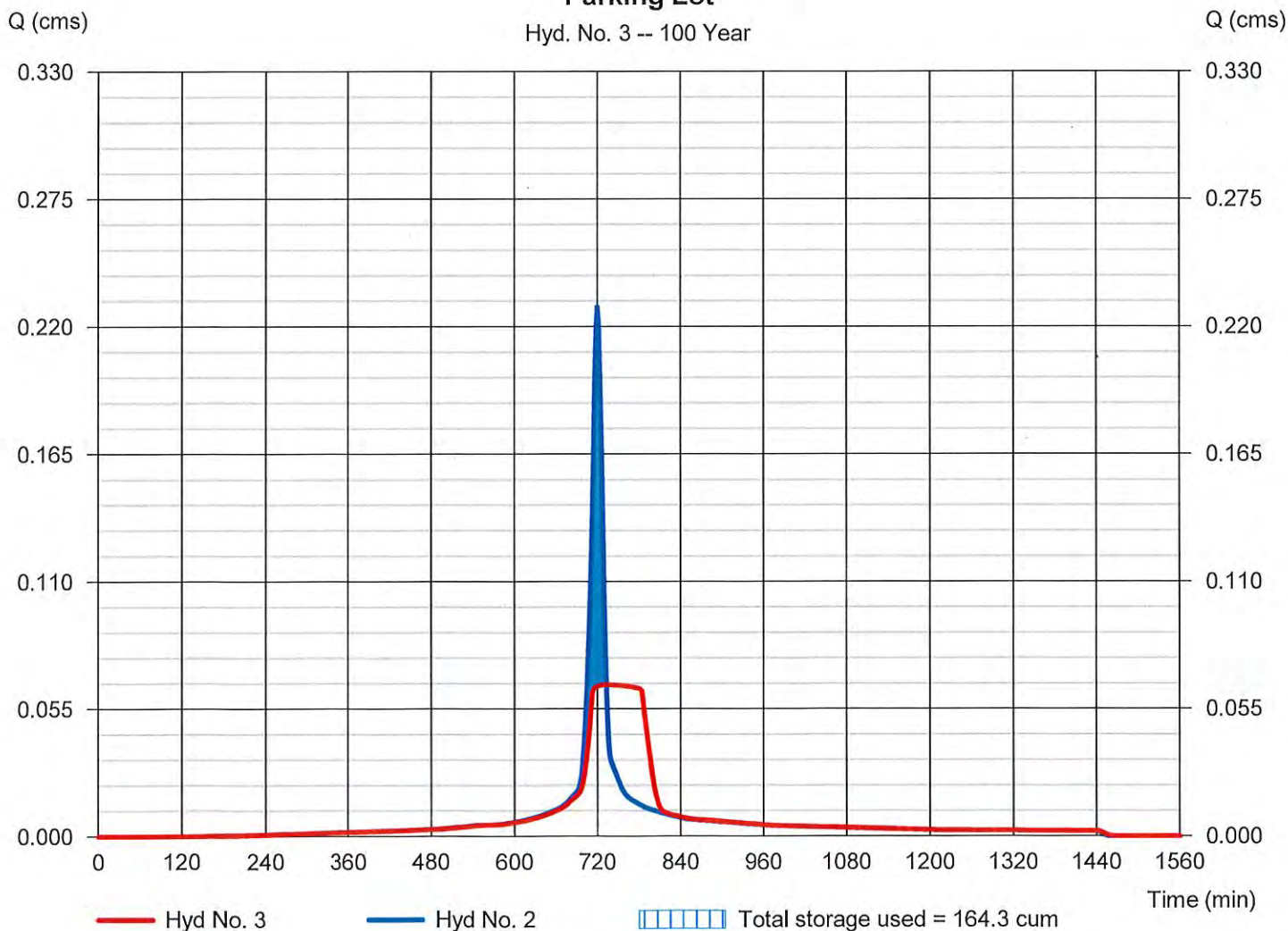
Parking Lot

Hydrograph type	= Reservoir	Peak discharge	= 0.065 cms
Storm frequency	= 100 yrs	Time to peak	= 732 min
Time interval	= 2 min	Hyd. volume	= 644.1 cum
Inflow hyd. No.	= 2 - Post-Development-201	Max. Elevation	= 180.79 m
Reservoir name	= Parking Lot	Max. Storage	= 164.3 cum

Storage Indication method used.

### Parking Lot

Hyd. No. 3 -- 100 Year



# Hydraflow Rainfall Report

Hydraflow Hydrographs Extension for Autodesk® Civil 3D® by Autodesk, Inc. v2020

Thursday, 10 / 8 / 2020

Return Period (Yrs)	Intensity-Duration-Frequency Equation Coefficients (FHA)			
	B	D	E	(N/A)
1	0.0000	0.0000	0.0000	-----
2	85.4000	17.7800	2.0777	-----
3	0.0000	0.0000	0.0000	-----
5	125.9000	22.3520	2.1285	-----
10	151.1000	24.1300	2.1463	-----
25	185.1000	25.9080	2.1641	-----
50	211.4000	26.9240	2.1793	-----
100	237.5000	27.9400	2.1869	-----

File name: Windsor A 2007.IDF

$$\text{Intensity} = B / (T_c + D)^E$$

Return Period (Yrs)	Intensity Values (mm/hr)											
	5 min	10	15	20	25	30	35	40	45	50	55	60
1	0	0	0	0	0	0	0	0	0	0	0	0
2	112	84	68	58	50	45	40	37	34	31	29	27
3	0	0	0	0	0	0	0	0	0	0	0	0
5	140	108	88	75	66	59	53	48	45	41	39	36
10	158	123	101	87	76	68	61	56	52	48	45	42
25	182	143	118	101	89	80	72	66	61	56	53	49
50	200	158	131	112	99	88	80	73	67	62	58	55
100	218	173	144	123	109	97	88	80	74	69	64	60

T<sub>c</sub> = time in minutes. Values may exceed 60.

Precip. file name: C:\Users\NMrad\Documents\NM\Hydrograph\ERCA-24hr &amp; Chicago 4hr.pcp

Storm Distribution	Rainfall Precipitation Table (mm)							
	1-yr	2-yr	3-yr	5-yr	10-yr	25-yr	50-yr	100-yr
SCS 24-hour	0	53	0	68	78	90	99	108
SCS 6-Hr	0	0	0	0	0	0	0	0
Huff-1st	0	0	0	0	0	0	0	0
Huff-2nd	0	0	0	0	0	0	0	0
Huff-3rd	0	0	0	0	0	0	0	0
Huff-4th	0	0	0	0	0	0	0	0
Huff-Indy	0	0	0	0	0	0	0	0
Custom	0	0	0	0	0	0	0	0

## STRESS TEST HYDROGRAPH RESULTS

# Hydrograph Report

Hydraflow Hydrographs Extension for Autodesk® Civil 3D® by Autodesk, Inc. v2020

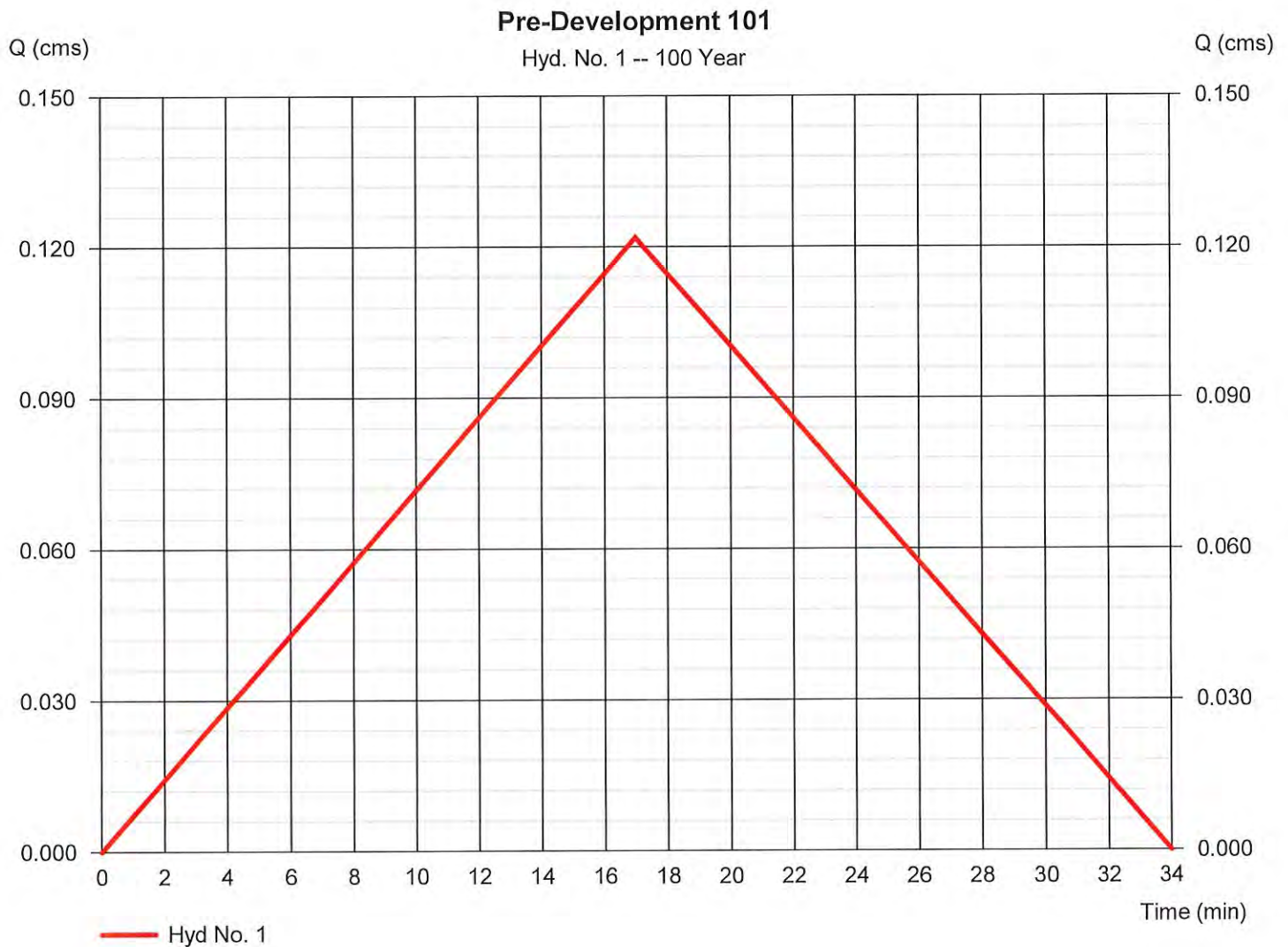
Thursday, 10 / 8 / 2020

## Hyd. No. 1

Pre-Development 101

Hydrograph type	= Rational	Peak discharge	= 0.122 cms
Storm frequency	= 100 yrs	Time to peak	= 17 min
Time interval	= 1 min	Hyd. volume	= 124.3 cum
Drainage area	= 0.670 hectare	Runoff coeff.	= 0.49*
Intensity	= 134.791 mm/hr	Tc by TR55	= 17.00 min
IDF Curve	= Windsor A 2007.IDF	Asc/Rec limb fact	= 1/1

\* Composite (Area/C) =  $[(0.320 \times 0.70) + (0.350 \times 0.30)] / 0.670$





# Hydrograph Report

Hydraflow Hydrographs Extension for Autodesk® Civil 3D® by Autodesk, Inc. v2020

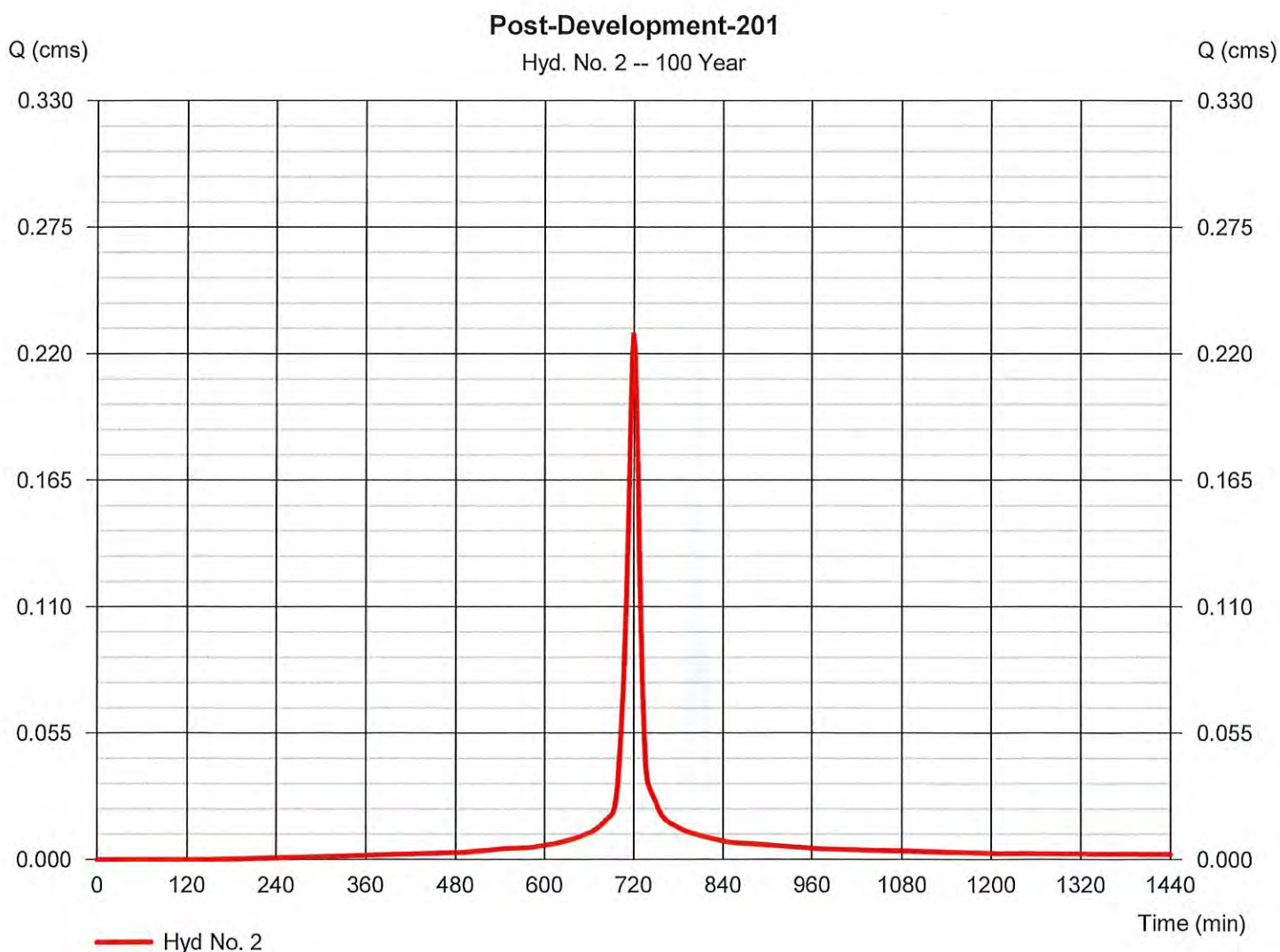
Thursday, 10 / 8 / 2020

## Hyd. No. 2

Post-Development-201

Hydrograph type	= SCS Runoff	Peak discharge	= 0.229 cms
Storm frequency	= 100 yrs	Time to peak	= 720 min
Time interval	= 2 min	Hyd. volume	= 644.1 cum
Drainage area	= 0.669 hectare	Curve number	= 95*
Basin Slope	= 0.0 %	Hydraulic length	= 0 m
Tc method	= User	Time of conc. (Tc)	= 11.62 min
Total precip.	= 150.00 mm	Distribution	= Type II
Storm duration	= 24 hrs	Shape factor	= 484

\* Composite (Area/CN) =  $[(0.120 \times 80) + (0.507 \times 98)] / 0.669$





# Hydrograph Report

Hydraflow Hydrographs Extension for Autodesk® Civil 3D® by Autodesk, Inc. v2020

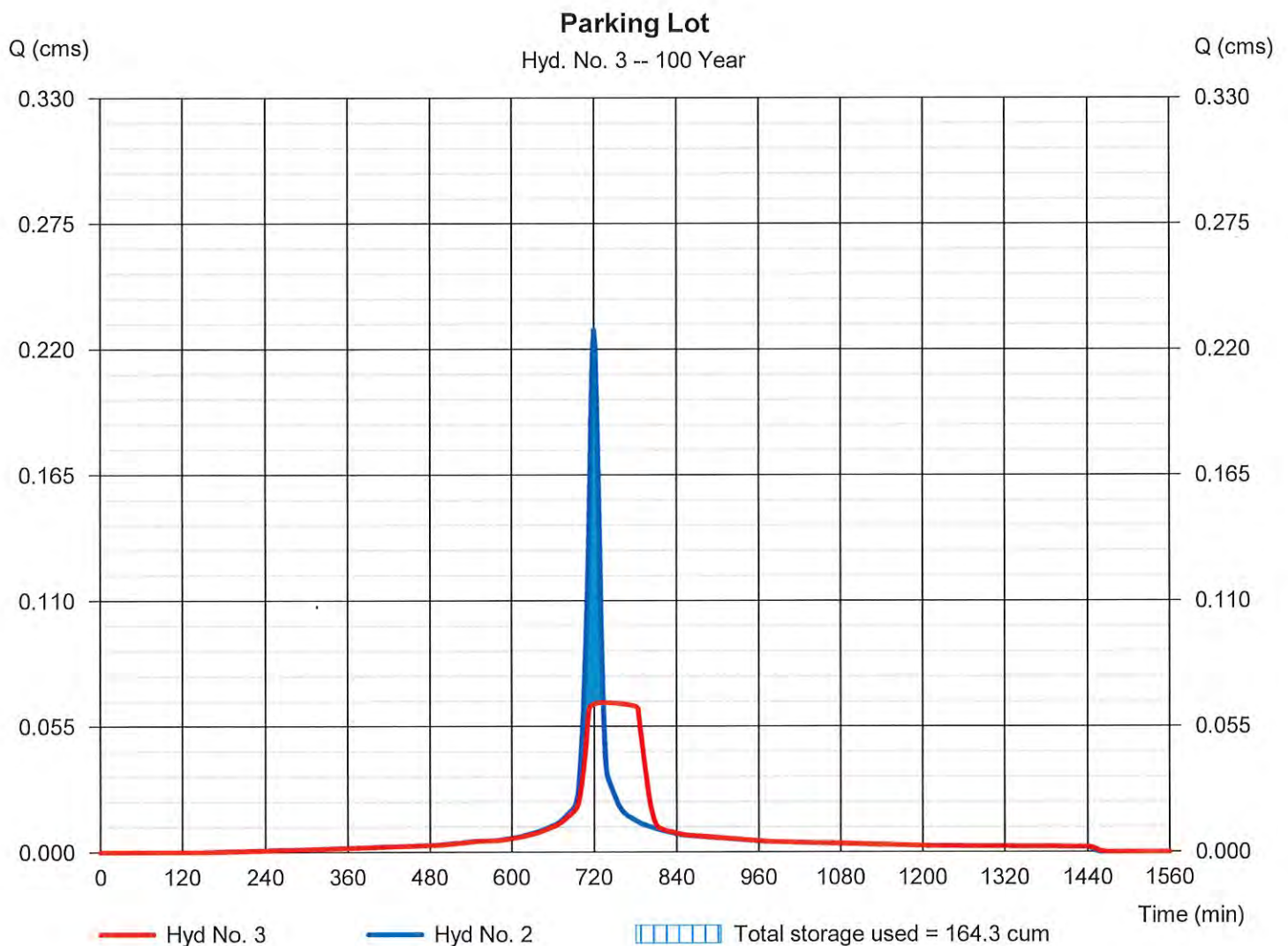
Thursday, 10 / 8 / 2020

## Hyd. No. 3


### Parking Lot

Hydrograph type	= Reservoir	Peak discharge	= 0.065 cms
Storm frequency	= 100 yrs	Time to peak	= 732 min
Time interval	= 2 min	Hyd. volume	= 644.1 cum
Inflow hyd. No.	= 2 - Post-Development-201	Max. Elevation	= 180.79 m
Reservoir name	= Parking Lot	Max. Storage	= 164.3 cum

Storage Indication method used.



<b>3885 SANDWICH STREET, WINDSOR</b> <b>SANITARY SEWER DESIGN SHEET (Ultimate DA Design)</b>									
<b>DESIGN CRITERIA</b> RESIDENTIAL 3.5 COMMERCIAL 10.0 INDUSTRIAL 15.0 INSTITUTIONAL 15.0 SCHOOL 15.0 AVERAGE DAILY PER CAPITA FLOW 0.208 L/sec PEAK EXTRANEAL FLOW 1+144(P/1000) <sup>1.48</sup> L/s HARMON'S PEAK FACTOR 1.48 MINIMUM PIPE SIZE 200 mm									
Project: 20-028 Client:									
2 m/s									
STREET OR BASEMENT	Area Included	LOCATION		To Node	DESIGN AREA SERVED (m <sup>2</sup> )		DESIGN POPULATION (PERSONS)		PEAK FACTOR
		From Node			INCREMENT	TOTAL	INCREMENT	TOTAL	
SANDWICH	A1	SAN CLEAN OUT 1		SAN CLEAN OUT 2	0.669	0.669	1	525	4
	-	SAN CLEAN OUT 2		TIE-IN	0.000	0.000	1	525	4
TOTAL POPULATION CALCULATION = 550 UNITS * 35 = 19,250									
SEWER DATA									
MAXIMUM FLOW					PEOPLE				
INTEGRATION (L/s)	SEWAGE INFILTRATION (L/s)	INFILTRATION (L/s)	PEAK FLOW (L/s)	PEAK FLOW (L/s)	Length (m)	Capacity (L/s)	Velocity (m/s)	Rate (%)	Upstream Elevation (m)
INTEGRATION (L/s)	SEWAGE INFILTRATION (L/s)	INFILTRATION (L/s)	PEAK FLOW (L/s)	PEAK FLOW (L/s)	Length (m)	Capacity (L/s)	Velocity (m/s)	Rate (%)	Upstream Elevation (m)
0.140	10.11	10.11	10.25	10.25	9.73	20.746	0.66	48.43	178.300
0.000	10.11	10.11	10.11	10.11	22.87	21.004	0.67	48.16	178.212
Date: 20-02-2020 Design By: GOWTHAM SIVAKUMAR Project No: 20-028 Checked and stamped: SHUREEL TUNO									
<b>BAIRDIAE</b> architecture + engineering									

MIXED USE DEVELOPMENT																																
STORM SEWER DESIGN SHEET (5-YEAR EVENT, Computed To)																																
LOCATION					AREA (ha)				FLOW					SEWER DATA							PROFILE											
Area ID	Area Included	From	To	C=	C=	C=	C=	Indiv 2.78 AC	Accum 5 2.78AC	Time of Conc.	Design Storm	Rainfall Intensity	Peak Flow (L/sec)	Qtotal (L/s)	Dia. (m) Actual	Dia. (mm)	Type	Slope (%)	Length (m)	Capacity (L/s)	Velocity (m/s)	Flow Time (min)	Ratio O/Q full	Upstream Elevation	Downstream Elevation							
		Node	Node	0.95	0.20																			Invert (m)	Invert (m)							
FHC APARTMENTS																																
A1	BUILDING AND PARKING LOT	CB #10	MH #2	0.198				0.52	0.52	10.00	5	107.72	56.33	56.33	0.300	300	PVC	0.35	27.78	57.2	0.80	0.58	99%	179.155	179.058							
A2	BUILDING AND PARKING LOT	MH #2	MH# 3	0.381				1.01	1.53	10.58	5	105.01	160.58	160.58	0.450	450	PVC	0.35	64.80	168.5	1.06	1.02	95%	179.058	178.831							
A1	BUILDING AND PARKING LOT	MH# 3	MH# 4	0.090				0.24	1.77	11.60	5	100.60	177.74	177.74	0.450	450	PVC	0.40	42.80	180.2	1.13	0.63	99%	178.831	178.660							
A1		MH# 4	EXISTING COMBINED	0.000				0.00	1.77	12.23	5	98.07	173.27	173.27	0.450	450	PVC	1.00	7.00	284.9	1.79	0.07	61%	178.660	178.590							
Q = 2.78 AIR, where Q= Peak Flow in Litres per Second (l/s) A= Area in hectares (ha) I= Rainfall Intensity (mm/hr) R= Runoff Coefficient														<div> <b>BAIRD AE</b> architecture + engineering</div> <div>Consultant: Baird AE - Architects &amp; Engineers Date: October 8, 2020 Design: Peter Sarkis Project No: 20-028 Dwg. Reference: Mixed use Development Stamp: NADIM MRAD</div>																		

**TABLE 1: STORAGE AVAILABILITY****MH/CB Storage**

MH/CB #	Lid Elev. (m)	Invert Elev. (m)	Depth (m)	Section Area (m <sup>2</sup> )	Volume (m <sup>3</sup> )
MH 2	181.12	179.19	1.93	1.13	2.18
MH 3	180.75	178.89	1.86	1.13	2.10
MH 4	180.74	178.69	2.05	1.13	2.32
CB x 10	181.00	179.17	1.20	0.36	4.32
<b>Available MH/CB Storage:</b>					<b>10.9 m<sup>3</sup></b>

**Pipe Storage**

MH/CB #		Length	Dia.	Area	Volume
From	To	(m)	(mm)	(m <sup>2</sup> )	(m <sup>3</sup> )
MH 2	MH 4	77.0	200	0.0314	2.4
MH 3	MH 4	27.8	375	0.1104	3.1
MH 4	MH D	107.6	450	0.1590	17.1
<b>Available Pipe Storage:</b>					<b>22.6 m<sup>3</sup></b>

4.05      **Total Available Storage: 33.5 m<sup>3</sup>**

## Appendix C

---

### HYDROGRAPH MODEL OUTPUT

## Appendix D

---

### STORMWATER QUALITY UNIT AND GRADING PLAN



# Hydro First Defense® - HC

## Water Quality Flow Rate Worksheet

Rev. 9.5



Project Name: **3885 SANDWICH STREET**  
 Street: **3885 SANDWICH STREET**  
 Province: **ONTARIO**  
 Designer: **G.S.**

Report Date:   
 City: **WINDSOR**  
 Country: **CANADA**  
 email:   
 Paste

### Treatment Parameters:

Structure ID: **MH#4**  
 TSS Goal: **70 % Removal**  
 TSS Particle Size: **Fine**  
 Water Quality Flow: **60 L/s**  
 Peak Storm Flow: **60 L/s**  
 Peak Storm Return: **100 yrs**

### RESULTS SUMMARY

Model	TSS
FD-3HC	57.2%
FD-4HC	68.5%
<b>FD-5HC</b>	<b>74.4%</b>
FD-6HC	77.9%
FD-8HC	81.5%

### Performance Statement:

The Hydro International stormwater treatment system, model FD-5HC, achieves the water quality objective of 74.4% TSS using Fine particle size distribution, providing continuous treatment positive removal for the water quality flow of 60 L/s.

### Model Specification:

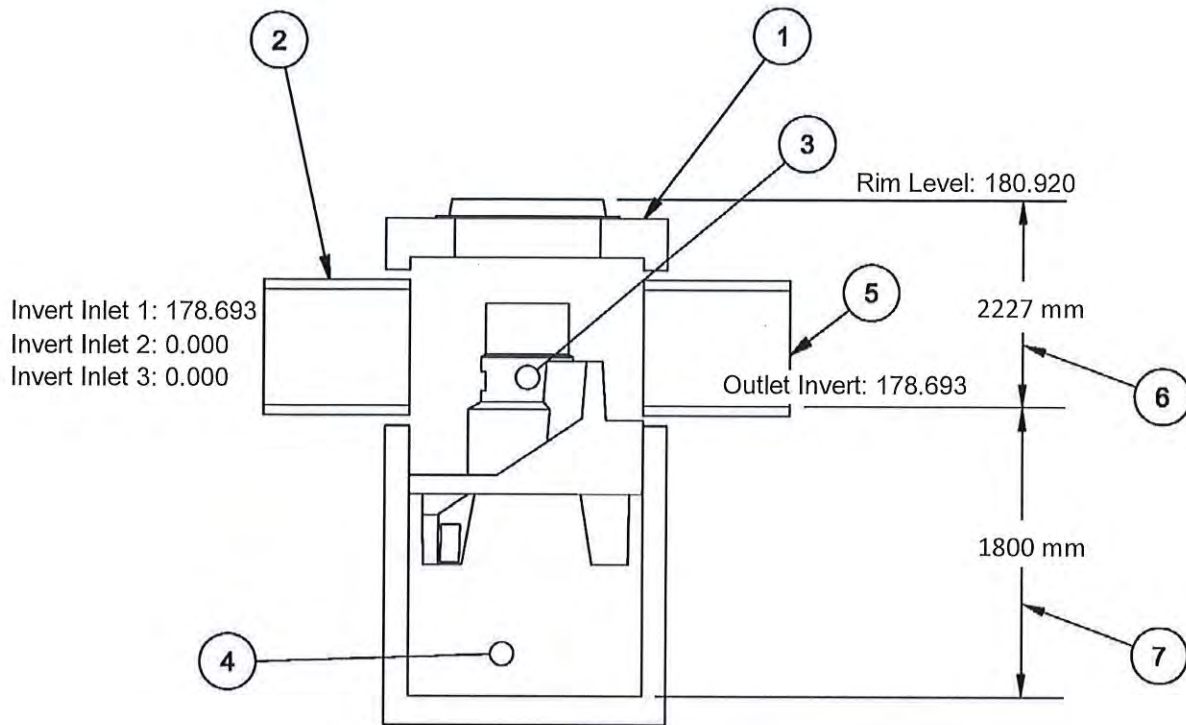
Selected Model: **FD-5HC**  
 Diameter: **1500 mm**  
 Design WQ Flow: **60.00 L/s**  
 No Bypass Flow: **42.00 L/s**  
 Peak Flow Capacity: **566.00 L/s**  
 Sediment Storage: **0.84 m³**  
 Oil Storage: **1136.00 L**

### Installation Configuration:

Placement: **Offline**  
 Outlet Pipe Size: **300 mm** OK  
 Inlet Pipe 1 Size: **450 mm** OK  
 Inlet Pipe 2 Size: **0 mm** OK  
 Inlet Pipe 3 Size: **0 mm** OK

Rim Level: **180.920 m** **Calc Invs.**  
 Outlet Pipe Invert: **178.693 m** OK  
 Invert Pipe 1: **178.693 m** OK  
 Invert Pipe 2: **0.000 m** Inlet below outlet will reduce treatment capacity  
 Invert Pipe 3: **0.000 m** Inlet below outlet will reduce treatment capacity

### Designer Notes:












**FD-5HC Specification**

1	Vortex Chamber Diameter	1500 mm
2	Inlet Pipe Diameter	450 mm
3	Oil Storage Capacity	1136 L
4	Min. Provided Sediment Storage Capacity	0.84 m <sup>3</sup>
5	Outlet Pipe Diameter	300 mm
6	Rim to Invert	2227 mm
7	Invert to Sump	1800 mm
	<b>Total Depth</b>	<b>4027 mm</b>

All drawing elevations are metres.

**Designer Notes:**



GRAPHIC NOTES	 N	 3/4" = 1'	 MANHOLE	 PROPOSED ELEVATION	 SLOPE TO CURB
	 TOP OF CURB	 EXISTING ELEVATIONS	 MATCH EXISTING AROUND ELEVATIONS	 PROPOSED CURB AND GUTTER	 PORTION TO BE REMOVED AND FILLED IN
	 OVER LAND AND CONFERENCE				

MINIMUM PAVEMENT DESIGN		
LAYER	MATERIAL	MINIMUM THICKNESS PARKING LOT, mm
ASPHALTIC CONCRETE	OPSS 1150 HL3 SURFACE ASPHALT	40
	OPSS 1150 HL4 BASE ASPHALT	60
CRAN 8-40 BASE	OPSS 1010	200



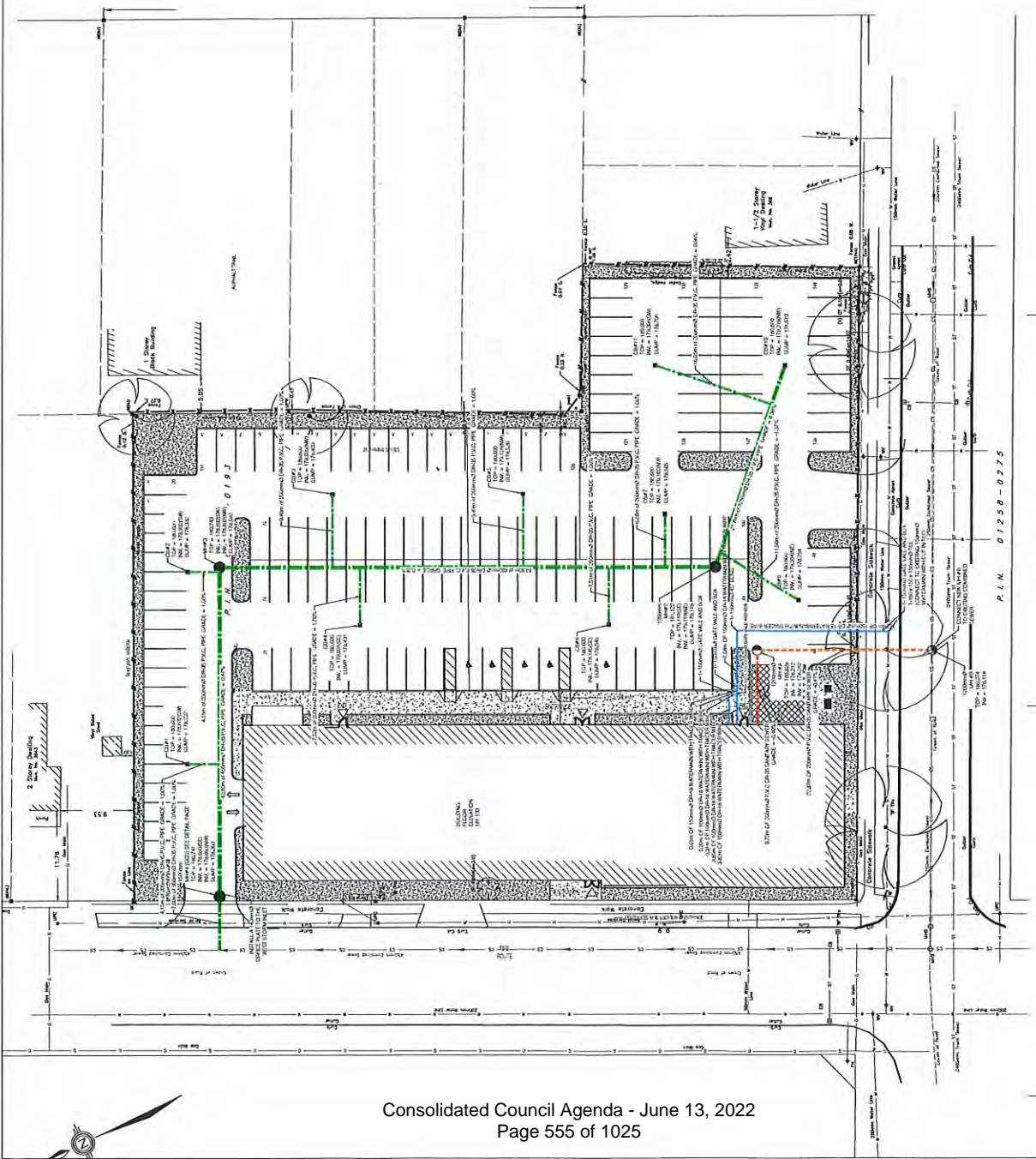
Date	Revision
NOV 25, 2019	Issued for Tender
FEB 11, 2020	Revised for Submission
APR 17, 2020	Revised for Submission
MAY 28, 2020	Revised for Submission
JUN 11, 2020	Revised for Permit
JUN 16, 2020	Issued for Tender
JUN 25, 2020	Revised for Submission
AUG 04, 2020	Revised for Submission

[illegible]

**ATTENTION**  
CONTRACTOR IS RESPONSIBLE FOR CONFIRMING  
THE EXACT LOCATION AND PROTECTION OF EXISTING  
UTILITIES DURING CONSTRUCTION.

--- SANITARY SEWER DRAINAGE AREA

GENERAL NOTES: REFER TO MECHANICAL AND ELECTRICAL PLANS.



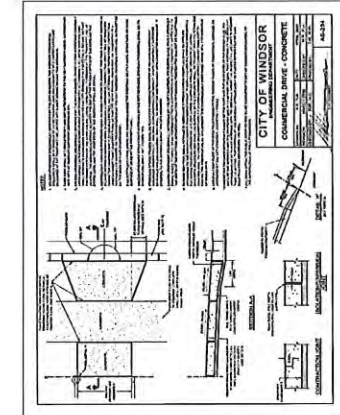
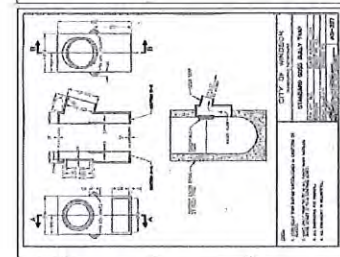
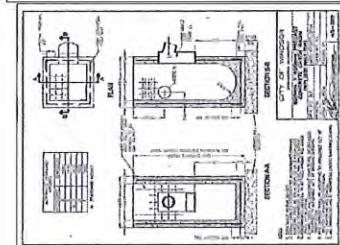
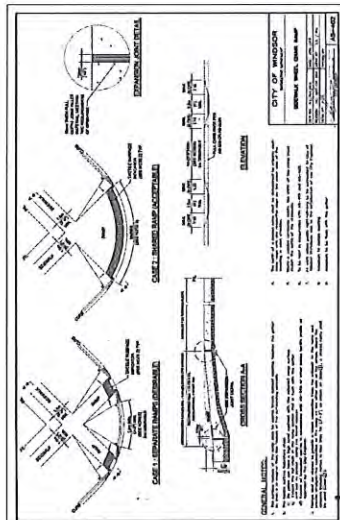
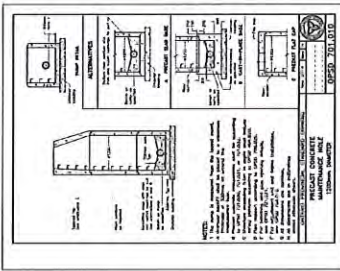
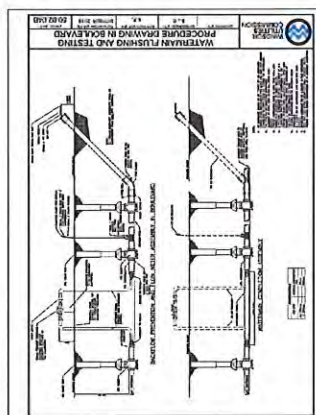
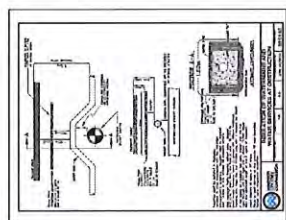
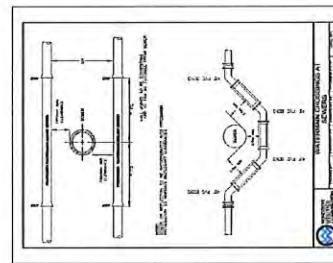
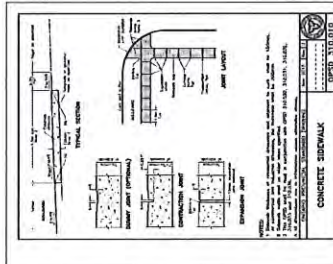
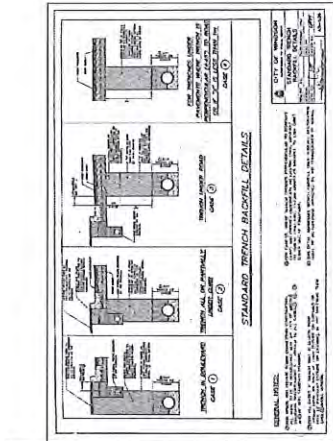
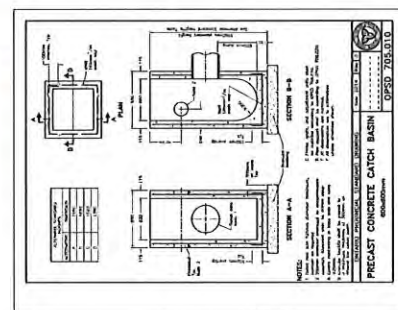
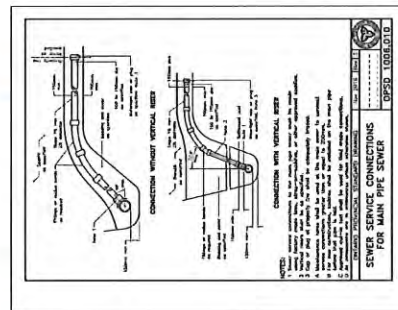
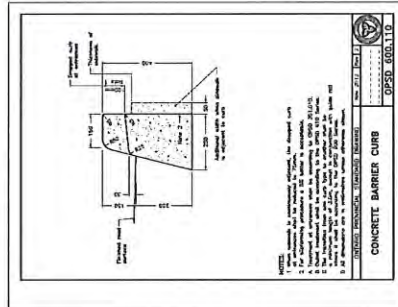
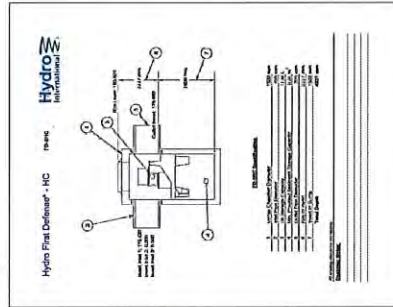
CHAPPELL AVE



Date	Revision
NOV 26, 2019	Issued for Tender
FEB 11, 2020	Revised for Submission
APR 17, 2020	Revised for Submission
MAY 04, 2020	Revised for Submission
JUN 11, 2020	Revised for Tender
JUN 19, 2020	Revised for Submission
JUN 30, 2020	Revised for Submission
AUG 04, 2020	Revised for Submission

**BAIRD|AE** architecture + engineering

1000 - 267 Polkster Street  
Windsor, ON N9A 4K4  
102 - 27 Princess Street  
Leamington, ON N8H 2K9





# APPENDIX 'L'

## Urban Design Brief





# TABLE OF CONTENTS

INTRODUCTION .....3

SITE DESIGN AND ORIENTATION .....4

BUILT FORM .....7

SITE CIRCULATION .....8

ARCHITECTURAL DESIGN.....10

PUBLIC SPACE.....12

CONCLUSION.....13

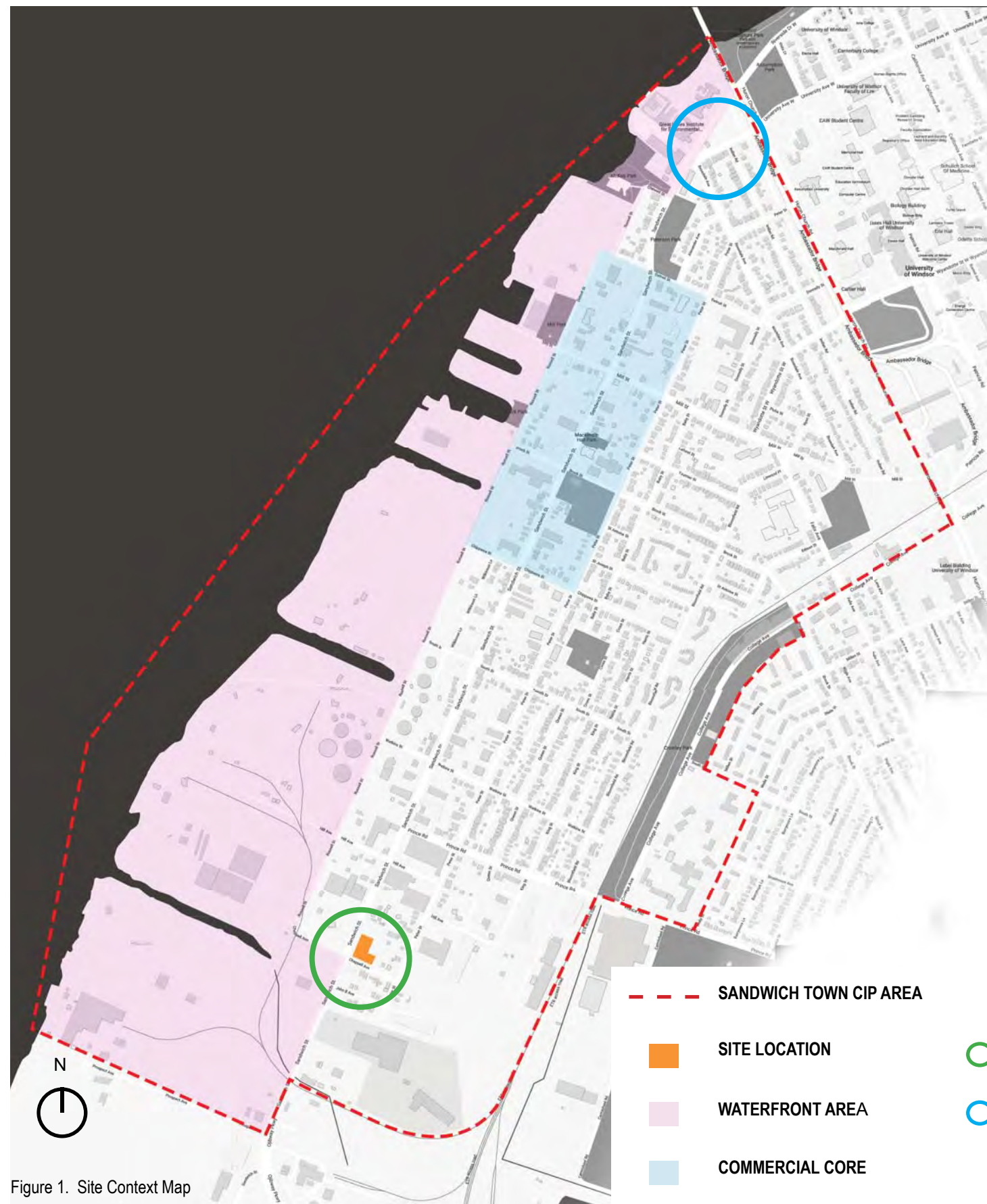


Figure 1. Site Context Map

## SITE INTRODUCTION

THE PROPOSED DEVELOPMENT IS SITUATED AT SOUTHERN EDGE OF THE SANDWICH TOWN COMMUNITY IMPROVEMENT PLAN AREA AT THE INTERSECTION OF SANDWICH STREET AND CHAPPELL AVE. THIS 11 STOREY RESIDENTIAL BUILDING WILL ACCOMMODATE COMMERCIAL SPACE ON THE FIRST FLOOR AND FRAME THE SOUTHERN GATEWAY OF SANDWICH TOWN WITH AN IMPROVED PEDESTRIAN FRIENDLY PRESENCE.

THROUGH THE USE OF MATERIALS THAT PLAY HOMAGE TO THE SANDWICH HERITAGE CONSERVATION DISTRICT, THE PROPOSAL AIMS TO BUILD THE LINK BETWEEN THE SOUTHERN GATEWAY AND THE ESTABLISHED COMMERCIAL CORE. ATTENTION WAS GIVEN TO THE RESIDENTIAL AND COMMERCIAL ENTRANCES TO ESTABLISH A PEDESTRIAN SCALE THROUGH ARCHITECTURAL FEATURES AND LANDSCAPE INTERVENTIONS.



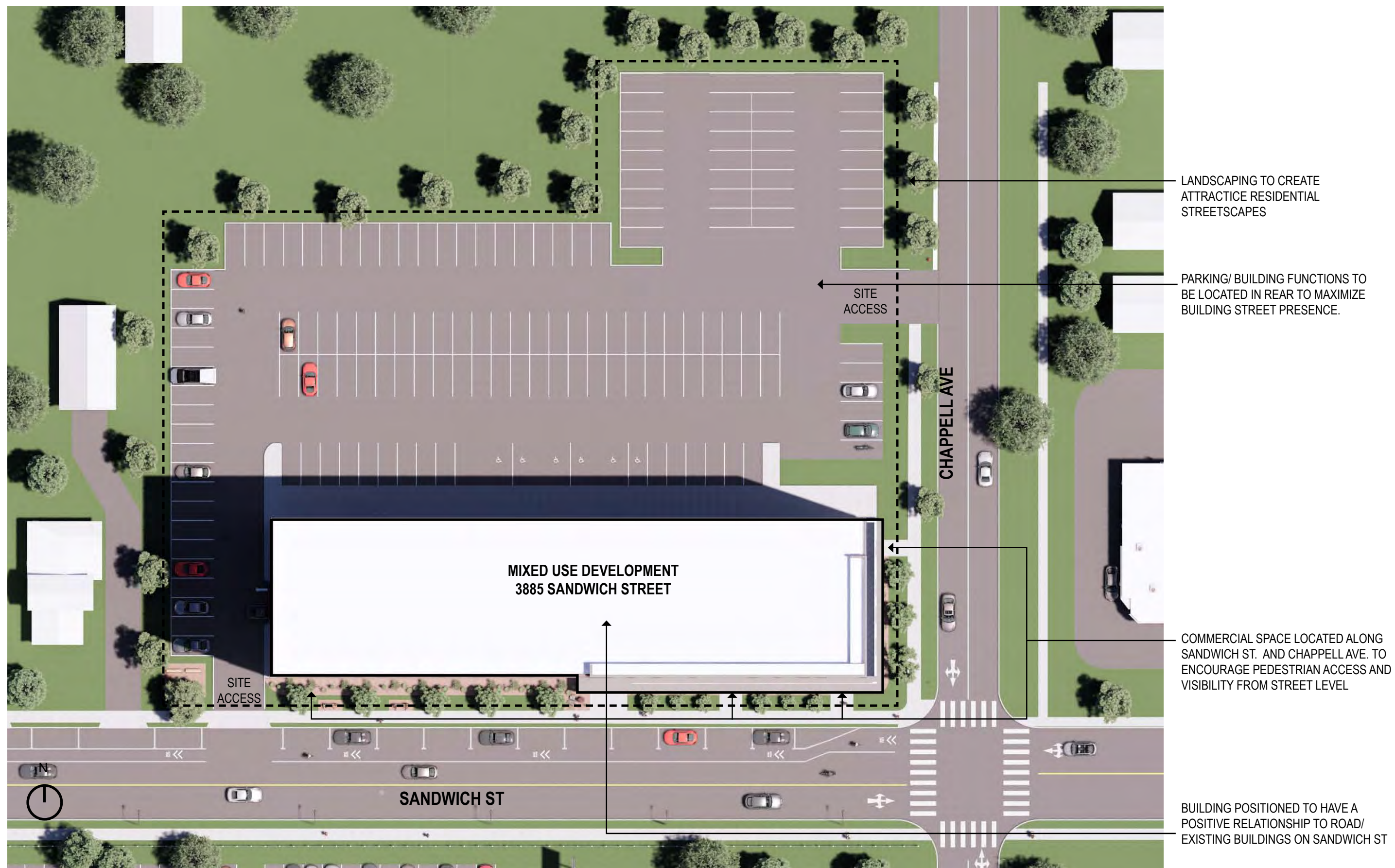


Figure 2. Site Plan

GUIDANCE

Olde Sandwich Towne Community Improvement Plan

6.2.1 Appearance and Community Image

- Improve the visual and perceived appearance and community image of OST.
- Improve the appearance of the gateways into OST with street furnishings, lighting, signage and ornamental plantings.
- Create a pedestrian friendly environment along Sandwich Street.
- Ensure consistency in building mass, treatment and promotion of building use along the Sandwich streetscape.
- Establish a consistent street edge along Sandwich Street and introduce new activity to the street.
- Promote visual interest, walkability and sense of place for visitors.
- Install iconic pieces of public art that help to tell a story, provide visual interest, and create landmarks in OST.

6.2.3 Commercial Development and Business Attraction

- Attract a diverse range of retail businesses and cultural activities to OST.
- Improve the overall image and ability to attract new businesses to the commercial core of OST.
- Improve the appearance and image of industrial properties on the Waterfront.

6.2.3 Commercial Development and Business Attraction

- Attract a diverse range of retail businesses and cultural activities to OST.
- Improve the overall image and ability to attract new businesses to the commercial core of OST.
- Improve the appearance and image of industrial properties on the Waterfront.

City Of Windsor Official Plan

2.5.5.2 Mixed Use Policies

- To encourage multi-functional areas which integrate compatible commercial,institutional, open space and residential uses.
- To encourage a compact form of mixed use development. declining or obsolete industrial areas;
- To provide opportunities to create and maintain special area identities and focal points within Windsor.
- To provide public places for strolling, recreation, conversation and entertainment.
- To increase the use of walking, cycling and public transportation within the designated Mixed Use area by fostering a strong live-work-shopping-recreation relationship.

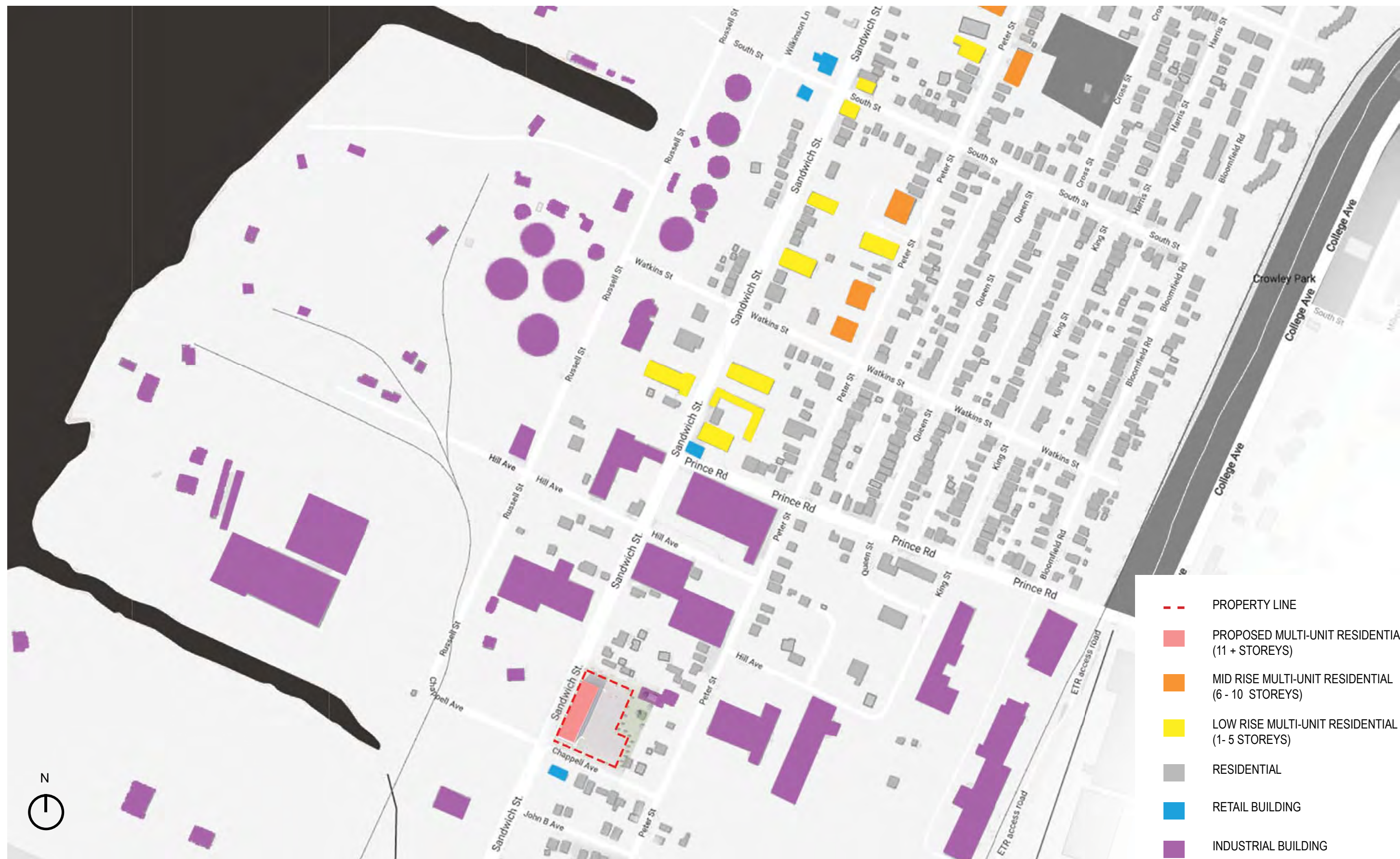
Policy 11.8.2.2

- Residential areas where the housing stock is in need of maintenance, rehabilitation and/or repair;
- Declining commercial or mixed use areas where there are a number of vacant or underutilized properties;
- Declining or obsolete industrial areas;
- Areas in which there are land use conflicts as a result of incompatible uses;
- Areas that have deficient municipal services such as parks, sewers and roads; and
- Areas that have the potential to be new employment areas.

RESPONSE

The proposed development signify the Southern gateway into old sandwich town (OST). Through its architectural articulation and a materiality that plays homage to the brick buildings of the OST commercial core, the proposed development establishes a link to the more developed North OST. This link is further developed by allocating over 5000sf of retail space along Sandwich St. and Chappell, there activating the pedestrian realm and creating a vibrant neighborhood. The proposal presents a visual and perceived improvement to the pedestrian scale of the Southern gateway into OST.





- PROPERTY LINE
- PROPOSED MULTI-UNIT RESIDENTIAL (11 + STOREYS)
- MID RISE MULTI-UNIT RESIDENTIAL (6 - 10 STOREYS)
- LOW RISE MULTI-UNIT RESIDENTIAL (1- 5 STOREYS)
- RESIDENTIAL
- RETAIL BUILDING
- INDUSTRIAL BUILDING

Figure 4. Figure ground



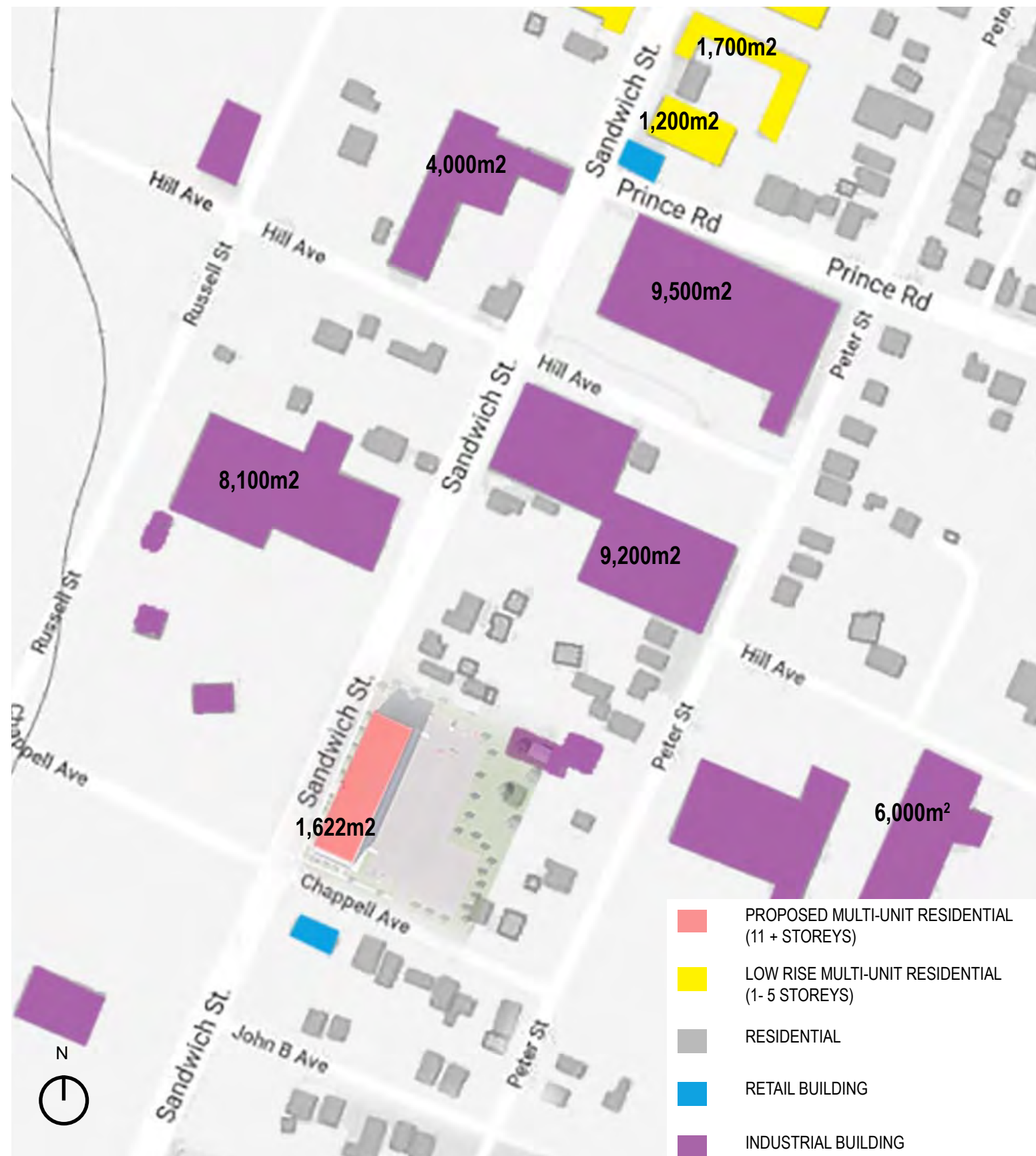


Figure 5. Site Massing Area Map

## GUIDANCE

### City of Windsor Official Plan

#### 8.2.2.5 Built Form

Council will ensure that the design of new development:

- is complementary to adjacent development in terms of its overall massing, orientation, setback and exterior design, particularly character, scale and appearance;
- Provides links with pedestrian, cycle, public transportation and road networks; and
- Maintains and enhances valued heritage resources and natural area features and functions.
- Encourages the creation of attractive residential streetscapes through architectural design that reduces the visual dominance of front drive garages, consideration of rear lanes where appropriate, planting of street trees and incorporation of pedestrian scale amenities.

#### 8.2.2.5 Built Form

Council will ensure that the design of extensive areas of redevelopment achieves the following:

- provides a development pattern that support a range of uses and profiles;
- defines the perimeter of such an area by a distinct edge which may be formed by roads, elements of the Greenway System or other linear elements;
- contains activity centres or nodes which are designed to serve the area and which may be identified by one or more landmarks;
- provides transportation links to adjacent areas; and
- maintains and enhances valued historic development patterns or heritage resources.
- is complementary to adjacent development in terms of overall massing, orientation, setback and exterior design, particularly character, scale and appearance.

## RESPONSE

The proposed development is contextually located in a predominantly Industrial district at the South end of the Sandwich CIP area. It occupies a significantly smaller massing footprint to its surrounding built industrial context. Although the proposed development is taller than the surrounding context it establishes an improved pedestrian scale through attractive architectural and landscape features, and also improves and activates the pedestrian realm by allocating retail along Sandwich st. and Chappell Ave. The proposed development built form will provide the missing link of sandwich town and create a new visual landmark.



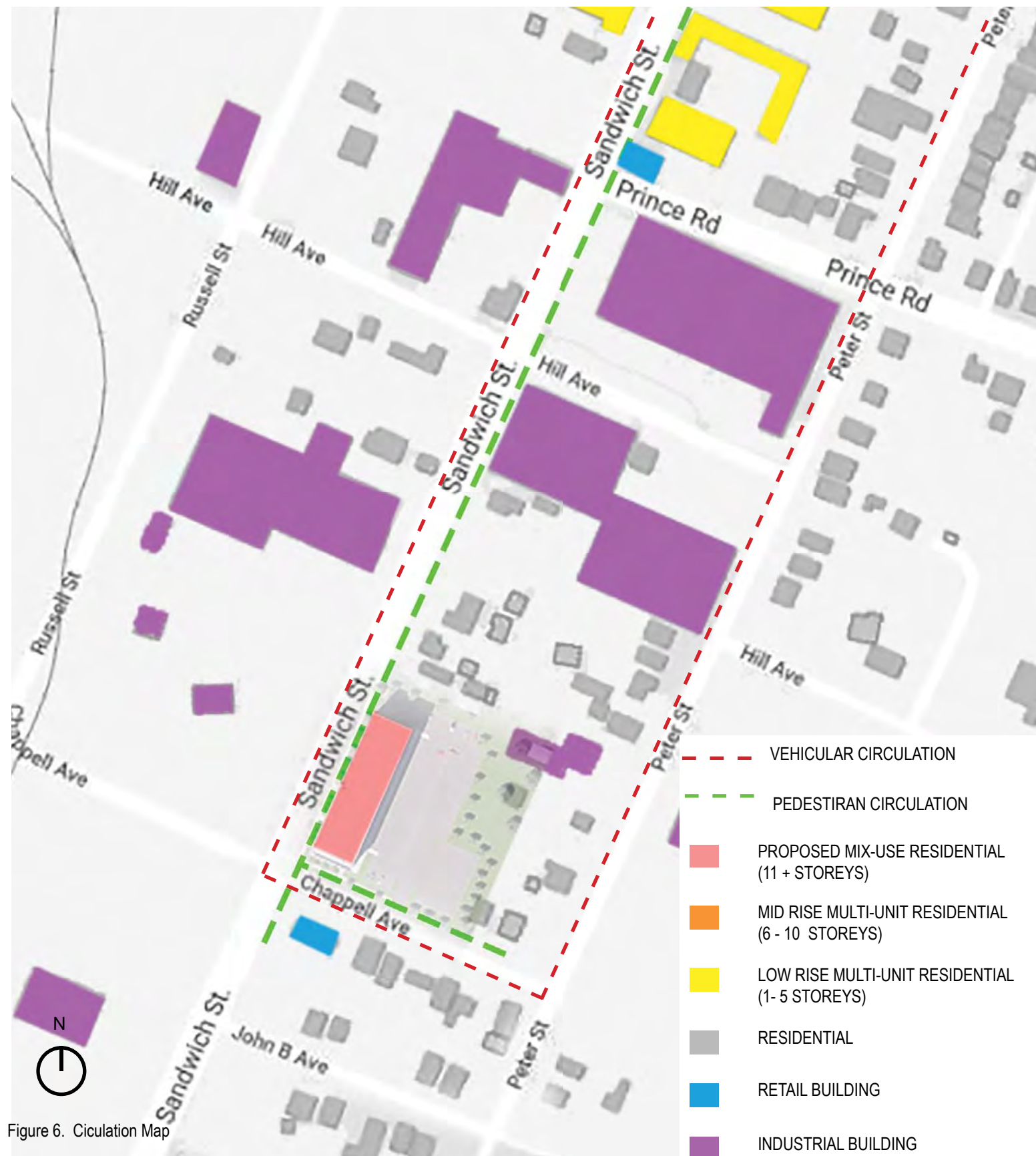


Figure 6. Circulation Map

## GUIDANCE

### City of Windsor Official Plan

#### Policy 8.2.2.5 Gateways

Building entrances should be well defined and accessible to pedestrians and the handicapped persons with disabilities.

- provide a sense of welcome and arrival;
- assist in orientation;
- create a memorable image; and
- contribute to the social, cultural, historic or thematic character of the area being defined

#### Policy 8.2.2.7

Council will ensure that a proposed development or infrastructure undertaking enhances the image of Windsor, its districts and/or its neighbourhoods by complementing and contributing to:

- the activity of the area together with the character, scale, appearance and design features of existing buildings
- the landmarks in the area;
- the consistency and continuity of the area with its surroundings;
- linkages within, to and from the area.

#### Policy 8.4.1.1

To integrate barrier-free pedestrian routes in the design of urban spaces.

## RESPONSE

The proposed development will be a landmark project that will establish the Southern Gateway into the Old Sandwich Town (OST). It will activate and improve the pedestrian realm through the addition of retail along Sandwich St. and Chappell Ave. The landscaping features along Sandwich St. will promote pedestrian circulation and create an overall welcoming and memorable image of the Southern gateway into the OST.



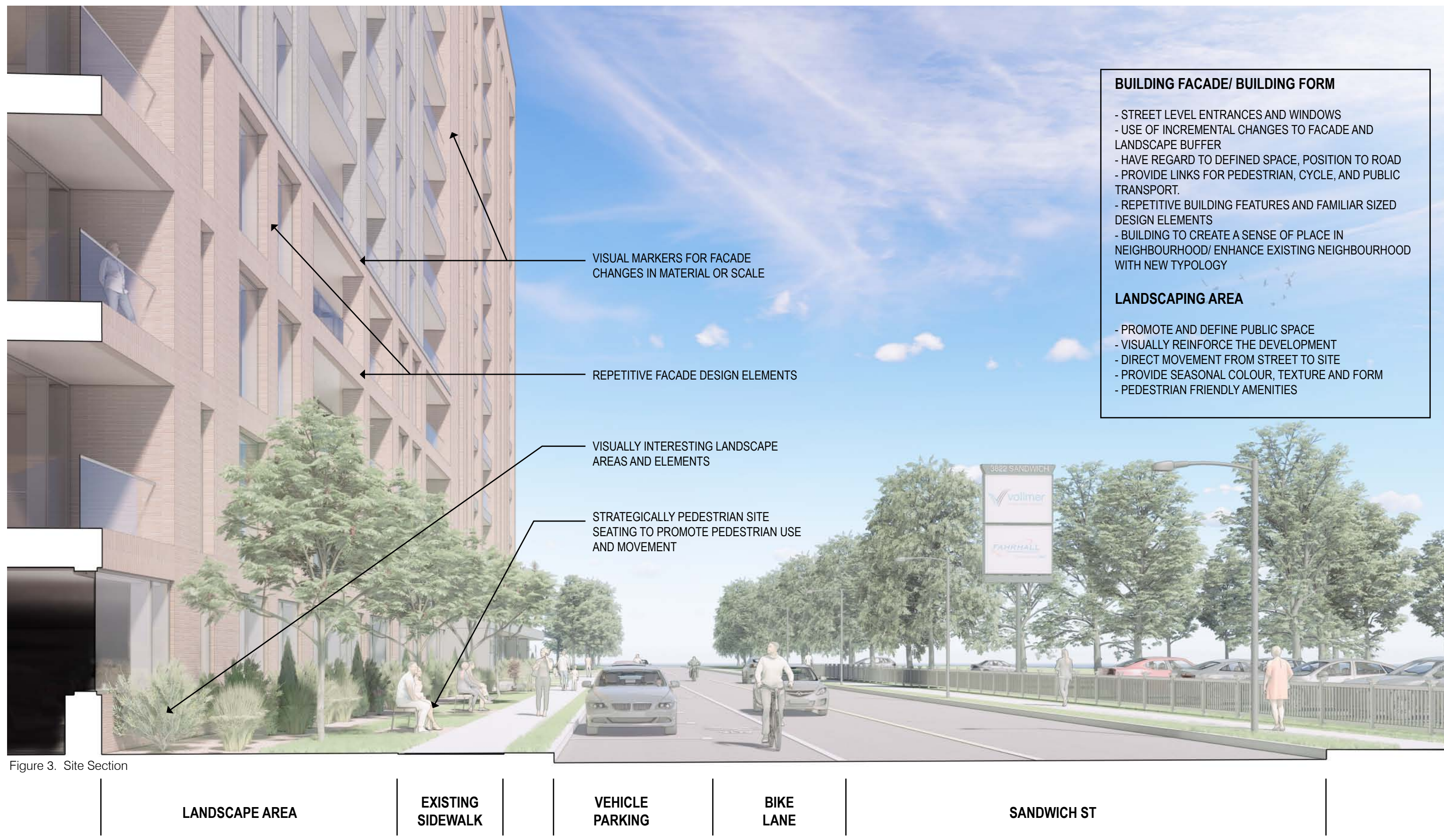


Figure 3. Site Section



# GUIDANCE

## City of Windsor Official Plan

### Policy 8.3.2.2

Council will encourage buildings and spaces that establish a pedestrian scale by promoting:

- the placement of continuous horizontal features on the first two storeys adjacent to the road;
- the repetition of landscaping elements, such as trees, shrubs or paving modules; and
- the use of familiar sized architectural elements such as doorways and windows.

### Policy 8.7.1.1

To achieve a varied development pattern which supports and enhances the urban experience.

### Policy 8.7.1.2

To achieve a complementary design relationship between new and existing development,while accommodating an evolution of urban design styles.

### Policy 8.7.1.3

To maximize the variety and visual appeal of building architecture.

### Policy 8.7.1.4

To integrate art and landscaping with the built form.

### Policy 8.7.1.5

To enhance the unique character of a district, neighborhood, prominent building or grouping of buildings.

### Policy 8.7.1.6

To ensure that signs respect and enhance the character of the area in which they are located.

### Policy 8.7.1.7

To achieve external building designs that reflect high standards of character, appearance, design and sustainable design features.

### Policy 8.7.2.1

Council will ensure that the design of new development:

- is complementary to adjacent development in terms of its overall massing, orientation, setback and exterior design, particularly character, scale and appearance;
- provides links with pedestrian, cycle, public transportation and road networks.
- maintains and enhances valued heritage resources and natural area features and functions.
- Encourages the creation of attractive residential streetscapes through architectural design that reduces the visual dominance of front drive garages, consideration of rear lanes where appropriate, planting of street trees and incorporation of pedestrian scale amenities.

# GUIDANCE

## City of Windsor Official Plan

### Landscaping 8.5.2.5

Council will encourage the use of landscaping to:

- Promote a human scale;
- promote defined public spaces;
- accentuate or screen adjacent building forms;
- frame desired views or focal objects;
- visually reinforce a location;
- direct pedestrian movement;
- demarcate various functions within a development;
- provide seasonal variation in form, colour, texture and representation;
- assist in energy conservation; and
- mitigate the effects of inclement weather.

### Protection from Elements 8.6.2.1

Council may encourage design measures such as awnings, canopies, arcades, or recessed ground floor facades to offer pedestrian protection from inclement weather

### Policy 8.7.2.3

Council will ensure that proposed development within an established neighborhood is designed to function as an integral and complementary part of that area’s existing development

- pattern by having regard for:
- massing;
- building height;
- architectural proportion;
- volumes of defined space;
- lot size;
- position relative to the road; and
- building area to site area ratios.
- the pattern, scale and character of existing development; and,
- exterior building appearance

### Policy 8.7.2.5

Council will require new development to support the creation of continuous building facades along Mainstreets through the street level presence of:

- community facilities, retail shops, and other frequently visited uses; and
- architectural features and elements which can be experienced by pedestrians.

### Policy 8.7.2.6

Council will encourage the buildings facades to be visually interesting through extensive use of street level entrances and windows. Functions which do not directly serve the public, such as loading bays and blank walls, should not be located directly facing the street.





FACADE STEPBACK AT FOURTH FLOOR

THE FACADE WILL INCORPORATE BRICK AND OTHER MATERIALS THAT RELATES TO THE COMMERCIAL CORE AND OLD SANDWICH TOWN

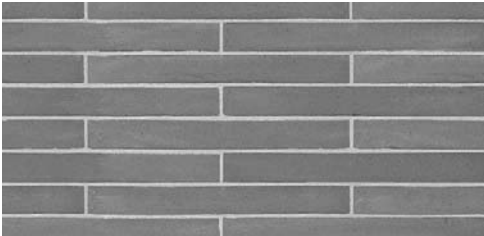
CONTINUOUS CANOPY HIGHLIGHTING AND FRAMING RETAIL SPACE ALONG SANDWICH ST. AND CHAPPELL AVE

LANDSCAPING AND PEDESTRIAN SEATING ALONG SANDWICH ST. AND CHAPPELL AVE. TO PROMOTE PEDESTRIAN USE AND MOVEMENT

Figure 7. Perspective



PRIMARY BRICK 1



PRIMARY BRICK 2



METAL ACCENT



# GUIDANCE

## City of Windsor Official Plan

**Objective 8.8.1.1** To use public space to enhance the image of Windsor.

**Objective 8.8.1.2** To clearly define the boundaries and edges of public space and their access points to form an identifiable, safe and inviting space.

**Objective 8.8.1.3** To create a variety of public spaces which accommodate a broad range of activities and encourage year round use.

**Policy 8.8.2.1** For the purpose of this Plan, public space includes all lands within public rights-of-way, open space areas, elements of the Green way System and other privately owned areas intended for public use.

**Policy 8.8.2.2** Council will promote the design of public spaces to define and complement the image of Windsor and its neighborhoods.

**Policy 8.8.2.6** Council will promote the creation of public spaces which accommodate a range of human social contact, from individual contemplation and private conversations to group activities and festivities

**Policy 8.8.2.7** Council will ensure that spaces which are privately-owned but publicly accessible, such as plazas, landscaped setbacks, or courtyards, support the function and enhance the appearance of the streetscape.

**Policy 8.9.2.1** Council may identify views and vistas which:

- contribute to the image of Windsor;
- provide orientation for residents and visitors; and
- foster a sense of anticipation and arrival.

**Policy 8.9.2.7** Council will promote the use of framing elements to enhance significant views and vistas in Windsor and will consider the installation of landscaping elements and light standards; the siting, profile and massing of a proposed development; and the location of infrastructure and other urban elements in the implementation of this policy.

**Policy 8.9.2.9** Council will ensure that the transportation system is designed and laid out to provide the best vantage points for significant views and vistas.

**Policy 8.11.2.10** Council will promote the development of Mainstreets at the locations identified on Schedule G: Civic Image. Such Mainstreets will be designed to:

- promote a diverse mixture of commercial, residential and other appropriate land uses along the road;
- encourage pedestrian activity and movement along the streetscape; and
- provide and/or enhance the unique character of the surrounding neighborhood.

**Policy 8.11.2.14** Council will promote the designation of cycling routes and segregation of movement by design features such as distinctive surface treatments, painted lines and symbols subject to appropriate design and engineering guidelines.

**Policy 8.11.2.15** Council will ensure the ease of orientation along the pedestrian and cycle networks through the provision of signs, route maps and key views.

**Policy 8.11.2.16** Council will consider the use of gateways, signs, decorative sidewalks, sculpture and other features at points along roads and/or routes where it is appropriate to emphasize the entrances to the city or its neighborhoods.

**Policy 8.11.2.17** Council will ensure that seating along roads is provided as required and is designed to:

- provide comfort for pedestrians at waiting areas, bus stops and near public facilities and institutions;
- support activities along the road in commercial or mixed use areas;
- support conversation and social interaction through the appropriate location and orientation of seating;
- provide a degree of protection from inclement weather;
- provide seating surfaces in proportion to the intensity of activities and the size of the space; and
- encourage an active street-life in all seasons.

**Policy 8.11.2.18** Council may support sidewalk cafes subject to appropriate design guidelines.

**Policy 8.11.2.19** Council will encourage the partial screening of surface parking lots through the use of low fences, walls, berms and other landscape elements, and through the location of lots away from street view, while still permitting views for orientation and safety

**Policy 8.11.2.21** Council will encourage parking lots that avoid large expanses fronting the road













Figure 8. Perspective

Based on urban the urban study review, it is in our opinion that the proposal adheres to the design direction of Old Sandwich Town Community Improvement Plan and the City of Windsor Official Plan.

The proposal addressed site design and orientation, built form, public realm, landscape design, architectural design and will be of high quality to meet the City of Windsor Standards.

This project will be a missing link for Sandwich Town, linking and marking the arrival and exit of the neighbourhood.





**SOIL & MATERIALS ENGINEERING INC.**  
**CONSULTING ENGINEERS**

**Report on**

Geotechnical Investigation for the  
Proposed 11-Storey Multi-Use Development  
3885 Sandwich Street, Windsor, Ontario

**Report Issued to**

Tunio Developments Inc.  
2089 Osbond Road  
Innisfil, ON L9S 0A9

**Attention: Khurram Tunio**  
[khurramtunio@yahoo.com](mailto:khurramtunio@yahoo.com)

**Date of Report**

May 21, 2021

**Job No.**

20G085  
Rev.: 00

**Distribution of Report**

1 e-copy – Tunio Developments Inc.  
1 e-copy – Baird AE

*GEOTECHNICAL ENGINEERING AND CONSTRUCTION MATERIALS INSPECTION & TESTING*

2000 Legacy Park Drive • Windsor, Ontario • N8W 5S6

Phone 519-966-8863 • Fax 519-966-8870 • email: [ctsoil@ctsoil.com](mailto:ctsoil@ctsoil.com) • [www.ctsoil.com](http://www.ctsoil.com)

Consolidated Council Agenda - June 13, 2022

Page 572 of 1025

---

## EXECUTIVE SUMMARY

In accordance with the request and authorization from Mr. Khurram Tunio, representing Tunio Developments Inc., Soil & Materials Engineering Inc. completed a geotechnical investigation for the proposed 11-storey multi-use building development on 3885 Sandwich Street, Windsor, Ontario.

The geotechnical investigation includes seven conventional augered and sampled testholes and two specialty Marchetti-type flat plate dilatometer (DMT) probe tests. Subsequent lab testing on the samples obtained during the investigation was completed. Based on the results of the geotechnical investigation, Soil & Materials Engineering Inc. offers the following conclusions regarding the geotechnical recommendations:

- Ultimate Limit States Factored Bearing Resistance for Isolated square footings depending on depth of placement, capacity ranging from 90 kPa to 120 kPa. Continuous strip footings similarly varying with depth 75 to 105 kPa.
- Serviceability Limit States Unfactored Geotechnical Resistance for Isolated square footings depending on depth of placement, allowable stress ranging from 60 kPa to 80 kPa. Continuous strip footings similarly varying with depth 50 kPa to 60 kPa. These limits ensure less than 25 mm of settlement, for limited size footings.
- Alternative foundation considerations both intermediate and deep
- Floor slab-on-grade design and exterior pavement design recommendations
- Excavation recommendations that include safe side walls from the bottom at a minimum gradient of 1H:1V commencing 1.2m from the base of the excavation in accordance with Provincial Regulations, after dewatering, if excavations are less than 3 metres deep.
- Pavement construction
- A class "E" site classification for seismic site response.

Table of Contents

	<u>Page</u>
Executive Summary	
<b>1.0 Introduction</b>	<b>5</b>
<b>2.0 Investigative Program</b>	<b>6</b>
<b>3.0 Site Conditions</b>	<b>8</b>
3.1 Geological Condition	8
3.2 Soil Condition	8
3.2.1 Pavement Composition	9
3.2.2 Fill	9
3.2.3 Sand	9
3.2.3.1 Sand with Silt	9
3.2.3.2 Silty Sand	9
3.2.4 Varved Clay	9
3.2.5 Silty Clay	9
3.2.6 Sand and Silt Lenses	10
3.2.7 Abladed Till	10
3.2.8 Limestone	10
3.3 Groundwater	10
<b>4.0 Discussion and Recommendations</b>	<b>12</b>
4.1 Foundations	12
4.1.1 Ultimate Limit States	12
4.1.2 Serviceability Limit States	13
4.1.3 Alternate Intermediate Foundation Considerations	14
4.1.3.1 Drilled Pier Foundations	14
4.1.3.2 Helical Pier Foundations	15
4.1.3.3 Rammed Aggregate Piers	15
4.1.4 Reinforced Concrete Raft Foundation	16
4.1.5 Deep Foundation Considerations	16
4.2 Floor Slab-On-Grade	18
4.2.1 Floor Slab-on-Raft	19
4.3 Lateral Soil Pressures	20
4.4 Excavation and Construction	20
4.5 Pavement Construction	21
4.6 Seismic Site Conditions	22
<b>5.0 Closure</b>	<b>23</b>

**List of Tables**

Table 1: Depth of Testholes

Table 2: U.L.S Factored Bearing Resistance

Table 3: Geotechnical Resistance (Unfactored) at S.L.S

Table 4: Pavement Structure Recommendation

**List of Drawings**

Drawings 1 through 2A: Site Maps

Drawings 3 through 12: Logs of Testholes

Drawing 13: Consolidation Testing

Drawing 14: Atterberg Limits

**List of Appendices**

Appendix 'A' – Purcka Geo-Structural Preliminary Settlements Analysis

Appendix 'B' – Bedrock Cores

Appendix 'C' – WEAP Analysis using Geotechnical Subsurface Data



## 1.0 INTRODUCTION

Mr. Khurram Tunio, representing Tunio Developments Inc., authorized Soil & Materials Engineering Inc. to complete a Geotechnical Investigation for the proposed 11-storey multi-use building development located at 3885 Sandwich Street, Windsor, Ontario. The area of development is a currently a vacant grassland.

The scope of work is to carry out a geotechnical investigation for project design purposes and to prepare a geotechnical report based on soil borings and laboratory testing.

This report discusses the results of our investigation with respect to the proposed development. The results of the fieldwork and laboratory testing programs were used to determine the relevant soil and groundwater parameters at this site. The recommendations contained in this report refer to the geotechnical aspect of the soil conditions encountered in the exploratory holes.

This report has been prepared using Systeme Internationale (S.I.) metric units. Field and laboratory testing has been completed in general accordance with the applicable American Society for Testing and Materials (ASTM) standards. The engineering principles applied in the development of the recommendations herein are in general accordance with the Canadian Foundation Engineering Manual 4th Edition, 2007 (CFEM), National Building Code of Canada, 2006 (NBC), and Canadian Highway Bridge Design Code (CSA-S6-06).

## 2.0 INVESTIGATIVE PROGRAM

The field work portion of the geotechnical investigation consisted of ten augered and sampled testholes; conventional testholes (Testhole 2, and P1 through P5, & TW Sample), and two Specialty Marchetti-type flat blade dilatometer probes (1-DMT and 3-DMT) completed in accordance with ASTM D6635 at the approximate locations indicated on the Site Maps (Drawing 1 and 2). Testholes P01 through P06 were denoted for the use of pavement construction, testholes 1-3 were used for foundation design (shallow, intermediate, and deep). The advancement of the testholes was facilitated with a truck-mounted power auger machine owned and operated by C.T. Soil & Materials Testing Inc. The truck-mounted drill unit is equipped with hollow stem augers, solid stem augers, and conventional soil and rock sampling tools. The testholes were completed during the period of December 2020 and February 2021 under the direction of a geotechnical engineer. The testhole information from the geotechnical investigation is presented in graphical form in the Log of Testholes (Drawings 3 through 12). A summary of the Testholes for the geotechnical investigation are shown in the following table.

**TABLE 1: Depth of Testholes**

Testhole	Depth (m)
1-DMT	34.15 + 2.05 of Bedrock
2	15.25
3-DMT	29
P1 through P6	2
TW- Sample	10.65

Soil samples were retrieved at frequent intervals of depth using the Standard Penetration Test Method (ASTM D1586). To assess the in-situ soil strength, field vane tests were conducted where applicable in accordance with ASTM D2573 as well as through the DMT probe. Relatively undisturbed sample was obtained at selective depth using a thin-walled sampler (Shelby tube). The retained soil samples were field logged, placed in suitable containers and transported to our laboratory for further

detailed examination and testing. Coring was completed in accordance with ASTM D2113, recorded on the logs, and presented on testhole 1-DMT (Drawing 3).

Testhole 1-DMT was advanced through the soil column and into the bedrock. Casing was set into the rock and the testholes were further advanced by continuous diamond core sampling (Nx size). Upon retrieval of individual run of the core barrel, the rock core was logged and placed in wooden core boxes for detailed examination and testing in the laboratory. Field Rock Quality Designation (RQD) was determined on each sample run.

The testhole locations were staked out in the field by Soil & Materials Engineering Inc. A level survey was completed in order to relate the testholes to a common datum. The site benchmark used for this geotechnical investigation is the top of fire hydrant on the north side of Chappell Avenue as indicated on the site maps (Drawings 1 and 2). The benchmark was given an assigned elevation of El. 181.25 metres. The depths and elevations presented in this report were derived for the sole benefit of the geotechnical analysis and stratigraphic evaluation by the geotechnical engineer. The depths and elevations presented in this report should not be used by others, for any other purpose.

The laboratory testing included a detailed visual and tactile examination of the retrieved samples along with soil moisture content, unit weight determinations, consolidation testing in accordance with ASTM D2435 (Drawing 13), and plasticity index in accordance with ASTM D4318 (Drawing 14) on selected soil samples. The soil moisture content and unit weight results are presented on the Testhole Logs (Drawings 3 through 12). Laboratory or field pocket penetrometer results to assess undrained shear strength were taken on selected samples and plotted on the enclosed logs.

### 3.0 SITE AND SUBSURFACE CONDITIONS

The new 11-storey development will be located within a vacant rectangular piece of grassland located on 3885 Sandwich Street, Windsor, Ontario, east corner of the Chappell Avenue and Sandwich Street intersection. The new development will consist of an 11-storey mixed use rectangular building and the bulk of the property is flat and generally level with grade changes of  $\pm 0.5$  metres.

#### 3.1 Geological Condition

The site is located within the western part of an extensive clay plain formation (Essex Clay Plain) characterized by fairly uniform geologic features. Standing between the basins of Lake Erie and Lake St. Clair, the surface is, essentially, a till plain overlying middle Devonian sedimentary rocks (limestone and shale). Limestone underlies most of the Essex County area; shale beneath the northern and eastern portion of Windsor. The thickness of the soil overburden in the general area is approximately 35 metres. We reached bedrock at this site at 34 metres.

At various sites in the region, gas and/or oil pockets, and/or artesian sulphur water are encountered near or immediately below the bedrock surface.

No major faults or dislocations due to the breaking of the Palaeozoic bedrock are reported in South-western Ontario.

#### 3.2 Soil Condition

The stratigraphic and interpreted boundaries in the testholes were obtained from 38 mm diameter samples retrieved from 165 mm diameter auger holes. The soil undrained shear strength consistency, as provided on the enclosed Testhole Logs (Drawings 3 through 12), are based on "N"-values determined from the Standard Penetration Test Method (ASTM D1586), reactions to the DMT (ASTM D6635), and field vane shear tests (ASTM D2573) results, as well as visual and tactile examination of the soil samples.

### **3.2.1 Pavement Composition**

At ground surface testholes 2, P1, P2, P4, and P5 encountered an initial surface asphalt layer of 50 mm, few of the testholes were followed closely by gravel fill, 150 mm to 250 mm below ground surface.

### **3.2.2 Fill**

At ground surface and below the above pavement composition, the testholes encountered a heterogeneous mixture of black organic sandy topsoil, clay, glass, sand, gravel, brick, and cinder within various testholes. The fill varies in depth from 0.35 m to 1.05 m.

### **3.2.3 Sand**

Below the topsoil, the site is underlain by fine to medium grained sand, silt content varies within testholes. The deposit encountered in Testhole 2 consists of medium-grained sand, brown colour, is in a general “compact” state, and terminates at 3.05 metres below grade.

#### **3.2.3.1 Sand with Silt**

Sand with silt is encountered in testholes P1, P3 and P6 terminating at the depth of the testholes, 1.95 metres below grade. The sand with silt is dry, fine to medium grained, brown in colour, and has a “loose” to “compact” compactness condition.

#### **3.2.3.2 Silty Sand**

Silty Sand is encountered in most of the investigated testholes. The silty sand is fine to medium grained sand with silt and varies between 1.95 metres and 2.3 metres below grade. The deposit of fine-grained sandy silt is brown in colour, and is generally found to be in a “loose” to “compact” state.

### **3.2.4 Varved Clay**

Beneath the non-cohesive sand deposit, the testholes encountered cohesive clay varved with silt and fine sand laminations. The varved clay is grey in colour and has a “soft” undrained shear strength consistency. The cohesive soil is present to a depth of 20 metres below grade, the undrained shear strength increases to “firm” below 12.0 metres of depth.

### **3.2.5 Silty Clay**

The varved clay is underlain by grey silty clay containing embedded sand and gravel. The structure and texture of the deposit has glacio-fluvial characteristics. The undrained shear strength is



greater than the overlying varved deposit, having a “stiff” undrained shear strength, somewhat decreasing with increased depth. The depth extent of the silty clay was found to extend from 20 metres until 30 metres below grade.

### **3.2.6 Sand and Silt Lenses**

Within the major silty clay stratum, non-cohesive, water bearing lenses of gravel, sand and silt can be found. Such lenses were observed in some of the retained samples.

### **3.2.7 Abladed Till**

Beneath the non-cohesive soil, the testholes interpreted an encountered a stratum of ablated till (hardpan). The ablated till has a soil matrix composed of “hard” silty fine sand with clay and frequent occurrences of embedded cobbles, boulders and limestone ledge-rock.

### **3.2.8 Limestone (Bedrock)**

Testhole 1-DMT was advanced into the underling sound bedrock. Sound bedrock (free of underlying soil) was encountered at 34 metres below ground surface.

The bedrock was sampled using an Nx size diamond core barrel. Core recovery was 100% in the sound bedrock. Rock Quality Designation (RQD) was determined to be 100%. The rock is limestone to dolomitic limestone with frequent light to medium hydrocarbon staining, occasional distinct strolitic bedding and occasional porous zones. The sub-horizontal fracturing follows planes of weakness. Fracture location and frequency are mapped on the enclosed Log of Testholes. Bedrock core photos can be found in Appendix ‘B’

## **3.3 Groundwater**

The groundwater level and the depth that the testhole remained open when measured after the completion of drilling is recorded on the enclosed Log of Testholes.

Free flowing groundwater was flowing in through the sand layer 3 metres below ground surface. Additionally, water level was recorded at 2.75 metres after 24 hours upon completion. Inferred water levels are also plotted on the enclosed Testhole Logs (Drawings 3 through 12), although water levels can be as high as ground surface at various times of the year.

---

The long-term lowest level of the groundwater at this site is generally associated with the interface of the brown and grey silty clay soil, the grey colour indicating permanently saturated conditions. Therefore, based on the testhole information, it appears that the long-term lowest level of groundwater level is located approximately 3.1 metres below existing grades.

Perched water may be encountered in the surficial topsoil and fill materials at wetter times of the year. Perched groundwater conditions result from the fill materials or soil fissures having the capacity to temporarily store water from rain and snow melt before it percolates to the long-term level of the groundwater table.

## 4.0 DISCUSSION AND RECOMMENDATIONS

Tunio Developments Inc. is proposing to develop an 11-storey multi-use development on 3885 Sandwich Street, Windsor, Ontario. The existing property is currently a vacant grassland undeveloped, with past development as revealed during the investigation. The building is basementless at the time of this report with 11 stories above grade, however, recent findings in this report may lead to alternative considerations. Beyond the building footprint the property will be used for car parking, as shown on the site maps (Drawings 1 and 2).

### 4.1 Foundations

The exploratory testholes completed at this site reveal variable shallow subsurface conditions in terms of soil composition and compactness condition or shear strength consistency. Below the fill, the site is characterized by “loose” to “compact” non-cohesive sand layer to a depth of 3.05 metres below grade. Beneath the sand, “soft” to “firm” varved clay is encountered to a depth of 20.0 metres followed by “stiff” silty clay containing embedded sand and gravel to a depth of 30 metres below grade. Presumably following the silty clay, albladed till can be found overlaying the bedrock to a depth of 34 metres below grade, the depth extent of the investigation. The water table at the time of the investigation was measured to be 2.75 metres below grade at the time of the fieldwork portion of the geotechnical investigation.

The bearing capacity of shallow foundations was therefore calculated for this report based on the presence of a non-cohesive soil, overlying “soft” varved marginally over-consolidated cohesive soil. (i.e. two layer or three-layer soil model). The following subsections presents geotechnical resistance referenced to the ultimate limit states and the serviceability limit states for conventional shallow spread foundations at this site

#### 4.1.1 Ultimate Limit States

The factored net geotechnical resistance at ultimate limit states (geotechnical resistance factor,  $\Phi$ , of 0.5) that may be used for conventional spread footing foundations are presented below.

**TABLE 2: U.L.S Factored Bearing Resistance**

Depth (m)	Factored Geotechnical Resistance, $\Phi R$ , at U.L.S. (kPa)*	
	Isolated Square	Continuous Strip
u/s fill/topsoil to El. 178	120	105
El. 177.5 to El. 176	90	75

\* Some locations may require increased footing width (lower bearing pressure) in order to achieve the stipulated performance criteria.

The above factored geotechnical resistance at ultimate limit states incorporates 0.50 as an applied resistance factor,  $\Phi$ , to the ultimate geotechnical resistance. These values are net of the lowest surcharge pressure on the soil surrounding the footing.

#### 4.1.2 Serviceability Limit States

For geotechnical calculation purposes, the gross bearing pressure at serviceability limit states has been taken for that pressure to generate 30 mm of total settlement beneath the footing, thereby generally assuring less than 25 mm differential settlement between any two foundation units. The composition of the unfactored loads to generate the calculated settlement will be dependent on the long-term sustained loading conditions which will include 100% of the dead loads and likely only a portion of the live loads and no component for transient loads such as wind or earthquake.

Geotechnical resistance at serviceability limit states that may be used for conventional spread footing foundations less than 3 metres (isolated square) and 2.0 m (continuous strip) are presented below.

**TABLE 3: Geotechnical Resistance (Unfactored) at S.L.S.**

Depth (m)	Geotechnical Resistance at S.L.S. (kPa)	
	Isolated Square	Continuous Strip
u/s fill/topsoil to El. 178	80	70
El. 177.5 to El. 176	60	50

\* Some locations may require increased footing width (lower bearing pressure) in order to achieve the stipulated performance criteria.

Foundations exceeding the above design chart or maximum footing width may be feasible; however, a detailed geo-structural interaction analysis must be completed for proper evaluation.

#### **4.1.3 Alternate Intermediate Foundation Considerations**

Sections 4.1.1 and 4.1.2 present the foundation requirements for foundations placed on native soil at this site and may not have the capacity needed for the structural design. As a result, consideration should be given to alternate foundation systems such as drilled caisson foundations and intermediate depth rammed aggregate piers foundations. These will strengthen and give a better ULS, but it will not help with settlement (SLS).

##### **4.1.3.1 Drilled Pier Foundations**

Conventional drilled pier concrete foundations can provide high load capacities. Drilled pier foundations are well suited for end-bearing into rock or dense till and have been successfully used in stiff clays, as is the case for this site. We do not recommend advancing caisson foundations to the sandy gravel due to the documented presence of pressurized natural gas at this site.

For a drilled pier foundation with a minimum diameter of 0.75 metres and a maximum diameter of 1.2 metres, socketed into the “stiff” silty clay a minimum of four times the caisson diameter (3 metres, terminating at a depth of approximately 12 metres), the net allowable end-bearing capacity is calculated as 250 kPa.

Drilled pier foundations must be spaced at a distance greater than three times the caisson diameter. If placed within three times the caisson diameter, a capacity reduction will occur due to the group effect. A closely spaced pile group can act as a “block” whereby the soil between adjacent piles is dragged down between them, shaft resistance develops around the perimeter of the group only, and end-resistance develops under the whole of the pile-soil block.



For successful installation and result, the bottom of the excavation must be properly cleaned to avoid large settlements. Once the excavation has completed and verification of a clean auger base, concrete must be placed during one continuous operation to avoid soil cuttings in suspension to settle to the top of the concrete resulting in a defect. Pumping is the best method for concrete placement, although a tremie may be used with adequate safeguards. With either method, the concrete placement must be fast and continuous.

Free-falling concrete must be placed through a central chute, making it fan down the centre of the hole, well clear of the walls of the shaft. This results in adequate compaction below the upper 1.5 m. Vibration of the concrete in the upper 1.5 m is required to produce uniform strength concrete. Concrete slumps designed equal or exceeding 120 mm must not be vibrated but gently rodded.

If Drilled Pier foundations are selected, we recommend Soil & materials Engineering Inc be retained to complete an analysis provided that the building loads become known.

#### **4.1.3.2 Helical Pier Foundations**

Helical pier foundations are not recommended for this site due to the shear strength of the soil leading to a unpractical and uneconomical design. The helical piers would be taken to bedrock, at this time we recommend driven piles over helical piers. If helical piers wish to be considered, we recommend contacting PURCKA Geo-Structural Engineers for additional assessment for the design and installation of helical piles and micropiles..

#### **4.1.3.3 Rammed Aggregate Piers**

Rammed aggregate piers within the existing fill materials are an appropriate technology to improve stiffness and support as well as provide to all conventional spread footing foundations. The displacement process allows for installation with no spoils and eliminates the need for casing. Its performance and cost-effective qualities make it ideal for soils for that are subject to caving.

We recommend contacting a proprietary contractor (such as Geopier or similar) for design and implementation of intermediate-depth foundations. Soil & Materials Engineering Inc. can finalize contract with the Rammed Aggregate Pier designer.

#### **4.1.4 Reinforced Concrete Raft Foundation**

[Soil & Materials Engineering Inc.](#)

If a reinforced concrete raft foundation is a considerable design, a full basement should be considered. PURCKA Geo-Structural Engineers Inc. should be retained to complete a finite element analysis (FEA) once building loads are determined.

#### **4.1.5 Deep Foundation Considerations**

Driven piles have also been considered for this project. Alternate types of driven piles may be considered, however the resistance factors assumed in the geotechnical design may be affected, and therefore Soil & Materials Engineering Inc. should be contacted to review alternate proposals prior to implementation.

Shaft resistance is only mobilized when the driven pile end-bears in a soil stratum that offers resistance equal to or less than the overlying strata. In cases where piles are driven to rock or unyielding till, the load capacity must be supported by driving resistance and load tests.

We recommend that the tips of piles penetrating the sandy gravel layer and bearing on limestone to be Type I reinforced according to Ontario Provincial Standard Drawing (OPSD) 3000.100, revision 2. Splices should be completed in accordance with OPSD 3000.150, revision 1 provided the dynamic impedance of the pile is not changed at the splice section (in order to keep-valid the Pile Driver Analyzer (PDA) testing).

Soil and Materials Engineering Inc. has selected an HP310x110 (typical) that conform to CAN/CSA G40.20/G40.21, Grade 350W for the subject of this analysis. The selected HP would be driven to “dense” sand till at this site, 34.0 metres below grade and will mobilize an Ultimate Capacity of 1500 kN. This Ultimate capacity was calculated using GRLWeap, an engineering software that analyzes pile drivability in current site soil conditions. The factored ULS axial geotechnical resistance is anticipated to be 540 kN. This capacity can be verified in the field with a combination of inspection blow counts (Hiley Formula) correlated to a sufficient number of PDA field tests. Verification of that capacity, however, would require sufficient field testing to ensure capacity availability. If, however, the geotechnical resistance factor,  $\Phi$ , is to be increased beyond 0.5 and it is structurally permitted, then the following Code requirements must be considered.

According to Table 8.2 of the CFEM we extract the permissible geotechnical resistance factors:

**Soil & Materials Engineering Inc.**

- a. Deep Foundations – Static Analysis for Compression,  $\Phi = 0.4$
- b. Deep Foundations – Dynamic Analysis for Compression,  $\Phi = 0.4$
- c. Deep Foundations – Field-measured Dynamic Test (PDA),  $\Phi = 0.5$
- d. Deep Foundations – Field static load test,  $\Phi = 0.6$

Using the Canadian Highway Design Bridge Code, the same geotechnical resistance factors for a typical understanding can be used, in combination with a ULS and SLS consequence factor. The consequence level for this project would be considered high, therefore  $\psi = 0.9$ .

Given the potential for a “false set”, we recommend increasing the PDA testing frequency from 2% to 10% of all the piles.

We recommend that the geotechnical consultant provide continuous inspection during installation of all driven piles, including dynamic measurements using the PDA. We recommend that a minimum of one full-scale static load test be completed prior to installation of production piles, and that dynamic measurements and analysis be completed during installation of the test pile to verify driving requirements for the production piles.

Immediately upon completion of pile driving, the elevation of the top of pile must be measured through survey means. If subsequent piling of the pile in a pile group results in a vertical upward movement greater than 3 mm of a previously driven pile, then the affected pile must be retapped.

#### **4.1.6 Foundation Design (General)**

All the factored geotechnical resistance bearing pressures at ultimate limit states incorporate a factor,  $\Phi$ , of 0.5 against shear failure of the underlying soil strata (in accordance with the Canadian Foundation Design Manual, 4<sup>th</sup> Edition and Ontario Building Code (2012)). The expected total and differential settlements for footings constructed as outlined previously will be 20 mm and 25 mm, respectively.

The settlement of such foundations must be assessed in conjunction with the existing overburden pressure, foundation size and construction procedure. The existing grade should not be adjusted positively greater than 300 mm over current conditions without an impact assessment on projected settlements.

Some continuous footings or isolated square footings may be designed to be constructed at different elevations in the soil. In this case, dewatering to a level of 600 mm deeper than the intended excavation will be required along with sloping the footings such that the soil slope is cut no steeper than 2.5H:1V with a maximum slope height of one metre. Successive sloped sections must have a crest-toe separation of greater than 1.5 metres.

We recommend all soil bearing surfaces be inspected and approved by the Geotechnical Consultant to confirm that the soil exposed corresponds with the testhole observations and the design assumptions of the soil consistency. All exterior footings constructed adjacent to unheated areas must have a minimum of 1.2 metres of soil cover, or synthetic insulation of equal thermal value for protection against frost heave.

The native sand and clay at this site is sensitive and is subject to disturbance when exposed to construction traffic and adverse weather conditions. We therefore recommend placing a thin mat of lean concrete (mud mat) on the bearing surface immediately after inspection and approval in order to preserve its integrity.

If dewatering prior to construction is ineffectual, then it may be possible to construct foundations using the “dig-and-pour” method wherein concrete is placed immediately behind the footing excavation. We recommend full time inspection and verification by the geotechnical consultant if the dig-and-pour method is selected. A site coordination meeting prior to foundation excavation should be held with the geotechnical consultant present.

## **4.2 Floor Slab-on-Grade**

The existing organic soil and fill materials must be removed from beneath the floor slab areas. Immediately following excavation to final subgrade level for the proposed floor, we recommend placement of the approved granular under floor fill in order to avoid deterioration of the soil surface caused by construction traffic and adverse weather conditions.

**Soil & Materials Engineering Inc.**

The exposed subgrade should be proof rolled in the presence of the Geotechnical Consultant. Any "soft" areas encountered during proof-rolling and inspection must be subexcavated and replaced with approved fill such as Granular "B Type II" (OPSS 1010) and compacted to at least 98% of its Standard Proctor maximum dry density. Fill used to raise the grade of the floor slab should be constructed similarly.

Presuming a service loading condition of less than 15 kPa, immediately beneath the floor slab, we recommend placing a minimum of 300 mm of Granular "A" (OPSS 1010) compacted to 100% of its Standard Proctor maximum dry density to provide uniform and adequate subgrade bearing surface. The 300 mm granular "A" (OPSS 1010) should be underlain by a woven geotextile, Mirafi HP270, or a product of equal performance. The slab construction should incorporate a capillary break consisting of 100 mm of clearstone gravel (Granular "O", OPSS 1010) if the top of the floor slab will not be located above the general area grade. Heavier loaded floor slabs should be specifically reviewed by this office.

An appropriately placed vapour retarder is recommended beneath all slabs on grade constructions that include moisture sensitive floorings or areas requiring humidity control.

#### **4.2.1 Floor Slab-on-Raft**

We recommend placing a granular drainage layer covered with a vapour retarder on the raft slab and cast a floor slab on the granular base. The granular base must incorporate drainage with a suitable outlet.



### 4.3 Lateral Soil Pressures

Elevator shaft will be incorporated under the building, the backfill material should be a free draining granular material, such as Granular "B Type I" (OPSS 1010). The native site soil is not suitable for use as backfill against the retaining walls. Due to the relatively impermeable nature of the native silty clay and the presence of near-surface fill deposits, the granular backfill will tend to accumulate water. To provide drainage around the walls, a perimeter and subfloor drainage system should be installed at the footing level and suitably outletted.

The following unfactored soil parameters can be used to calculate the lateral earth pressure against the basement walls incorporating a drained backfill.

$$P_h = K (\gamma h + q)$$

where:  $P_h$  = the horizontal earth pressure at depth,  $h$  (kN/m<sup>2</sup>)

$K$  = the coefficient of earth pressure (at rest)

= 0.4 for compacted Granular "B Type I" backfill

$\gamma$  = the unit weight of backfill

= 19.4 kN/m<sup>3</sup> for imported Granular "B Type I" fill

$h$  = the depth below ground surface (m)

$q$  = the surcharge load, including live and transient loads (kN/m<sup>2</sup>)

The above pressures are applicable only if free draining granular fill is placed against the damp-proofed or water proofed walls with an appropriate perimeter drainage system at the footing level.

### 4.4 Excavation and Construction

Excavations through the surficial fill materials and native sandy silt to the depth of the prevailing groundwater table at approximately 0.75 metres below existing grades should have side walls safely sloped at a minimum gradient of 1H:1V in accordance with Ontario Provincial Regulations (O. Reg. 213/91, s. 234(1)). Side slopes cut at a steeper rate than this will require field assessment by the Geotechnical Consultant.

Excavations below a depth of 2.75 metres will encounter fully saturated conditions and will require dewatering to ensure stable side slope and excavation base. Dewatering should be completed prior to excavation.

**Soil & Materials Engineering Inc.**

If vertical walls are intended, excavations below a depth of 1.2 metres should be shored. The temporary shoring design can be completed by PURCKA Geo-Structural Engineers Inc. once project geometry is established.

Due to past developments on site, we anticipate encounters of relict farm-related drainage or foundation features at various locations across the site. We recommend all foundations, subgrades, and trench side walls to be inspected by this office at the time of excavation.

#### **4.5 Pavement Construction**

The fill and topsoil along with any other unsuitable materials should be completely stripped for the pavement subgrade. After stripping operations have been completed the exposed subgrade materials should be proof rolled in the presence of the geotechnical consultant. Excavations for subgrade within some areas may require additional fill removal if particularly incompetent. The exposed subgrade should be proof rolled, in the presence of the Geotechnical Consultant, to verify the general competency of the subgrade. General fill material required to raise the general subgrade can be composed of Granular "B Type I" (OPSS 1010) compacted to 98% of its Standard Proctor maximum dry density.

Once the subgrade has been proof-rolled and shaped to promote drainage (minimum 2% rate), the pavement materials can be placed on the approved subgrade. Back-of-curb subgrade drainage and catch basin stub-drains (2.5 metres long, keyed into the subgrade, and radiating in all four directions) are recommended for all pavements.

Table 4, below, addresses the recommended pavement composition for this project.

**TABLE 4: Pavement Structure Recommendation**

Thickness of Pavement (mm)		
Material	Parking	Driveways/Truck
HL-3 (OPSS 1150)	40	50
HL-4 (OPSS 1150)	50	60
Granular 'A' (OPSS 1010)	150	150
Granular 'B Type II' (OPSS 1010)	200	300
Geotextile (Mirafi HP370, or equal)	One layer	One layer

The granular base should be compacted to 100% of its Standard Proctor maximum dry density in lift thicknesses commensurate with the employed compaction equipment. All asphalt concrete should be placed in accordance with the Ontario Provincial Standards and compacted to 92% of the Marshall maximum theoretical density.

In areas of tight turning radii or standing loaded trucks, consideration should be given to the use of Portland cement concrete pavement comprised of 200 mm of CSA A23.1 concrete (5% to 8% air content for 20 mm nominal aggregate with a maximum water-cement ratio of 0.45) on 350 mm of Granular "A" (OPSS 1010) compacted to 100% of its Standard Proctor maximum dry density.

#### 4.6 Seismic Site Classification

The Ontario Building Code allows the site to be classified based on the weighted average shear strength or standard penetration resistance profile within the upper 30 metres of the soil column. Based on investigations at testholes for this site investigation terminating at a depth of 35 metres, we recommend the site to receive a Class "E" classification for seismic site response resulting from the weighted average shear strength and standard penetration resistance profile of the cohesive and non-cohesive soils encountered below the testhole depths. Further investigation may suggest otherwise.

## 5.0 CLOSURE

This report presents our interpretation of factual information obtained from the investigation and is intended for the use of the design engineer. Where comments are made related to construction, they are provided only in order to highlight aspects of construction that could affect the design of the project.

The number of boreholes required to determine the localized underground conditions between testholes affecting construction would be much greater than has been carried out for design purposes.

Further examination and investigation should be carried out in order to verify the adequacy of the information for construction that may affect the contractor with regards to construction techniques, schedule, equipment capabilities, cost sequencing, etc. This report addresses the geotechnical aspects of the subsurface conditions at the site pertinent to the proposed project only.

All testholes were tightly backfilled at completion. The Owner retains responsibility for future maintenance and pedestrian safety of the property.

It is beyond the scope of this investigation and report to address any issues related to health or environmental aspects of the proposed works.

Should local site conditions differ materially from that contained in this report, contact this office immediately for guidance. Do not hesitate to contact us should questions arise concerning the contents of this report. We would be pleased to meet with you at your convenience.

Regards,  
**Soil & Materials Engineering Inc.**

B. Purcka, EIT  
Geotechnical Engineering Division

T. O'Dwyer, P.Eng.  
Consulting Engineer



Drawings/Enclosures

**Soil & Materials Engineering Inc.**

# SITE MAP



## LEGEND:

- Test Hole
- ⊕ Monitoring Well
- ⊕ Test Well
- ☒ DMT
- ☐ Inclinator
- Test Pit



CT Soil & Materials Testing Inc  
2000 Legacy Park Dr  
Windsor, ON N8W 5S6  
Telephone: (519) 966-8863  
Fax: (519) 966-8870

**Project: 11-Storey Mixed Development**

**Location: 3885 Sandwich Street, Windsor, ON**

**Number: 20G085**

### Notes:

**Testhole Locations**

Client:  
*Tunio Developments Inc.*

**DRAWING**

**1**



# SITE MAP



## LEGEND:

- Test Hole
- ◆ Monitoring Well
- ⊕ Test Well
- ⊠ DMT
- Inclinator
- Test Pit



CT Soil & Materials Testing Inc  
2000 Legacy Park Dr  
Windsor, ON N8W 5S6  
Telephone: (519) 966-8863  
Fax: (519) 966-8870

**Project: 11-Storey Mixed Development**

**Location: 3885 Sandwich Street, Windsor, ON**

**Number: 20G085**

### Notes:

**Testhole Locations**

Client:  
*Tunio Developments Inc.*

**DRAWING**

**2**



# SITE MAP



## LEGEND:

- Soil Boring
- ⊕ Monitoring Well
- ⊕ Test Well
- ⊕ DMT
- ⊕ Inclinator
- ⊕ Test Pit



CT Soil & Materials Testing Inc  
2000 Legacy Park Dr  
Windsor, ON N8W 5S6  
Telephone: (519) 966-8863  
Fax: (519) 966-8870

Project: 11-Storey Mixed Development  
Location: 3885 Sandwich Street, Windsor, ON  
Number: 20G085

## Notes:

Testhole Locations

DRAWING

**2A**



**C.T. SOIL & MATERIALS  
TESTING INC.**  
WINDSOR ONTARIO

Client: **Tunio Developments Inc.**

Project: **11-Storey Mixed Development**

**Location:** 3885 Sandwich Street, Windsor, ON

## EQUIPMENT DATA

Machine: **Diedrich D50 Truck**  
Method: **83 mm I.D. H/S Auger**  
Size: **165 mm O.D.**  
Date: **2/22/2021** TO **2/24/2021**

[illegible]

REVIEWING PROFESSIONAL:  
**B. Purcka, B.A.Sc., EIT**  
Soil & Materials  
Engineering Inc.

**C.T. SOIL & MATERIALS  
TESTING INC.**  
2000 Legacy Park Drive  
WINDSOR, ONTARIO, N8W 5S6  
ph. (519) 966-8863, fx. (519) 966-8870  
email [ctsoil @ ctsoil.com](mailto:ctsoil@ctsoil.com)





**C.T. SOIL & MATERIALS  
TESTING INC.**  
WINDSOR ONTARIO

Client: **Tunio Developments Inc.**

Project: **11-Storey Mixed Development**

**Location: 3885 Sandwich Street, Windsor, ON**

## EQUIPMENT DATA

Machine: Diedrich D50 Truck  
Method: 83 mm I.D. H/S Auger  
Size: 165 mm O.D.  
Date: 2/22/2021 TO 2/24/2021

SOIL LITHOLOGY			SAMPLE			SHEAR STR.(kPa)				BULK UNIT WGHT (kN/m <sup>3</sup> )	WATER CONTENT (%)				WELL DATA	ELEV. (m)	REMARKS & GRAIN SIZE DISTRIBUTION % GR SA SI CL	
EL/DPT (m)	DESCRIPTION	DMT DEPTH (m)	SYMBOL	SAMP ID	TYPE	N-VALUE	DILATOMETER: Su (bars) ▼ FIELD VANE: Peak X Rem. ⊗ LAB TEST: Unc. ■ P.F. □				PENETR. RESISTANCE DMT Phi (◆) ◆							
							STANDARD ● DYN.CONE ○				<div> <div>Wp</div> <div>W</div> <div>Wl</div> </div>							
11.20 169.65	DMT Interpretation, silty clay (CL-ML) Su = 40 kPa	7.0	CLAY															
		7.1	CLAY															
		7.2	CLAY															
		7.3	CLAY															
		7.4	CLAY															
		7.5	CLAY															
		7.6	CLAY															
		7.7	CLAY															
		7.8	CLAY															
		7.9	CLAY															
		8.0	CLAY															
		8.1	CLAY															
		8.2	CLAY															
		8.3	CLAY															
		8.4	CLAY															
		8.5	CLAY															
		8.6	CLAY															
		8.7	CLAY															
		8.8	CLAY															
		8.9	CLAY															
		9.0	CLAY															
		9.1	CLAY															
		9.2	CLAY															
		9.3	CLAY															
		9.4	CLAY															
		9.5	CLAY															
		9.6	CLAY															
		9.7	CLAY															
		9.8	CLAY															
		9.9	CLAY															
		10.0	CLAY															
		10.1	CLAY															
		10.2	CLAY															
10.3	CLAY																	
10.4	CLAY																	
10.5	CLAY																	
10.6	CLAY																	
10.7	CLAY																	
10.8	CLAY																	
10.9	CLAY																	
11.0	CLAY																	
11.1	CLAY																	
11.2	CLAY																	
11.3	CLAY																	
11.4	CLAY																	
11.5	CLAY																	
11.6	CLAY																	
11.7	CLAY																	

REVIEWING PROFESSIONAL:  
**B. Purcka, B.A.Sc., EIT**  
Soil & Materials  
Engineering Inc.

**C.T. SOIL & MATERIALS  
TESTING INC.**  
2000 Legacy Park Drive  
WINDSOR, ONTARIO, N8W 5S6  
ph. (519) 966-8863, fx. (519) 966-8870  
email [ctsoil @ ctsoil.com](mailto:ctsoil@ctsoil.com)





C.T. SOIL & MATERIALS  
TESTING INC.  
WINDSOR ONTARIO

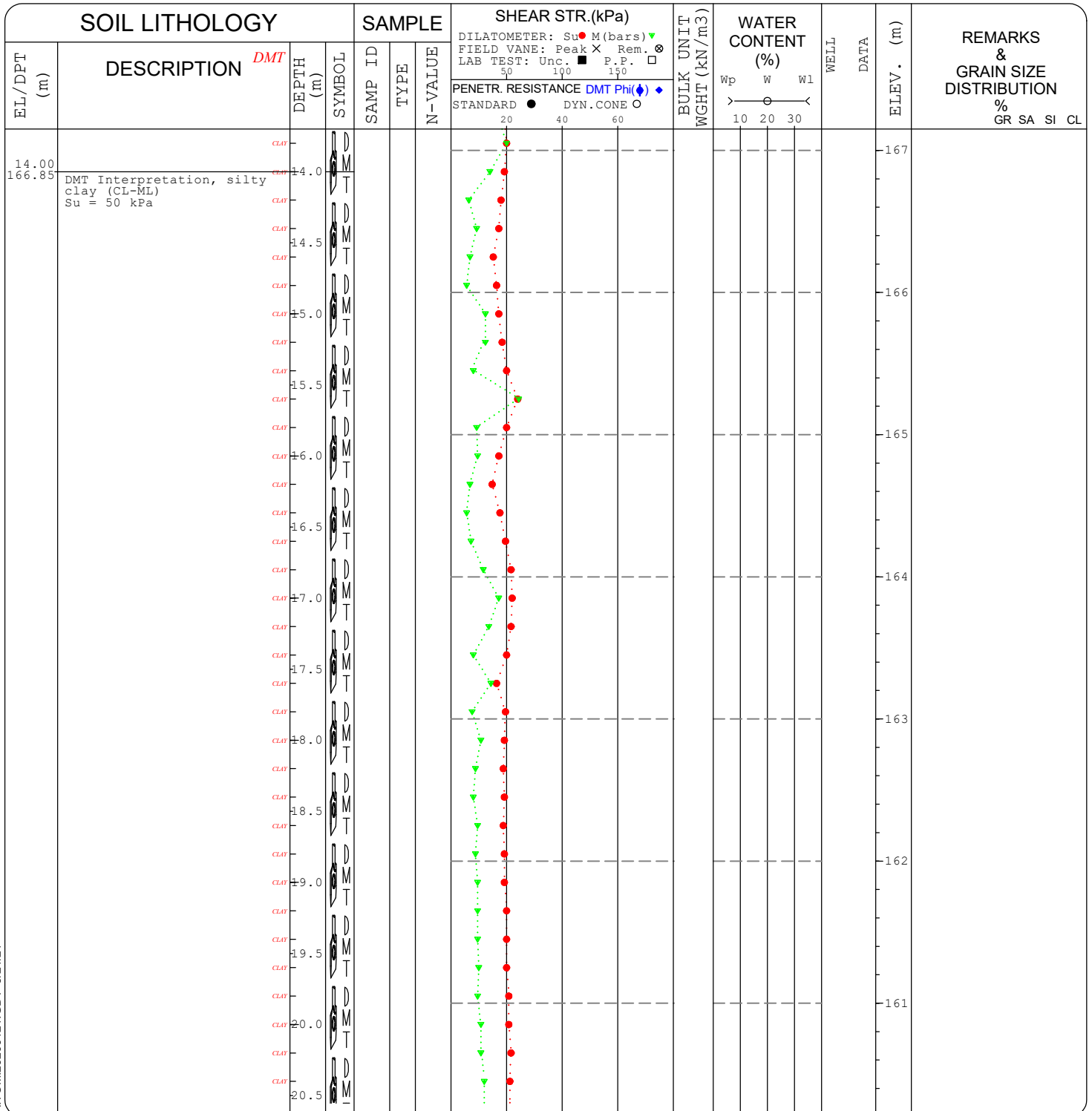
Client: Tunio Developments Inc.

Project: 11-Storey Mixed Development

Location: 3885 Sandwich Street, Windsor, ON

## EQUIPMENT DATA

Machine: Diedrich D50 Truck  
Method: 83 mm I.D. H/S Auger  
Size: 165 mm O.D.  
Date: 2/22/2021 TO 2/24/2021



REVIEWING PROFESSIONAL:  
**B. Purcka, B.A.Sc., EIT**  
Soil & Materials  
Engineering Inc.

C.T. SOIL & MATERIALS  
TESTING INC.  
2000 Legacy Park Drive  
WINDSOR, ONTARIO, N8W 5S6  
ph. (519) 966-8863, fx. (519) 966-8870  
email ctsoil @ ctsoil.com







C.T. SOIL & MATERIALS  
TESTING INC.  
WINDSOR ONTARIO

Client: Tunio Developments Inc.

Project: 11-Storey Mixed Development

Location: 3885 Sandwich Street, Windsor, ON

## EQUIPMENT DATA

Machine: Diedrich D50 Truck  
Method: 83 mm I.D. H/S Auger  
Size: 165 mm O.D.  
Date: 2/22/2021 TO 2/24/2021

SOIL LITHOLOGY			SAMPLE			SHEAR STR.(kPa)			BULK UNIT WGHT (kN/m <sup>3</sup> )	WATER CONTENT (%)			WELL DATA	ELEV. (m)	REMARKS & GRAIN SIZE DISTRIBUTION % GR SA SI CL
EL/DPT (m)	DESCRIPTION	DEPTH (m)	SYMBOL	SAMP ID	TYPE	N-VALUE	DILATOMETER: Su (bars) ▼	FIELD VANE: Peak X Rem. □		Wp	W	Wl			
20.60 160.25	DMT Interpretation, silty clay (CL-ML) Su = 60 kPa	CLAY	DMT				PENETR. RESISTANCE DMT Phi (kPa) ◆	STANDARD							
		21.0	DMT												
		21.5	DMT												
22.00 158.85	DMT Interpretation, silty clay (CL-ML) Su = 65 kPa	CLAY	DMT												
		22.5	DMT												
		23.0	DMT												
		23.5	DMT												
		24.0	DMT												
		24.5	DMT												
		25.0	DMT												
		25.5	DMT												
		26.0	DMT												
		26.5	DMT												
		27.0	DMT												

REVIEWING PROFESSIONAL:  
**B. Purcka, B.A.Sc., EIT**  
Soil & Materials  
Engineering Inc.

C.T. SOIL & MATERIALS  
TESTING INC.  
2000 Legacy Park Drive  
WINDSOR, ONTARIO, N8W 5S6  
ph. (519) 966-8863, fx. (519) 966-8870  
email ctsoil @ ctsoil.com





C.T. SOIL & MATERIALS  
TESTING INC.  
WINDSOR ONTARIO

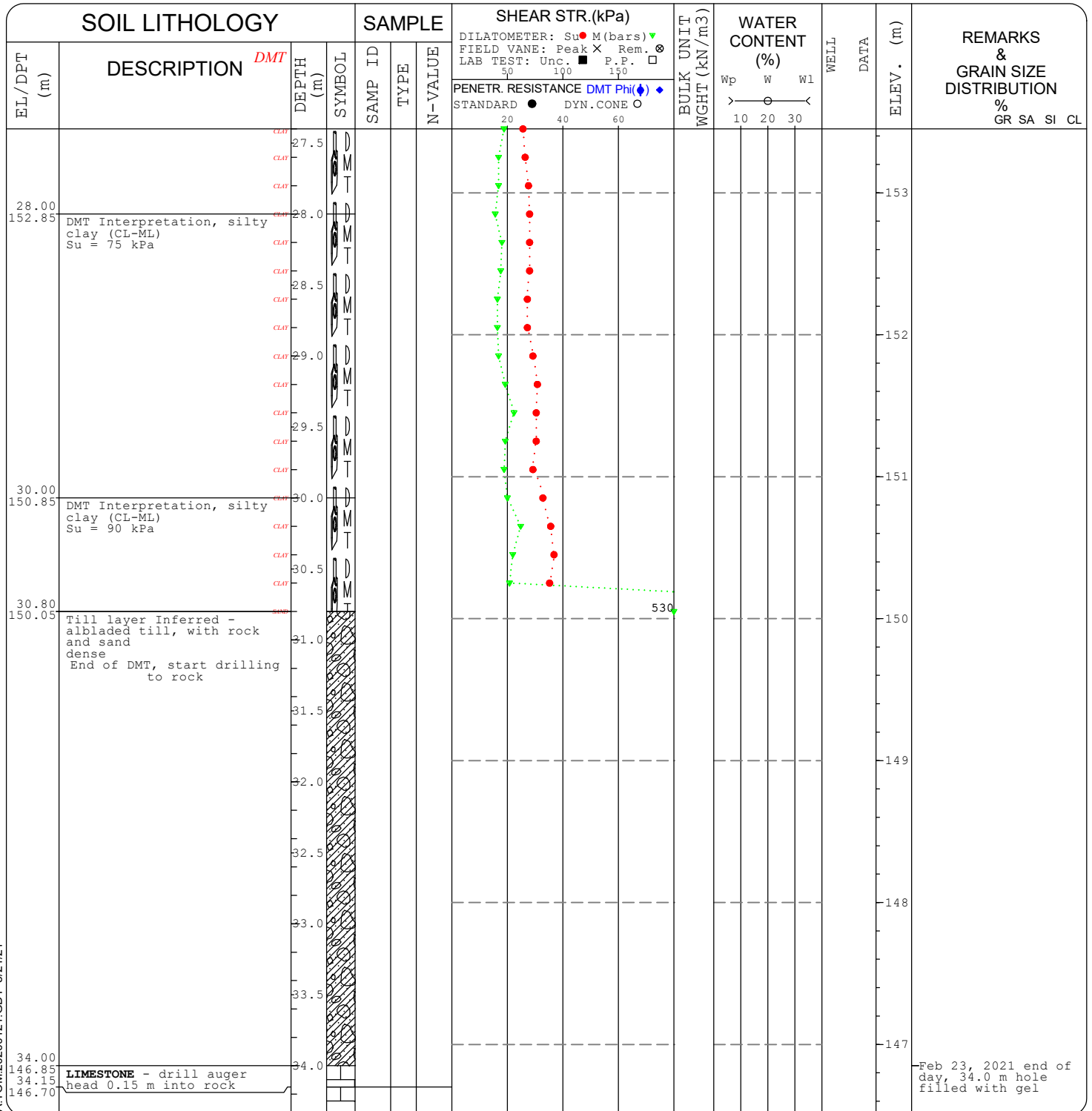
Client: Tunio Developments Inc.

Project: 11-Storey Mixed Development

Location: 3885 Sandwich Street, Windsor, ON

## EQUIPMENT DATA

Machine: Diedrich D50 Truck  
Method: 83 mm I.D. H/S Auger  
Size: 165 mm O.D.  
Date: 2/22/2021 TO 2/24/2021



REVIEWING PROFESSIONAL:  
**B. Purcka, B.A.Sc., EIT**  
Soil & Materials  
Engineering Inc.

C.T. SOIL & MATERIALS  
TESTING INC.  
2000 Legacy Park Drive  
WINDSOR, ONTARIO, N8W 5S6  
ph. (519) 966-8863, fx. (519) 966-8870  
email ctsoil @ ctsoil.com





**C.T. SOIL & MATERIALS  
TESTING INC.**  
WINDSOR ONTARIO

Client: **Tunio Developments Inc.**

Project: **11-Storey Mixed Development**

**Location: 3885 Sandwich Street, Windsor, ON**

## EQUIPMENT DATA

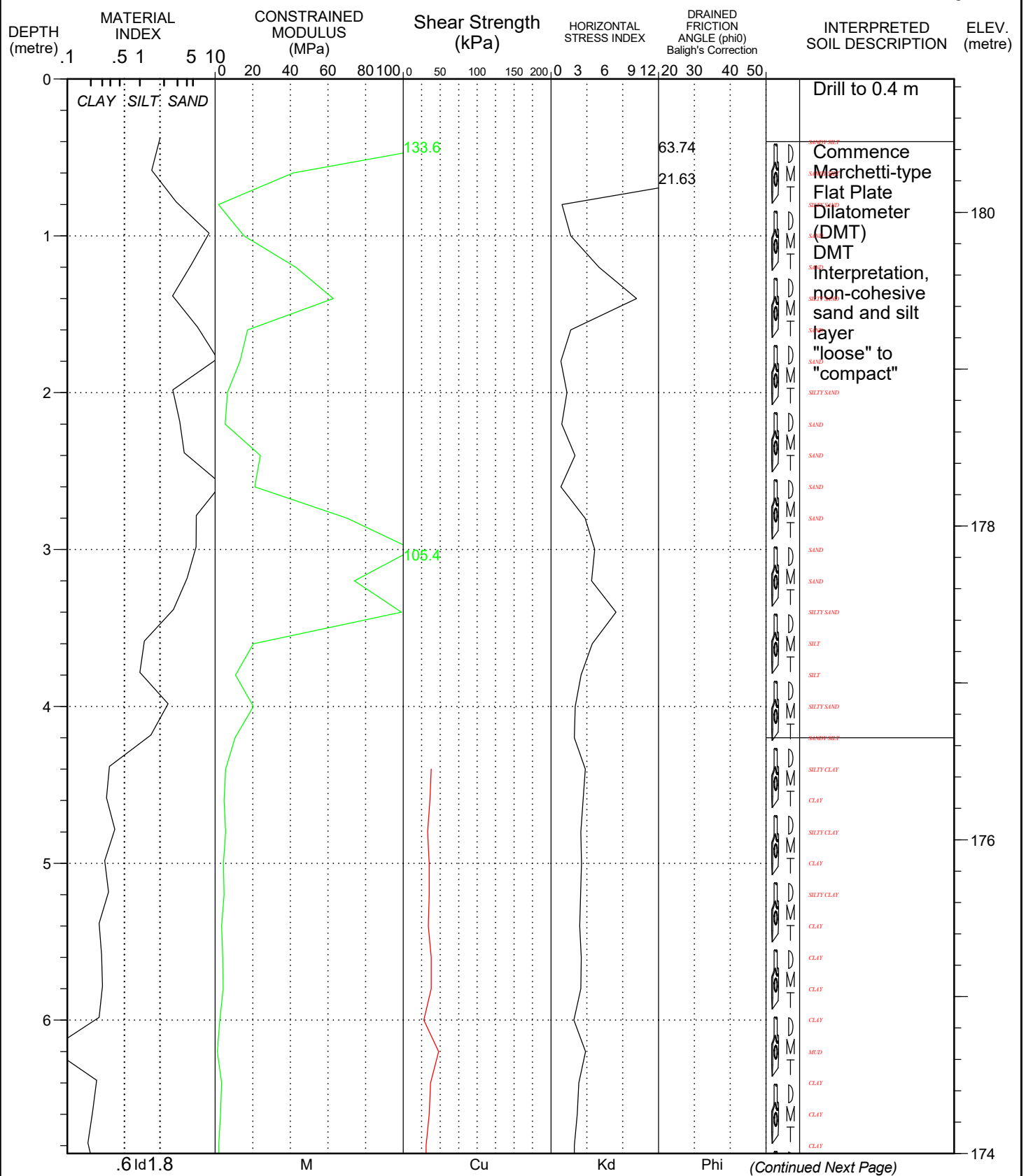
Machine: Diedrich D50 Truck  
Method: 83 mm I.D. H/S Auger  
Size: 165 mm O.D.  
Date: 2/22/2021 TO 2/24/2021


[illegible]

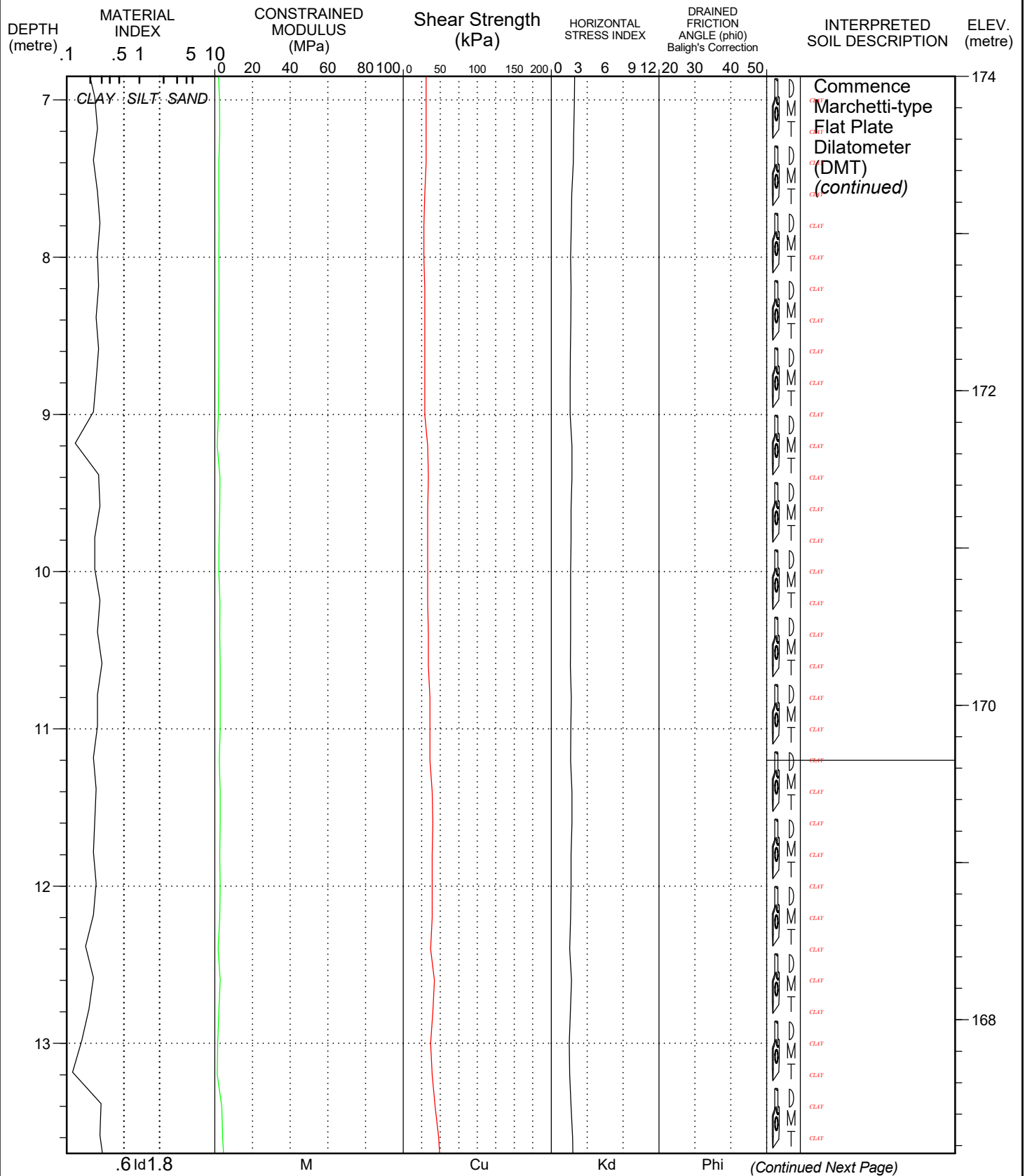
REVIEWING PROFESSIONAL:  
**B. Purcka, B.A.Sc., EIT**  
Soil & Materials  
Engineering Inc.


**C.T. SOIL & MATERIALS  
TESTING INC.**  
2000 Legacy Park Drive  
WINDSOR, ONTARIO, N8W 5S6  
ph. (519) 966-8863, fx. (519) 966-8870  
email [ctsoil @ ctsoil.com](mailto:ctsoil@ctsoil.com)



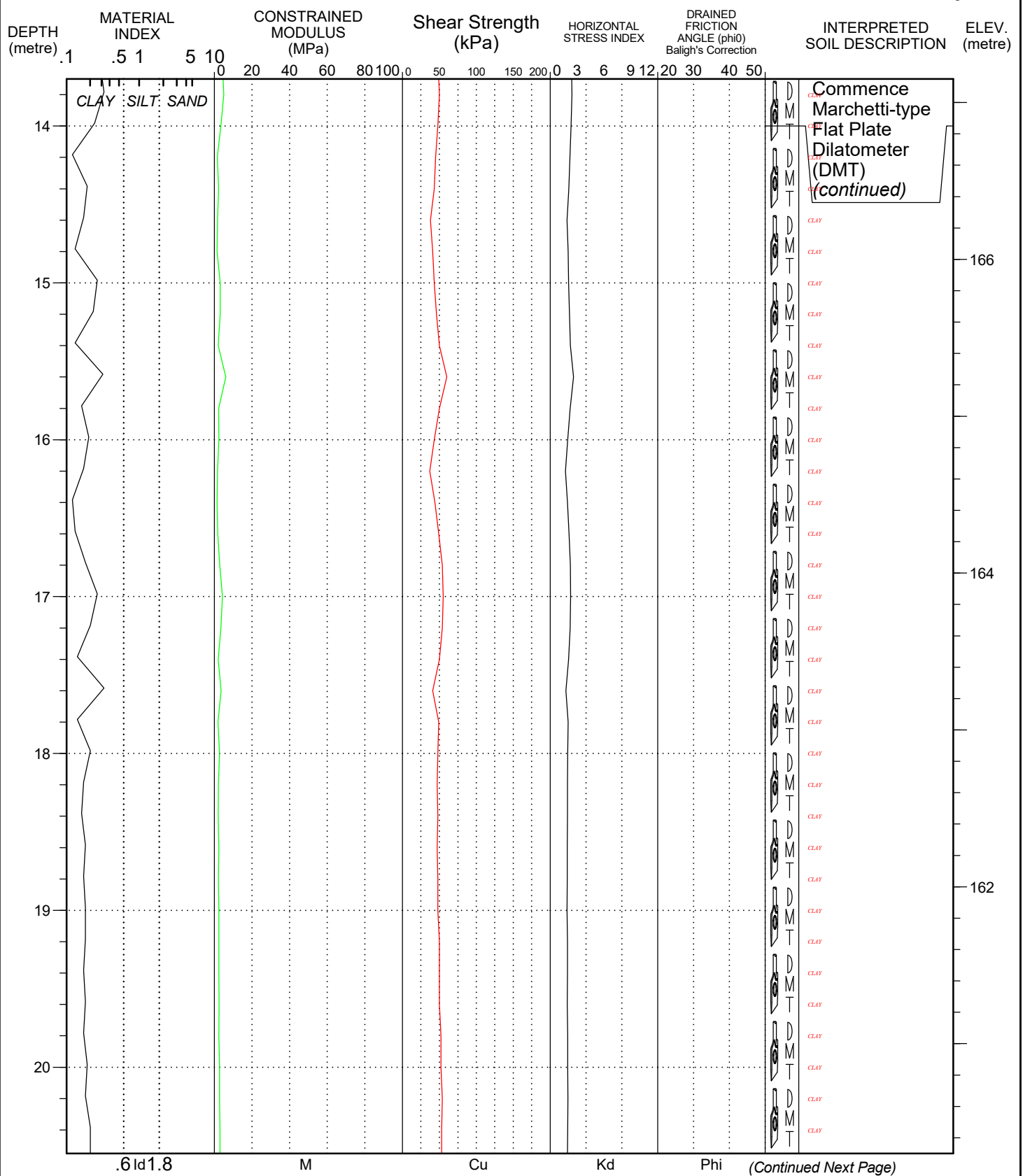



 <p>CT Soil &amp; Materials Testing Inc 2000 Legacy Park Dr Windsor, ON N8W 5S6 Telephone: (519) 966-8863 Fax: (519) 966-8870</p>	<b>LOG OF 1-DMT</b>		
	11-Storey Mixed Development 3885 Sandwich Street, Windsor, ON		
	PROJECT NO.: 20G085	REVIEWED BY: B. Purcka, B.A.Sc., EIT	DATE ADVANCED: 2/22/2021

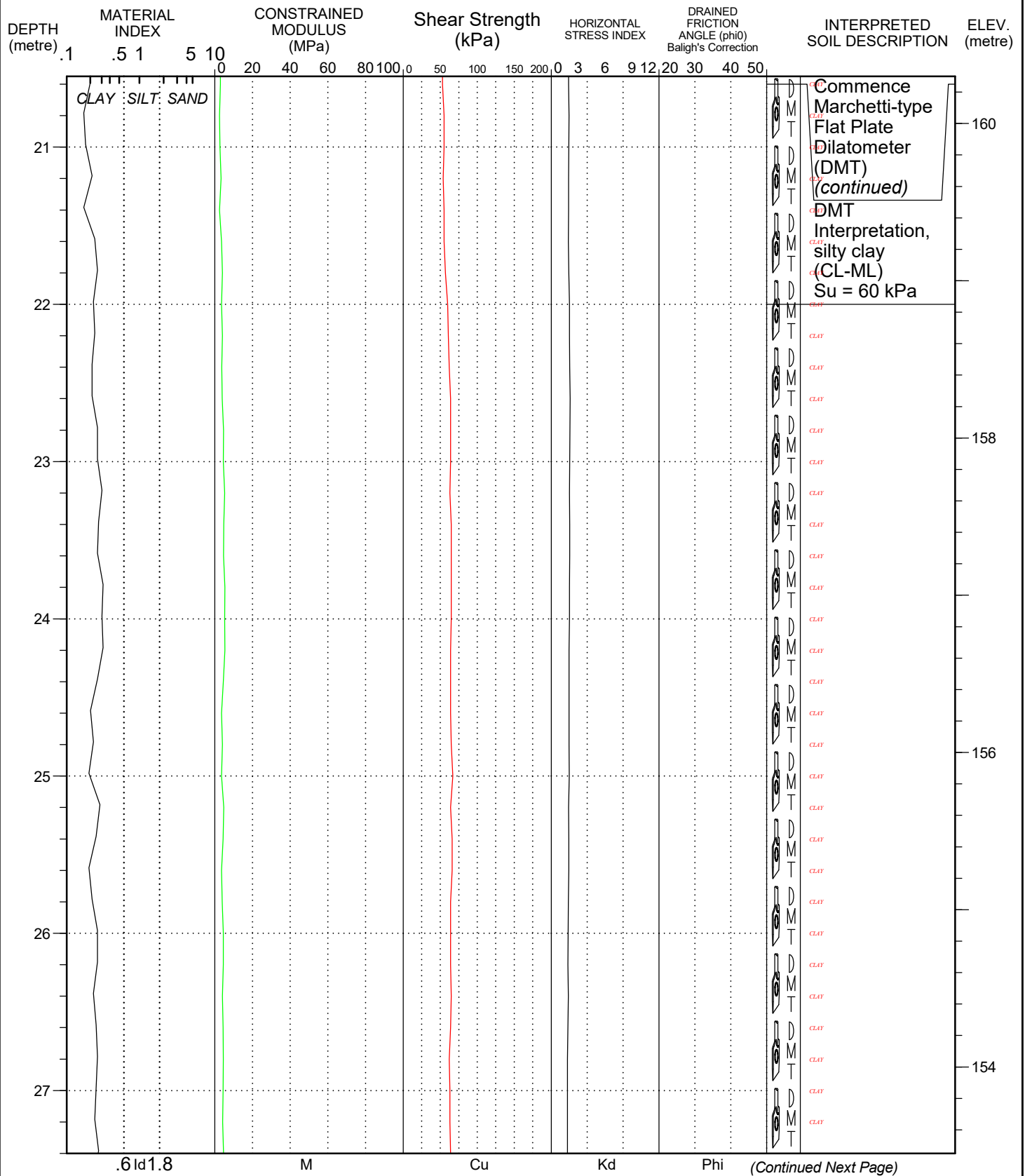


 <p>CT Soil &amp; Materials Testing Inc                  2000 Legacy Park Dr                  Windsor, ON N8W 5S6                  Telephone: (519) 966-8863                  Fax: (519) 966-8870</p>	<b>LOG OF 1-DMT</b>		
	11-Storey Mixed Development 3885 Sandwich Street, Windsor, ON		
	PROJECT NO.: 20G085	REVIEWED BY: B. Purcka, B.A.Sc., EIT	DATE ADVANCED: 2/22/2021





 <p>CT Soil &amp; Materials Testing Inc 2000 Legacy Park Dr Windsor, ON N8W 5S6 Telephone: (519) 966-8863 Fax: (519) 966-8870</p>	<b>LOG OF 1-DMT</b>		
	11-Storey Mixed Development 3885 Sandwich Street, Windsor, ON		
	PROJECT NO.: 20G085	REVIEWED BY: B. Purcka, B.A.Sc., EIT	DATE ADVANCED: 2/22/2021



CT DMT LOG 20G085.GPJ BETA.TOM.20200121.GDT 5/21/21



CT Soil & Materials Testing Inc  
 2000 Legacy Park Dr  
 Windsor, ON N8W 5S6  
 Telephone: (519) 966-8863  
 Fax: (519) 966-8870

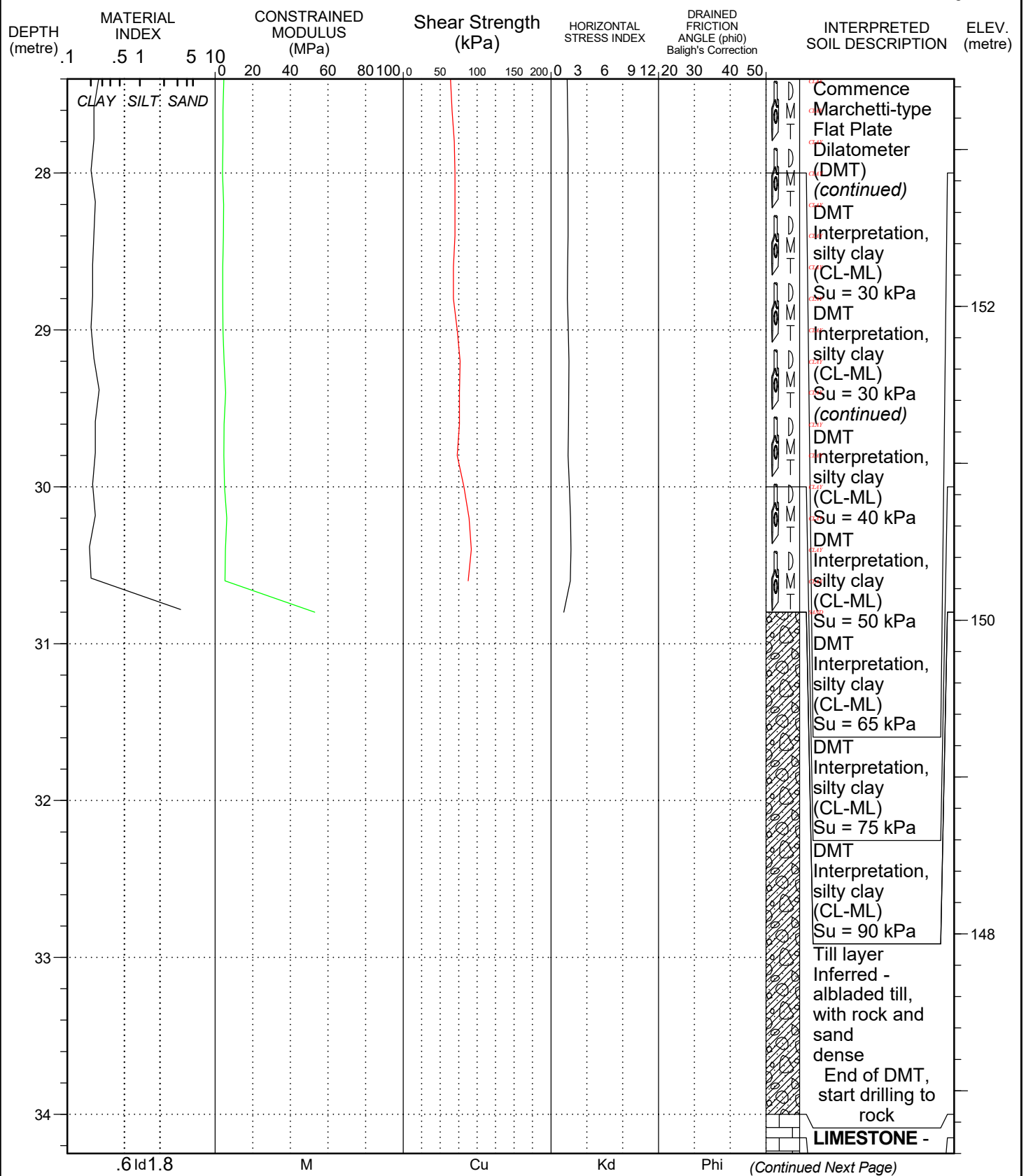
## LOG OF 1-DMT


11-Storey Mixed Development  
 3885 Sandwich Street, Windsor, ON

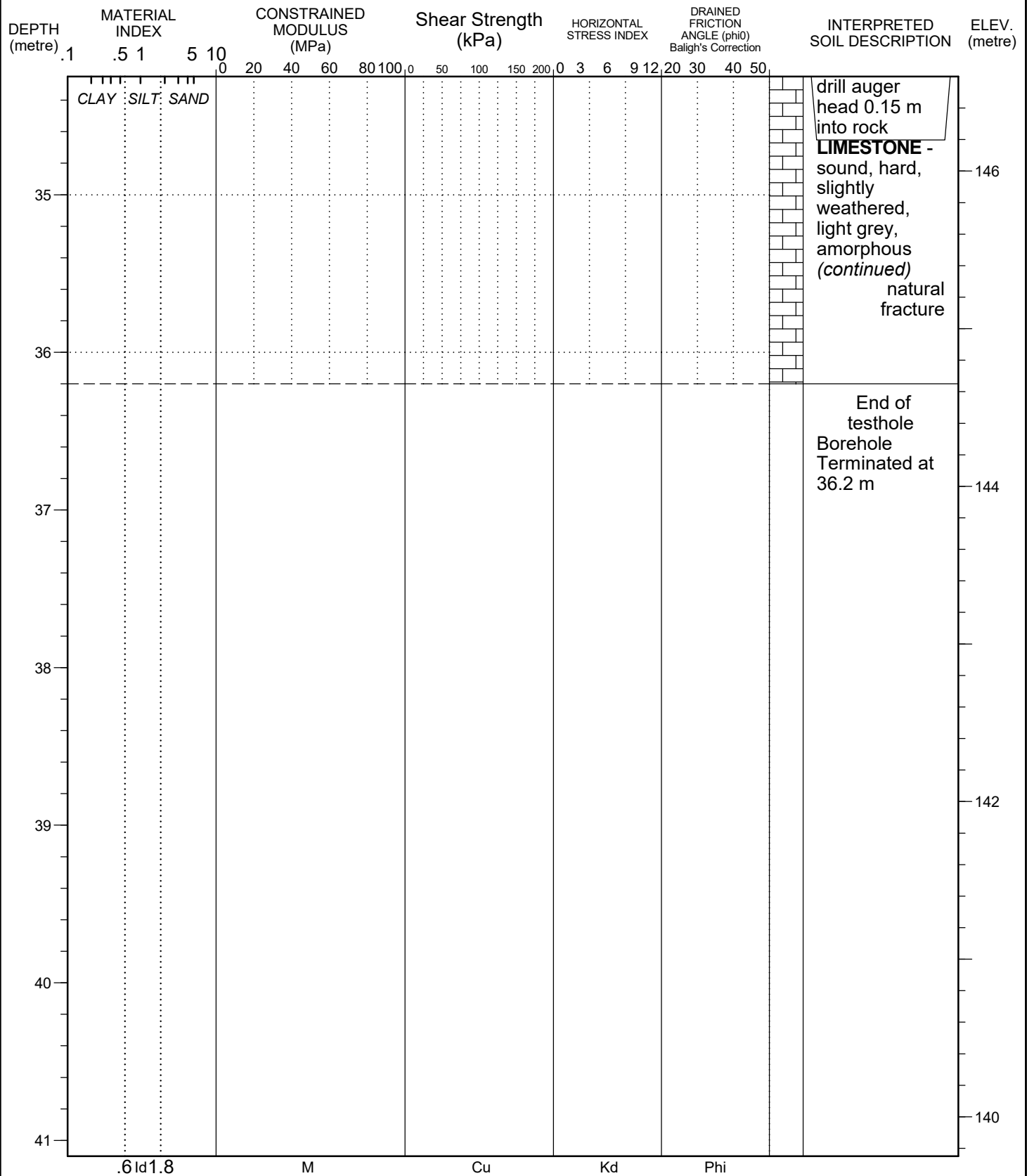
PROJECT NO.:  
 20G085

REVIEWED BY:  
 B. Purcka, B.A.Sc., EIT

DATE ADVANCED:  
 2/22/2021



 <p>CT Soil &amp; Materials Testing Inc                  2000 Legacy Park Dr                  Windsor, ON N8W 5S6                  Telephone: (519) 966-8863                  Fax: (519) 966-8870</p>	<b>LOG OF 1-DMT</b>		
	11-Storey Mixed Development 3885 Sandwich Street, Windsor, ON		
	PROJECT NO.: 20G085	REVIEWED BY: B. Purcka, B.A.Sc., EIT	DATE ADVANCED: 2/22/2021



CT Soil & Materials Testing Inc  
2000 Legacy Park Dr  
Windsor, ON N8W 5S6  
Telephone: (519) 966-8863  
Fax: (519) 966-8870

## LOG OF 1-DMT

11-Storey Mixed Development  
3885 Sandwich Street, Windsor, ON

PROJECT NO.:  
20G085

REVIEWED BY:  
B. Purcka, B.A.Sc., EIT

DATE ADVANCED:  
2/22/2021



C.T. SOIL & MATERIALS  
TESTING INC.  
WINDSOR ONTARIO

Client: Tunio Developments Inc.

Project: 11-Storey Mixed Development

Location: 3885 Sandwich Street, Windsor, ON

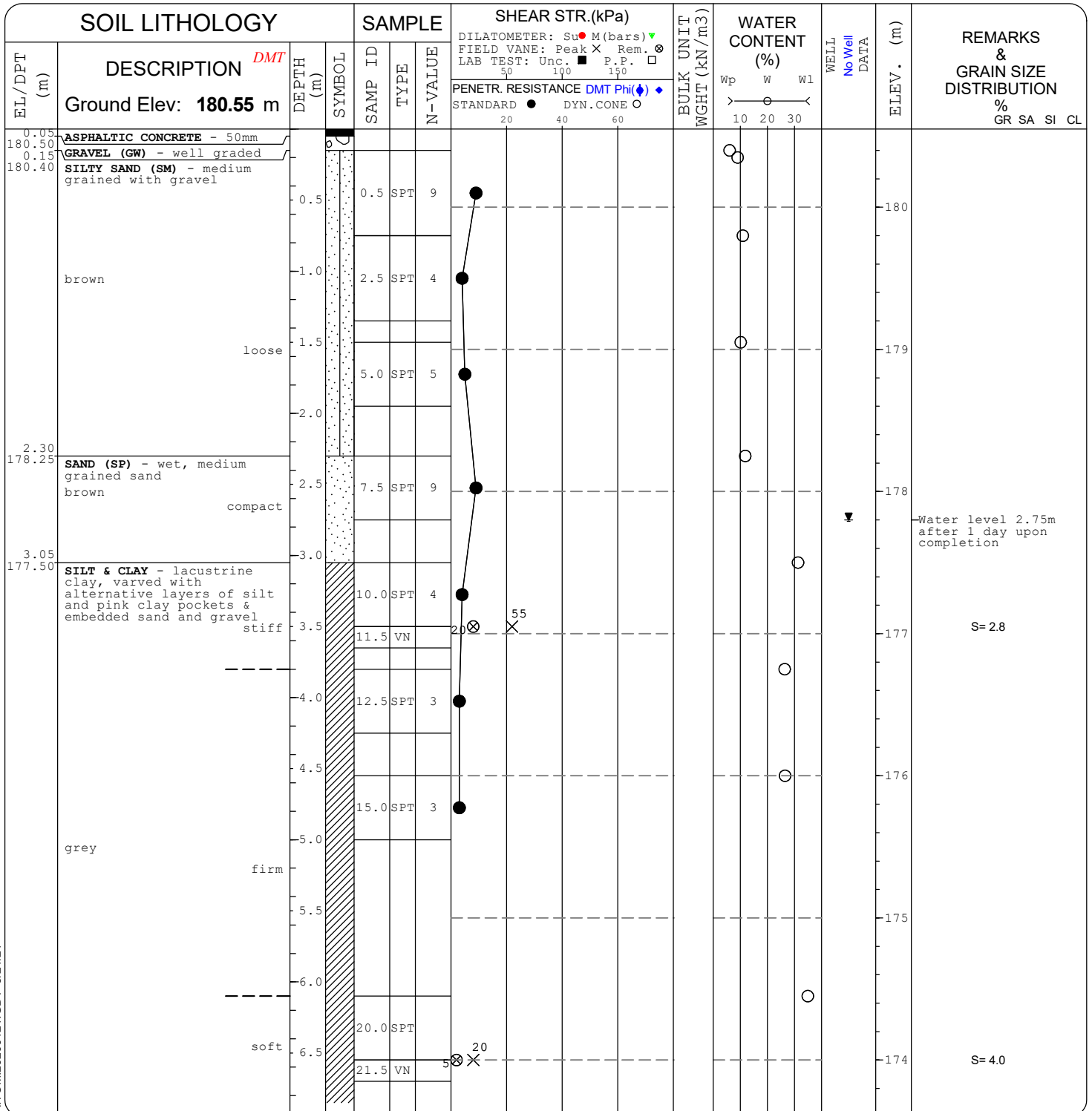
## EQUIPMENT DATA

Machine: Diedrich D50 Truck

Method: 83 mm I.D. H/S Auger

Size: 165 mm O.D.

Date: 12/22/2020 TO 12/22/2020



REVIEWING PROFESSIONAL:  
**B. Purcka, B.A.Sc., EIT**  
Soil & Materials  
Engineering Inc.

C.T. SOIL & MATERIALS  
TESTING INC.  
2000 Legacy Park Drive  
WINDSOR, ONTARIO, N8W 5S6  
ph. (519) 966-8863, fx. (519) 966-8870  
email ctsoil @ ctsoil.com







C.T. SOIL & MATERIALS  
TESTING INC.  
WINDSOR ONTARIO

Client: Tunio Developments Inc.

Project: 11-Storey Mixed Development

Location: 3885 Sandwich Street, Windsor, ON

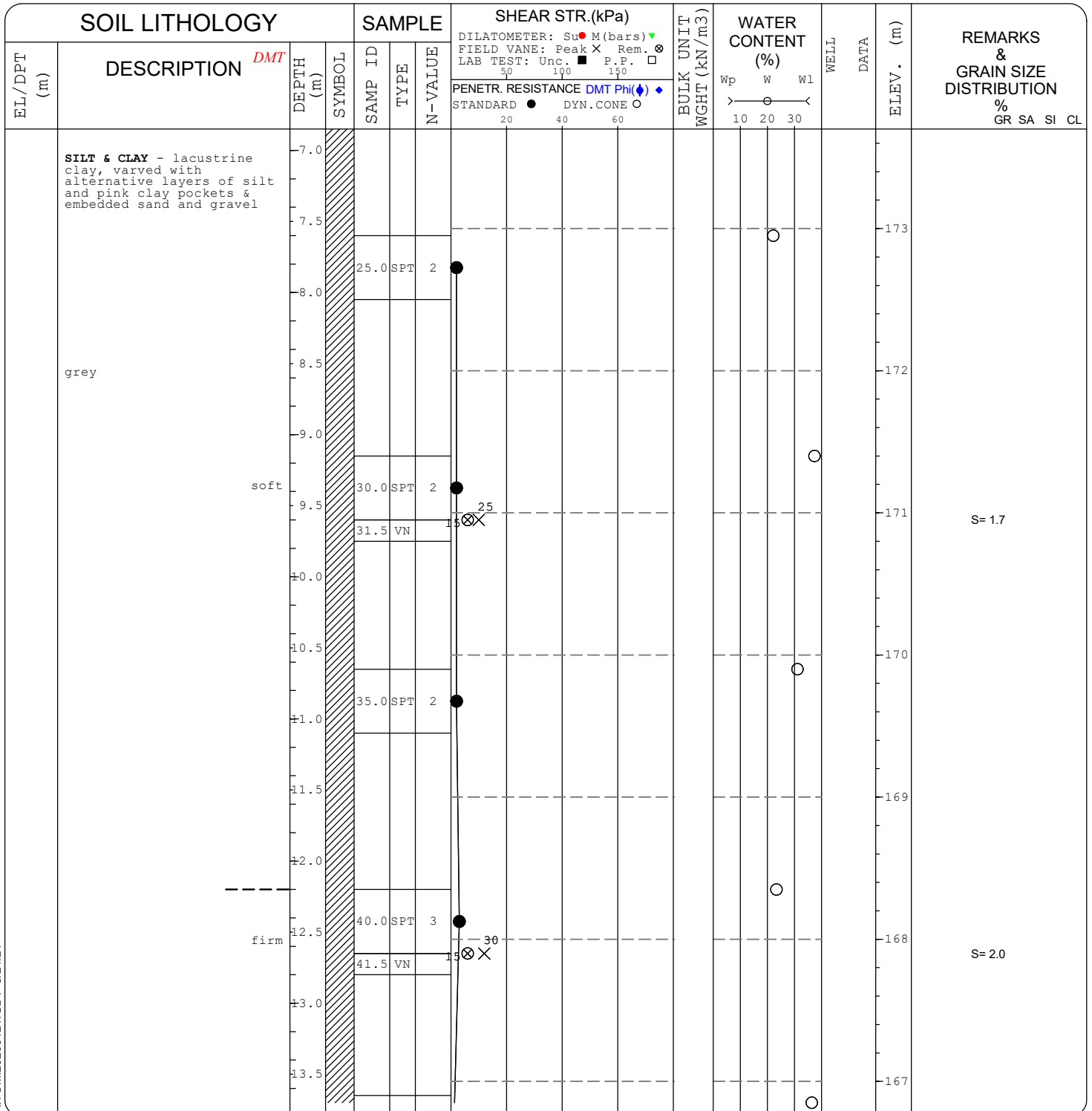
## EQUIPMENT DATA

Machine: Diedrich D50 Truck

Method: 83 mm I.D. H/S Auger

Size: 165 mm O.D.

Date: 12/22/2020 TO 12/22/2020



REVIEWING PROFESSIONAL:  
**B. Purcka, B.A.Sc., EIT**  
Soil & Materials  
Engineering Inc.

C.T. SOIL & MATERIALS  
TESTING INC.  
2000 Legacy Park Drive  
WINDSOR, ONTARIO, N8W 5S6  
ph. (519) 966-8863, fx. (519) 966-8870  
email ctsoil @ ctsoil.com





C.T. SOIL & MATERIALS  
TESTING INC.  
WINDSOR ONTARIO

Client: Tunio Developments Inc.

Project: 11-Storey Mixed Development

Location: 3885 Sandwich Street, Windsor, ON

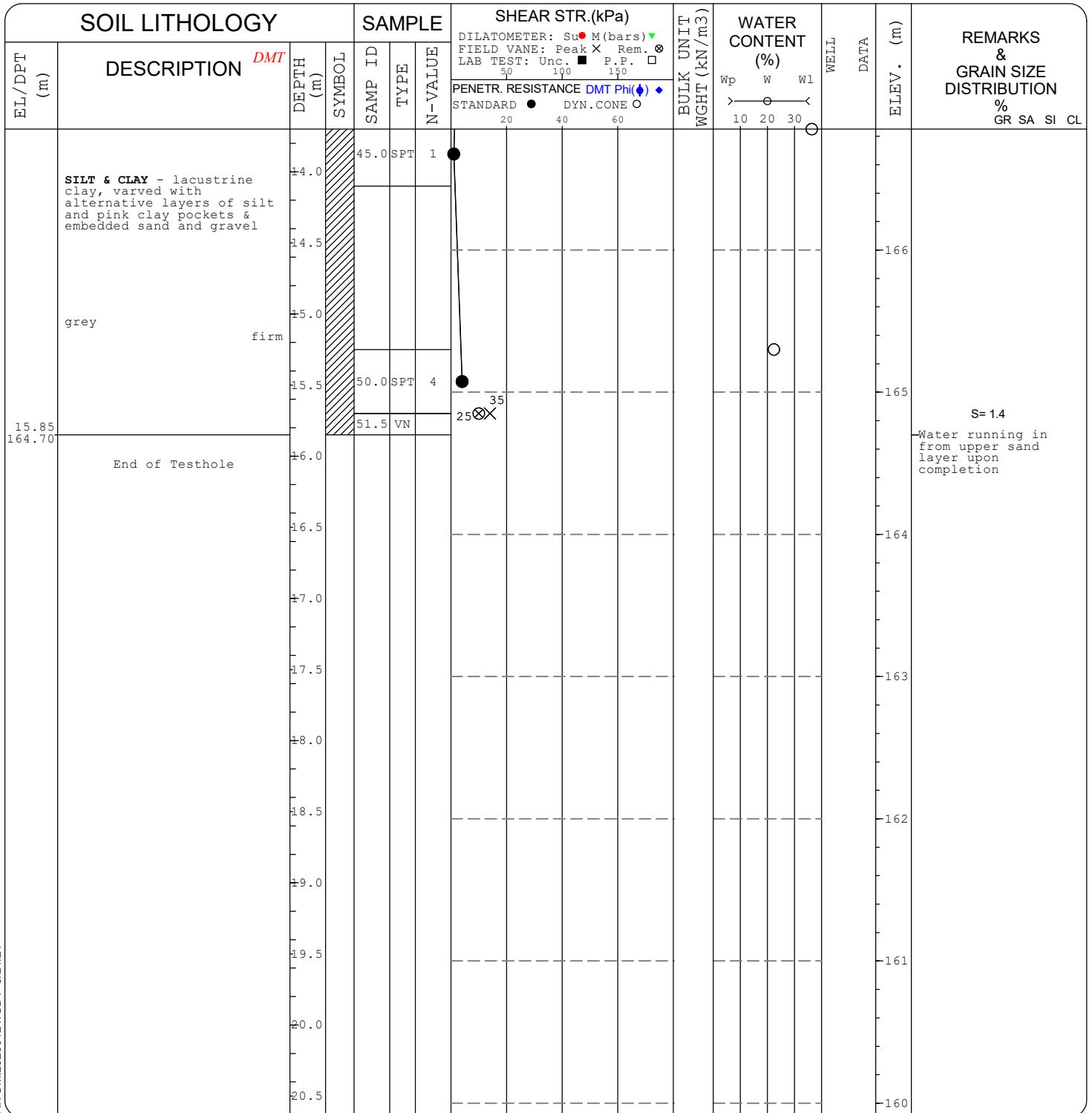
## EQUIPMENT DATA

Machine: Diedrich D50 Truck

Method: 83 mm I.D. H/S Auger

Size: 165 mm O.D.

Date: 12/22/2020 TO 12/22/2020



REVIEWING PROFESSIONAL:  
**B. Purcka, B.A.Sc., EIT**  
Soil & Materials  
Engineering Inc.

C.T. SOIL & MATERIALS  
TESTING INC.  
2000 Legacy Park Drive  
WINDSOR, ONTARIO, N8W 5S6  
ph. (519) 966-8863, fx. (519) 966-8870  
email ctsoil @ ctsoil.com



Date: 12/23/2021 TO 12/23/2021

CTMET 20G085.GPJ BETA.TOM.20200121.GDT 5/21/21





C.T. SOIL & MATERIALS  
TESTING INC.  
WINDSOR ONTARIO

Client: Tunio Developments Inc.

Project: 11-Storey Mixed Development

Location: 3885 Sandwich Street, Windsor, ON

## EQUIPMENT DATA

Machine: Diedrich D50 Truck

Method: Direct Push

Size: 96 mm x 15 mm

Date: 12/23/2021 TO 12/23/2021

SOIL LITHOLOGY				SAMPLE			SHEAR STR.(kPa)				BULK UNIT WGHT (kN/m3)	WATER CONTENT (%)			WELL DATA	ELEV. (m)	REMARKS & GRAIN SIZE DISTRIBUTION % GR SA SI CL	
EL/DPT (m)	DESCRIPTION	DMT	DEPTH (m)	SYMBOL	SAMP ID	TYPE	N-VALUE	DILATOMETER: Su (bars) ▼				FIELD VANE: Peak X Rem. ⊗						
								LAB TEST: Unc. ■ P.P. □ ⊗				PENETR. RESISTANCE DMT Phi(◆) ◆						
								STANDARD ● DYN. CONE ○				Wp W Wl						
								20	40	60		10	20	30				
		SILTY CLAY	7.0	DMT				●	●	●								
		SILTY CLAY		DMT				●	●	●								
		SILTY CLAY	7.5	DMT				●	●	●								
		SILTY CLAY		DMT				●	●	●								
		SILTY CLAY		DMT				●	●	●								
		SILTY CLAY	8.0	DMT				●	●	●						173		
		SILTY CLAY		DMT				●	●	●								
		SILTY CLAY		DMT				●	●	●								
		SILTY CLAY	8.5	DMT				●	●	●								
		SILTY CLAY		DMT				●	●	●								
		SILTY CLAY		DMT				●	●	●						172		
		SILTY CLAY	9.0	DMT				●	●	●								
		SILTY CLAY		DMT				●	●	●								
		SILTY CLAY		DMT				●	●	●								
		SILTY CLAY	9.5	DMT				●	●	●								
		SILTY CLAY		DMT				●	●	●								
		SILTY CLAY		DMT				●	●	●						171		
		SILTY CLAY		DMT				●	●	●								
		SILTY CLAY	10.0	DMT				●	●	●								
		SILTY CLAY		DMT				●	●	●								
		SILTY CLAY		DMT				●	●	●								
		SILTY CLAY	10.5	DMT				●	●	●								
		SILTY CLAY		DMT				●	●	●								
		SILTY CLAY		DMT				●	●	●						170		
		SILTY CLAY		DMT				●	●	●								
		SILTY CLAY	11.0	DMT				●	●	●								
		SILTY CLAY		DMT				●	●	●								
		SILTY CLAY		DMT				●	●	●								
		SILTY CLAY	11.5	DMT				●	●	●								
		SILTY CLAY		DMT				●	●	●								
		SILTY CLAY		DMT				●	●	●						169		
		SILTY CLAY	12.0	DMT				●	●	●								
		SILTY CLAY		DMT				●	●	●								
		SILTY CLAY		DMT				●	●	●								
		SILTY CLAY	12.5	DMT				●	●	●								
		SILTY CLAY		DMT				●	●	●								
		SILTY CLAY		DMT				●	●	●						168		
		SILTY CLAY		DMT				●	●	●								
		SILTY CLAY	13.0	DMT				●	●	●								
		SILTY CLAY		DMT				●	●	●								
		SILTY CLAY		DMT				●	●	●								
		SILTY CLAY	13.5	DMT				●	●	●								
		SILTY CLAY		DMT				●	●	●								
12.50																		
168.25																		



Date: 12/23/2021 TO 12/23/2021

[illegible]





C.T. SOIL & MATERIALS  
TESTING INC.  
WINDSOR ONTARIO

Client: Tunio Developments Inc.

Project: 11-Storey Mixed Development

Location: 3885 Sandwich Street, Windsor, ON

## EQUIPMENT DATA

Machine: Diedrich D50 Truck

Method: Direct Push

Size: 96 mm x 15 mm

Date: 12/23/2021 TO 12/23/2021

SOIL LITHOLOGY			SAMPLE			SHEAR STR.(kPa)			BULK UNIT WGHT (kN/m <sup>3</sup> )	WATER CONTENT (%)			WELL DATA	ELEV. (m)	REMARKS & GRAIN SIZE DISTRIBUTION % GR SA SI CL
EL/DPT (m)	DESCRIPTION <i>DMT</i>	DEPTH (m)	SYMBOL	SAMP ID	TYPE	N-VALUE	DILATOMETER: Su (bars) ▼ FIELD VANE: Peak X Rem. □ LAB TEST: Unc. ■ P.P. □	PENETR. RESISTANCE DMT Phi (kPa) ◆ STANDARD ● DYN. CONE ○		Wp	W	Wl			
160.25	DMT Interpretation, silty clay (CL-ML) Su = 50 kPa	16.0	DMT											160	
		21.0	DMT												
		21.5	DMT												
		22.0	DMT												
		22.5	DMT												
		23.0	DMT												
		23.5	DMT												
		24.0	DMT												
24.50	DMT Interpretation, silty clay (CL-ML) Su = 60 kPa	24.5	DMT											156	
156.25		25.0	DMT												
		25.5	DMT												
		26.0	DMT												
		26.5	DMT												
		27.0	DMT												

REVIEWING PROFESSIONAL:

**B. Purcka, B.A.Sc., EIT**Soil & Materials  
Engineering Inc.C.T. SOIL & MATERIALS  
TESTING INC.

2000 Legacy Park Drive

WINDSOR, ONTARIO, N8W 5S6

ph. (519) 966-8863, fx. (519) 966-8870

email ctsoil @ ctsoil.com





C.T. SOIL & MATERIALS  
TESTING INC.  
WINDSOR ONTARIO

Client: Tunio Developments Inc.

Project: 11-Storey Mixed Development

Location: 3885 Sandwich Street, Windsor, ON

## EQUIPMENT DATA

Machine: Diedrich D50 Truck

Method: Direct Push

Size: 96 mm x 15 mm

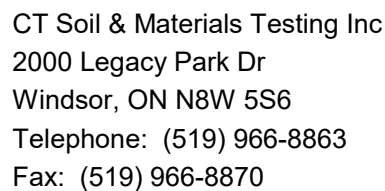
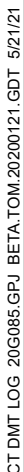
Date: 12/23/2021 TO 12/23/2021

SOIL LITHOLOGY			SAMPLE			SHEAR STR.(kPa)			BULK UNIT WGHT (kN/m <sup>3</sup> )	WATER CONTENT (%)			WELL DATA	ELEV. (m)	REMARKS & GRAIN SIZE DISTRIBUTION % GR SA SI CL
EL/DPT (m)	DESCRIPTION	DEPTH (m)	SYMBOL	SAMP ID	TYPE	N-VALUE	DILATOMETER: Su (bars) M (bars)	FIELD VANE: Peak X Rem. X		Wp	W	Wl			
28.80 151.95	CLAY	27.5	DMT				20							153	
	CLAY	28.0	DMT				20							152	
	CLAY	28.5	DMT				20							151	
	CLAY	29.0	DMT				20							150	
	CLAY	29.5	DMT				20							149	
	CLAY	30.0	DMT				20							148	
	CLAY	30.5	DMT				20							147	
	CLAY	31.0	DMT				20								
	CLAY	31.5	DMT				20								
	CLAY	32.0	DMT				20								
	CLAY	32.5	DMT				20								
	CLAY	33.0	DMT				20								
	CLAY	33.5	DMT				20								
	CLAY	34.0	DMT				20								

REVIEWING PROFESSIONAL:  
**B. Purcka, B.A.Sc., EIT**  
Soil & Materials  
Engineering Inc.

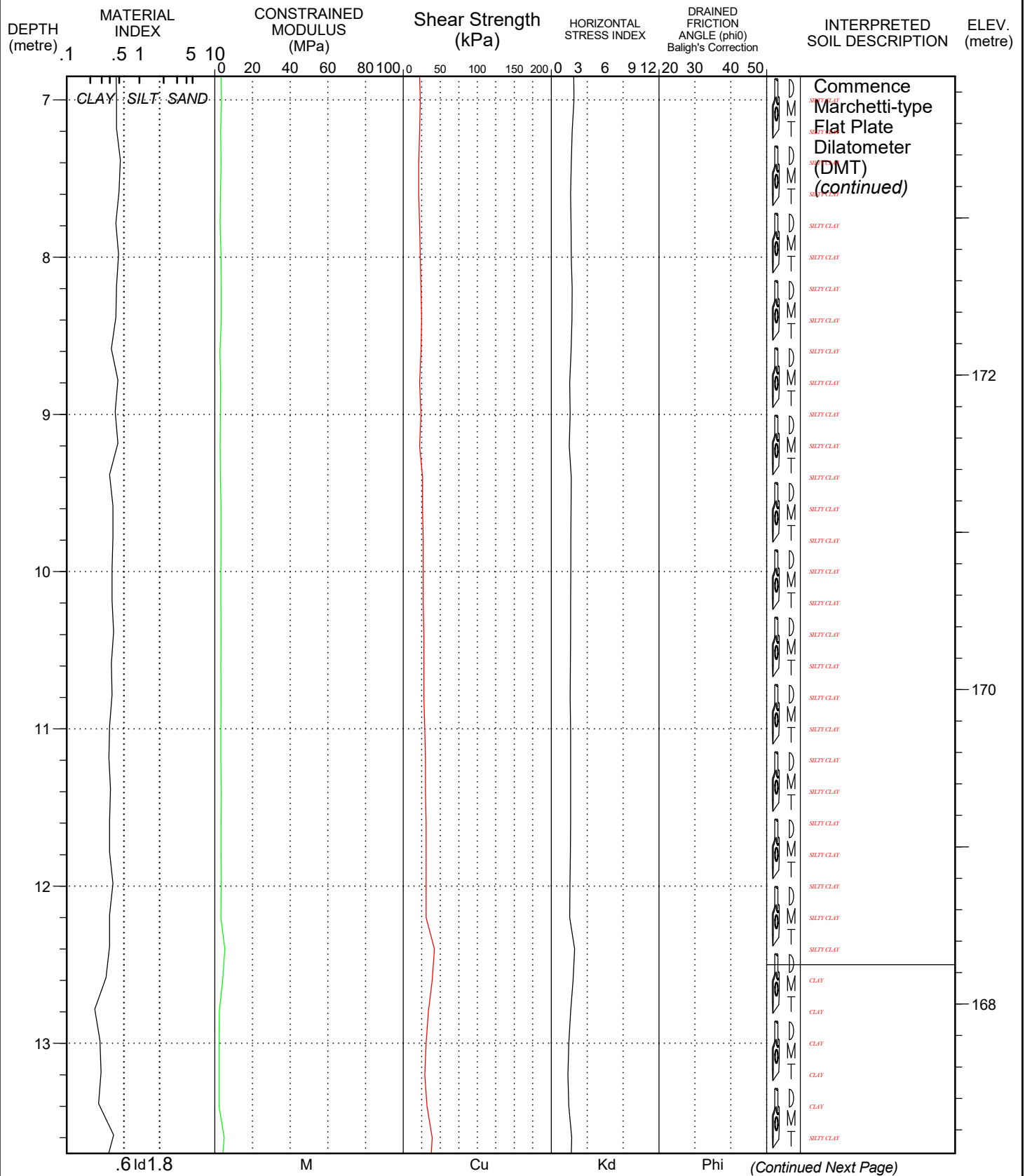
C.T. SOIL & MATERIALS  
TESTING INC.  
2000 Legacy Park Drive  
WINDSOR, ONTARIO, N8W 5S6  
ph. (519) 966-8863, fx. (519) 966-8870  
email ctsoil @ ctsoil.com




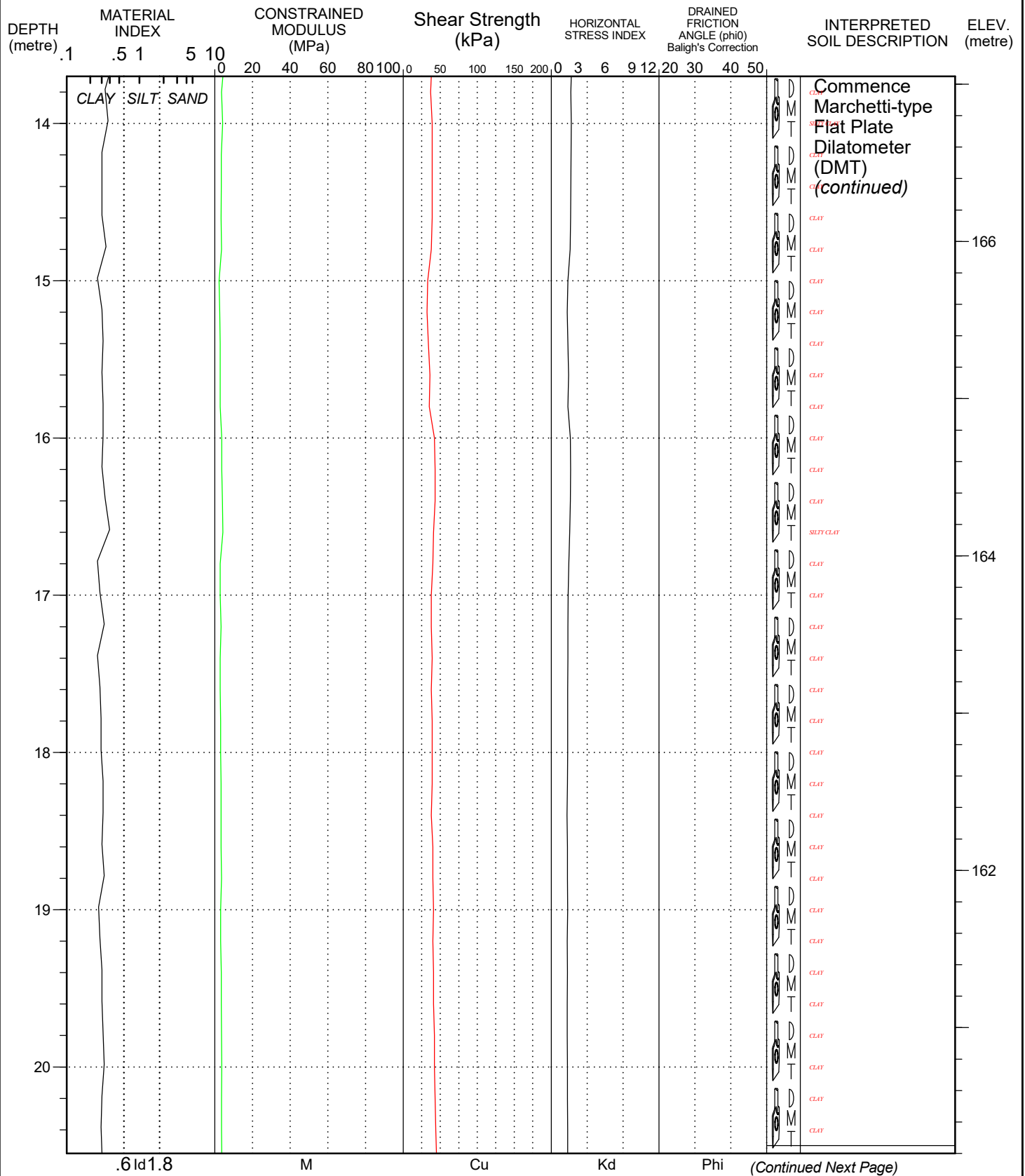



11-Storey Mixed Development  
3885 Sandwich Street, Windsor, ON

DATE ADVANCED:  
12/23/2021

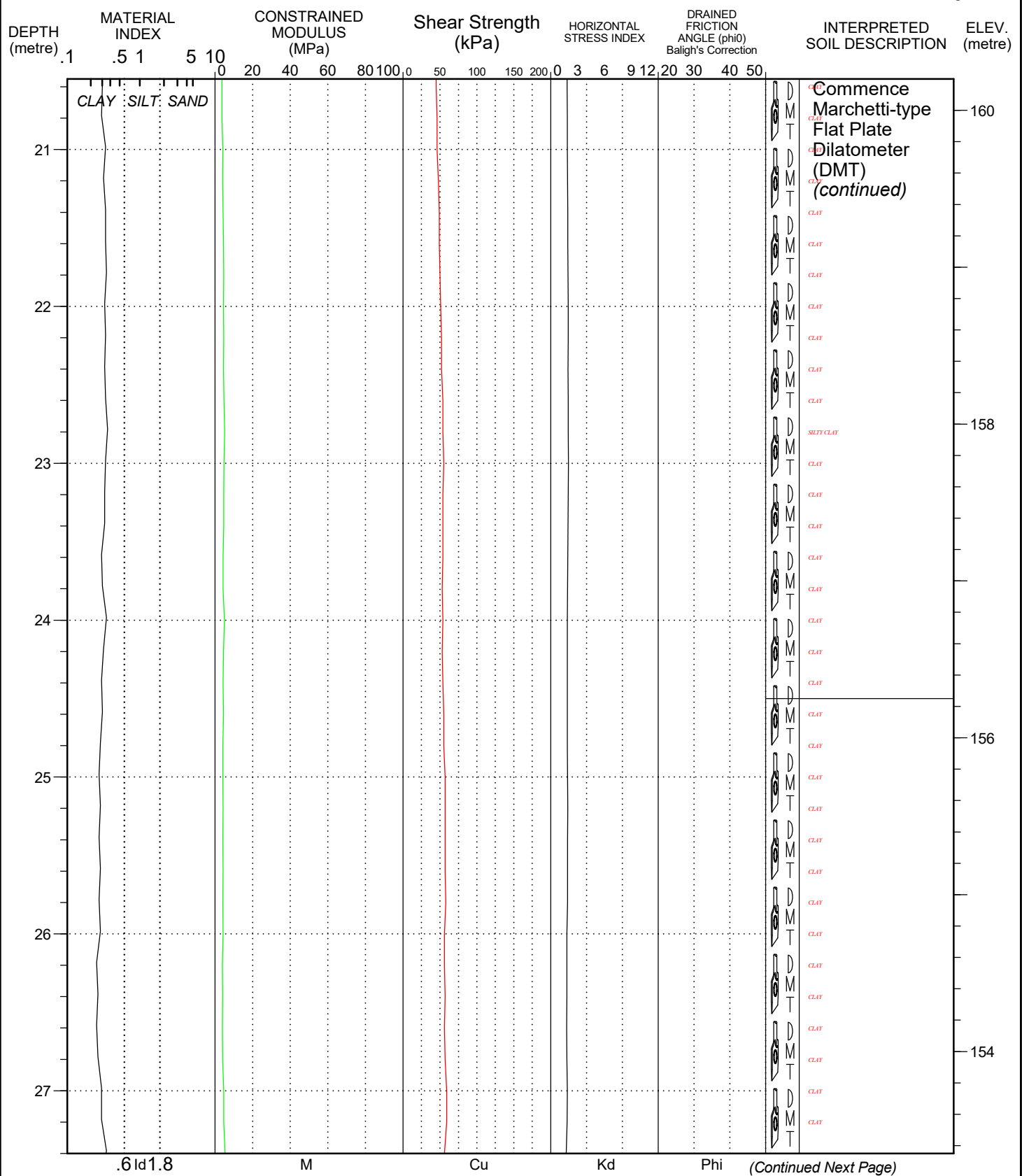



 <p>CT Soil &amp; Materials Testing Inc                  2000 Legacy Park Dr                  Windsor, ON N8W 5S6                  Telephone: (519) 966-8863                  Fax: (519) 966-8870</p>	<b>LOG OF 3-DMT</b>		
	11-Storey Mixed Development 3885 Sandwich Street, Windsor, ON		
	PROJECT NO.: 20G085	REVIEWED BY: B. Purcka, B.A.Sc., EIT	DATE ADVANCED: 12/23/2021

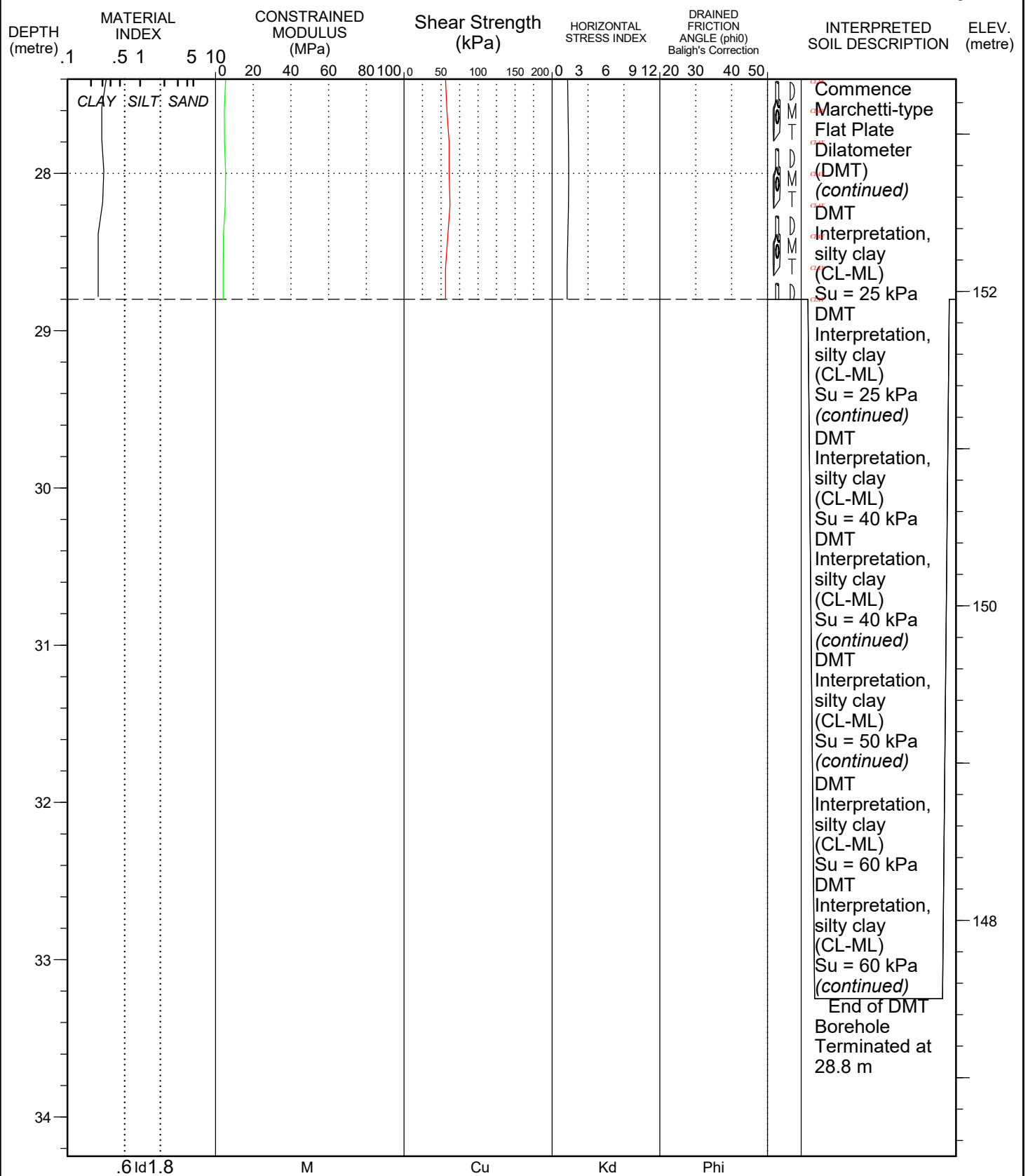



 <p>CT Soil &amp; Materials Testing Inc 2000 Legacy Park Dr Windsor, ON N8W 5S6 Telephone: (519) 966-8863 Fax: (519) 966-8870</p>	<b>LOG OF 3-DMT</b>		
	11-Storey Mixed Development 3885 Sandwich Street, Windsor, ON		
	PROJECT NO.: 20G085	REVIEWED BY: B. Purcka, B.A.Sc., EIT	DATE ADVANCED: 12/23/2021





 <p>CT Soil &amp; Materials Testing Inc 2000 Legacy Park Dr Windsor, ON N8W 5S6 Telephone: (519) 966-8863 Fax: (519) 966-8870</p>	<b>LOG OF 3-DMT</b>		
	11-Storey Mixed Development 3885 Sandwich Street, Windsor, ON		
	PROJECT NO.: 20G085	REVIEWED BY: B. Purcka, B.A.Sc., EIT	DATE ADVANCED: 12/23/2021



	<b>LOG OF 3-DMT</b>	
	CT Soil & Materials Testing Inc 2000 Legacy Park Dr Windsor, ON N8W 5S6 Telephone: (519) 966-8863 Fax: (519) 966-8870	
	PROJECT NO.: 20G085	REVIEWED BY: B. Purcka, B.A.Sc., EIT
		DATE ADVANCED: 12/23/2021

CT DMT LOG 20G085.GPJ BETA.TOM.20200121.GDT 5/21/21



C.T. SOIL & MATERIALS  
TESTING INC.  
WINDSOR ONTARIO

Client: Tunio Developments Inc.

Project: 11-Storey Mixed Development

Location: 3885 Sandwich Street, Windsor, ON

## EQUIPMENT DATA

Machine: Diedrich D50 Truck

Method: S/S Auger

Size: 115 mm O.D.

Date: 12/22/2020 TO 12/22/2020

SOIL LITHOLOGY			SAMPLE			SHEAR STR.(kPa)			BULK UNIT WGHT (kN/m <sup>3</sup> )	WATER CONTENT (%)			WELL No Well DATA	ELEV. (m)	REMARKS & GRAIN SIZE DISTRIBUTION % GR SA SI CL
EL/DPT (m)	DESCRIPTION <i>DMT</i>	DEPTH (m)	SYMBOL	SAMP ID	TYPE	N-VALUE	DILATOMETER: Su M(bars) ▼	FIELD VANE: Peak X Rem. ⊗		Wp	W	Wl			
0.05 180.05	ASPHALTIC CONCRETE - 50mm														
	FILL - heterogeneous mixture consisting gravel, topsoil, sand, glass and brick pieces, dry to moist	0.5		0.5	SPT	6									
1.05 179.05	SAND WITH SILT (SP-SM) - damp medium grained sand with silt	1.0		2.5	SPT	8									
	brown compact	1.5													
1.95 178.15		2.0		5.0	SPT	16									
	End of Testhole	2.0													Testhole open and dry upon completion
		2.5													
		3.0													
		3.5													
		4.0													
		4.5													
		5.0													
		5.5													
		6.0													
		6.5													

REVIEWING PROFESSIONAL:

B. Purcka, B.A.Sc., EIT

Soil & Materials  
Engineering Inc.C.T. SOIL & MATERIALS  
TESTING INC.

2000 Legacy Park Drive

WINDSOR, ONTARIO, N8W 5S6

ph. (519) 966-8863, fx. (519) 966-8870

email ctsoil @ ctsoil.com





C.T. SOIL & MATERIALS  
TESTING INC.  
WINDSOR ONTARIO

Client: Tunio Developments Inc.

Project: 11-Storey Mixed Development

Location: 3885 Sandwich Street, Windsor, ON

## EQUIPMENT DATA

Machine: Diedrich D50 Truck

Method: S/S Auger

Size: 115 mm O.D.

Date: 12/22/2020 TO 12/22/2020

SOIL LITHOLOGY			SAMPLE			SHEAR STR.(kPa)			BULK UNIT WGHT (kN/m <sup>3</sup> )	WATER CONTENT (%)			WELL No Well DATA	ELEV. (m)	REMARKS & GRAIN SIZE DISTRIBUTION % GR SA SI CL
EL/DPT (m)	DESCRIPTION <i>DMT</i>	DEPTH (m)	SYMBOL	SAMP ID	TYPE	N-VALUE	DILATOMETER: Su M (bars) ▼	FIELD VANE: Peak X Rem. ⊗		Wp	W	Wl			
0.05 180.35	ASPHALTIC CONCRETE - 50mm														
0.45 179.95	FILL - mixed fill consisting topsoil and gravel														
	SILTY SAND (SM) - damp medium grained sand with gravel and silt	0.5		0.0	SPT	6								180	
	loose	1.0		2.5	SPT	6								179	
	brown	1.5													
	compact	1.95 178.45		5.0	SPT	12									
	End of Testhole	2.0													Testhole open and dry upon completion
		2.5												178	
		3.0													
		3.5												177	
		4.0													
		4.5												176	
		5.0													
		5.5												175	
		6.0													
		6.5												174	

REVIEWING PROFESSIONAL:  
**B. Purcka, B.A.Sc., EIT**  
Soil & Materials  
Engineering Inc.

C.T. SOIL & MATERIALS  
TESTING INC.  
2000 Legacy Park Drive  
WINDSOR, ONTARIO, N8W 5S6  
ph. (519) 966-8863, fx. (519) 966-8870  
email ctsoil @ ctsoil.com





C.T. SOIL & MATERIALS  
TESTING INC.  
WINDSOR ONTARIO

Client: Tunio Developments Inc.

Project: 11-Storey Mixed Development

Location: 3885 Sandwich Street, Windsor, ON

## EQUIPMENT DATA

Machine: Diedrich D50 Truck

Method: S/S Auger

Size: 115 mm O.D.

Date: 12/22/2020 TO 12/22/2020

SOIL LITHOLOGY			SAMPLE			SHEAR STR.(kPa)			BULK UNIT WGHT (kN/m <sup>3</sup> )	WATER CONTENT (%)			WELL No Well DATA	ELEV. (m)	REMARKS & GRAIN SIZE DISTRIBUTION % GR SA SI CL
EL/DPT (m)	DESCRIPTION <i>DMT</i>	DEPTH (m)	SYMBOL	SAMP ID	TYPE	N-VALUE	DILATOMETER: Su M (bars) ▼	FIELD VANE: Peak X Rem. ⊗		Wp	W	Wl			
	Ground Elev: 180.55 m						LAB TEST: Unc. ■ P.P. □	PENETR. RESISTANCE DMT Phi(♦) ◆							
							STANDARD ● DYN. CONE ○								
							20 40 60			10 20 30					
0.60	FILL - heterogeneous mixture of topsoil, gravel, cinders and brick pieces	0.5	XXXX	0.0	SPT	11				○				180	
179.95	SAND WITH SILT (SP-SM) - damp fine to medium grained sand with silt	1.0	XXXX	2.0	SPT	10				○					
	brown compact	1.5													
1.95		1.5								○				179	
178.60	End of Testhole	2.0				5.0									Testhole open and dry upon completion
		2.5												178	
		3.0													
		3.5												177	
		4.0													
		4.5												176	
		5.0													
		5.5												175	
		6.0													
		6.5												174	

REVIEWING PROFESSIONAL:  
**B. Purcka, B.A.Sc., EIT**  
Soil & Materials  
Engineering Inc.

C.T. SOIL & MATERIALS  
TESTING INC.  
2000 Legacy Park Drive  
WINDSOR, ONTARIO, N8W 5S6  
ph. (519) 966-8863, fx. (519) 966-8870  
email ctsoil @ ctsoil.com







C.T. SOIL & MATERIALS  
TESTING INC.  
WINDSOR ONTARIO

Client: **Tunio Developments Inc.**

Project: **11-Storey Mixed Development**

Location: **3885 Sandwich Street, Windsor, ON**

## EQUIPMENT DATA

Machine: **Diedrich D50 Truck**

Method: **S/S Auger**

Size: **115 mm O.D.**

Date: **12/22/2020 TO 12/22/2020**

SOIL LITHOLOGY			SAMPLE			SHEAR STR.(kPa)			BULK UNIT WGHT (kN/m <sup>3</sup> )	WATER CONTENT (%)			WELL No Well DATA	ELEV. (m)	REMARKS & GRAIN SIZE DISTRIBUTION % GR SA SI CL
EL/DPT (m)	DESCRIPTION <i>DMT</i>	DEPTH (m)	SYMBOL	SAMP ID	TYPE	N-VALUE	DILATOMETER: Su (bars) M (bars) X Rem. (bars)	FIELD VANE: Peak X Rem. (bars)		Wp	W	Wl			
0.05	ASPHALTIC CONCRETE - 50mm														
180.50	GRAVEL (GW) - well graded														
180.35	FILL - heterogeneous mix of topsoil and gravelly sand														
0.90		0.5		0.5	SPT	7								180	
179.65	SILTY SAND (SM) - damp medium to coarse grained with gravel	1.0		2.5	SPT	5									
	brown	1.5												179	
1.95				5.0	SPT	8									
178.60	End of Testhole	2.0													Testhole open and dry upon completion
		2.5												178	
		3.0													
		3.5												177	
		4.0													
		4.5												176	
		5.0													
		5.5												175	
		6.0													
		6.5												174	

REVIEWING PROFESSIONAL:  
**B. Purcka, B.A.Sc., EIT**  
Soil & Materials  
Engineering Inc.

C.T. SOIL & MATERIALS  
TESTING INC.  
2000 Legacy Park Drive  
WINDSOR, ONTARIO, N8W 5S6  
ph. (519) 966-8863, fx. (519) 966-8870  
email ctsoil @ ctsoil.com



TESTHOLE No. **P5**

Client: **Tunio Developments Inc.**

**Location:** 3885 Sandwich Street, Windsor, ON

Machine: **Diedrich D50 Truck**

Method: S/S Auger

Size: 115 mm O.D.

Date: 12/22/2020 TO 12/22/2020

[illegible]

REVIEWING PROFESSIONAL:  
**B. Purcka, B.A.Sc., EIT**  
Soil & Materials  
Engineering Inc.

**C.T. SOIL & MATERIALS  
TESTING INC.**  
2000 Legacy Park Drive  
WINDSOR, ONTARIO, N8W 5S6  
ph. (519) 966-8863, fx. (519) 966-8870  
email [ctsoil @ ctsoil.com](mailto:ctsoil@ctsoil.com)





C.T. SOIL & MATERIALS  
TESTING INC.  
WINDSOR ONTARIO

Client: Tunio Developments Inc.

Project: 11-Storey Mixed Development

Location: 3885 Sandwich Street, Windsor, ON

## EQUIPMENT DATA

Machine: Diedrich D50 Truck

Method: S/S Auger

Size: 115 mm O.D.

Date: 12/22/2020 TO 12/22/2020

SOIL LITHOLOGY				SAMPLE			SHEAR STR.(kPa)		BULK UNIT WGHT (kN/m <sup>3</sup> )	WATER CONTENT (%)			WELL No Well DATA	ELEV. (m)	REMARKS & GRAIN SIZE DISTRIBUTION % GR SA SI CL		
EL/DPT (m)	DESCRIPTION <i>DMT</i>	DEPTH (m)	SYMBOL	SAMP ID	TYPE	N-VALUE	DILATOMETER: Su <span style="color:red">●</span> M(bars) <span style="color:green">▼</span> FIELD VANE: Peak <span style="color:red">X</span> Rem. <span style="color:red">⊗</span> LAB TEST: Unc. <span style="color:black">■</span> P.P. <span style="color:black">□</span>			PENETR. RESISTANCE DMT Phi( <span style="color:blue">◆</span> ) <span style="color:blue">◆</span>							
							STANDARD <span style="color:black">●</span> DYN. CONE <span style="color:black">○</span>			Wp W Wl							
	Ground Elev: 180.65 m						20 40 60			10 20 30							
0.35 180.30	FILL - heterogeneous mixture of topsoil, gravel, cinders and brick pieces		XXXX	0.0	SPT	5											
	SAND WITH SILT (SP-SM) - damp fine to medium grained silty sand with gravel	0.5															
		loose	1.0		2.0	SPT		3									
		brown	1.5														
		compact		5.0	SPT	12											
1.95 178.70	End of Testhole	2.0													Testhole open and dry upon completion		
		2.5															
		3.0															
		3.5															
		4.0															
		4.5															
		5.0															
		5.5															
		6.0															
		6.5															

REVIEWING PROFESSIONAL:  
**B. Purcka, B.A.Sc., EIT**  
Soil & Materials  
Engineering Inc.

C.T. SOIL & MATERIALS  
TESTING INC.  
2000 Legacy Park Drive  
WINDSOR, ONTARIO, N8W 5S6  
ph. (519) 966-8863, fx. (519) 966-8870  
email ctsoil @ ctsoil.com





C.T. SOIL & MATERIALS  
TESTING INC.  
WINDSOR ONTARIO

Client: Tunio Developments Inc.

Project: 11-Storey Mixed Development

Location: 3885 Sandwich Street, Windsor, ON

## EQUIPMENT DATA

Machine: Diedrich D50 Truck  
Method: 83 mm I.D. H/S Auger  
Size: 165 mm O.D.  
Date: 2/26/2021 TO 2/26/2021

SOIL LITHOLOGY			SAMPLE			SHEAR STR.(kPa)			BULK UNIT WGHT (kN/m <sup>3</sup> )	WATER CONTENT (%)			WELL No Well DATA	ELEV. (m)	REMARKS & GRAIN SIZE DISTRIBUTION % GR SA SI CL
EL/DPT (m)	DESCRIPTION <i>DMT</i>	DEPTH (m)	SYMBOL	SAMP ID	TYPE	N-VALUE	DILATOMETER: Su M (bars) ▼	FIELD VANE: Peak X Rem. ⊗		PENETR. RESISTANCE DMT Phi (kPa) ◆	Wp	W			
	Ground Elev: m														
	Drill to 10.65 m														
		0.5													
		1.0													
		1.5													
		2.0													
		2.5													
		3.0													
		3.5													
		4.0													
		4.5													
		5.0													
		5.5													
		6.0													
		6.5													

REVIEWING PROFESSIONAL:  
**B. Purcka, B.A.Sc., EIT**  
Soil & Materials  
Engineering Inc.

C.T. SOIL & MATERIALS  
TESTING INC.  
2000 Legacy Park Drive  
WINDSOR, ONTARIO, N8W 5S6  
ph. (519) 966-8863, fx. (519) 966-8870  
email ctsoil @ ctsoil.com





C.T. SOIL & MATERIALS  
TESTING INC.  
WINDSOR ONTARIO

Client: Tunio Developments Inc.

Project: 11-Storey Mixed Development

Location: 3885 Sandwich Street, Windsor, ON

## EQUIPMENT DATA

Machine: Diedrich D50 Truck  
Method: 83 mm I.D. H/S Auger  
Size: 165 mm O.D.  
Date: 2/26/2021 TO 2/26/2021

SOIL LITHOLOGY			SAMPLE			SHEAR STR.(kPa)			BULK UNIT WGHT (kN/m <sup>3</sup> )	WATER CONTENT (%)			WELL DATA	ELEV. (m)	REMARKS & GRAIN SIZE DISTRIBUTION % GR SA SI CL
EL/DPT (m)	DESCRIPTION <i>DMT</i>	DEPTH (m)	SYMBOL	SAMP ID	TYPE	N-VALUE	DILATOMETER: Su M (bars) ▼	FIELD VANE: Peak X Rem. ◉		Wp	W	Wl			
							LAB TEST: Unc. ■ P.P. □								
							PENETR. RESISTANCE DMT Phi(♦) ◆								
							STANDARD ● DYN. CONE ○								
							20 40 60			10 20 30					
10.65	SILTY CLAY - with embedded sand and gravel grey	10.65		35.0	TW				19.0						Consolidation Test was completed at 10.65 m
11.40	firm	11.40		37.0	VN	15	30								S=2.0
	End of Testhole	11.5													

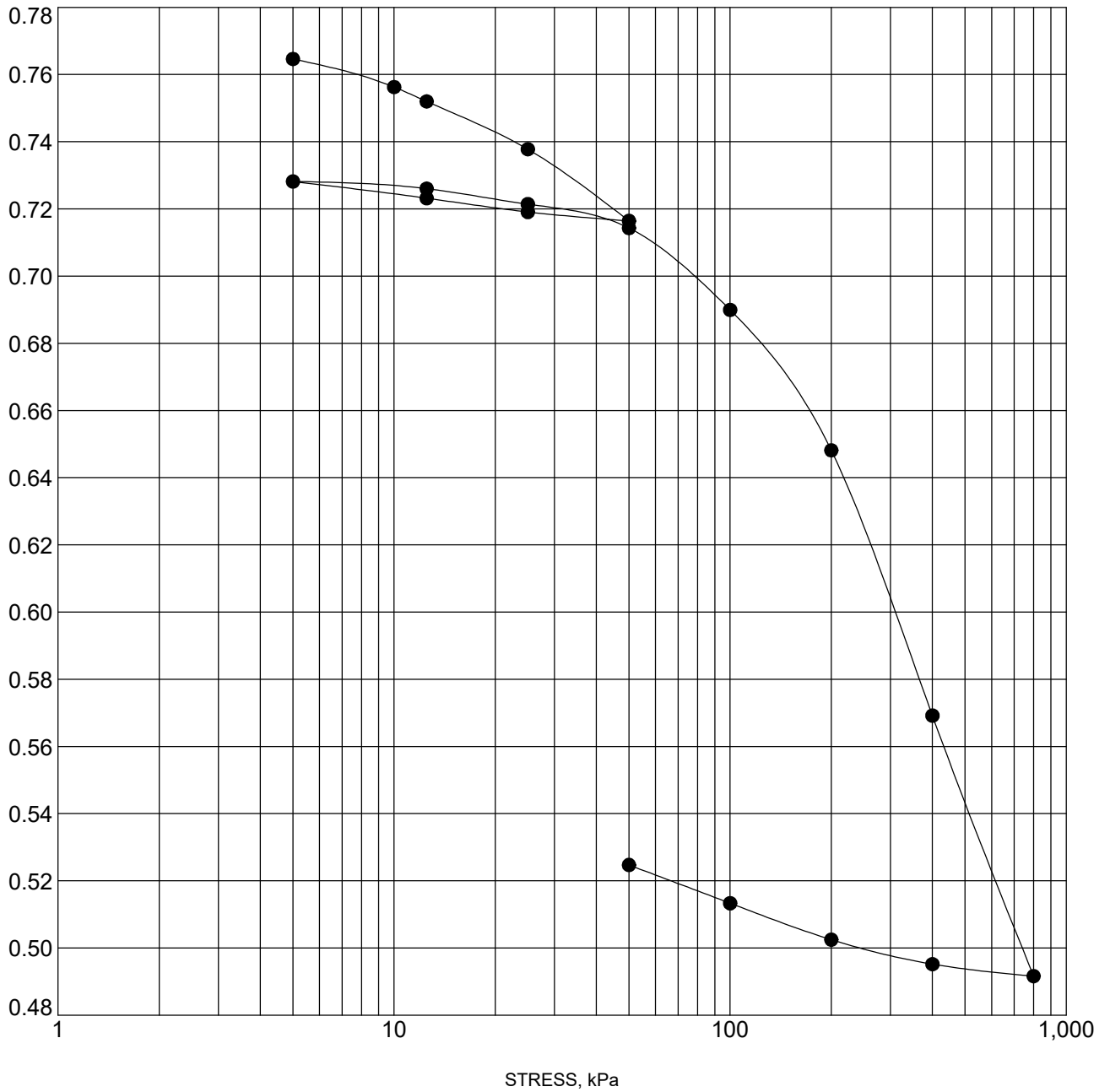
REVIEWING PROFESSIONAL:  
**B. Purcka, B.A.Sc., EIT**  
Soil & Materials  
Engineering Inc.

C.T. SOIL & MATERIALS  
TESTING INC.  
2000 Legacy Park Drive  
WINDSOR, ONTARIO, N8W 5S6  
ph. (519) 966-8863, fx. (519) 966-8870  
email ctsoil @ ctsoil.com





VOID RATIO



Specimen Identification		Classification	$\gamma_d$	MC%
● TW Sample	10.65	SILTY CLAY (CL-ML)	14.9	27

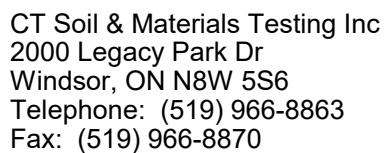
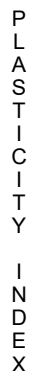


CT Soil & Materials Testing Inc  
 2000 Legacy Park Dr  
 Windsor, ON N8W 5S6  
 Telephone: (519) 966-8863  
 Fax: (519) 966-8870

## CONSOLIDATION TEST

Project: 11-Storey Mixed Development  
 Location: 3885 Sandwich Street, Windsor, ON  
 Number: 20G085

Drawing 1



Project: 11-Storey Mixed Development  
Location: 3885 Sandwich Street, Windsor, ON  
Number: 20G085

Consolidated Council Agenda - June 13, 2022  
Page 632 of 1025

# APPENDIX A

PURCKA Geo-Structural Preliminary Settlements Analysis



**PURCKA**  
GEO-STRUCTURAL  
ENGINEERS INC.

Settlement Analysis for Typical Shallow Spread Strip  
Footings Based on SME data from DMT (20G085)

DMT Settlements v. 1.0.1.16

Enclosures

*Report – pages 1 through 3*

*Typical Strip Footing – pages 4 through 6*

*Typical Square Footing – pages 7 through 9*



May 21, 2021

**Ref No. 2021-010**

**Soil & Materials Engineering Inc.**

Attention: Tom O'Dwyer, P.Eng., P.E.  
Consulting Engineer  
[todwyer@ctsoil.com](mailto:todwyer@ctsoil.com)

**Re: Preliminary Geo-Structural Interaction and Settlement Analysis for the Proposed 11-Storey Development, 3885 Sandwich Street, Windsor, Ontario**

In accordance with the request and authorization from Tom O'Dwyer, P.Eng. representing Soil & Materials Engineering Inc. (C.T. Soil), Purcka Geo-Structural Engineers Inc. completed a preliminary geo-structural interaction for the settlement of the proposed exterior strip and square footings for the proposed 11-Storey Development located at 3885 Sandwich Street, Windsor, Ontario.

Purcka Geo-Structural assumed conventional shallow spread (strip) foundations will be constructed at the subject project and isolated squares for column loads. The strip footings are assumed to be at each exterior wall and at each interior corridor wall. The building loads were not provided at the time of this analysis and have been assumed since this is a preliminary analysis. Once actual structural loading and footing sizes are determined, Purcka Geo-Structural Engineers Inc. should be contacted for a refined analysis. Note that 100% of the dead load and 75% of the live load are generally used in a settlement analysis on cohesive soil. It was assumed that the four load bearing walls, two exterior walls and two corridor walls, run lengthwise along the building with each totaling 66 metres.

Exterior Wall - 2	D <sub>L</sub> = undefined kN/m	L <sub>L</sub> = undefined kN/m
Corridor Wall - 2	D <sub>L</sub> = undefined kN/m	L <sub>L</sub> = undefined kN/m

Soil conditions were provided to Purcka GSE by Soil & Materials Engineering Inc. Ref. 20G085 report dated April 10, 2021. Relevant subsurface data was collected using a speciality Marchetti-type flat plate dilatometer performed on December 23, 2020 and February 26, 2021. Based on the site map (SME



Drawings 1 and 2) provided by Soil & Materials Engineering Inc., 1-DMT and 3-DMT were selected as representational data used for the analysis (SME Drawing No.3 and No.5), attached.

The preliminary settlement analysis was completed using DMT Settlements (SMDT) v 1.0.1.16 with the following assumptions.

- The loading is uniform along the entire wall foundation, a uniform long term sustained pressure of 70 kPa acting on a 1.2 m wide strip footing (this assumption is sufficient for a preliminary analysis).
- A uniform long term sustained pressure of 80 kPa acting on a 3 m square footing (this assumption is sufficient for a preliminary analysis).
- The soil conditions provided by Soil & Materials Engineering Inc. are correct and reliable for use by Purcka GSE.

The settlement calculation report is attached on pages 4 through 9 and discussed in greater detail. The following is intended as a brief summary of the results.

	<b>Below the Centre</b>	<b>Below the Corner</b>	<b>Below the Long Side</b>	<b>Below the Short Side</b>
Strip Settlement, <b>S</b>	32 mm	18 mm	31 mm	19 mm
Squares Settlement, <b>S</b>	33 mm	23 mm	Same as short	27 mm

The above settlements are assumed to occur beneath a flexible foundation. Once the Structural Engineer has determined foundation reactions and size, the results should be provided to Purcka Geo-Structural Engineers Inc for a refined analysis using Finite Element Analysis FEA program, Sigma/W, for anticipated performance.

We trust this preliminary report is presented in a manner suitable for your review and evaluation. If questions arise concerning our findings, opinions, or any other aspect of the subject project, do not hesitate to contact us.

Regards,  
**Purcka Geo-Structural Engineers Inc.**



T.O'Dwyer, P.Eng., P.E.  
Consulting Engineer

B. Purcka, EIT  
Director



# Settlements Calculation

Purcka Geo-Structural Engineer

Soil & Material Engineering Inc

2021-010 / SME 20G085

3885 Sandwich St.

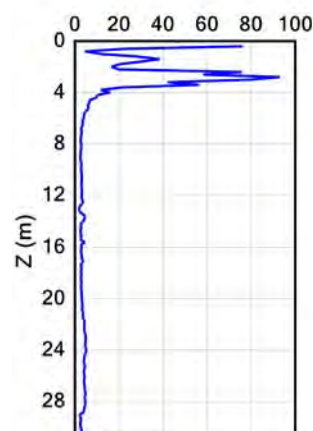
## LOAD DESCRIPTION

$$\begin{array}{c} V = 1260 \text{ kN} \\ \text{---} B = 1.2 \text{ m} \\ L = 15.0 \text{ m} \end{array}$$

$$\begin{array}{c} q = 70 \text{ kPa} \\ \downarrow \downarrow \downarrow \downarrow \\ L = 15.0 \text{ m} \quad Z_a = 1.5 \text{ m} \end{array}$$

## CONSTRAINED MODULUS M (MPa)

(manual profile input)



## CALCULATION OPTIONS

Lower limit of Constrained Modulus assigned in the calculation

0.70 MPa

Thickness of calculation layer

0.20 m

End of Calculation

at end of assigned profile

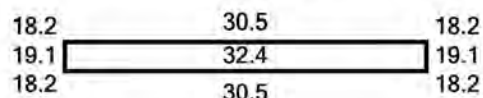
## SETTLEMENTS CALCULATION

(one-dimensional conventional method)

$$S = \sum \frac{\Delta \sigma_v}{M} \Delta z$$

Calculation Point	Settlements	Z Stop
	[mm]	[m]
below the center	32.4	30.70
below the corner	18.2	30.70
below the median point of short side	19.1	30.70
below the median point of long side	30.5	30.70

## Settlements [mm]

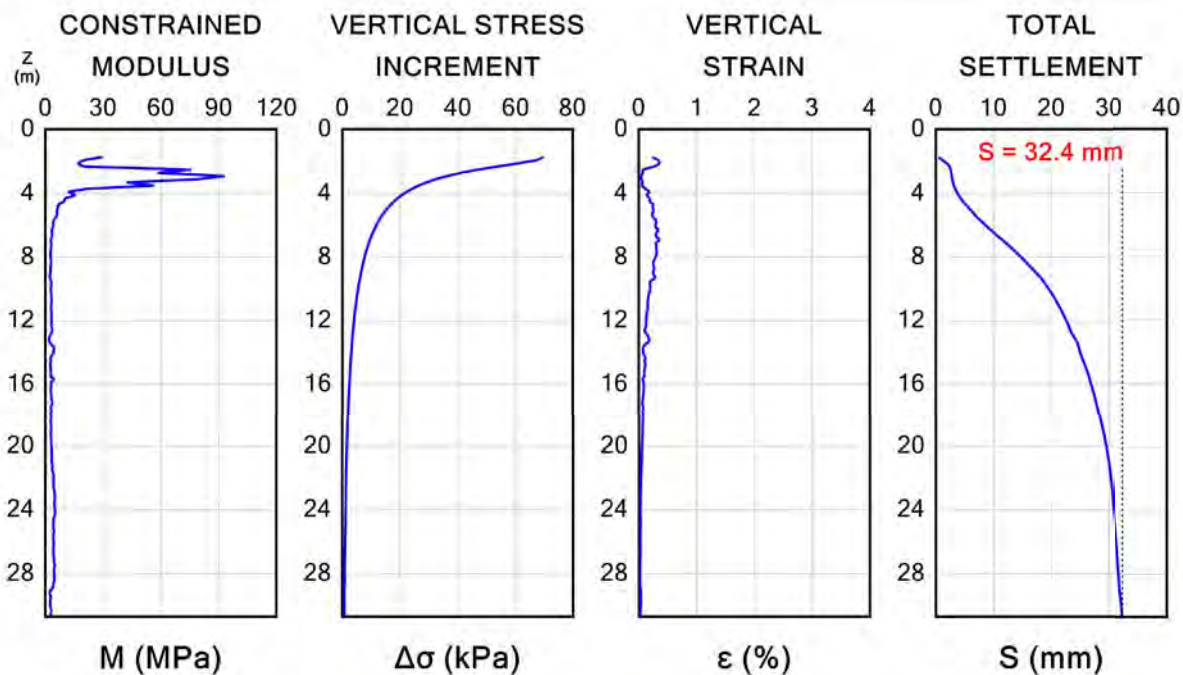


The calculated settlements are obtained using the interpretation formulae and the calculation method recommended in the TC16 DMT Report(2001). It is the designer's responsibility to use alternative procedures if considered preferable.

### SETTLEMENTS CALCULATION - below the center

Purcka Geo-Structural Engineer  
Soil & Material Engineering Inc

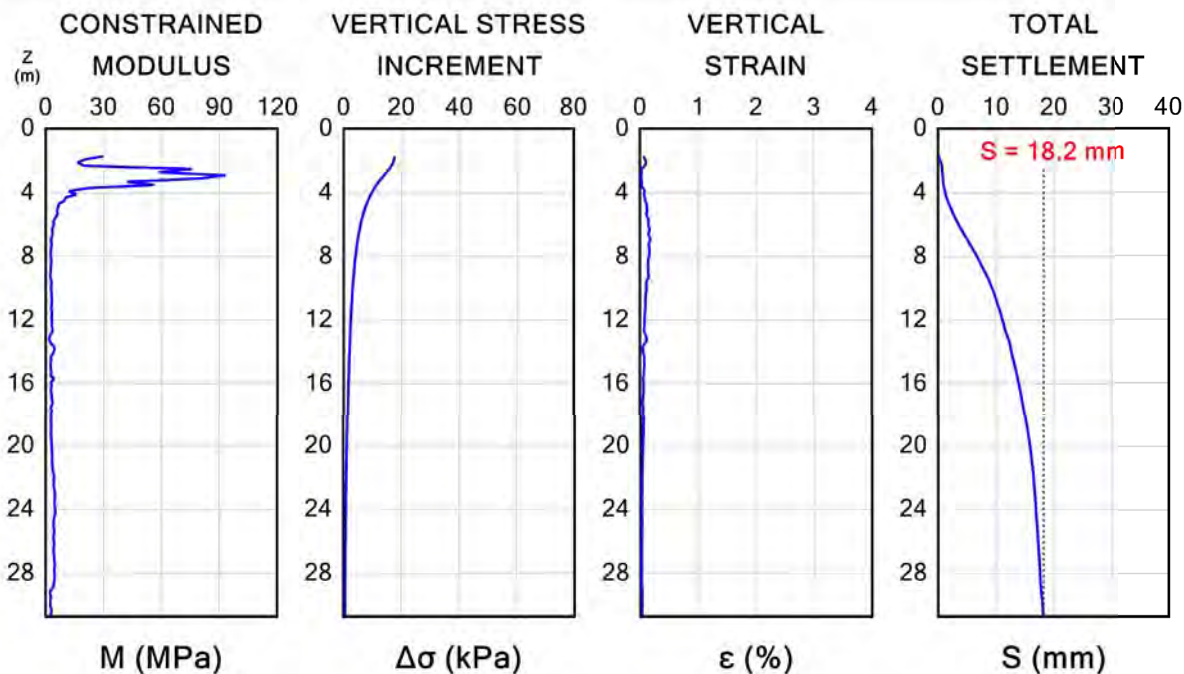
2021-010 / SME 20G085  
3885 Sandwich St.



### SETTLEMENTS CALCULATION - below the corner

Purcka Geo-Structural Engineer  
Soil & Material Engineering Inc

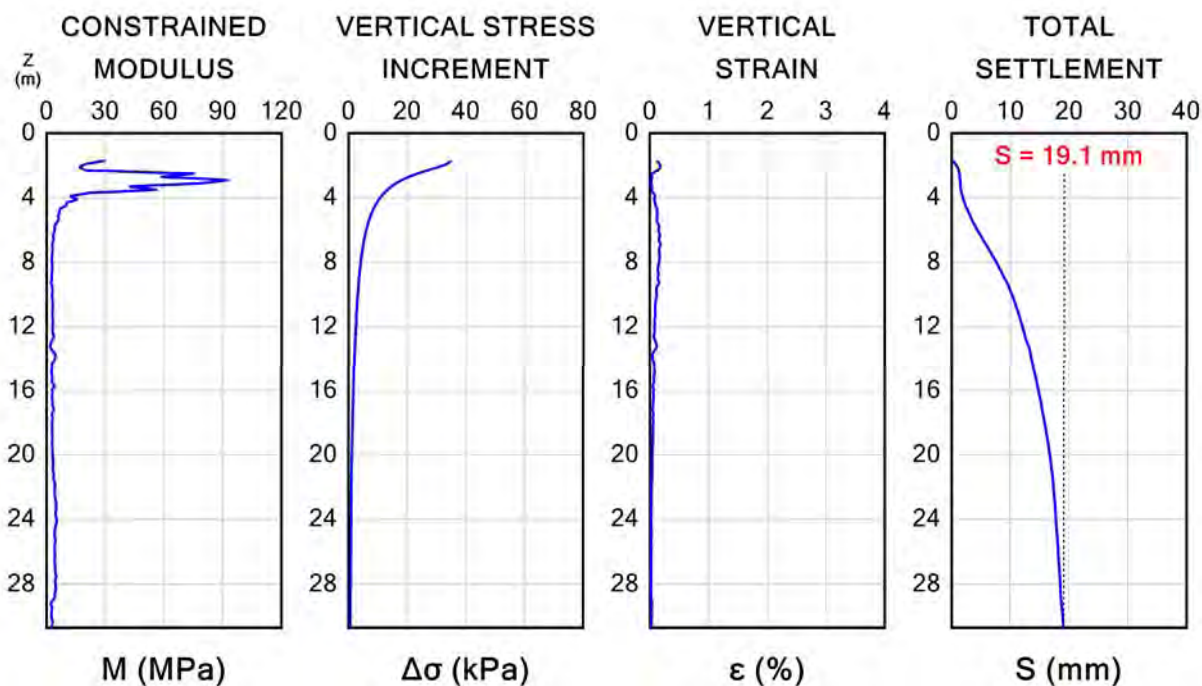
2021-010 / SME 20G085  
3885 Sandwich St.



### SETTLEMENTS CALCULATION - below the median point of short side

Purcka Geo-Structural Engineer  
Soil & Material Engineering Inc

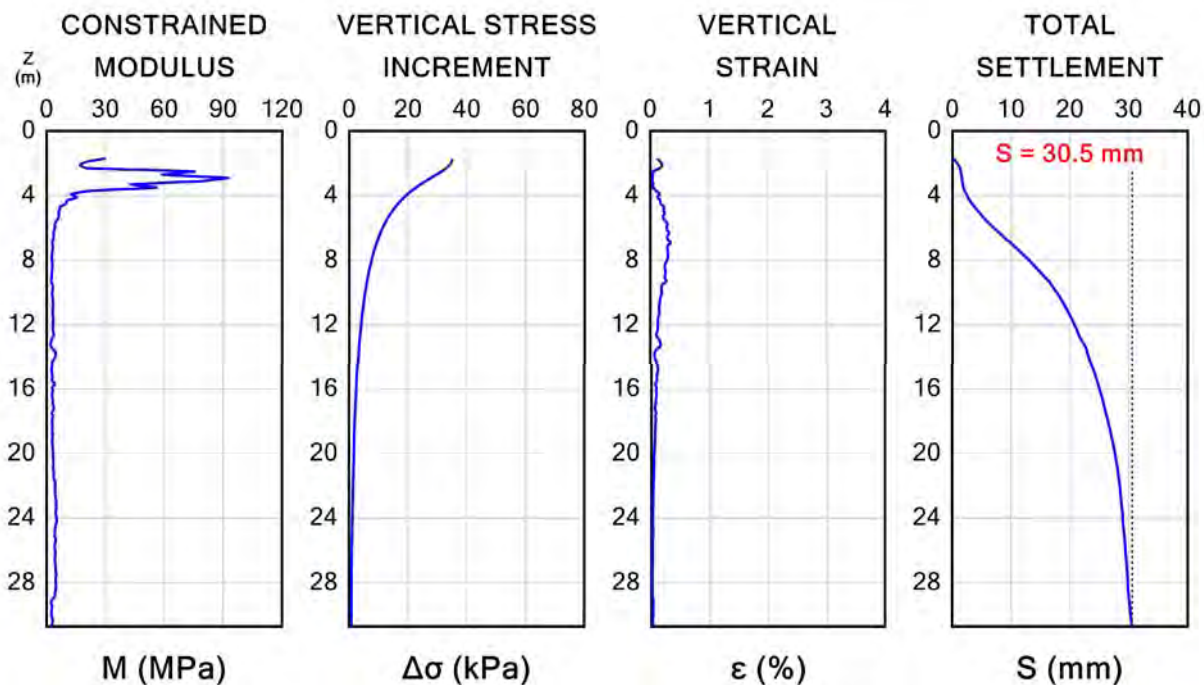
2021-010 / SME 20G085  
3885 Sandwich St.



### SETTLEMENTS CALCULATION - below the median point of long side

Purcka Geo-Structural Engineer  
Soil & Material Engineering Inc

2021-010 / SME 20G085  
3885 Sandwich St.







# Settlements Calculation

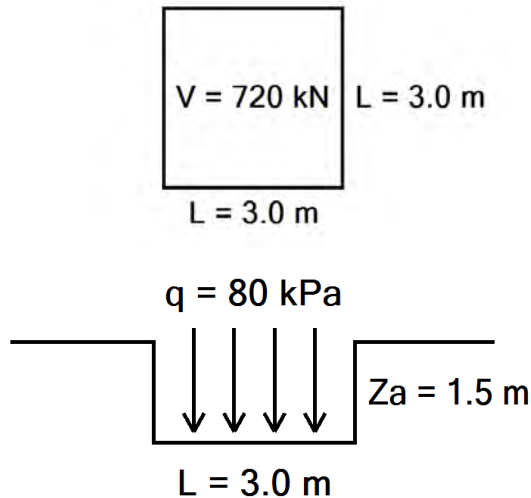
Purcka Geo-Structural Engineer

Soil & Material Engineering Inc

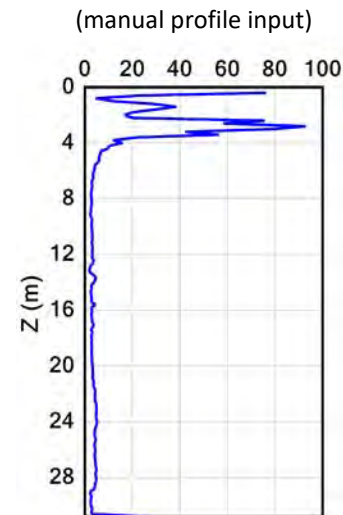
2021-010 / SME 20G085

3885 Sandwich St.

## LOAD DESCRIPTION



## CONSTRAINED MODULUS M (MPa)



## CALCULATION OPTIONS

Lower limit of Constrained Modulus assigned in the calculation

0.70 MPa

Thickness of calculation layer

0.20 m

End of Calculation

at end of assigned profile

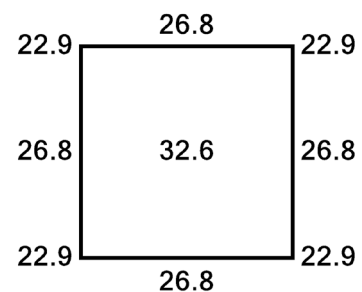
## SETTLEMENTS CALCULATION

(one-dimensional conventional method)

$$S = \sum \frac{\Delta \sigma_v}{M} \Delta z$$

Calculation Point	Settlements	Z Stop
	[mm]	[m]
below the center	32.6	30.70
below the corner	22.9	30.70
below the median point of the side	26.8	30.70

## Settlements [mm]

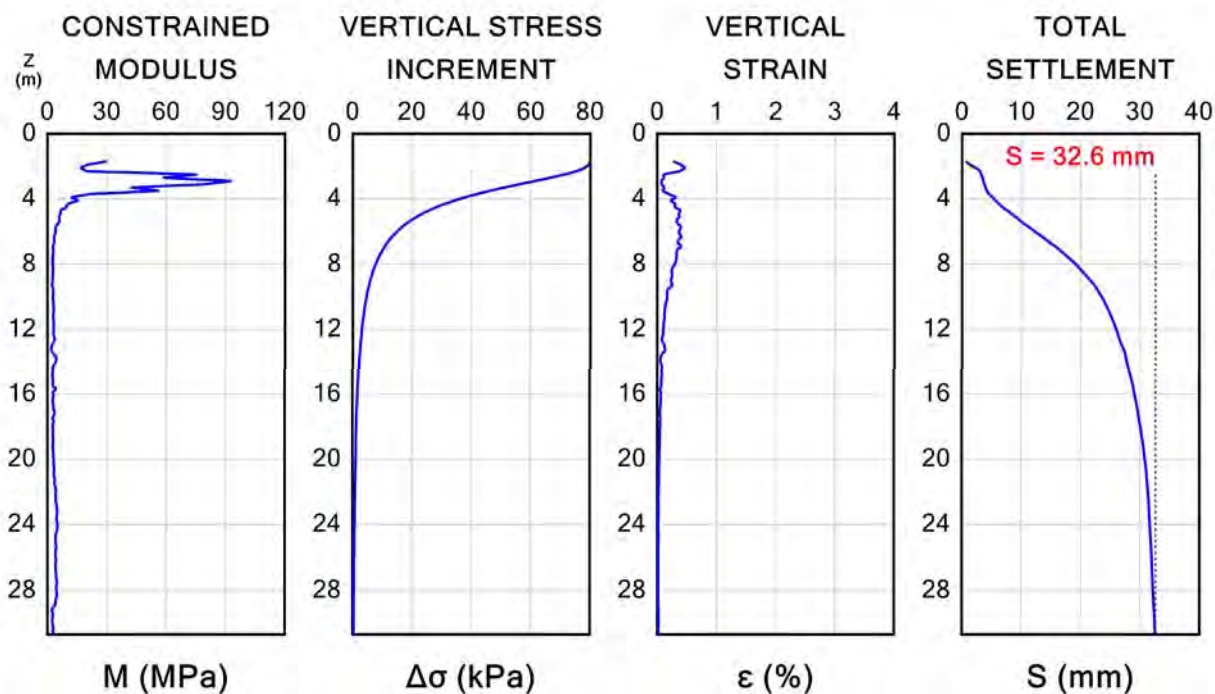


The calculated settlements are obtained using the interpretation formulae and the calculation method recommended in the TC16 DMT Report(2001). It is the designer's responsibility to use alternative procedures if considered preferable.

### SETTLEMENTS CALCULATION - below the center

Purcka Geo-Structural Engineer  
Soil & Material Engineering Inc

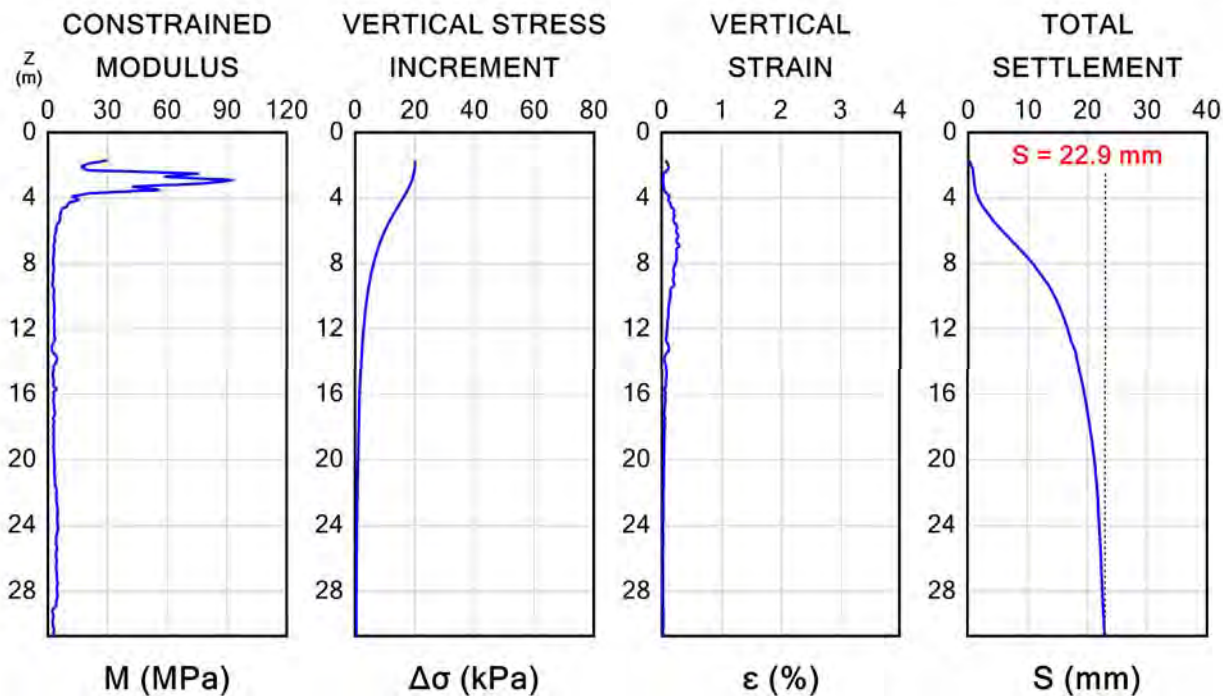
2021-010 / SME 20G085  
3885 Sandwich St.



### SETTLEMENTS CALCULATION - below the corner

Purcka Geo-Structural Engineer  
Soil & Material Engineering Inc

2021-010 / SME 20G085  
3885 Sandwich St.



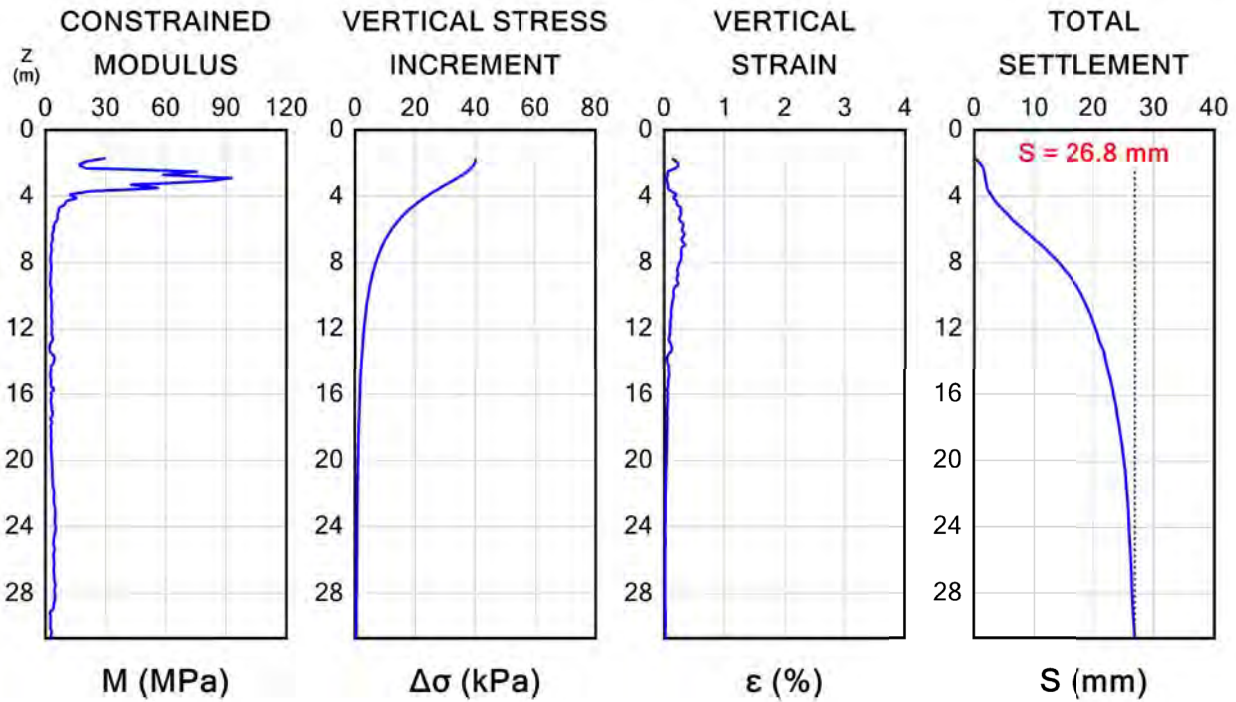
# SETTLEMENTS CALCULATION - below the median point of the side

Purcka Geo-Structural Engineer

2021-010 / SME 20G085

Soil & Material Engineering Inc

3885 Sandwich St.



# APPENDIX B

Bedrock Cores

Photo Taken: February 24, 2021

*Figure 1: Site Photo of Cores (1-DMT) Run 1: 34.15 m to 36.2 m*



*Figure 1: Site Photo of Cores (1-DMT) Run 1: 34.15 m to 36.2 m*



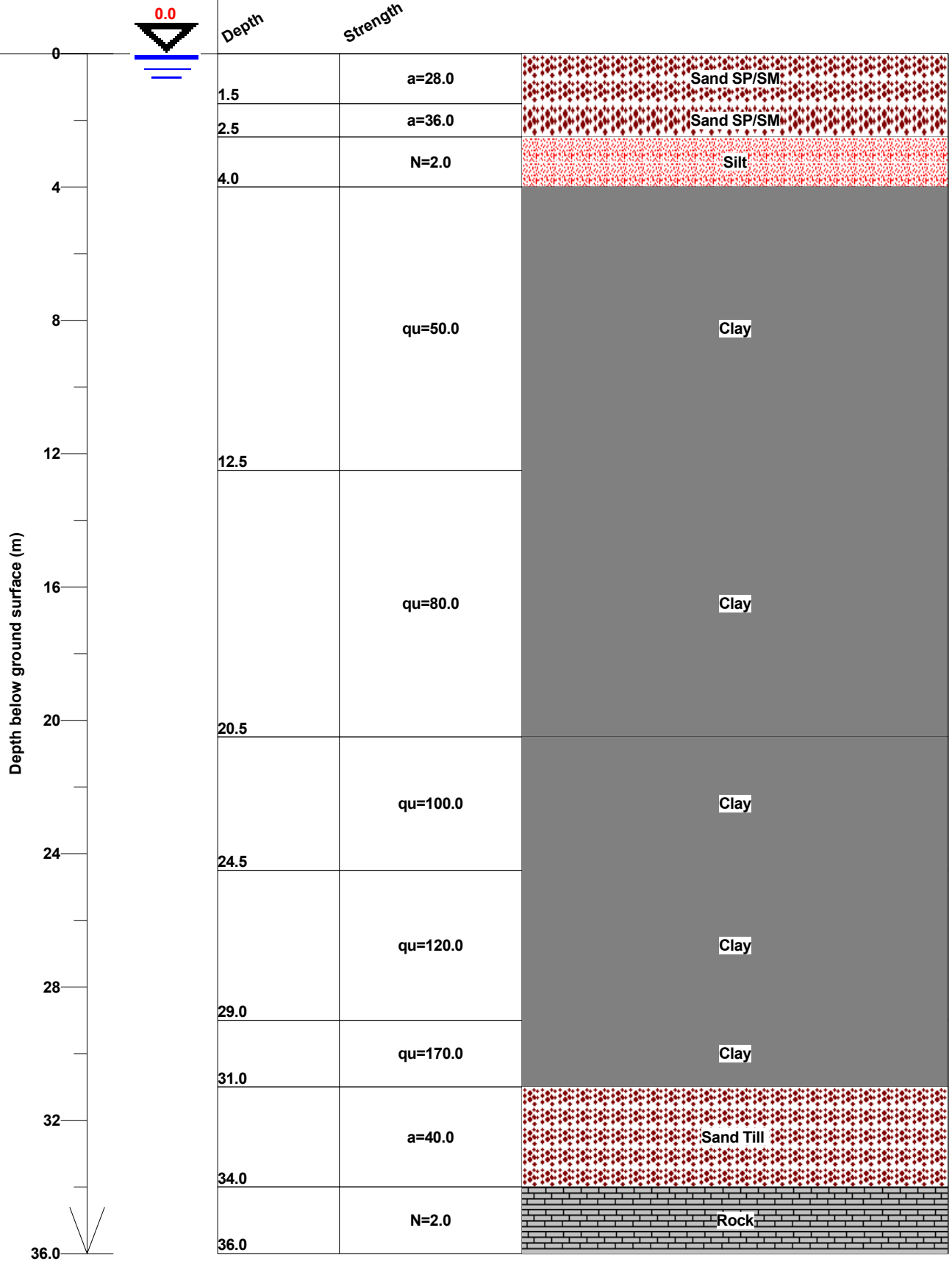
# APPENDIX C

## WEAP Analysis Using Geotechnical Subsurface Data

*Drawing 1: Soil Profile for Driveability Analysis*

*Drawing 2: Driveability Graph*

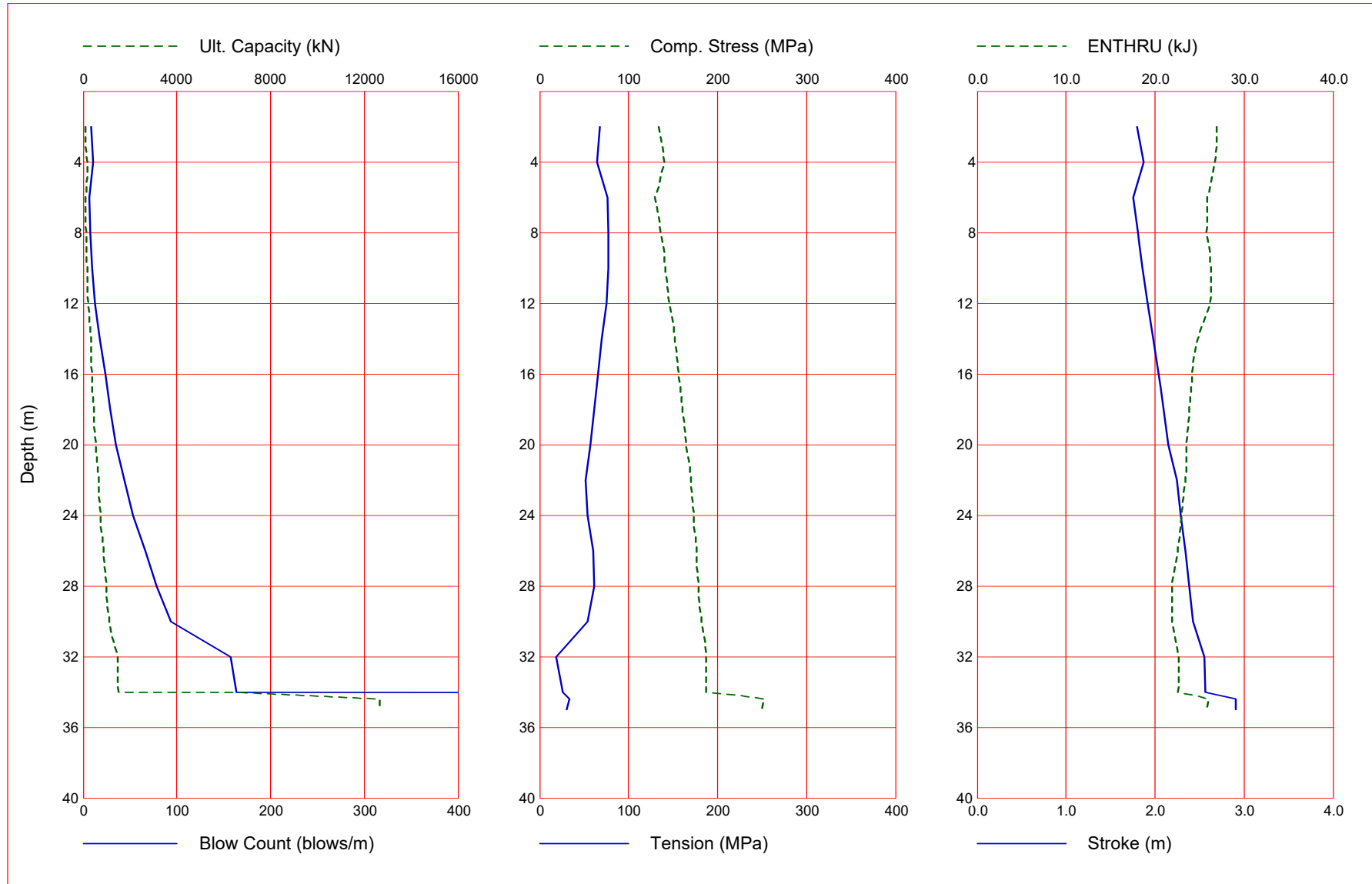
*Drawing 3: Driveability Table*



Soil & Material Engineering Inc  
11 Storey Development

2021 Apr 08  
GRLWEAP Version 2010

Gain/Loss 1 at Shaft and Toe 0.833 / 1.000



Soil & Material Engineering Inc  
11 Storey Development

2021 Apr 08  
GRLWEAP Version 2010

Gain/Loss 1 at Shaft and Toe 0.833 / 1.000

Depth m	Ultimate Capacity kN	Friction kN	End Bearing kN	Blow Count blows/m	Comp. Stress MPa	Tension Stress MPa	Stroke m	ENTHRU kJ
2.0	113.8	7.8	106.0	8.5	134.143	-67.304	1.80	26.9
4.0	169.4	35.9	133.4	10.7	140.626	-64.135	1.87	26.7
6.0	100.6	79.1	21.5	6.9	129.319	-75.879	1.75	25.9
8.0	145.9	124.5	21.5	8.1	136.311	-77.550	1.81	25.8
10.0	193.4	171.9	21.5	9.9	140.950	-77.133	1.86	26.3
12.0	242.9	221.4	21.5	12.5	145.354	-75.235	1.91	26.2
14.0	326.4	292.1	34.4	18.0	151.717	-69.445	1.98	24.8
16.0	403.7	369.3	34.4	23.6	156.391	-65.373	2.04	24.2
18.0	480.9	446.6	34.4	29.1	160.590	-61.655	2.10	23.8
20.0	558.2	523.8	34.4	35.4	164.480	-57.516	2.15	23.5
22.0	657.4	614.4	43.0	43.7	169.892	-52.040	2.24	23.4
24.0	752.4	709.5	43.0	52.8	173.251	-53.485	2.29	23.0
26.0	868.1	816.5	51.6	66.0	176.671	-59.972	2.34	22.6
28.0	979.2	927.6	51.6	78.5	178.366	-61.843	2.38	21.9
30.0	1126.8	1053.8	73.0	93.6	181.340	-53.433	2.43	21.9
32.0	1472.8	1142.6	330.2	157.8	187.529	-18.506	2.55	22.7
34.0	1517.5	1187.3	330.2	163.6	187.539	-25.947	2.57	22.6
34.4	12644.9	1187.3	11457.6	9999.0	251.759	-33.496	2.91	26.0
35.0	12644.9	1187.3	11457.6	9999.0	250.398	-30.811	2.91	25.8

Refusal occurred; no driving time output possible



**PARKING JUSTIFICATION REPORT  
MIXED USED DEVELOPMENT  
3885 SANDWICH STREET**

**WINDSOR, ONTARIO**

PROJECT NO. 20-028

DATED: MAY 25, 2022



27 Princess St., Unit 102  
Leamington, ON N8H 2X8  
519.326.6161 T F 1.844.842.9188  
[bairdAE.ca](http://bairdAE.ca)



## TABLE OF CONTENT

<b>1.</b>	<b>INTRODUCTION .....</b>	<b>1</b>
1.1	Background .....	1
1.2	Development Proposal .....	1
<b>2.0</b>	<b>EXISTING CONDITIONS .....</b>	<b>2</b>
2.1	Road Network Characteristics .....	2
2.2	Existing Parking Areas .....	2
2.3	Future Parking Areas .....	3
<b>3.0</b>	<b>PROPOSED CONDITION .....</b>	<b>4</b>
3.1	City of Windsor By-law Section 8600-CD 2.1 .....	4
3.2	Parking Demand Rate Based on ITE .....	5
<b>3.0</b>	<b>CONCLUSION .....</b>	<b>6</b>

## TABLES

Table 1: Roadway .....	2
Table 2: Parking Spaces and Location .....	4
Table 3: Minimum Parking Requirement (By-Law) .....	4

## FIGURES

Exhibit 1: Location Map  
Exhibit 2: Parking Stalls

## APPENDIX

Appendix A Site Plan and Background Information  
Appendix B By-Law and ITE Parking Generation Manual  
Appendix C Photos

## 1. INTRODUCTION

### 1.1 Background

Baird AE has been retained to prepare a Parking Study in support of the site plan application for the proposed mix-use high-rise development on Sandwich Street in City of Windsor. The land is currently zoned as Commercial District (CD) per the City of Windsor's planning department.

The purpose of the study is to determine the adequacy of parking supply to meet the requirements of the proposed development. The site currently consists of grass and gravel section.

The proposed development location is shown in Exhibit 1 below.



**Exhibit 1 - Location Plan**

### 1.2 Development Proposal

The development is 0.67ha in size which will consist of new high-rise building with retail section on ground floor, parking spaces and landscape area. The site is bounded by Chappell Avenue to the south, Sandwich Street to the west, and residential development to the north and east.

The new building is 11-Storey high consist of parking space and retail store of 6697 ft<sup>2</sup> (622.17m<sup>2</sup>) in size on ground floor and 150 residential units with 70 single bedrooms units and 80 double bedroom units on all other floors.

## 2.0 EXISTING CONDITIONS

### 2.1 Road Network Characteristics

The existing road network and lane configuration are described in Table 1. To avoid confusion in the road network direction, it is assumed that Sandwich Street runs in an east-west fashion.

**Table 1: Roadway**

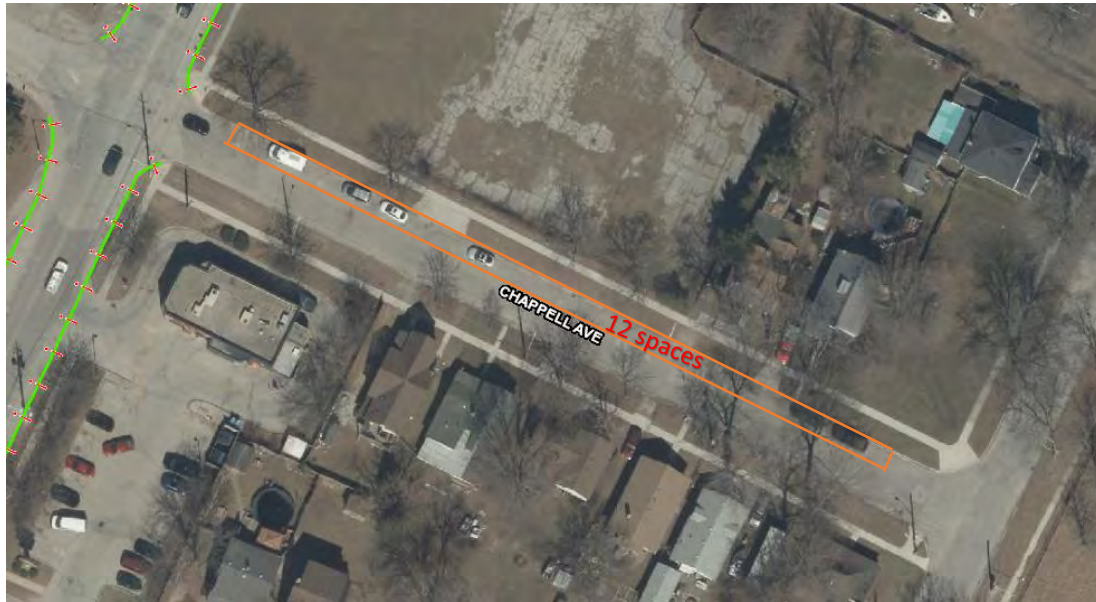
Component	Sandwich Street	Chappell Avenue
Direction	East-West	North-South
Speed limits	50 km/h	50 km/h
Cycling Facilities	No	No
On-Street Parking	No	Yes

The subject development is located in the western part of the city with transit services within the study area. These transit buses provide residents and visitors to travel to/from urban center and then to all parts of the city.

### 2.2 Existing Parking Areas

As shown in City of Windsor parking map (Appendix A), a substantial amount of existing parking is located within the vicinity of the development. Chappell Avenue is two-way streets with an urban cross-section and on-street parking is allowed on one side with no restriction at all times. Whereas, no on-street parking is available on Sandwich Street.

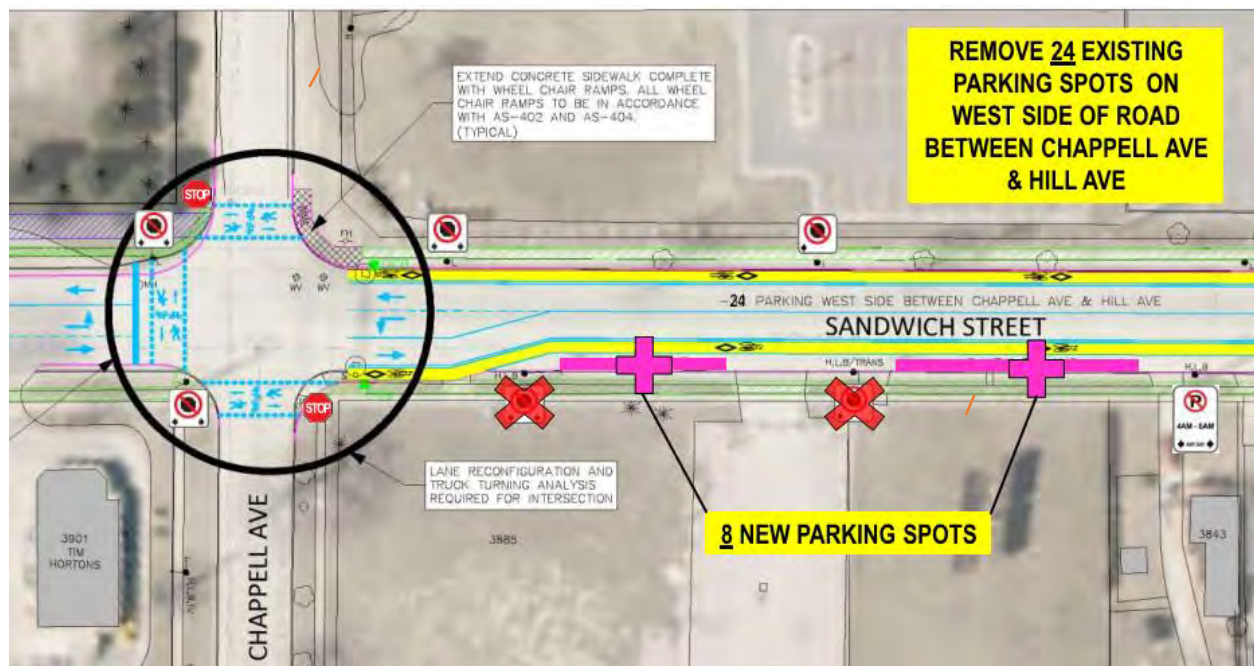
Based on aerial image, approximately 12 stalls are available at Chappell Avenue for resident and visitor use. Exhibit 2 shows the location of the parking.



**Exhibit 2 – Parking Stalls**

### 2.3 Future Parking Areas

Based on City of Windsor reconstruction plan of Sandwich Street, approximately 8 new stalls will be available on Sandwich Street in front of development. Exhibit 3 shows the location of the parking.



**Exhibit 3 – Future Parking Stalls**

### 3.0 PROPOSED CONDITION

The proposed development will include new 11-storey apartment building, commercial area of 6697 ft<sup>2</sup> on ground floor, asphalt parking area and landscape area. The following table provides the breakdown of provided parking spaces in site plan. A detailed site plan is provided in the Appendix A.

**Table 2: Parking Spaces and Location**

No	Description	Area / Units	Proposed Spaces	Lost Space	Location
1	Resident Parking	150	134	None	surface
2	Retail parking	6.6k sq.ft	22	None	surface
Total Spaces			156		

### 3.1 City of Windsor By-law Section 8600-CD 2.1

The proposed development falls within the City of Windsor Zoning By-Law 8600-CD2.1 (Zone Map 4) Commercial District.

As outlined in the City of Windsor by-law (table 24.20.5.1), the parking requirement for combine use building is 1.25 space per 1 dwelling. Further, the convenient store required 1 space for 22.5 sq.m. The required number of parking spaces for the development is provided in the Table 3 and details are provided in Appendix B.

**Table 3: Minimum Parking Requirement (By-Law)**

Land use	Levels	Parking Rate	Area / Units	Parking Required	Parking Provided	Surplus /Deficit
Multi-Storey Resident	10 Levels	1.25 space per 1 dwelling	150	187	156	(60)
Retail Store	1 level	1 Space per 22.5 m <sup>2</sup>	6,697sq.ft	29		
Total				216		

The total required parking spaces is 216 spaces based on the City's By-Law standards and assumptions. There is a deficit of 60 spaces. Additional 20+ street parking is available on the street of Sandwich Street and Chappell Avenue.



The site is well-located for travel by transit, walking and cycling alternative as alternative to driving. The site is located on Sandwich Street connecting between west Windsor and downtown. Bus route 8 has no transit stop within 150m of development but a new transit stops within vicinity of development encourage resident and visitors to use transit and bicycles and as other mode of transportation. This measure will promote City of Windsor Transportation Demand Management (TDM) strategies for a safe, secure, green, innovative and integrated transportation system which promote transit and bicycle network facilities. Deficit in parking spaces will encourage resident and visitors to use bicycles and transit as other mode of transportation.

A sandwich Street improvement as shown in Exhibit 3 support bicycles route with several other elements improvement.

Also, applicant intend to provide car share spaces in the development to support resident population and commercial traffic. There are 22 spaces for commercial use. A reduction in parking demand for commercial space after 6:00pm will help resident population to use their spaces upon mutual agreement between resident and commercial units.

A parking relief is requested for the development based on City of Windsor by-law amendment (130-2017) which states 1 space for each dwelling unit. Further, these TDM measures that have been incorporated into proposal will serve to encourage travel options by transit, walking and cycling for resident.

There is more reduced parking demand as discussed in the following section of this report.

### ***3.2 Parking Demand Rate Based on ITE***

The development is investigated using the Institution of Transportation Engineering (ITE) Parking Generation, 5th Edition. The number of parking spots required for the development is calculated using the ITE Manual land use types ITE 222 (Multifamily – High Rise), ITE 814 (variety store), and ITE 851 (convenient market). The provided spaces are shown in Table 3, and details are provided in Appendix B.

**Table 3: Minimum Parking Requirement (ITE Parking Manual)**

Land use	Units	ITE	Average Parking Rate	Parking Required	Parking Provided	Deficit /Surplus
Apartment Buildings	150 units	222	0.98	147	156	(15)
Convenience Store	3.8k	851	5.44	21		
Variety Store	2,900sq.ft	814	1.13	3		
Total				171		

Based on the ITE Manual, the required number of residential parking spaces is 180. The number of parking stalls provided is 156 spaces, the deficit of 15 spaces.

As discussed in Section 3.1, the applicant intends to support share parking between commercial and residential users. The commercial use is to serve a small retail and convenience store and should not create additional parking demand in the evening and on weekends when residential parking peaks.

It is noted that the development has 70 single bedroom apartment units which will create maximum parking requirement at 1:1 ratio (1 bedroom / 1 parking space). We believe this ratio will be mostly probably less. Further, additional 20 plus street parking is available on the street of Sandwich Street and Chappell Avenue to mitigate deficit parking requirements.

### **3.0 CONCLUSION**

This parking justification has been undertaken in accordance with City requirements in order to support Official Plan Amendment, Zoning By-law amendment and Site Plan applications for the site. Following are the findings and are as follows:

The development is well located for travel by transit and active transportation. The development is mix of commercial and residential use that make mostly trips by walking and by cycling. In addition, there are sidewalk on the nearby roadways and future options for cycling in the area as shown Exhibit 3.

The development promotes City of Windsor TDM strategies that increase the capacity of our existing transportation system. Further, the applicant intends to support share parking between commercial and residential users. Therefore, we believe the available existing and provided parking spaces are satisfactory to meet the City's by-law. Further, there is more reduced parking demand based on ITE Parking Manual.

We, therefore, anticipate no further changes to parking spaces will be required.

All of which is respectfully submitted.

**BAIRD AE INC.**  
**27 PRINCESS STREET, UNIT 102**  
**LEAMINGTON, ONTARIO N8H 2X8**

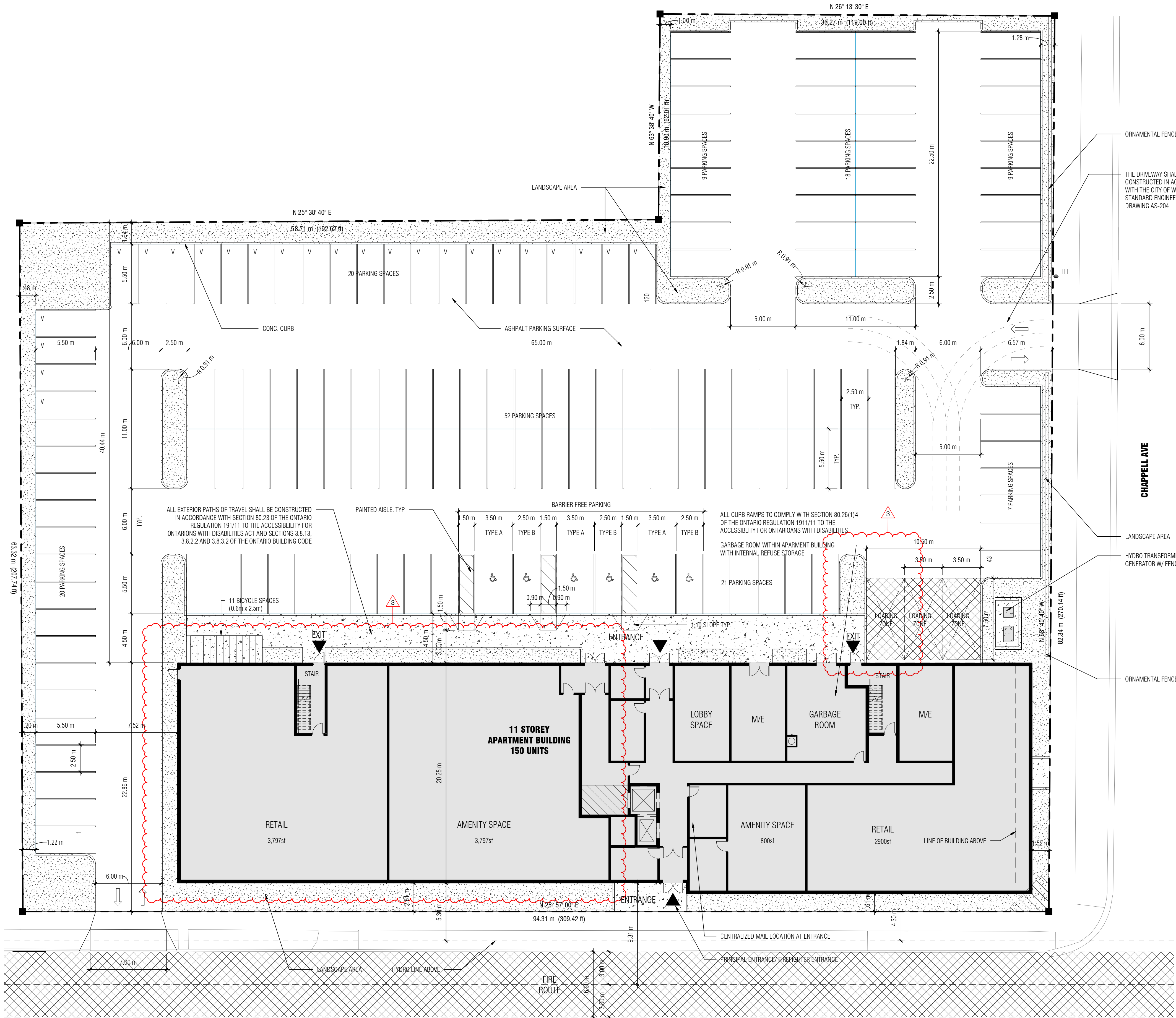
Shurjeel Tunio, P.Eng.  
Senior Project Manager  
**Baird AE**



## **Appendix A**

---

### **SITE PLAN AND BACKGROUND INFORMATION**



SITE DATA MATRIX			OBC REFERENCE
PROJECT DESCRIPTION: 11 STOREY MULTI UNIT RESIDENTIAL BUILDING 150 RESIDENTIAL UNITS			<input checked="" type="checkbox"/> PART 3 <input type="checkbox"/> PART 9 <input type="checkbox"/> PART 11
ZONING DESIGNATION:	CD2.1		
MAJOR OCCUPANCY:	RESIDENTIAL		
BUILDING CLASSIFICATION:	GROUP C		
SITE AREA	BUILDING AREA	GROSS AREA	
EXISTING: 6,694m <sup>2</sup>	EXISTING: N/A	EXISTING:	N/A
PROPOSED: 6,694m <sup>2</sup>	PROPOSED: 1,622m <sup>2</sup>	PROPOSED:	19,122.04m <sup>2</sup>
TOTAL: 6,694m <sup>2</sup>	TOTAL: 1,622m <sup>2</sup>	TOTAL:	19,122.04m <sup>2</sup>
LOT COVERAGE	MINIMUM LOT WIDTH	BUILDING HEIGHT	
MAXIMUM: N/A	REQUIRED: N/A	MAXIMUM:	14m
PROVIDED: 1,623m <sup>2</sup>	PROVIDED: 94.8m	PROVIDED:	37m
MINIMUM FRONT YARD DEPTH	MINIMUM REAR YARD DEPTH	MINIMUM SIDE YARD DEPTH	
MAXIMUM: N/A	REQUIRED: N/A	MAXIMUM:	N/A
PROVIDED: 1.61m	PROVIDED: 40.44m	PROVIDED:	1.52m
PARKING	BICYCLE SPACES	LOADING SPACES	
USE CLASSIFICATION	EXISTING: 0	EXISTING:	0
PROPOSED: 156 (6 BF, 3 TYPE A & 3 TYPE B) 1.04 RATIO	PROPOSED: 11	PROPOSED:	3
REQUIRED: 188 (6 BF, 3 TYPE A & 3 TYPE B) 1.25 RATIO	REQUIRED: 11	REQUIRED:	3
TOTAL: 156 (6 BF, 3 TYPE A & 3 TYPE B) 1.04 RATIO	TOTAL: 11	TOTAL:	3
LANDSCAPE AREA	AMENITY SPACE	SCREENING FENCE LENGTH	
EXISTING: N/A	REQUIRED: 900m <sup>2</sup>	EXISTING:	N/A
PROPOSED: 868m <sup>2</sup>	PROPOSED: 1294m <sup>2</sup>	PROPOSED:	N/A
TOTAL: 868.56m <sup>2</sup>	TOTAL: N/A	TOTAL:	N/A
NOTE: 1. ALL EXTERIOR PATHS OF TRAVEL SHALL BE CONSTRUCTED IN ACCORDANCE WITH SECTION 80.23 OF ONTARIO REGULATION 191/11 TO THE ACCESSIBILITY FOR ONTARIANS WITH DISABILITIES ACT AND SECTIONS 3.8.1.3 AND 3.8.3.2 OF THE ONTARIO BUILDING CODE. 2. ALL CURB RAMP SHALL BE CONSTRUCTED IN ACCORDANCE WITH SECTION 80.26(1) OF THE ONTARIO REGULATION 191/11 TO THE ACCESSIBILITY FOR ONTARIANS WITH DISABILITIES ACT AND SECTION 3.8.3.1.8 OF THE ONTARIO BUILDING CODE.			

102 - 27 Princess Street  
Leamington ON N8H 2X8

BAIRDIAE

architecture + engineering

1000 - 267 Pelissier Street  
Windsor ON N9A 4K4

A DIVISION OF MATTHEW J. BAIRD CONSULTING SERVICES INC.

3 SITE REVISIONS 2 22.05.25

2 SITE REVISIONS 21.06.06

1 ISSUED FOR OPA/ZBA/SPC 20.11.09

No. Description Date

Revision Schedule

SEAL

ONTARIO ASSOCIATION  
OF  
ARCHITECTS  
PAC. WELD  
LICENCE  
7289

MIXED-USE DEVELOPMENT

SANDWICH ST., WINDSOR ON

SHEET TITLE

SITE PLAN

20-028

SP-1

2022-05-25 3:23:07 PM

Consolidated Council Agenda - June 13, 2022  
Page 660 of 1025















Zoning District Map 4

LEGEND:

- Zone Boundary<sup>1</sup>
- Specific Zoning Exemptions<sup>2</sup>
- Specific Zoning Regulation<sup>5</sup>
- Specific Temporary Zoning Exemptions<sup>3</sup>
- Registered Plan Parcel Limits
- Ownership Parcel Limits
- Municipal Boundary Line
- Inland Watercourse Flood Prone Area<sup>4</sup>
- Detroit River/Lake St.Clair Flood Prone Area<sup>4</sup>

NOTES:

1. Each Zoning District symbol corresponds to a zoning district set out in the text of By-law 8600 (i.e. CD1.1 - Commercial District 1.1).

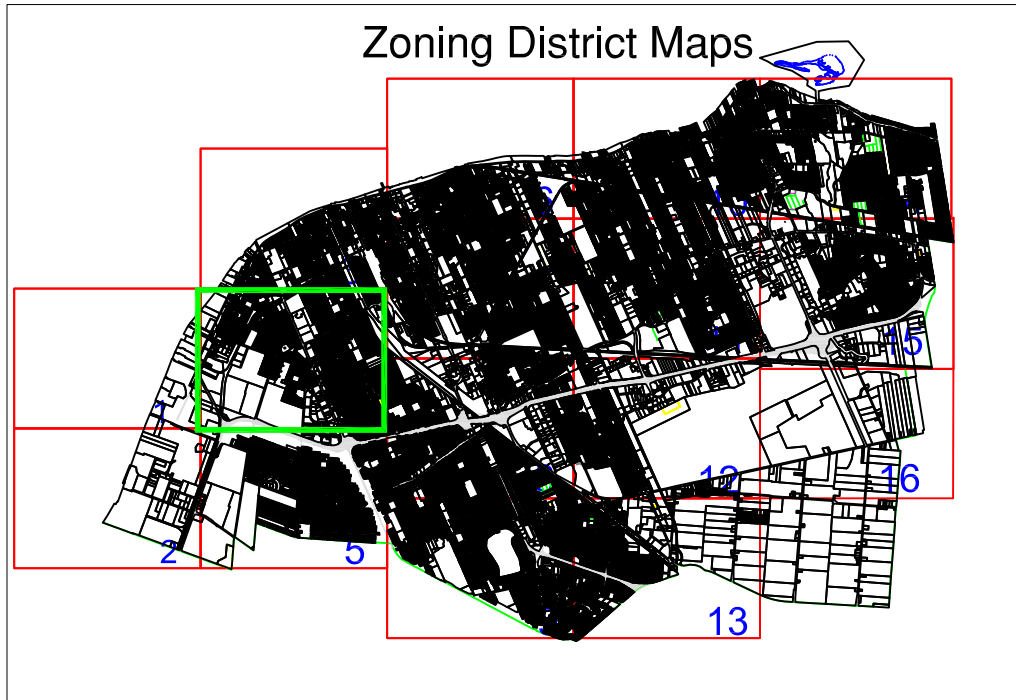
2. An H symbol preceding the zoning district symbol represents a hold on the issuance of a building permit until specific development provisions have been satisfied. Subsection 5.4 specifies the cases permitted until such time as the H symbol is removed by an amending bylaw approved by Council.

3. See Subsection 20(1) and the relevant clause for the specific special provisions.

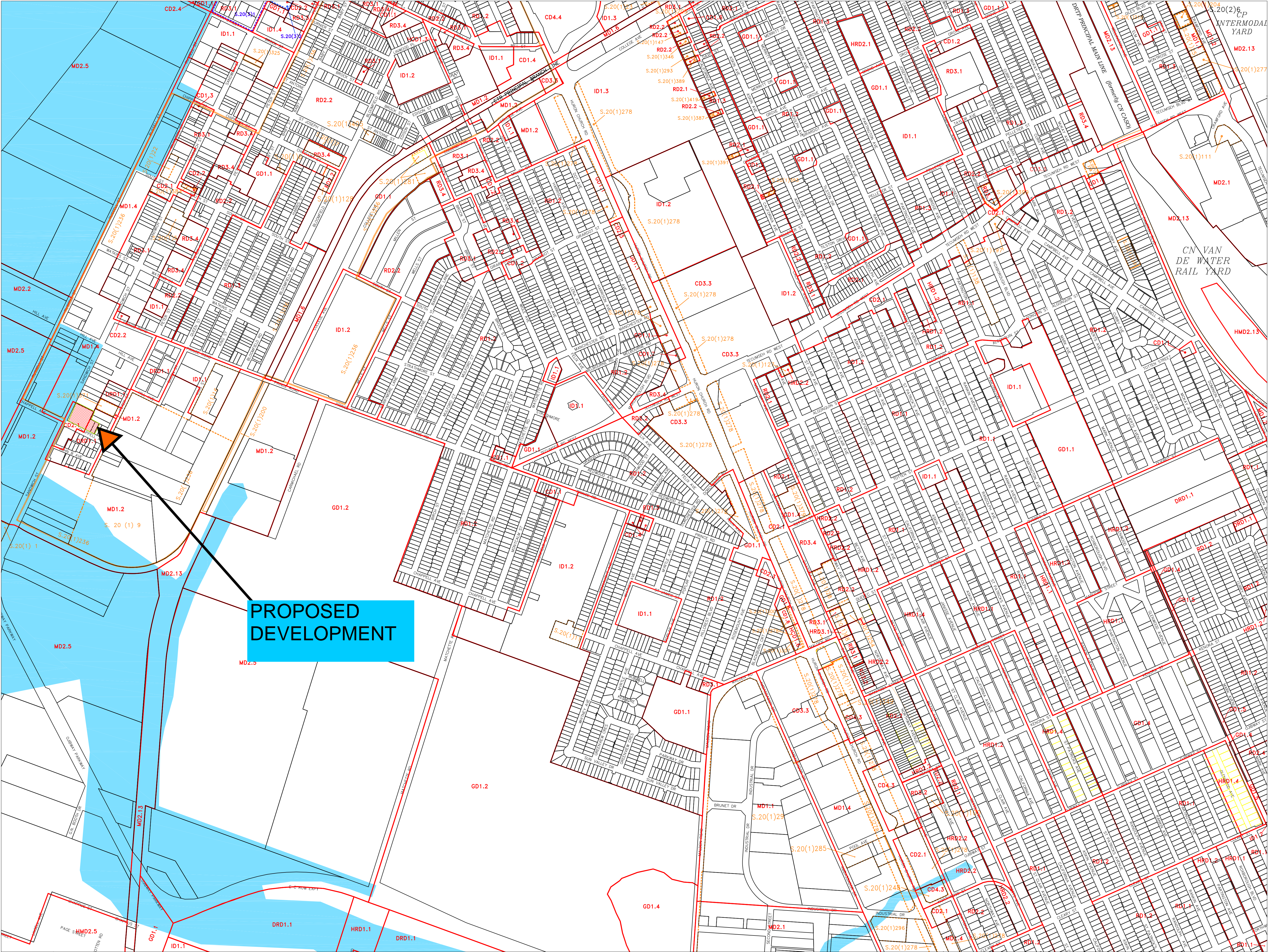
4. Represents the approximate limits of land subject of potential flooding along the Detroit River, Lake St. Clair and inland watercourses as determined by the Essex Region Conservation Authority (ERCA). Within these areas, buildings or structures are generally restricted and possibly prohibited. Application for building permits will be referred to ERCA for its review and the issuance of permits prior to the issuance of any building permit by the City of Windsor.

5. See Subsection 20(3) and the relevant clause for the specific special provisions.

DATE:		BY-LAW #		DATE:		BY-LAW #	
06/05/04	107-2003	06/05/04	107-2003	01/18/21	3-2021	01/18/21	3-2021
10/05/04	107-2003	07/05/04	109-2004	08/31/21	123-2021	08/31/21	123-2021
07/05/04	109-2004	07/05/04	109-2004	12/07/21	176-2021	12/07/21	176-2021
07/06/03	330-2003	07/06/03	330-2003	12/15/21	144-2021	12/15/21	144-2021
25/11/04	346-2004	25/11/04	346-2004				
25/11/04	361-2004	25/11/04	361-2004				
21/02/05	2-2005	21/02/05	2-2005				
21/02/05	25-2005	21/02/05	25-2005				
01/03/05	47-2005	01/03/05	47-2005				
18/04/05	71-2005	18/04/05	71-2005				
20/04/05	75-2005	20/04/05	75-2005				
24/04/05	313-2004	24/04/05	313-2004				
06/07/05	121-2005	06/07/05	121-2005				
28/05/05	130-2005	28/05/05	130-2005				
08/08/05	171-2005	08/08/05	171-2005				
08/08/05	194-2005	08/08/05	194-2005				
13/10/05	239-2005	13/10/05	239-2005				
24/10/05	225-2005	24/10/05	225-2005				
Revised:				Revised:			
13/08/06	8/L 327, 200	13/08/06	8/L 327, 200				
30/07/06	110-2006	30/07/06	110-2006				
14/08/06	147-2006	14/08/06	147-2006				
21/08/06	146-2006	21/08/06	146-2006				
23/10/06	178-2006	23/10/06	178-2006				
13/11/06	184-2006	13/11/06	184-2006				
04/09/07	214-2006	04/09/07	214-2006				
08/08/08	80-2008	08/08/08	80-2008				
08/08/08	85-2008	08/08/08	85-2008				
31/12/08	198-2008	31/12/08	198-2008				
16/03/09	160-2007	16/03/09	160-2007				
11/02/10	10-2010	11/02/10	10-2010				
22/10/10	146-2010	22/10/10	146-2010				
14/03/11	20-2011	14/03/11	20-2011				
14/03/11	27-2011	14/03/11	27-2011				
23/03/11	28-2011	23/03/11	28-2011				
30/03/11	65-2011	30/03/11	65-2011				
08/04/11	71-2011	08/04/11	71-2011				
08/04/11	80-2011	08/04/11	80-2011				
28/12/07	233-2007	28/12/07	233-2007				
13/09/08	324-2004	13/09/08	324-2004				
22/11/11	132-2011	22/11/11	132-2011				
22/12/11	208-2011	22/12/11	208-2011				
10/01/12	209-2011	10/01/12	209-2011				
06/06/12	52-2012	06/06/12	52-2012				
28/09/12	126-2012	28/09/12	126-2012				
30/10/12	30-2009	30/10/12	30-2009				
30/10/12	24-2009	30/10/12	24-2009				
07/11/12	164-2012	07/11/12	164-2012				
08/05/13	67-2013	08/05/13	67-2013				
21/05/13	56-2013	21/05/13	56-2013				
08/10/13	159-2013	08/10/13	159-2013				
22/10/13	154-2013	22/10/13	154-2013				
22/10/13	167-2013	22/10/13	167-2013				
13/03/14	16-2014	13/03/14	16-2014				
09/06/14	76-2014	09/06/14	76-2014				
13/08/14	114-2014	13/08/14	114-2014				
27/08/14	101-2013	27/08/14	101-2013				
25/06/15	67-2015	25/06/15	67-2015				
29/07/15	8/L 8600 1.20.30.5	29/07/15	8/L 8600 1.20.30.5				
02/08/16	1-2016	02/08/16	1-2016				
07/05/17	56-2017	07/05/17	56-2017				
08/23/17	63-2017	08/23/17	63-2017				
08/23/17	108-2017	08/23/17	108-2017				
08/23/17	110-2017	08/23/17	110-2017				
08/23/17	117-2017	08/23/17	117-2017				
05/24/18	177-2017	05/24/18	177-2017				
06/19/18	63-2018	06/19/18	63-2018				
09/04/18	120-2018	09/04/18	120-2018				
03/01/19	168-2018	03/01/19	168-2018				
06/25/19	64-2019	06/25/19	64-2019				
11/01/19	97-2019	11/01/19	97-2019				
12/18/19	120-2019	12/18/19	120-2019				
12/18/19	128-2019	12/18/19	128-2019				
09/23/20	111-2020	09/23/20	111-2020				
09/25/20	110-2020	09/25/20	110-2020				
10/06/20	122-2020	10/06/20	122-2020				
12/01/20	146-2020	12/01/20	146-2020				
01/27/20	170-2020	01/27/20	170-2020				



Zoning District Map 4



This forms Part of Schedule 'A'.

City of Windsor Zoning By-law 8600

Scale 1:6000



POINTS OF INTEREST LEGEND

- Elementary Schools:**
  - 1 Bellevue
  - 2 Central
  - 3 Christ the King
  - 4 Coronation
  - 5 Corpus Christi
  - 6 David Maxwell
  - 7 Dougal
  - 8 Dr. David Suzuki
  - 9 Eastwood
  - 10 Ecole Elementaire Louise-Charron
  - 11 Ecole Georges P. Vanier
  - 12 Ecole L'Envollee
  - 13 Ecole Monseigneur Jean Noel
  - 14 Ecole Saint-Edmond
  - 15 Ecole Sainte-Therese
  - 16 F.W. Begley
  - 17 Ford City
  - 18 Forest Glade - Primary Learning Centre
  - 19 Forest Glade - Main Site
  - 20 General Brock
  - 21 Giles Campus
  - 22 Glenwood
  - 23 J.J. Lassalle
  - 24 Herman Academy Elementary
  - 25 Hetherington
  - 26 Hugh Beaton
  - 27 Immaculate Conception
  - 28 John A. McWilliam
- Post-Secondary:**
  - 1 Adult and Continuing Education
  - 2 Direction(JAR), P.A.S.S. and Turning Point
  - 3 Glenagard-Osmond Bldg
  - 4 INCUBATOR: Hybrid Laboratory
  - 5 Maryvale Adolescent Special Education
  - 6 New Beginnings I and II
  - 7 St. Clair College
  - 8 St. Clair College Media Plex
  - 9 St. Michael's Adult Learning
  - 10 University of Windsor
  - 11 University of Windsor - St. Denis Centre
  - 12 University of Windsor - School of Social Work/ Centre for Executive and Professional Education
  - 13 University of Windsor - School of Creative Arts
  - 14 University of Windsor - School of Music/Visual Arts and Film Production Program
- Community Centres:**
  - 1 Adult Knox Herman Recreation Complex
  - 2 Centre for Seniors
  - 3 CST John Atkinson Memorial Community Centre
  - 4 Forest Glade Community Centre
  - 5 Gino A. Marcus Community Centre
  - 6 Glenagard Court Community Centre
  - 7 Little River Golf Course
  - 8 Mackenzie Hall Cultural Centre
  - 9 Malden Visitors Centre
  - 10 Ojibway Nature Centre
  - 11 Optimist Community Centre
  - 12 Roseland Golf and Curling Club
  - 13 Capri Pizzeria Recreation Complex
  - 14 WFCU Centre
  - 15 Willistead Heritage Complex
  - 16 Windsor Water World
- Customer Care Centres:**
  - 1 Adult Knox Herman Recreation Complex
  - 2 Centre for Seniors
  - 3 CST John Atkinson Memorial Community Centre
  - 4 Forest Glade Community Centre
  - 5 Gino A. Marcus Community Centre
  - 6 Glenagard Court Community Centre
  - 7 Little River Golf Course
  - 8 Mackenzie Hall Cultural Centre
  - 9 Malden Visitors Centre
  - 10 Ojibway Nature Centre
  - 11 Optimist Community Centre
  - 12 Roseland Golf and Curling Club
  - 13 Capri Pizzeria Recreation Complex
  - 14 WFCU Centre
  - 15 Willistead Heritage Complex
  - 16 Windsor Water World
- Public Libraries:**
  - 1 Bellevue
  - 2 Central
  - 3 Fontainebleau
  - 4 Forest Glade - Optimist
  - 5 John Muir
  - 6 Nikola Budimir Memorial
  - 7 Riverside
  - 8 Seminole
  - 9 W.F. "Bill" Chisholm
  - 10 Windsor Community Museum
- Secondary Schools:**
  - 1 Assumption College/Middle School
  - 2 Catholic Central
  - 3 Ecole Secondaire de Lamothe-Cadillac
  - 4 Ecole Secondaire E.J. Lajeunesse
  - 5 F.J. Brennan
  - 6 Herman Academy Secondary
  - 7 Holy Names
  - 8 Kennedy Collegiate
  - 9 Massey
  - 10 Riverside
  - 11 St. Joseph's
  - 12 Walkerville Collegiate
  - 13 Westview Freedom Academy
- Arenas:**
  - 1 Adult Knox Arena
  - 2 Forest Glade Arena
  - 3 Lions Outdoor Rink
  - 4 Capri Pizzeria Recreation Complex
  - 5 WFCU Centre
- French Immersion:**
  - 1 Adult Knox Arena
  - 2 Forest Glade Arena
  - 3 Lions Outdoor Rink
  - 4 Capri Pizzeria Recreation Complex
  - 5 WFCU Centre
- Parklands:**
  - 1 Adult Knox Arena
  - 2 Forest Glade Arena
  - 3 Lions Outdoor Rink
  - 4 Capri Pizzeria Recreation Complex
  - 5 WFCU Centre
- Major Special Event Areas:**
  - 1 Adult Knox Arena
  - 2 Forest Glade Arena
  - 3 Lions Outdoor Rink
  - 4 Capri Pizzeria Recreation Complex
  - 5 WFCU Centre
- Indoor/Outdoor Pools:**
  - 1 Adult Knox Arena
  - 2 Forest Glade Arena
  - 3 Lions Outdoor Rink
  - 4 Capri Pizzeria Recreation Complex
  - 5 WFCU Centre

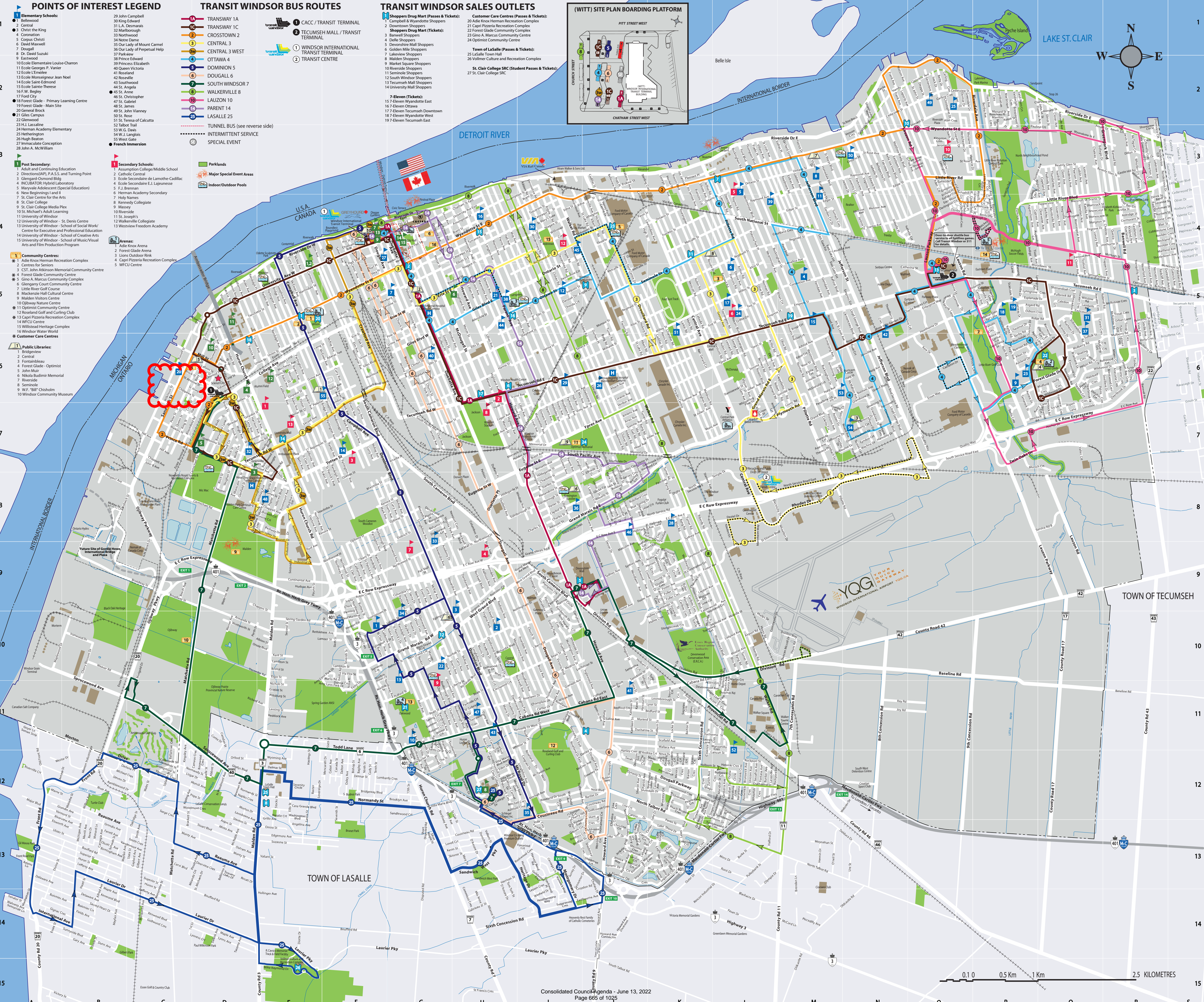
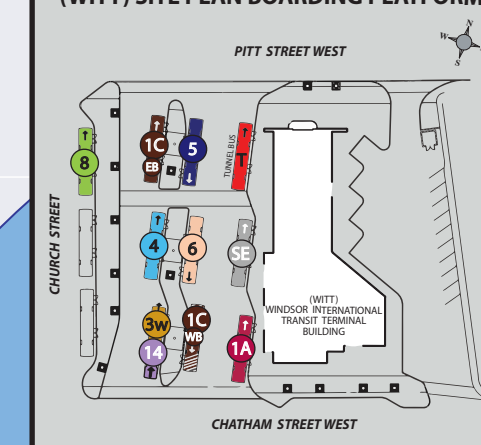
TRANSIT WINDSOR BUS ROUTES

- TRANSWAY 1A**
- TRANSWAY 1C**
- CROSSTOWN 2**
- CENTRAL 3**
- CENTRAL 3 WEST**
- OTTAWA 4**
- DOMINION 5**
- DOUGALL 6**
- SOUTH WINDSOR 7**
- WALKERVILLE 8**
- LAUZON 10**
- PARENT 14**
- LASALLE 25**
- TUNNEL BUS** (see reverse side)
- INTERMITTENT SERVICE**
- SPECIAL EVENT**

TRANSIT WINDSOR SALES OUTLETS

- Shoppers Drug Mart (Passes & Tickets):**
  - 1 Campbell & Wyandotte Shoppers
  - 2 Downtown Shoppers
  - 3 Shoppers Drug Mart (Tickets)
  - 4 Banwell Shoppers
  - 5 Delta Shoppers
  - 6 Devonshire Mall Shoppers
  - 7 Golden Mile Shoppers
  - 8 Lakeside Shoppers
  - 9 Malden Shoppers
  - 10 Market Square Shoppers
  - 11 Riverside Shoppers
  - 12 South Windsor Shoppers
  - 13 Tecumseh Mall Shoppers
  - 14 University Mall Shoppers
- Customer Care Centres (Passes & Tickets):**
  - 20 Adult Knox Herman Recreation Complex
  - 21 Capri Pizzeria Recreation Complex
  - 22 Forest Glade Community Complex
  - 23 Gino A. Marcus Community Centre
  - 24 Optimist Community Centre
- Town of LaSalle (Passes & Tickets):**
  - 25 LaSalle Town Hall
  - 26 Vollmer Culture and Recreation Complex
- St. Clair College SRC (Student Passes & Tickets):**
  - 27 St. Clair College SRC
- 7-Eleven (Tickets):**
  - 17-Eleven Wyandotte East
  - 16-7-Eleven Ottawa
  - 17-7-Eleven Tecumseh Downtown
  - 18-7-Eleven Wyandotte West
  - 19-7-Eleven Tecumseh East

(WITT) SITE PLAN BOARDING PLATFORM







## Legend

### Schools

- Elementary School
- Elementary and Secondary
- Secondary School



### Crossing Guard Locations

### School Parking Special Zone:

- Bus Bay
- Kiss and Ride
- School Bus Loading Zone

### School Parking Limits

- No Parking At All Times
- No Parking With Exception
- Residential On Street Permit Park
- No Stopping At All Times
- No Stopping or Parking with Exce
- No Restriction At All Times
- Limited Parking Restrictions
- Street Meter Parking
- Parking With Exception
- Alternate Side No Parking At All T
- Limited Alternate Side No Parking
- Residential Alternate On Street Pi



### Kiss And Ride Zones

### 5 Minute Walking Buffer

### Windsor Aerial 2019

- Red: Band\_1
- Green: Band\_2
- Blue: Band\_3

1: 3,733

## Notes

189.6 0 94.82 189.6 Meters

© Latitude Geographics Group Ltd.

This map is a user generated static output from an Internet mapping site and is for reference only. Data layers that appear on this map may or may not be accurate, current, or otherwise reliable.

THIS MAP IS NOT TO BE USED FOR NAVIGATION



# Sandwich Street Reconstruction

## Proposed Bike Lanes & Multi-use Trails for Sandwich Street



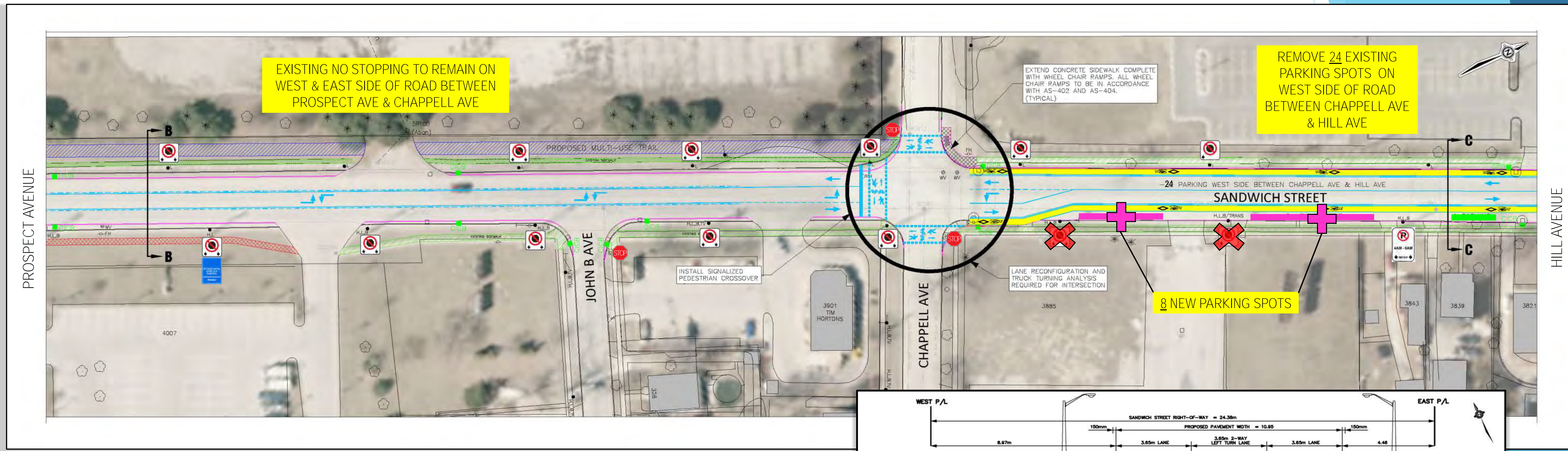
LEGEND

EXISTING PARKING TO REMAIN

EXISTING PARKING TO BE REMOVED

NO PARKING

NEW PARKING



### Chappell Avenue and Hill Avenue

39 – 24 = 15 EXISTING PARKING SPOTS TO REMAIN & 8 NEW PARKING SPOTS ADDED

LEGEND

L

 - LIGHT STANDARD

☼

 - TREE

✱

 - SHRUB

H

 - HYDRO POLE

H/L/TV

 - TV POLE WITH TELECOMMUNICATIONS

-

 - GAS LINE

MHO

 - MAINTENANCE HOLE (PRIVATE)

●

 - MUNICIPAL MAINTENANCE HOLE

-

 - MUNICIPAL SEWERS (SANITARY, STORM AND COMBND)

-

 - EXISTING CURBLINE

-

 - PROPOSED CURBLINE

★

 - STORM OUTLET

wv

 - WATER VALVE

⊙

 - FIRE HYDRANT

⊠

 - EXISTING CATCHBASIN TO BE REMOVED

□

 - EXISTING CATCHBASIN

CICB

 - PROPOSED CURB INLET CATCHBASIN

-

 - PROPOSED PAVEMENT MARKINGS

- PROPOSED BIKE LANE (1.5m)

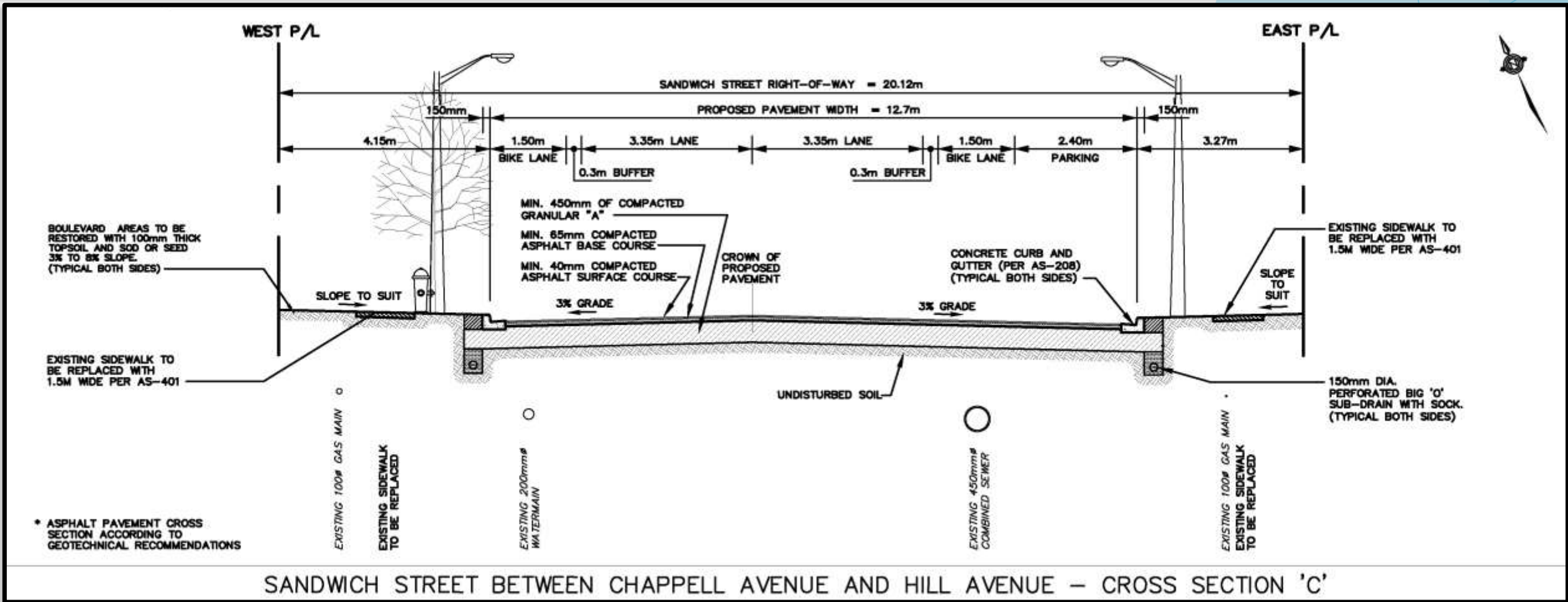
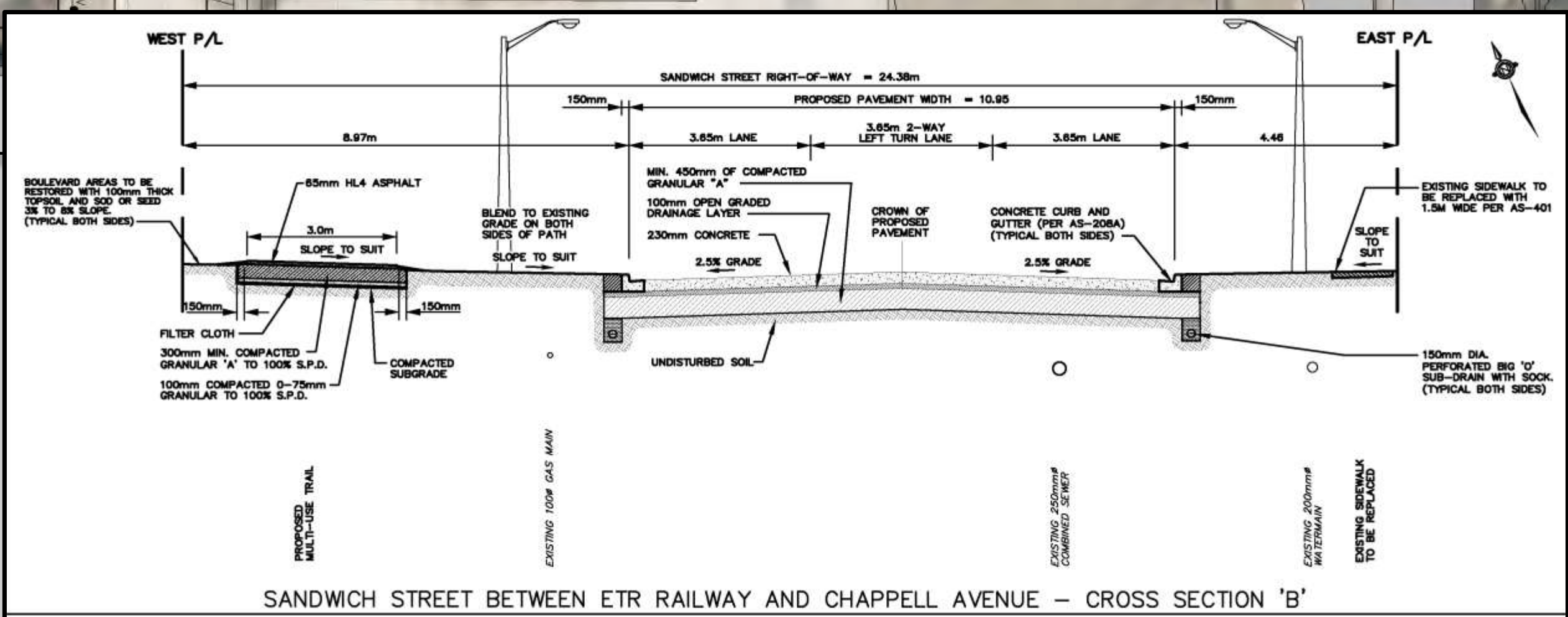
- PROPOSED BIKE LANE WITH BUFFER (2.0m)

- PROPOSED SIDEWALK (1.5m)

- PROPOSED MULTI-USE TRAIL (3.0m)

- REMOVE & REPLACE EXISTING SIDEWALKS

- REMOVE EXISTING SIDEWALKS





## Appendix B

---

### BY-LAW AND ITE PARKING GENERATION

(AMENDED by B/L 130-2017, Sept. 28, 2017)

- .1 Excluding lands in the *Central Business District*, for lands in any *Business Improvement Area* and for all lands within a *Commercial District* located:
- .1 on the north and south side of Wyandotte Street West between Dougall Avenue and Patricia Road;
  - .2 on the north and south side of University Avenue between Dougall Avenue and Randolph Avenue;
  - .3 on the north and south side of Tecumseh Road East between Forest Avenue and Chilver Road, Cadillac Street and Larkin Road, and Westcott Road and Rossini Boulevard;
  - .4 on the east and west side of Ouellette Avenue between Giles Boulevard and Tecumseh Road;

for an existing *building*, the required minimum number of *parking spaces* shall be as shown opposite the respective use in Table 24.20.3.1:

TABLE 24.20.3.1 – REQUIRED PARKING SPACES BUSINESS IMPROVEMENT AREAS & OTHER DEFINED AREAS	
USE	PARKING RATE – MINIMUM
Bake Shop	0
Business Office	0
Convenience Store	0
Food Convenience Store	0
Food Outlet – Take-out	0
Medical Office	1 for each 27 m² GFA
Pawnshop	0
Personal Service Shop	0
Pharmacy	0
Professional Studio	0
Repair Shop – Light	0
Restaurant	None for the first 90 m² GFA AND 1 for each additional 15 m² GFA
Retail Store	0
Veterinary Office	1 for each 27 m² GFA
All other uses not listed above	Section 24.20.5 shall apply

**24.20.5 REQUIRED PARKING SPACES –ALL OTHER AREAS AND USES NOT LISTED IN TABLES 24.20.1.1 AND 24.20.3.1**

- .1 The required minimum number of parking spaces shall be as shown opposite the respective use as shown in Table 24.20.5.1:

TABLE 24.20.5.1 - REQUIRED PARKING SPACES
---

USE	PARKING RATE - MINIMUM
Adult Entertainment Parlour	1 for each 7.5 m² GFA
Art Gallery	1 for each 45 m² GFA
Automatic Car Wash	0
Automobile Repair Garage	1 for each 45 m² GFA
Automobile Sales Lot	1 for each 45 m² GFA
Bake Shop	1 for each 22.5 m² GFA
Bakery	1 for each 45m² GFA for the first 2,700 m² GFA and 1 for each additional 180 m²
Billiard Hall	1 for each 22.5 m² GFA
Bingo Hall	1 for each 22.5 m² GFA
Bowling Alley	4 per alley
Building Material Recycling Centre	1 for each 45 m² GFA
Business Office	1 for each 45 m² GFA
Church (including a Church Hall)	1 for each 5.5 m² GFA uses as a church, chapel or sanctuary AND 1 for each 36 m² GFA not used as a church, chapel or sanctuary
Club	1 for each 22.5 m² GFA
Coin Operated Car Wash	0
College Student Residence	1 for each 4 beds
Collision Shop	1 for each 45 m² GFA
Combined Use Building – Dwelling Units	1.25 for each dwelling unit
Commercial School	2.5 for each classroom or teaching area AND 1 for each 22.5 m² of GFA of cafeteria, auditorium, gymnasium and other area of assembly
Confectionary	1 for each 45 m² GFA for the first 2,700 m² GFA AND 1 for each additional 180 m²
Confectioner’s Shop	1 for each 22.5 m² GFA
Contractor’s Office	1 for each 45 m² GFA used as a business office AND 1 for each 200 m² GFA used as a warehouse
Convenience Store	1 for each 22.5 m² GFA
Convent or Monastery	1 for each 4 beds
Correctional Facility	1 for each 2 beds
Day Nursery	1.5 for each classroom or teaching area
Double-duplex Dwelling	4
Drive-through Food Outlet	1 for each 22.5 m² GFA
Drive-through Restaurant	1 for each 7.5 m² GFA
Duplex Dwelling	2
TABLE 24.20.5.1 - REQUIRED PARKING SPACES	
USE	PARKING RATE - MINIMUM



Elementary School	1.5 for each classroom or teaching area
Entertainment Lounge	1 for each 7.5 m² GFA
Exhibition Hall	1 for each 36 m² GFA
Financial Office	1 for each 45 m² GFA
Food Convenience Store	1 for each 22.5 m² GFA
Fraternity or Sorority House	1 for each 4 beds
Funeral Home	1 for each 5.5 m² GFA used for a chapel, sanctuary or reposing room
Games Arcade	1 for each 22.5 m² GFA
Garden Centre	1 for each 22.5 m² GFA
Gas Bar	1 for each 45 m² GFA
General Salvage Operation	1 for each 45 m² GFA for the first 2,700 m² GFA AND 1 for each additional 180 m²
Group Home	1
Health Studio	1 for each 36 m² GFA
Heavy Repair Shop	1 for each 45 m² GFA for the first 2,700 m² GFA AND 1 for each additional 180 m²
Hospital	1 for each bed
Hotel	1 for each guest room AND 1 for each 22.5 m² GFA used for a restaurant, convention hall, meeting room and other places of assembly.
Library	1 for each 45 m² GFA
Light Repair Shop	1 for each 45 m² GFA
Lodging House	1 for each 6 beds
Major Commercial Centre (exclusive of a hotel or motel)	1 for each 22.5 m² GFA
Marina	0.5 for each 1 boat docking space AND 1 for each 1 boar anchorage space
Material Transfer Centre	1 for each 45 m² GFA for the first 2,700 m² GFA AND 1 for each additional 180 m²
Medical Office	1 for each 13.5 m² GFA
Micro-brewery	1 for each 45 m² GFA
Minor Commercial Centre	1 for each 22.5 m² GFA and when the combined GFA of all restaurants and entertainment lounges Exceeds 30% of the GFA of the Centre, 1 for each 7.5 m² GFA of all restaurants and entertainment lounges in excess thereof
Mobile Home	1
TABLE 24.20.5.1 - REQUIRED PARKING SPACES	
USE	PARKING RATE - MINIMUM

Motel	1 for each guest room AND 1 for each 22.5 <sup>2</sup> GFA used for a restaurant, convention hall, meeting room and other places of assembly
Motor Vehicle Dealership	1 for each 45 m <sup>2</sup> GFA
Motor Vehicle Salvage Operation	1 for each 45 m <sup>2</sup> GFA for the first 2,700 m <sup>2</sup> GFA AND 1 for each additional 180 m <sup>2</sup>
Multiple Dwelling containing a maximum of 4 Dwelling units	1 for each dwelling unit
Multiple Dwelling containing a minimum of 5 Dwelling units	1.25 for each dwelling unit
Museum	1 for each 45 m <sup>2</sup> GFA
Outdoor Market	0
Pawnshop	1 for each 22.5 m <sup>2</sup> GFA
Personal Service Shop	1 for each 22.5 m <sup>2</sup> GFA
Pharmacy	1 for each 22.5 m <sup>2</sup> GFA
Place of Entertainment and Recreation	1 for each 36 m <sup>2</sup> GFA
Power Generation Plant	1 for each 200 m <sup>2</sup> GFA
Professional Studio	1 for each 45 m <sup>2</sup> GFA
Public Hall	1 for each 7.5 m <sup>2</sup> GFA
Residential Care Facility	1 for each 4 beds
Restaurant	1 for each 7.5 m <sup>2</sup> GFA
Retail Store	1 for each 22.5 m <sup>2</sup> GFA
Secondary School	1.5 for each classroom or teaching area AND 1 For each 22.5 m <sup>2</sup> of GFA of cafeteria , auditorium, gymnasium and other area of assembly
Self-storage Facility	2
Semi-Detached Dwelling	1 for each dwelling unit
Service Station	1 for each 45 m <sup>2</sup> GFA
Shelter	1 for each 6 beds
Single –unit Dwelling	1
Stacked Dwelling Unit	1 for each dwelling unit
Take-Out Food Outlet	1 for each 22.5 m <sup>2</sup> GFA
Temporary Outdoor Vendor’s Site	0
Theatre	1 for each 6 seats
Tourist Home	1 for each guest room AND 1 for each 22.5 m <sup>2</sup> GFA used for a restaurant, convention hall, meeting room and other places of assembly
TABLE 24.20.5.1 - REQUIRED PARKING SPACES	
USE	PARKING RATE - MINIMUM

Townhome Dwelling having an attached garage or carport	1 for each dwelling unit
Townhome Dwelling without an attached garage or carport	1.25 for each dwelling unit
Transport Terminal	5 parking spaces, or 1 for each 45.0 m <sup>2</sup> GFA, whichever is greater
University Student Residence	1 for each 4 beds
Veterinary Clinic	1 for each 13.5 m <sup>2</sup> GFA
Veterinary Office	1 for each 13.5 m <sup>2</sup> GFA
Warehouse	1 for each 200 m <sup>2</sup> GFA
Wholesale Store	1 for each 45 m <sup>2</sup> GFA
Workshop	1 for each 45 m <sup>2</sup> GFA for the first 2,700 m <sup>2</sup> GFA AND 1 for each additional 180 m <sup>2</sup>
All other commercial uses not specifically listed	1 for each 36 m <sup>2</sup> GFA
All other industrial uses not specifically listed	1 for each 45 m <sup>2</sup> GFA for the first 2,700 m <sup>2</sup> GFA AND 1 for each additional 180 m <sup>2</sup> GFA

(AMENDED by B/L 144-2015, Nov. 6, 2015; B/L 169-2018, Dec. 19/2018)

**24.20.7      CALCULATION OF REQUIRED PARKING SPACES**

- .1      The required number of parking spaces for each use listed in Tables 24.20.1.1, 24.20.3.1 and 24.20.5.1 is calculated as follows:
  - .1      The gross floor area of that part of a building designed and used for a parking area, parking space, visitor parking space, accessible parking space, bicycle parking space, loading space, automatic car wash or coin-operated car wash is not included in the calculation of required number of vehicle parking spaces.
  - .2      If a parking rate is expressed as a ratio of parking spaces to the gross floor area, the parking space requirement for a use is to be calculated by dividing the applicable gross floor area of the use by the applicable parking rate.
  - .3      If the calculation of the number of required parking spaces results in a number containing a fraction, the number shall be rounded DOWN to the nearest whole number, but in no case may there be less than one parking space, except when the parking rate is zero.
  - .4      If a building is occupied or proposed to be occupied by more than one main use, the required parking for each main use is calculated on the basis of the percentage of gross floor area devoted to that use plus the equivalent percentage of any common areas and shared accessory uses in the building.
  - .5      If a Combined use Building is occupied in part by a Minor Commercial Centre or a Major Commercial Centre, the total required number of parking spaces is the sum of the required number of parking spaces for each Dwelling Unit and for the Minor Commercial Centre of a Major

Commercial Centre.

24.20.10 SIZE OF PARKING SPACE

- .1 Each parking space shall have a minimum length of 5.5 metres and a minimum width of 2.5 metres, except where one side of the parking space is flanked by a wall or fence, each parking space shall have a minimum length of 5.5 metres and a minimum width of 3.5 metres.

24.22 VISITOR PARKING SPACE PROVISIONS

24.22.1 REQUIRED VISITOR PARKING SPACES

- .1 For a Townhome Dwelling without an attached garage or carport, Multiple Dwelling with a minimum of five dwelling units, or Dwelling Units in a Combined Use Building, a minimum of 15 percent of parking spaces shall be marked as visitor parking.
- .5 If the calculation of the number of visitor parking spaces results in a number containing a fraction, the number shall be rounded DOWN to the nearest whole number, but in no case shall there be less than one visitor parking space and one required parking space.

24.22.10 SIZE OF VISITIOR PARKING SPACE

- .1 Each visitor parking space shall have a minimum length of 5.5 metres and a minimum width of 2.5 metres, except where one side of the parking space is flanked by a wall or fence, each visitor parking space shall have a minimum length of 5.5 metres and a minimum width of 3.5 metres.

24.24 ACCESSIBLE PARKING SPACE PROVISIONS [ZNG/4046]  
(AMENDED by B/L 48-2014, April 15, 2014)

24.24.1 REQUIRED ACCESSIBLE PARKING SPACES

- .1 There shall be provided accessible parking spaces as shown in Table 24.24.1:

TABLE 24.24.1 – REQUIRED ACCESSIBLE PARKING SPACES		
TOTAL NUMBER OF PARKING SPACES IN PARKING AREA	REQUIRED NUMBER OF ACCESSIBLE PARKING SPACES – MINIMUM	
	TYPE A	TYPE B
1 to 25	1 space	0
26 to 100	2 percent of parking spaces	2 percent of parking spaces
101 to 200	1.5 percent of parking spaces	0.5 space plus 1.5 percent of parking spaces
201 to 1,000	0.5 space plus 1 percent of parking spaces	1 space plus 1 percent of parking spaces
1,001 or more	5 spaces plus 0.5 percent of parking spaces	5.5 spaces plus 0.5 percent of parking spaces

- .2 If the calculation of the number of required Type A and Type B accessible parking spaces results in a number containing a fraction, the number shall be rounded up to the nearest whole number:

# Multifamily Housing (High Rise) (222)

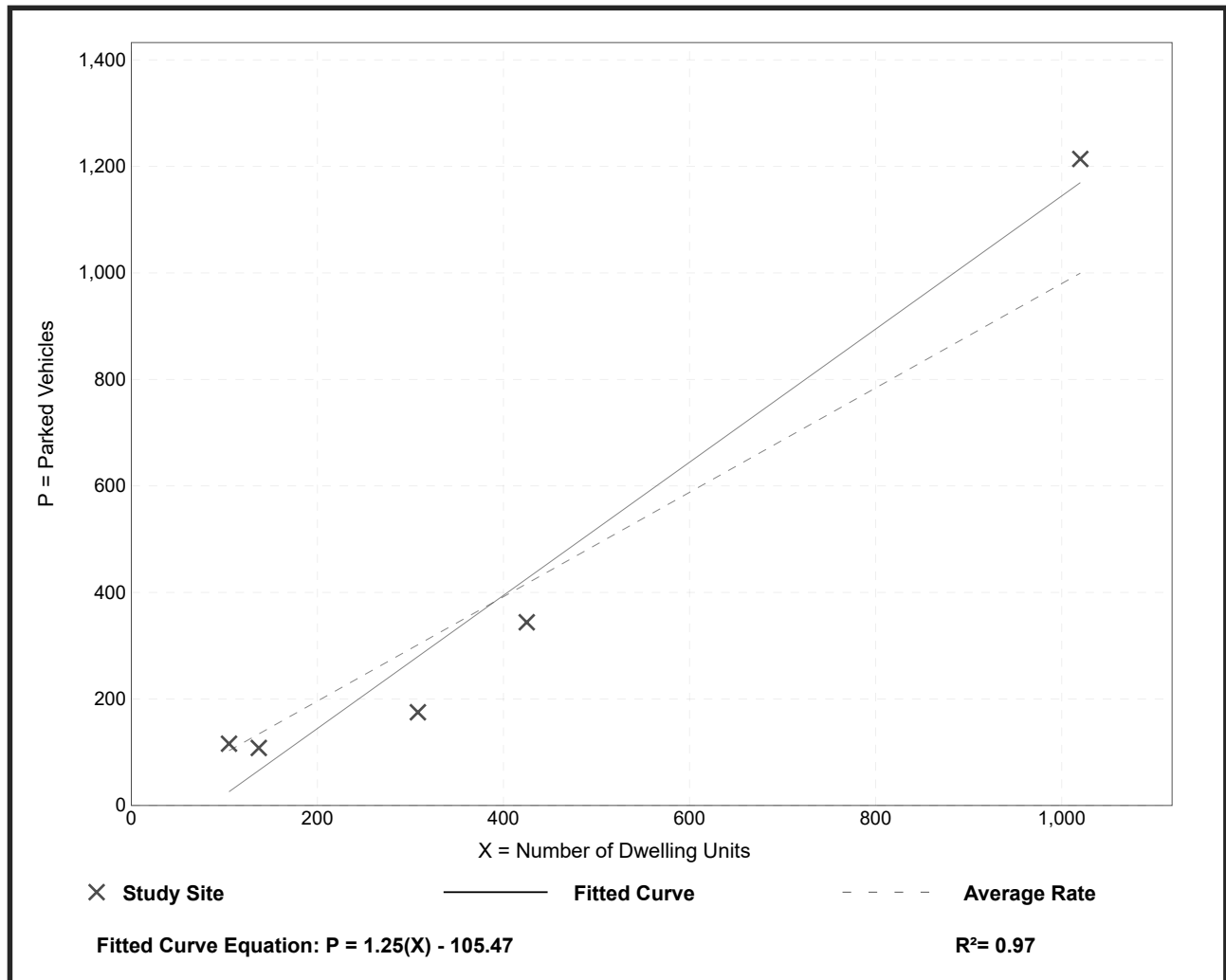
Peak Period Parking Demand vs: Dwelling Units  
 On a: Weekday (Monday - Friday)  
 Setting/Location: General Urban/Suburban (no nearby rail transit)  
 Peak Period of Parking Demand: 10:00 p.m. - 5:00 a.m.  
 Number of Studies: 5  
 Avg. Num. of Dwelling Units: 399

## Peak Period Parking Demand per Dwelling Unit

Average Rate	Range of Rates	33rd / 85th Percentile	95% Confidence Interval	Standard Deviation (Coeff. of Variation)
0.98	0.57 - 1.19	0.78 / 1.19	***	0.27 (28%)

## Data Plot and Equation

*Caution – Small Sample Size*



Parking Generation Manual, 5th Edition • Institute of Transportation Engineers



# Variety Store - Non-December

## (814)

Peak Period Parking Demand vs: 1000 Sq. Ft. GFA

On a: Weekday (Monday - Friday)

Setting/Location: General Urban/Suburban

Peak Period of Parking Demand: 5:00 - 7:00 p.m.

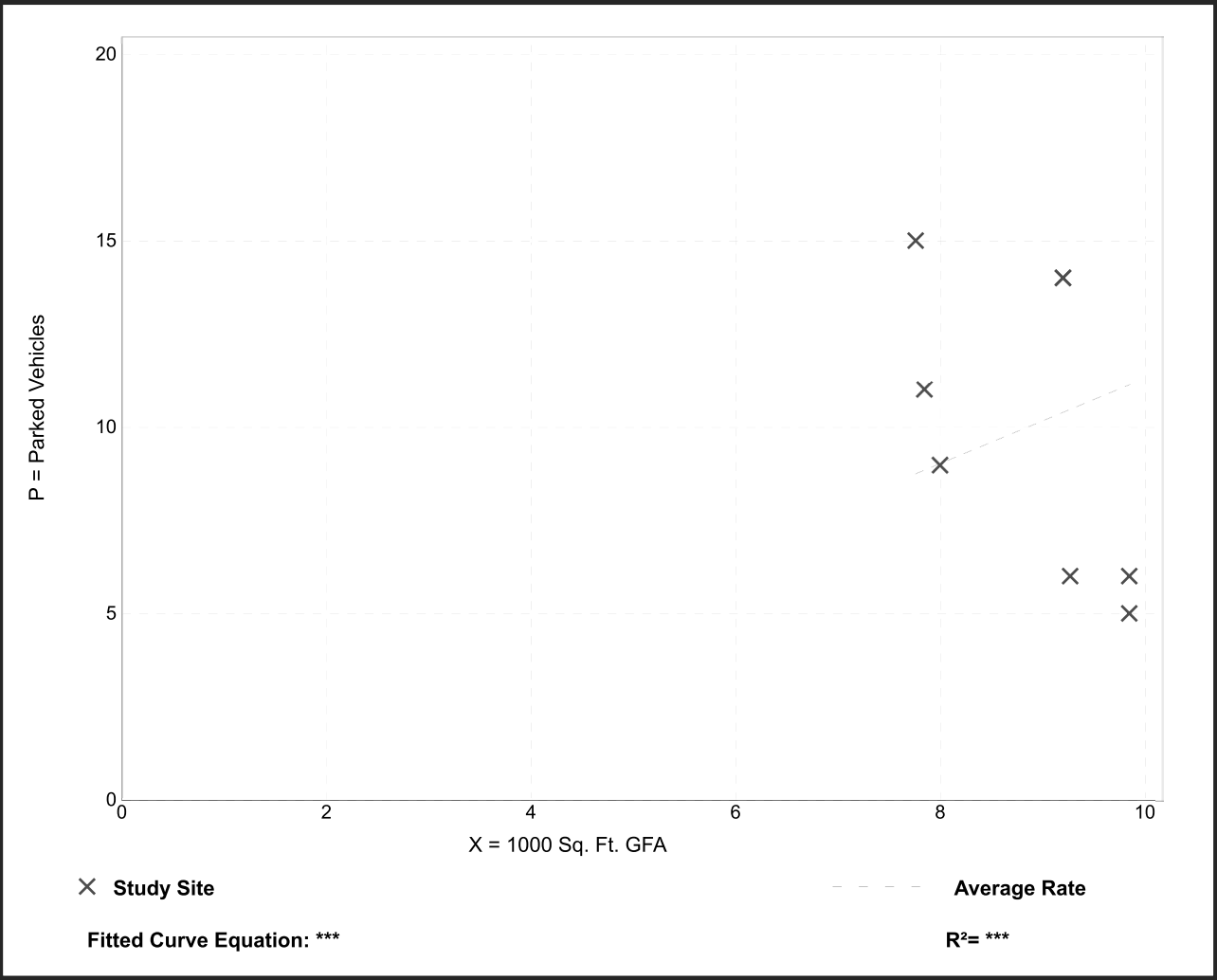
Number of Studies: 8

Avg. 1000 Sq. Ft. GFA: 8.8

### Peak Period Parking Demand per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	33rd / 85th Percentile	95% Confidence Interval	Standard Deviation (Coeff. of Variation)
1.13	0.51 - 1.93	0.65 / 1.79	***	0.52 (46%)

### Data Plot and Equation



Parking Generation Manual, 5th Edition • Institute of Transportation Engineers

# Convenience Market

## (851)

Peak Period Parking Demand vs: 1000 Sq. Ft. GFA

On a: Weekday (Monday - Friday)

Setting/Location: General Urban/Suburban

Peak Period of Parking Demand: Not Available

Number of Studies: 2

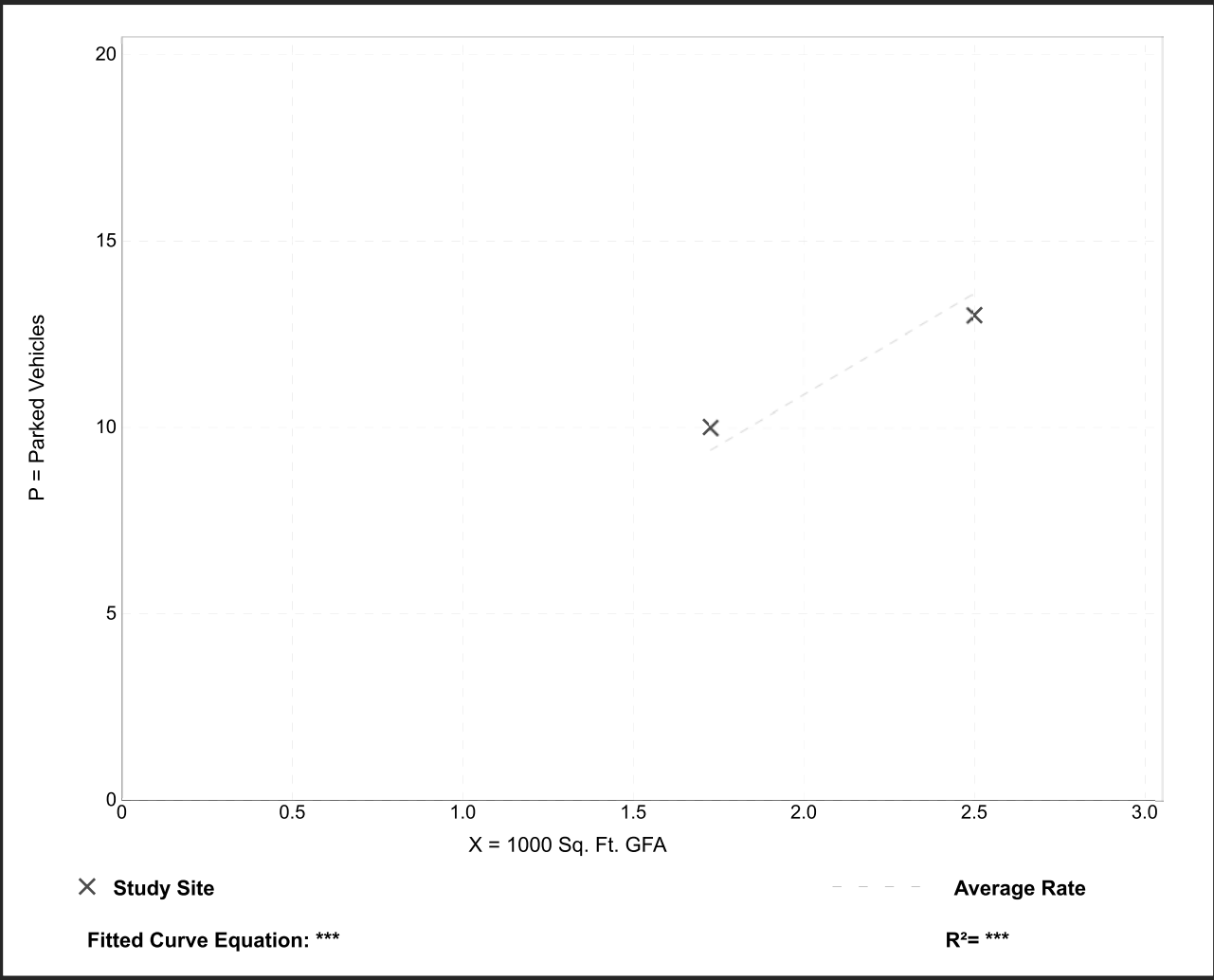
Avg. 1000 Sq. Ft. GFA: 2.1

### Peak Period Parking Demand per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	33rd / 85th Percentile	95% Confidence Interval	Standard Deviation (Coeff. of Variation)
5.44	5.20 - 5.79	*** / ***	***	***

Data Plot and Equation

Caution – Small Sample Size



Parking Generation Manual, 5th Edition • Institute of Transportation Engineers

## Appendix C

---

### PHOTOS



Looking East from Sandwich to Chappell Ave



Looking West from Sandwich to Chappell Ave



Looking North from Chappell Ave to Sandwich





## **TRAFFIC IMPACT STUDY MIXED USED DEVELOPMENT**

**3885 SANDWICH STREET  
WINDSOR, ONTARIO**

PROJECT NO. 20-028

DATED: MAY 26, 2022



27 Princess St., Unit 102  
Leamington, ON N8H 2X8  
519.326.6161 TF 1.844.842.9188  
[bairdAE.ca](http://bairdAE.ca)

# TABLE OF CONTENT

<b>1.0</b>	<b>INTRODUCTION.....</b>	<b>1</b>
1.1	Background.....	1
1.2	Proposed Development.....	2
1.3	Scope.....	2
1.4	Analysis Methodology.....	2
<b>2.0</b>	<b>EXISTING CONDITION .....</b>	<b>3</b>
2.1	Road Network Characteristics.....	3
2.2	Key Existing Intersection.....	3
2.3	Existing Traffic Volumes.....	3
<b>3.0</b>	<b>FUTURE CONDITION .....</b>	<b>4</b>
3.1	Growth Rate .....	4
3.2	Future Background Development .....	4
<b>4.0</b>	<b>DEVELOPMENT TRAFFIC .....</b>	<b>4</b>
4.1	Description of Project .....	4
4.2	Development Access.....	4
4.3	Trip Generation .....	5
4.4	Trip Distribution and Assignment.....	5
4.5	Future Condition.....	6
<b>5.0</b>	<b>INTERSECTION OPERATIONS .....</b>	<b>6</b>
<b>5.1</b>	<b>SIGNAL WARRANT .....</b>	<b>9</b>
<b>6.0</b>	<b>CONCLUSION AND RECOMMENDATION.....</b>	<b>10</b>
<b>7.0</b>	<b>CLOSURE.....</b>	<b>11</b>

**FIGURES**

Exhibit 1 - Location Plan .....	1
---------------------------------	---

**TABLES**

Table 1: Trip Generation .....	5
Table 2: Trip Distribution .....	6
Table 3: Background Conditions – Level of Service .....	6
Table 4: 2022 Conditions – Level of Service .....	7
Table 5: 2027 Conditions – Level of Service .....	7
Table 6: 2032 Conditions – Level of Service .....	8

**APPENDICES**

Appendix A Background Traffic Data and Other Related Information
Appendix B Future Background Traffic, Development Traffic and Total Traffic Volumes
Appendix C Capacity Analysis
Appendix D Site Plans, Warrants and Photos

## 1.0 INTRODUCTION

### 1.1 Background

BairdAE has been retained to prepare a Traffic Impact Assessment in support of proposed 11-storey apartment building on 3885 Sandwich Street, Windsor. The building will house new 150 residential units with 70 single bedroom units and 80 double bedroom units, and retail store on ground floor with total enclosed space of 6697ft<sup>2</sup>. The site is bounded by Chappell Avenue to the south, Sandwich Street to the west, and residential development to the north and east.

Two new accesses will be provided to the site as shown in Exhibit 1. The first access is located about 95m north of intersection of Sandwich Street W and Chappell Avenue and second access is located 64m east of intersection. The development is expected to be completed in 2022.

The traffic flow from development is predicted to produce 1295 daily vehicles, 122 morning vehicles and 111 evening peak vehicles.



**Exhibit 1 - Location Plan**

## ***1.2 Proposed Development***

As illustrated in site plan (Appendix D), the development site is approximately 0.67ha which will consist of new building with 150 units, retail section of 6697 ft<sup>2</sup> in size, parking spaces and landscape area. The development will have 188 parking spaces for residence and visitors. Two full accesses will be provided to access the site. the first access is located about 95m north of intersection of Sandwich Street W and Chappell Avenue and second access is located 64m east of intersection.

## ***1.3 Scope***

It is anticipated that the proposed development construction will begin in 2027 and as a result following future horizon periods (conditions) are established as part of this study:

- Existing Condition 2020; and
- 2022 Future Condition
- 2027 Future Condition
- 2032 Future Condition – 10-year horizon

The study has considered impacts of site generated traffic at the followings intersections:

- Sandwich Street and Chappell Avenue
- Sandwich Street and Access Road 1
- Chappell Avenue and Access Road 2

## ***1.4 Analysis Methodology***

A transportation analysis was completed to determine the existing and future operation conditions of intersection and individual turning movements. The operational analyses were primarily based on procedures set out in the Highway Capacity Manual (2010) with the assistance of Synchro 10. Several performance measures are used in the analysis of signalized and unsignalized intersections including the following:

- Level of Service (LOS) – a measure of the average vehicle delay experienced by the motorists attempting to travel through the intersection. LOS is measured from “A” to “F” with peak hour LOS in the “A” to “D” range being considered acceptable by most and a LOS of F representing unacceptable delays;
- Delay – the additional travel time experienced by a driver compared to free-flow conditions; and
- Queue Lengths – the Synchro Software measures both the 50th percentile and 95th percentile maximum queue lengths. The 50th percentile queue (the median) is the



maximum back of queue length during a typical traffic cycle. The 95th percentile queue is the maximum back of queue length during a typical traffic cycle with 95th percentile traffic volumes. The 95th percentile queue measures the queue length that 95 percent of the sample lies below. The 95th percentile critical queue lengths were identified for movements where the queue surpassed the estimated length of the storage bay.

Taken together, these measures provide an indication of delay and the number of vehicles that can be accommodated through an intersection.

## **2.0 EXISTING CONDITION**

### ***2.1 Road Network Characteristics***

The existing road network, lane configuration and existing traffic control for the study area are shown in Exhibit 2 and the details are described below:

**Sandwich Street** is the two-lane urban north-south arterial roadway with posted speed limit of 50 km/h at the close proximity to the development. The road turn into Ojibway parkway 500m west of the development with speed limit of 70km/h.

**Crawford Street** is a east-west two lane local roadway extending from Peter Street to Russell Street. It has a posted 50 km/h speed limit, with on-street parking permitted on both sides. It is stop controlled on its approach to the intersection with Sandwich Street.

### ***2.2 Key Existing Intersection***

The intersection of Sandwich Street with Chappell Avenue is 4-leg unsignalized intersection. There is one lane in each direction at the intersection. No exclusive turning lanes are provided at the intersection. Intersection is controlled by STOP sign on Chappell Avenue.

### ***2.3 Existing Traffic Volumes***

Recent traffic count and signal timing data was obtained from the City of Windsor for the intersection of Sandwich Street and Chappell Avenue. The existing traffic volumes and other relevant data are included in Appendix A.

### **3.0 FUTURE CONDITION**

#### ***3.1 Growth Rate***

The growth rate information was obtained from Windsor Area Long Range Transportation Study (WALTS) traffic growth chart. Based on chart, 20-year traffic growth (2.17 traffic volume 1997 and 2.22 traffic volume 2017) is approximately 1.1%, hence a conservative growth rate of 3% per year was assumed to reflect growth in background traffic volumes. The projected traffic volumes are provided in Appendix B.

#### ***3.2 Future Background Development***

The site is located at urban area and busiest intersection, it is almost impossible to ignore future potential development. The existing surrounding users include industries, restaurant and local residences. Based on site visit, there are no new development taking place within site vicinity hence therefore we assumed no major residential/commercial/industrial development is taking place.

### **4.0 DEVELOPMENT TRAFFIC**

This section will describe the development accesses, trip generation, trip distribution and ultimate peak hour traffic.

#### ***4.1 Description of Project***

As shown in site plan (see appendix D), the proposed development consists of 150 units with 70 single bedroom units and 80 double bedroom units with total enclosed space of 1480.6m<sup>2</sup>. Sight triangle at the southwest corner of the development is provided to improve sight lines for drivers at the intersection. It is assumed that the development will be constructed by 2022 (addition of 1295 Daily; 105 inbound traffic and 128 outbound traffic).

#### ***4.2 Development Access***

The proposed site accesses are provided from Sandwich Street and Chappell Avenue. The future access roads will be T-leg intersections. All approaches at the intersection will have one left-though-right share lane as shown in Exhibit 2 in Appendix B.

### 4.3 Trip Generation

The number of vehicle trips anticipated to be generated by the proposed development was calculated based on trip generation rates published by The Institution of Transportation Engineers (ITE) Trip Generation 9th Edition. Higher ITE Code 220 (Apartment) and ITE Code 826 (Retail) were used to estimate generated trips.

Description of Land use, ITE codes, unit sizes, trip generation rate and trip generation for daily and peak hours are provided in Table 1. Appendix B provides detailed calculations and all relevant charts.

Table 1: Trip Generation

Use	ITE	Units	AADT	Trip Generated			
				AM Hour		PM Hour	
				In	Out	In	Out
Apartment	220	150	998	15	61	60	33
Retail Center	826	6.69 kft <sup>2</sup>	297	22	24	8	10
Total			1295	37	85	68	43

There will be street parking on the Sandwich Street, hence, retail store traffic will not have significant impact on development's accesses. However, for modelling purpose, the retail traffic is added for worst case scenario.

### 4.4 Trip Distribution and Assignment

Given that site's highly urban location (proximity to a mix of residential, industrial, commercial and employments uses), the trips distribution is based on shortest route to reach arterial route E.C. Row Expressway. It is assumed that the 61 percent of volume generated from development will travel to/from south of Sandwich Street and 39 percent from/to north of Sandwich Street. At the intersection of Sandwich Street with Chappell Street, the traffic trip will be distributed similar to existing traffic movement. The site development traffic distribution is shown Table 2 and also provided in Figure 1.2, Figure 2.2, Figure 3.2 and Figure 4.2 (see Appendix B).

It is also assumed that the 60 percent of development traffic is from/to Access Road 1 and 40 percent from/to Access Road 2.

Table 2: Trip Distribution

From/To Sandwich Street	Distribution
North	39%
South	61%
Total	100%

#### 4.5 Future Condition

Development traffic volumes were added to the forecasted (2022, 2027 and 2032) background traffic volumes to obtain corresponding total traffic volumes at intersections. The projected total future volumes are provided in Figure 1.3, Figure 2.3, Figure 3.3 and Figure 4.3 (see Appendix B).

### 5.0 INTERSECTION OPERATIONS

The existing (2020) and forecasted 2022, 2027 and 2032 traffic volumes for the study intersections are evaluated using the Synchro/Sim Traffic software version 10 which automates the procedures contained in the Highway Capacity Manual 2010.

The existing and future peak hours analysis results are included in tables 3-6 and corresponding worksheets are included in Appendix C.

Table 3: Background Conditions – Level of Service

Sandwich St and Chappell Ave Intersection	A.M. Peak Hour			P.M. Peak Hour		
	LOS	v/c	Delay (sec)	LOS	v/c	Delay (sec)
2018						
EB LTR	B	0.1	12.6	B	0.14	13.9
WB LTR	C	0.03	17.0	C	0.03	17.5
NB LTR	A	0.04	1.1	A	0.04	1.3
SB LTR	A	0.0	0.0	A	0.01	0.2
Overall LOS	A			A		
2022						
EB LTR	B	0.1	12.6	B	0.16	14.4
WB LTR	C	0.03	17.3	C	0.03	18.6
NB LTR	A	0.0	1.2	A	0.05	1.4
SB LTR	A	0.0	0.0	A	0.01	0.2

Overall LOS	A			A		
2027						
EB LTR	B	0.13	13.5	C	0.21	16.5
WB LTR	C	0.04	20.1	C	0.05	22.6
NB LTR	A	0.05	1.4	A	0.06	1.6
SB LTR	A	0.0	0.0	A	0.01	0.2
Overall LOS	A			B		
2032						
EB LTR	B	0.16	14.9	C	0.27	19.0
WB LTR	C	0.06	23.8	D	0.06	28.2
NB LTR	A	0.06	1.5	A	0.07	1.9
SB LTR	A	0.0	0.0	A	0.01	0.2
Overall LOS	B			B		

**Note:** NB – Northbound SB – Southbound EB – Eastbound WB – Westbound; LTR – Left/Through/Right turn

Table 4: 2022 Conditions – Level of Service

Intersection	A.M. Peak Hour			P.M. Peak Hour		
	LOS	v/c	Delay (sec)	LOS	v/c	Delay (sec)
Sandwich Street and Chappell Avenue (Unsignalized)						
EB LTR	B	0.12	14.1	B	0.16	14.9
WB LTR	C	0.14	18.9	C	0.11	22.8
NB LTR	A	0.04	1.2	A	0.05	1.4
SB LTR	A	0.01	0.4	A	0.02	0.5
Overall LOS	A			A		
Sandwich Street and Access 1 (Unsignalized)						
WB LTR	B	0.12	14.2	C	0.08	16.1
NB LTR	A	0.26	0.0	A	0.25	0.0
SB LTR	A	0.01	0.3	A	0.01	0.4
Overall LOS	A			A		
Chappell Avenue and Access 2 (Unsignalized)						
EB LTR	A	0.01	3.7	A	0.02	3.9
WB LTR	A	0.01	0.0	A	0.01	0.0
SB LTR	A	0.04	8.7	A	0.02	8.7
Overall LOS	A			A		

Table 5: 2027 Conditions – Level of Service

Intersection	A.M. Peak Hour			P.M. Peak Hour		
	LOS	v/c	Delay (sec)	LOS	v/c	Delay (sec)
<b>Sandwich Street and Chappell Avenue (Unsignalized)</b>						
EB LTR	C	0.15	15.3	C	0.22	17.3
WB LTR	C	0.17	22.5	<b>D</b>	<b>0.15</b>	<b>28.9</b>
NB LTR	A	0.05	1.3	A	0.06	1.6



SB LTR	A	0.01	0.4	A	0.02	0.5
Overall LOS	A			B		
Sandwich Street and Access 1 (Unsignalized)						
WB LTR	B	0.14	15.5	C	0.09	18.2
NB LTR	A	0.29	0.0	A	0.29	0.0
SB LTR	A	0.0	0.3	A	0.01	0.4
Overall LOS	A			A		
Chappell Avenue and Access 2 (Unsignalized)						
EB LTR	A	0.01	3.4	A	0.02	3.7
WB LTR	A	0.01	0.0	A	0.01	0.0
SB LTR	A	0.04	8.7	A	0.02	8.7
Overall LOS	A			A		

Table 6: 2032 Conditions – Level of Service

Intersection	A.M. Peak Hour			P.M. Peak Hour		
	LOS	v/c	Delay (sec)	LOS	v/c	Delay (sec)
Sandwich Street and Chappell Avenue (Unsignalized)						
EB LTR	C	0.20	17.4	C	0.28	20.2
WB LTR	D	0.21	27.2	E	0.20	38.2
NB LTR	A	0.05	1.4	A	0.07	1.9
SB LTR	A	0.01	0.4	A	0.02	0.5
Overall LOS	B			B		
Sandwich Street and Access 1 (Unsignalized)						
WB LTR	C	0.16	17.0	C	0.11	20.7
NB LTR	A	0.32	0.0	A	0.32	0.0
SB LTR	A	0.0	0.3	A	0.01	0.4
Overall LOS	A			A		
Chappell Avenue and Access 2 (Unsignalized)						
EB LTR	A	0.01	3.3	A	0.02	3.5
WB LTR	A	0.01	0.0	A	0.01	0.0
SB LTR	A	0.04	8.8	A	0.02	8.7
Overall LOS	A			A		

Under 2022, 2027 and 2032 background condition, the Sandwich Street and Chappell Avenue intersection is projected to operate at an overall acceptable LOS during peak hours. Under 2032 existing condition delay is observed long delay for westbound turning vehicles during pm peak. However, sufficient capacity remains for this movement (v/c= 0.06) indicating sufficient gaps are available hence no mitigation measures are required.

Under 2022 post development conditions, the intersections analyzed are expected to operate at acceptable level of service during peak hours.

Under 2027 and 2032 post development conditions, the intersections analyzed are expected to operate at acceptable level of service during peak hours. However, the intersection of Sandwich Street with Chappell Avenue evening condition westbound turning vehicles observes higher delay. The level of service for westbound deteriorate from LOS D in 2027 to LOS E in 2032. However, sufficient capacity to remain ( $v/c = 38.4$ ) indicating enough queuing storage is available. Also, the westbound right turning vehicles are less than 60 vph. Hence, no turning lanes are required. Signal warrant analysis was conducted for this intersection.

Average queuing at the sites accesses to be minimal for all future conditions, hence, this reinforce the condition that dedicated turning lanes are not required.

## **5.1 Signal Warrant**

Transportation Association of Canada (TAC) traffic signal procedure requires 100 warrant points to trigger signal at the intersection. Using 2032 projected evening traffic volumes, the 86 warrants points indicate that traffic signal is not warranted. Details calculations are provided in Appendix D.

## 6.0 CONCLUSION AND RECOMMENDATION

Utilizing the morning and evening traffic data, the operating conditions were evaluated for 2020, 2022, 2027 and 2032 traffic conditions. The findings from these evaluations are summarized below.

- The proposed 11-storey mix-use high rise apartment building will have 150 units and 6697 ft<sup>2</sup> retail space on ground floor which will generated approximately 1295 daily; 105 inbound traffic and 128 outbound traffic.
- Based on conversation with client, the development construction will begin in 2022.
- The background growth rate is considered in the analysis as it represents worst case scenario i.e. 3%.
- Under existing and future background conditions, the study area intersections operate at acceptable level of service during morning and evening peak. However, under 2032 existing condition, the westbound turning lanes level of service is D. This delay is due to stop control and higher volumes on the Sandwich Street. However, there is sufficient capacity available for this movement ( $v/c = 0.06$ ) indicating sufficient gaps are available hence no mitigation measures are required
- Under the 2022 future post-development condition, the intersections are expected to operate at acceptable level of service during peak hours.
- Under the 2027 and 2032 future post-development condition, the intersection of Sandwich Street and Chappell Avenue are expected to operate at acceptable level of service during peak hours. However, the westbound turning movement at the intersection is forecast to have longer delays i.e. LOS D in 2027 and LOS E in 2032. However, sufficient gaps are available to accommodate this movement. Hence no improvements are required.
- All other intersection operates at acceptable level of service in 2022, 2027 and 2032 post-development conditions.
- The warrant for signalization is not required at the intersection of Sandwich Street and Chappell Avenue for 2032 post-development condition. It is expected that the City will continue monitor traffic at this location.

- An adequate sight line distance is provided for safe departure from the development.

## 7.0 CLOSURE

The information contained in this report is prepared for mixed use High-Rise Development in City of Windsor for future discussion regarding potential traffic impact on Sandwich Street, Chappell Avenue and access roads.

We trust that the above meets with your purpose. Should you have any questions, please do not hesitate to contact the undersigned. All of which is respectfully submitted.

All of which is respectfully submitted.

**BAIRD AE INC.**  
**27 PRINCESS STREET, UNIT 102**  
**LEAMINGTON, ONTARIO N8H 2X8**

Shurjeel Tunio, P.Eng.  
Senior Project Manager  
**Baird AE**

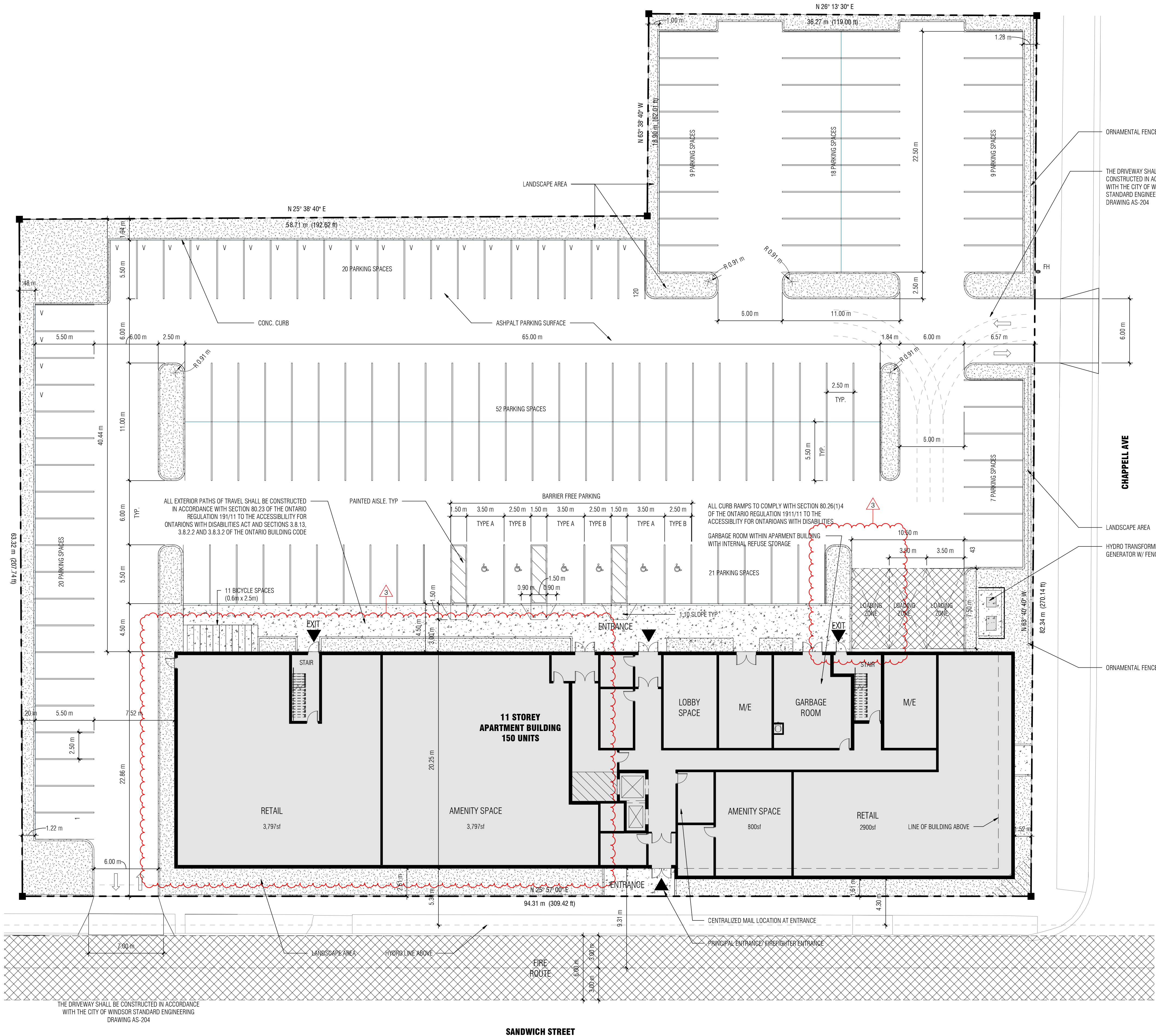


## Appendix A

---

### BACKGROUND TRAFFIC DATA AND OTHER RELATED INFORMATION





SITE DATA MATRIX			OBC REFERENCE
PROJECT DESCRIPTION: 11 STOREY MULTI UNIT RESIDENTIAL BUILDING 150 RESIDENTIAL UNITS			<input checked="" type="checkbox"/> PART 3 <input type="checkbox"/> PART 9 <input type="checkbox"/> PART 11
ZONING DESIGNATION:	CD2.1		
MAJOR OCCUPANCY:	RESIDENTIAL		
BUILDING CLASSIFICATION:	GROUP C		
SITE AREA	BUILDING AREA	GROSS AREA	
EXISTING: 6,694m <sup>2</sup>	EXISTING: N/A	EXISTING:	N/A
PROPOSED: 6,694m <sup>2</sup>	PROPOSED: 1,622m <sup>2</sup>	PROPOSED:	19,122.04m <sup>2</sup>
TOTAL: 6,694m <sup>2</sup>	TOTAL: 1,622m <sup>2</sup>	TOTAL:	19,122.04m <sup>2</sup>
LOT COVERAGE	MINIMUM LOT WIDTH	BUILDING HEIGHT	
MAXIMUM: N/A	REQUIRED: N/A	MAXIMUM:	14m
PROVIDED: 1,623m <sup>2</sup>	PROVIDED: 94.8m	PROVIDED:	37m
MINIMUM FRONT YARD DEPTH	MINIMUM REAR YARD DEPTH	MINIMUM SIDE YARD DEPTH	
MAXIMUM: N/A	REQUIRED: N/A	MAXIMUM:	N/A
PROVIDED: 1.61m	PROVIDED: 40.44m	PROVIDED:	1.52m
PARKING	BICYCLE SPACES	LOADING SPACES	
USE CLASSIFICATION	EXISTING: 0	EXISTING:	0
PROPOSED: 156 (6 BF, 3 TYPE A & 3 TYPE B) 1.04 RATIO	PROPOSED: 11	PROPOSED:	3
REQUIRED: 188 (6 BF, 3 TYPE A & 3 TYPE B) 1.25 RATIO	REQUIRED: 11	REQUIRED:	3
TOTAL: 156 (6 BF, 3 TYPE A & 3 TYPE B) 1.04 RATIO	TOTAL: 11	TOTAL:	3
LANDSCAPE AREA	AMENITY SPACE	SCREENING FENCE LENGTH	
EXISTING: N/A	REQUIRED: 900m <sup>2</sup>	EXISTING:	N/A
PROPOSED: 868m <sup>2</sup>	PROPOSED: 1294m <sup>2</sup>	PROPOSED:	N/A
TOTAL: 868.56m <sup>2</sup>	TOTAL: N/A	TOTAL:	N/A
NOTE: 1. ALL EXTERIOR PATHS OF TRAVEL SHALL BE CONSTRUCTED IN ACCORDANCE WITH SECTION 80.23 OF ONTARIO REGULATION 191/11 TO THE ACCESSIBILITY FOR ONTARIANS WITH DISABILITIES ACT AND SECTIONS 3.8.1.3 AND 3.8.3.2 OF THE ONTARIO BUILDING CODE. 2. ALL CURB RAMP SHALL BE CONSTRUCTED IN ACCORDANCE WITH SECTION 80.26(1) OF THE ONTARIO REGULATION 191/11 TO THE ACCESSIBILITY FOR ONTARIANS WITH DISABILITIES ACT AND SECTION 3.8.3.1.8 OF THE ONTARIO BUILDING CODE.			

102 - 27 Princess Street  
Leamington ON N8H 2X8

BAIRDIAE

architecture + engineering

1000 - 267 Pelissier Street  
Windsor ON N9A 4K4

A DIVISION OF MATTHEW J. BAIRD CONSULTING SERVICES INC.

3 SITE REVISIONS 2 22.05.25

2 SITE REVISIONS 21.06.06

1 ISSUED FOR OPA/ZBA/SPC 20.11.09

No. Description Date

Revision Schedule

SEAL

ONTARIO ASSOCIATION  
OF  
ARCHITECTS  
PAC. WEED  
LICENCE  
7289

MIXED-USE DEVELOPMENT

SANDWICH ST., WINDSOR ON

SHEET TITLE

SITE PLAN

PROJECT TITLE

JOB NUMBER

SHEET NUMBER

20-028

SP-1

2022-05-25 3:23:07 PM

GENERAL NOTES:  
1. This drawing is NOT to be scaled.  
2. This drawing, as an instrument of service, is provided by and is the property of Baird AE.  
3. The contractor must verify and accept responsibility for all dimensions and conditions on site and must notify Baird AE of any variation from the supplied information.  
4. This disclaimer is not responsible for the accuracy of survey, and the other disciplines information shown on this drawing. Refer to the appropriate consultant's drawings before proceeding with the work.  
5. Construction must conform to all applicable codes and requirements of authorities having jurisdiction.  
6. The contractor working from drawings not specifically marked For Construction must assume full responsibility and bear costs for any corrections or damages resulting from his work.

Consolidated Council Agenda - June 13, 2022  
Page 696 of 1025



TOPOGRAPHIC SURVEY  
OF  
PART OF LOTS 27 and 28,  
(EAST SIDE OF SANDWICH STREET)  
AND  
PART OF LOT 28,  
(WEST SIDE OF PETER STREET)  
REGISTERED PLAN 40  
IN THE  
CITY OF WINDSOR  
COUNTY OF ESSEX, ONTARIO

© VERHAEGEN • STUBBERFIELD • HARTLEY • BREWER • BEZAIRE INC.

SCALE = 1:250



"METRIC" DISTANCES AND COORDINATES SHOWN ON THIS PLAN  
ARE IN METRES AND CAN BE CONVERTED TO FEET BY  
DIVIDING BY 0.3048

LEGEND

ALL MONUMENTS SHOWN THUSLY □ ARE IRON BARS (IB) UNLESS OTHERWISE NOTED.  
SIB DENOTES 25mm X 25mm X 1.22m STANDARD IRON BAR  
SSIB DENOTES 25mm X 25mm X 0.61m SHORT STANDARD IRON BAR  
IB DENOTES 16mm X 16mm X 0.61m IRON BAR  
IB # DENOTES 19mm diameter X 0.61m ROUND IRON BAR  
C DENOTES CUT-CROSS  
CP DENOTES 5mm X 50mm STEEL PIN  
DENOTES SURVEY MONUMENT FOUND  
DENOTES SURVEY MONUMENT SET AND MARKED 1744  
WIT DENOTES WITNESS DENOTES PERPENDICULAR  
(S) DENOTES SET (M) DENOTES MEASURED (D) DENOTES DEED  
ORP DENOTES OBSERVED REFERENCE POINT (OU) DENOTES ORIGIN UNKNOWN  
(S/P) DENOTES SET PROPORTIONALLY (OU) DENOTES ORIGIN UNKNOWN  
(P) DENOTES REGISTERED PLAN 40  
(P1) DENOTES UNDATED PLAN OF SURVEY OF THE SUBJECT PROPERTY BY (1201)  
(P2) DENOTES PLAN OF SURVEY BY (1194) DATED NOVEMBER 30, 1978.  
(P3) DENOTES PLAN OF SURVEY BY (KVL) DATED JUNE 20, 1975  
(P4) DENOTES PLAN OF SURVEY BY (KVL) DATED JUNE 20, 1975  
(1744) DENOTES VERHAEGEN STUBBERFIELD HARTLEY BREWER BEZAIRE INC., O.L.S.  
(1194) DENOTES JOHN B. SWEETON INC., O.L.S.  
(1201) DENOTES CLARKE SURVEYORS INC., O.L.S.  
(KVL) DENOTES VERHAEGEN AND BEZAIRE LIMITED, O.L.S.

WHS DENOTES SEWER MANHOLE  
CB DENOTES CATCH BASIN  
LS# DENOTES LIGHT STANDARD CONCRETE  
LS# DENOTES LIGHT STANDARD STEEL  
LS# DENOTES LIGHT STANDARD WOOD  
UP# DENOTES UTILITY POLE CONCRETE  
UP# DENOTES UTILITY POLE STEEL  
UP# DENOTES UTILITY POLE WOOD  
GP DENOTES GUY POLE  
GW DENOTES GUY WIRE  
TDC DENOTES TOP OF CURB  
BDC DENOTES BOTTOM OF CURB  
FH DENOTES FIRE HYDRANT  
WM DENOTES WATER METER  
WVS DENOTES WATER VALVE (Service)  
WVM DENOTES WATER VALVE (Main)  
GM DENOTES GAS METER  
GV DENOTES GAS VALVE  
PecTV DENOTES CABLE TV PEDESTAL  
TRs DENOTES TRAFFIC SIGN  
SC DENOTES SHRUB  
SC DENOTES SEWER CLEANOUT  
INV DENOTES INVERT  
BOL DENOTES BOLLARD

DECIDUOUS AND CONIFEROUS TREES ARE DENOTED DT AND CT RESPECTIVELY.  
A PREFIX TO THE DESCRIPTION DESIGNATES THE NUMBER OF TREE TRUNKS WHEN  
TREES ARE CLUMPED TOGETHER AND A SUFFIX DENOTES THE TREE DIAMETER OR  
(NTS) NOT TO SCALE.

G (pipe size) DENOTES GAS LINE  
H (pipe size) DENOTES OVERHEAD HYDRO LINE  
CS (pipe size) DENOTES COMBINED SEWER  
SA (pipe size) DENOTES SANITARY SEWER  
ST (pipe size) DENOTES STORM SEWER  
W (pipe size) DENOTES WATER LINE

UNDERGROUND CABLE, HYDRO OR TELEPHONE LINES ARE PREFIXED WITH THE LETTER "U"  
(CABLE = uC HYDRO = uH TELEPHONE = uT)

INTEGRATION DATA

COORDINATES ARE DERIVED FROM GRID OBSERVATIONS USING THE CAN-NET  
NETWORK SERVICE AND ARE REFERRED TO UTM ZONE 17 (81 WEST LONGITUDE)  
NAD83 (CSRS) (2010.0)  
COORDINATE VALUES ARE TO AN URBAN ACCURACY IN ACCORDANCE WITH  
SECTION 14(2) OREG 216/10

POINT ID	NORTHING	EASTING
ORP-A	N4683887.90	E328283.37
ORP-B	N4684009.21	E328250.83

COORDINATES CANNOT, IN THEMSELVES, BE USED TO RE-ESTABLISH CORNERS  
OR BOUNDARIES SHOWN ON THIS PLAN.

ELEVATIONS

ELEVATIONS SHOWN ON THIS PLAN ARE IN METRES C.G.V.D. 28 (H.T.V. 2.0)

BENCH MARK

BENCH MARK 3026 ELEVATION 177.126m  
BRICK BUILDING, (Mun. No. 4027 SANDWICH ST. W.) 18.3m NORTHEAST OF  
RAILWAY CROSSING, TABLET IN SOUTHEAST CONCRETE FOUNDATION WALL,  
0.30m FROM SOUTHWEST CORNER AND 0.85m BELOW BRICK.

SITE BENCH MARK

TOP OF FIRE HYDRANT ALONG NORTH SIDE OF CHAPPELL STREET, (SEE PLAN), ELEVATION 181.25m

AREA

0.6697 Hectares, (1.655 Acres).

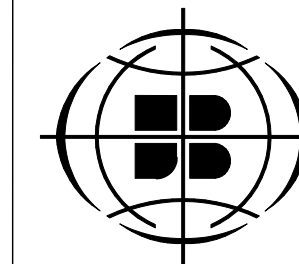
SURVEYOR'S CERTIFICATE

I CERTIFY THAT:

- THIS SURVEY AND PLAN ARE CORRECT AND IN ACCORDANCE WITH THE SURVEYS ACT,  
THE SURVEYORS ACT AND THE REGULATIONS MADE UNDER THEM.
- THIS SURVEY WAS COMPLETED ON THE 24th DAY OF MARCH, 2020.

DATE MARCH 26, 2020

ANDREW S. MANTHA  
ONTARIO LAND SURVEYOR  
for VERHAEGEN • STUBBERFIELD • HARTLEY  
BREWER • BEZAIRE INC.



VERHAEGEN  
LAND SURVEYORS  
A DIVISION OF J.D. BARNES LTD.

944 OTTAWA STREET, WINDSOR, ON, N8X 2E1

T: (519) 258-1772 F: (519) 258-1791 www.jdbarnes.com

DRAWN BY: A.S.M.

FILE: 20-47-541-00.dwg

CHECKED BY: A.S.M.

E-40-2  
(E/S SANDWICH)

REFERENCE NO.: 20-47-541-00

CAD Date: March 27, 2020 4:40 PM

CAD File: 20-47-541-00.dwg

NOTES

BEARINGS ARE UTM GRID DERIVED FROM OBSERVED REFERENCE POINTS "A" AND "B" BY  
REAL TIME NETWORK OBSERVATIONS.

DISTANCES ON THIS PLAN ARE GROUND AND CAN BE CONVERTED TO GRID BY MULTIPLYING  
BY THE COMBINED SCALE FACTOR OF 0.99994002

REGISTERED PLAN 40

P. I. N. 01258-0194

P. I. N. 01258-0179

P. I. N. 01258-0193

P. I. N. 01258-0180

P. I. N. 01258-0188

P. I. N. 01258-0183

P. I. N. 01258-0190

P. I. N. 01258-0186

P. I. N. 01258-0275

# Sandwich St @ Chappel Ave

## Morning Peak Diagram

### Specified Period

**From:** 7:00:00

**To:** 9:00:00

### One Hour Peak

**From:** 7:15:00

**To:** 8:15:00

**Municipality:** Windsor

**Site #:** 0000000001

**Intersection:** Sandwich St & Chappel Ave

**TFR File #:** 1

**Count date:** 21-Jul-2020

### Weather conditions:

Clear/Dry

### Person(s) who counted:

Cam

### \*\* Non-Signalized Intersection \*\*

**Major Road:** Sandwich St runs N/S

North Leg Total: 583

North Entering: 229

North Peds: 0

Peds Cross:  $\times$

Heavys	0	2	0	2
Trucks	1	9	0	10
Cars	3	213	1	217
<b>Totals</b>	<b>4</b>	<b>224</b>	<b>1</b>	

Heavys 10

Trucks 9

Cars 335

Totals 354

East Leg Total: 20

East Entering: 8

East Peds: 7

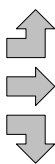
Peds Cross:  $\times$

Heavys	Trucks	Cars	Totals
17	5	16	38

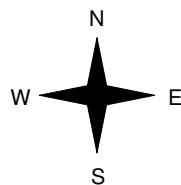


Chappel Ave

Heavys	Trucks	Cars	Totals
0	3	7	10
0	0	2	2
22	2	12	36
22	5	21	



Sandwich St



Cars	Trucks	Heavys	Totals
0	1	0	1
0	0	0	0
5	1	1	7
5	2	1	

Chappel Ave



Cars	Trucks	Heavys	Totals
12	0	0	12

Peds Cross:  $\times$

West Peds: 3

West Entering: 48

West Leg Total: 86

Cars	230	Cars	13	328	9	350
Trucks	12	Trucks	4	5	0	9
Heavys	25	Heavys	17	10	0	27
<b>Totals</b>	<b>267</b>	<b>Totals</b>	<b>34</b>	<b>343</b>	<b>9</b>	

Peds Cross:  $\times$

South Peds: 4

South Entering: 386

South Leg Total: 653

## Comments

# Sandwich St @ Chappel Ave

## Mid-day Peak Diagram

### Specified Period

**From:** 11:00:00

**To:** 14:00:00

### One Hour Peak

**From:** 12:15:00

**To:** 13:15:00

**Municipality:** Windsor

**Site #:** 0000000001

**Intersection:** Sandwich St & Chappel Ave

**TFR File #:** 1

**Count date:** 21-Jul-2020

### Weather conditions:

Clear/Dry

### Person(s) who counted:

Cam

**\*\* Non-Signalized Intersection \*\***

**Major Road:** Sandwich St runs N/S

North Leg Total: 526

North Entering: 284

North Peds: 1

Peds Cross:  $\times$

Heavys	0	8	0	8
Trucks	3	7	0	10
Cars	10	251	5	266
Totals	13	266	5	



Heavys	5
Trucks	8
Cars	229
Totals	242

East Leg Total: 24

East Entering: 13

East Peds: 9

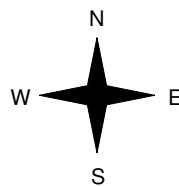
Peds Cross:  $\times$

Heavys	Trucks	Cars	Totals
21	6	27	54



Chappel Ave

Heavys	Trucks	Cars	Totals
0	1	3	4
0	0	1	1
26	3	20	49
26	4	24	



Sandwich St



Cars	Trucks	Heavys	Totals
4	1	0	5
2	0	0	2
6	0	0	6
12	1	0	

Chappel Ave



Cars	Trucks	Heavys	Totals
11	0	0	11

Peds Cross:  $\times$

West Peds: 0

West Entering: 54

West Leg Total: 108

Cars	277	Cars	15	222	5	242
Trucks	10	Trucks	3	6	0	9
Heavys	34	Heavys	21	5	0	26
Totals	321	Totals	39	233	5	



Peds Cross:  $\times$

South Peds: 5

South Entering: 277

South Leg Total: 598

## Comments

# Sandwich St @ Chappel Ave

## Afternoon Peak Diagram

### Specified Period

**From:** 15:00:00

**To:** 18:00:00

### One Hour Peak

**From:** 15:15:00

**To:** 16:15:00

**Municipality:** Windsor

**Site #:** 0000000001

**Intersection:** Sandwich St & Chappel Ave

**TFR File #:** 1

**Count date:** 21-Jul-2020

### Weather conditions:

Clear/Dry

### Person(s) who counted:

Cam

### \*\* Non-Signalized Intersection \*\*

**Major Road:** Sandwich St runs N/S

North Leg Total: 778

North Entering: 435

North Peds: 0

Peds Cross:  $\times$

Heavys	1	4	0	5
Trucks	1	6	1	8
Cars	6	411	5	422
Totals	8	421	6	

Heavys	5
Trucks	2
Cars	336
Totals	343

East Leg Total: 28

East Entering: 8

East Peds: 7

Peds Cross:  $\times$

Heavys	Trucks	Cars	Totals
25	2	13	40

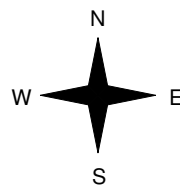


Chappel Ave

Heavys	Trucks	Cars	Totals
0	0	5	5
0	0	0	0
29	0	29	58
29	0	34	



Sandwich St



Cars	Trucks	Heavys	Totals
3	1	0	4
0	0	0	0
4	0	0	4
7	1	0	

Chappel Ave



Cars	Trucks	Heavys	Totals
14	5	1	20

Peds Cross:  $\times$

West Peds: 0

West Entering: 63

West Leg Total: 103

Cars	444
Trucks	6
Heavys	33
Totals	483



Cars	7	328	9	344
Trucks	1	1	4	6
Heavys	24	5	1	30
Totals	32	334	14	

Peds Cross:  $\times$

South Peds: 2

South Entering: 380

South Leg Total: 863

## Comments



# Sandwich St @ Chappel Ave

## Total Count Diagram

**Municipality:** Windsor  
**Site #:** 0000000001  
**Intersection:** Sandwich St & Chappel Ave  
**TFR File #:** 1  
**Count date:** 21-Jul-2020

**Weather conditions:**  
Clear/Dry  
**Person(s) who counted:**  
Cam

**\*\* Non-Signalized Intersection \*\***

**Major Road:** Sandwich St runs N/S

North Leg Total: 4794  
North Entering: 2445  
North Peds: 3  
Peds Cross:  $\times$

	Heavys	Trucks	Cars	Totals
North	1	47	0	48
East	10	45	2	57
South	42	2281	17	2340
<b>Totals</b>	<b>53</b>	<b>2373</b>	<b>19</b>	

	Heavys	Trucks	Cars	Totals
North	47	36	2266	2349
East	36	2266	2349	
South	47	36	2266	2349
<b>Totals</b>	<b>47</b>	<b>36</b>	<b>2266</b>	

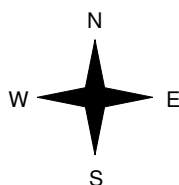
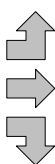
East Leg Total: 146  
East Entering: 68  
East Peds: 57  
Peds Cross:  $\times$

Heavys	Trucks	Cars	Totals
140	24	118	282



Chappel Ave

Heavys	Trucks	Cars	Totals
3	7	36	46
0	0	6	6
146	12	130	288
149	19	172	



Sandwich St

Cars	Trucks	Heavys	Totals
18	3	0	21
3	1	0	4
38	4	1	43
59	8	1	

Chappel Ave



Cars	Trucks	Heavys	Totals
63	14	1	78

Peds Cross:  $\times$   
West Peds: 9  
West Entering: 340  
West Leg Total: 622

	Cars	Trucks	Heavys	Totals
West	2449	61	194	2704
South	73	13	139	225
East	2212	26	44	2282
<b>Totals</b>	<b>2325</b>	<b>51</b>	<b>184</b>	



	Cars	Trucks	Heavys	Totals
West	2449	61	194	2704
South	73	13	139	225
East	2212	26	44	2282
<b>Totals</b>	<b>2325</b>	<b>51</b>	<b>184</b>	

Peds Cross:  $\times$   
South Peds: 17  
South Entering: 2560  
South Leg Total: 5264

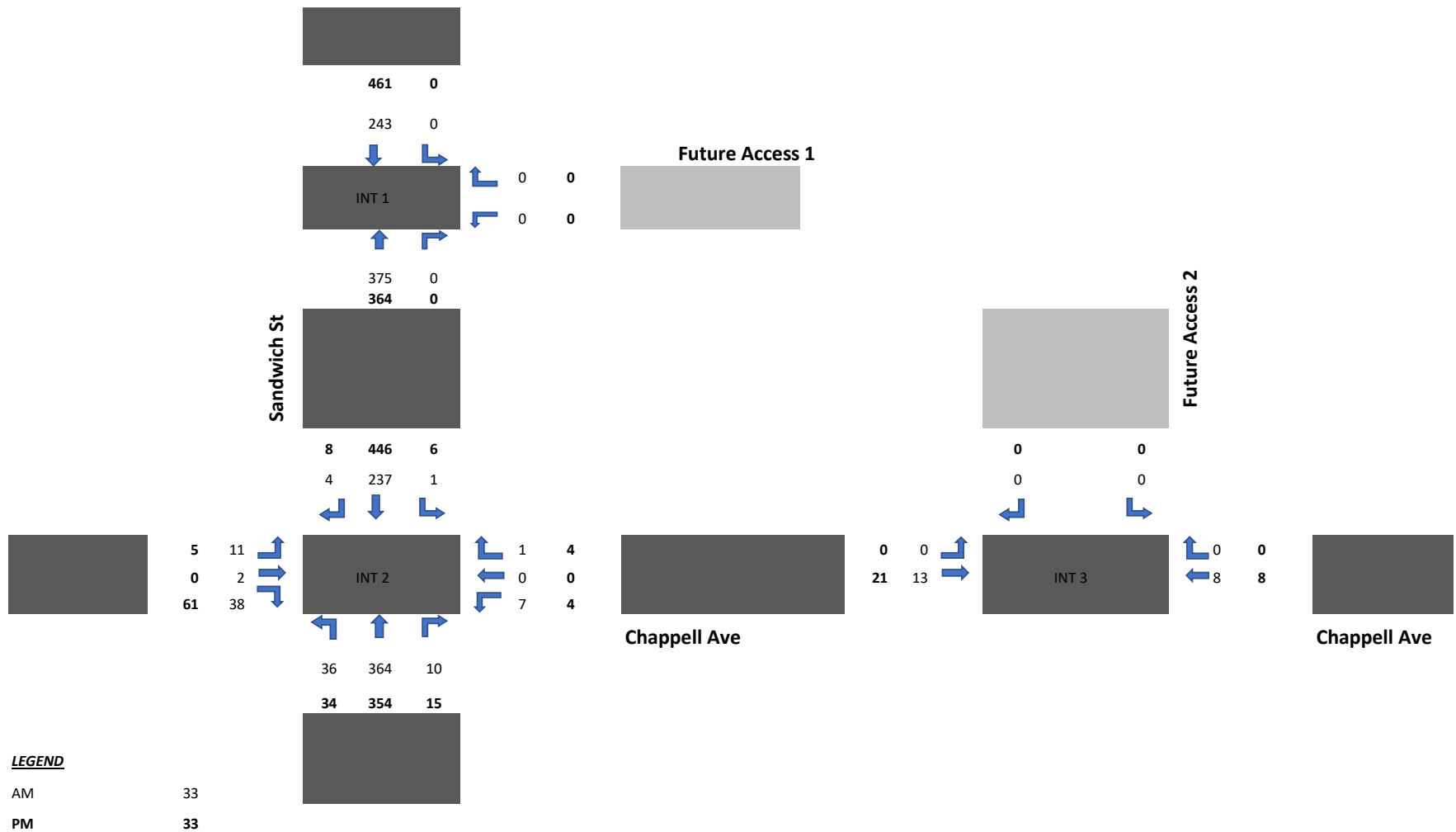
## Comments

## **Appendix B**

---

### **FUTURE TRAFFIC, DEVELOPMENT TRAFFIC AND TOTAL TRAFFIC VOLUMES**

# MIXED USE HIGH-RISE BUILDING



**PROJECT NAME**  
3885 Sandwich Street, Windsor

**SHEET TITLE**  
2022 BACKGROUND TRAFFIC



**DATE:** 26-May-22

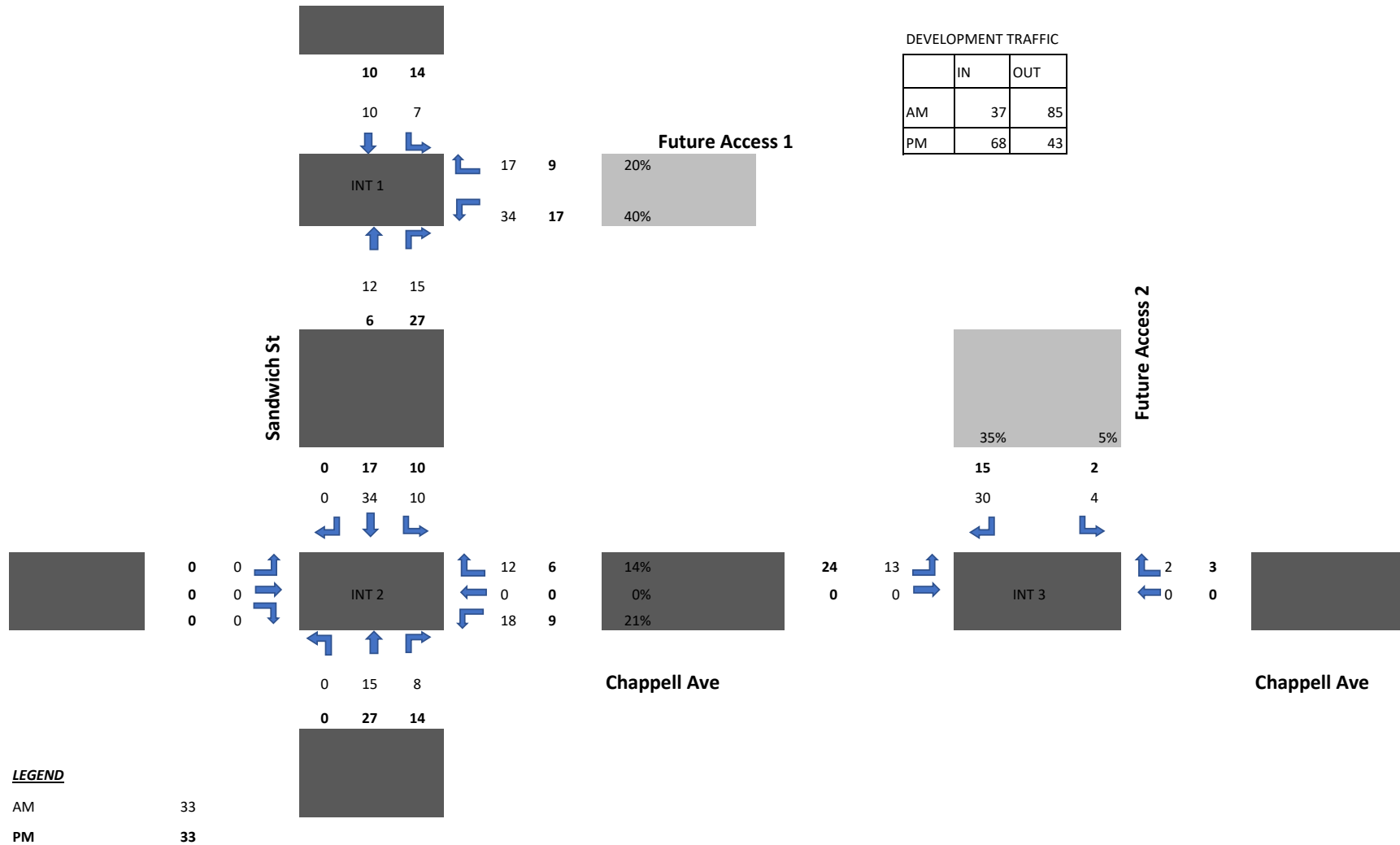
**PROJECT NO:** 20-028

**FIGURE NO:** Figure 1.1

# MIXED USE HIGH-RISE BUILDING

DEVELOPMENT TRAFFIC

	IN	OUT
AM	37	85
PM	68	43



**PROJECT NAME** 3885 Sandwich Street, Windsor

**SHEET TITLE** 2022 DEVELOPMENT TRAFFIC

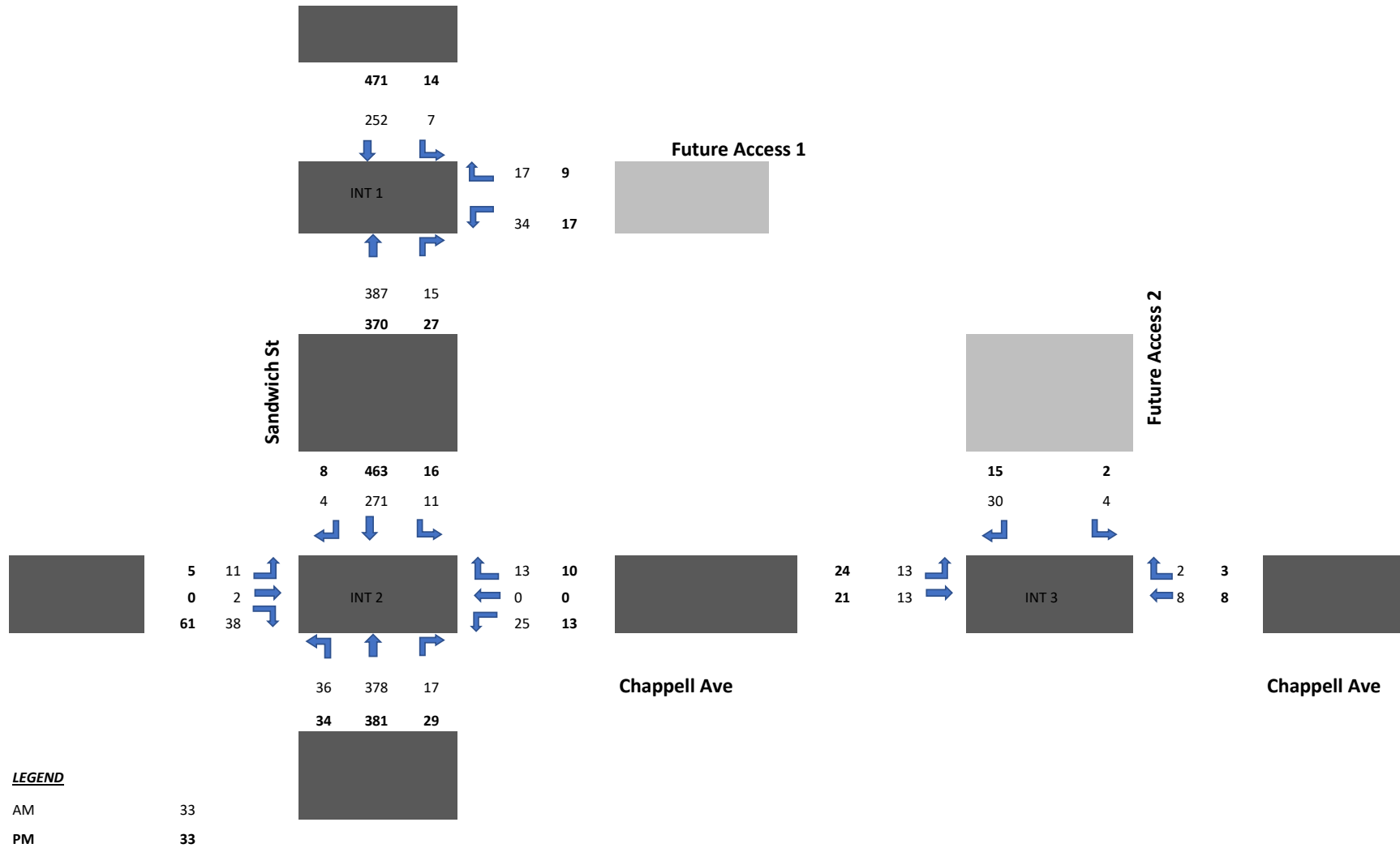


**DATE:** 26-May-22

**PROJECT NO:** 20-028

**FIGURE NO:** Figure 1.2

# MIXED USE HIGH-RISE BUILDING



**PROJECT NAME** 3885 Sandwich Street, Windsor

**SHEET TITLE** 2022 TOTAL TRAFFIC



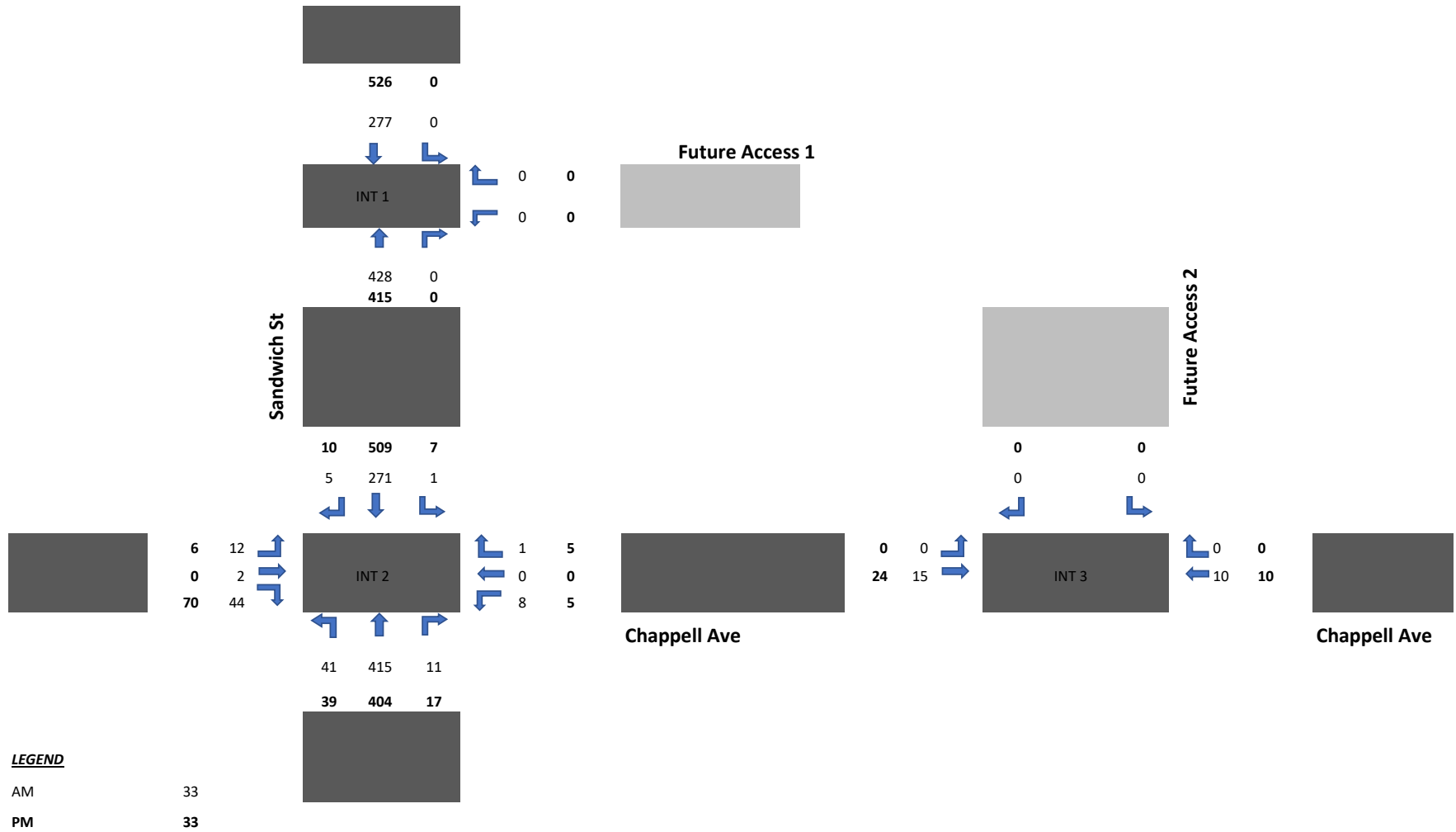
**DATE:** 26-May-22

**PROJECT NO:** 20-028

**FIGURE NO:** Figure 1.3



# MIXED USE HIGH-RISE BUILDING

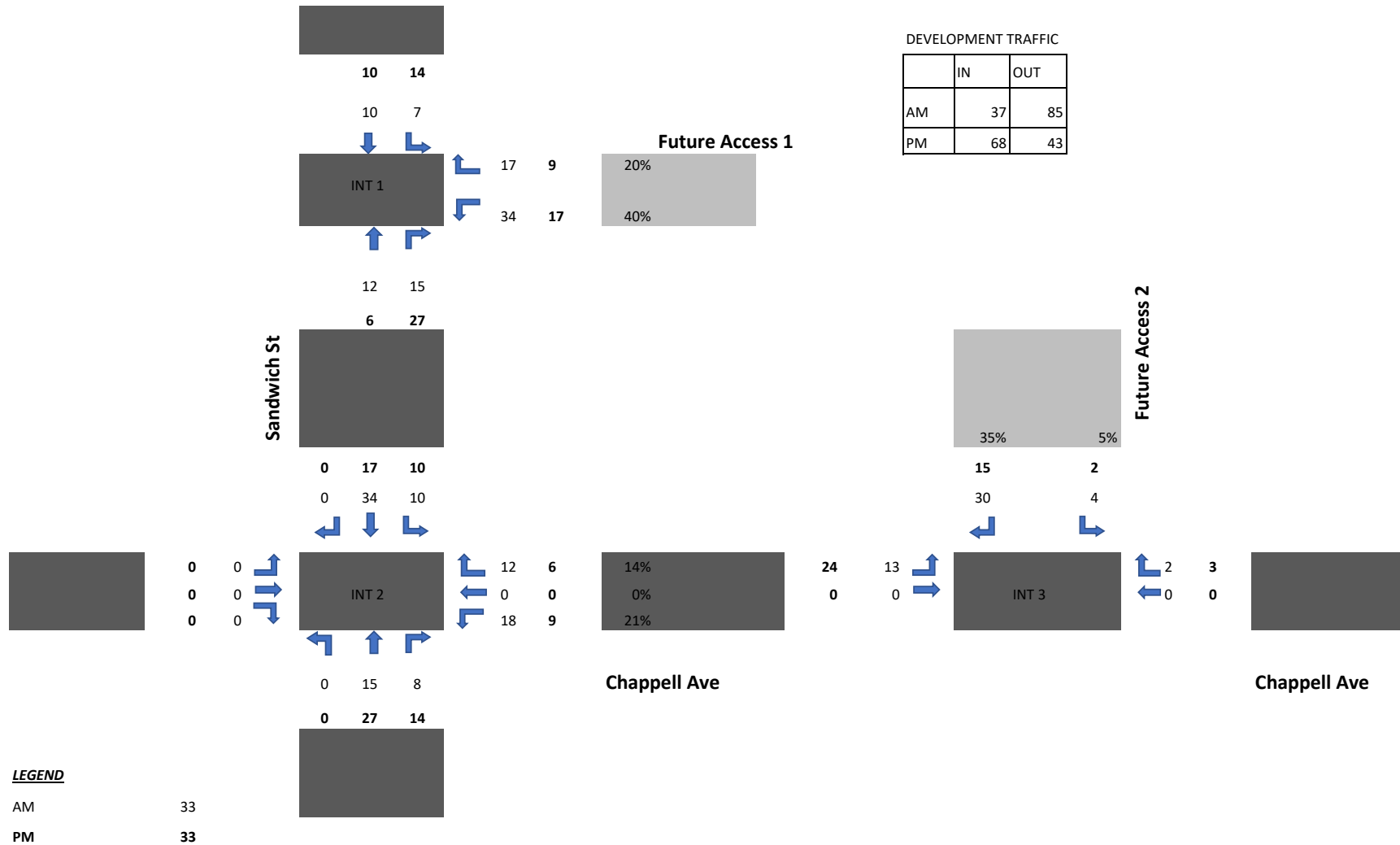


<b>PROJECT NAME</b>	3885 Sandwich Street, Windsor	<b>DATE:</b>	26-May-22
<b>SHEET TITLE</b>	2027 BACKGROUND TRAFFIC	<b>PROJECT NO:</b>	20-028
		<b>FIGURE NO:</b>	Figure 2.1

# MIXED USE HIGH-RISE BUILDING

DEVELOPMENT TRAFFIC

	IN	OUT
AM	37	85
PM	68	43



**PROJECT NAME** 3885 Sandwich Street, Windsor

**SHEET TITLE** 2027 DEVELOPMENT TRAFFIC

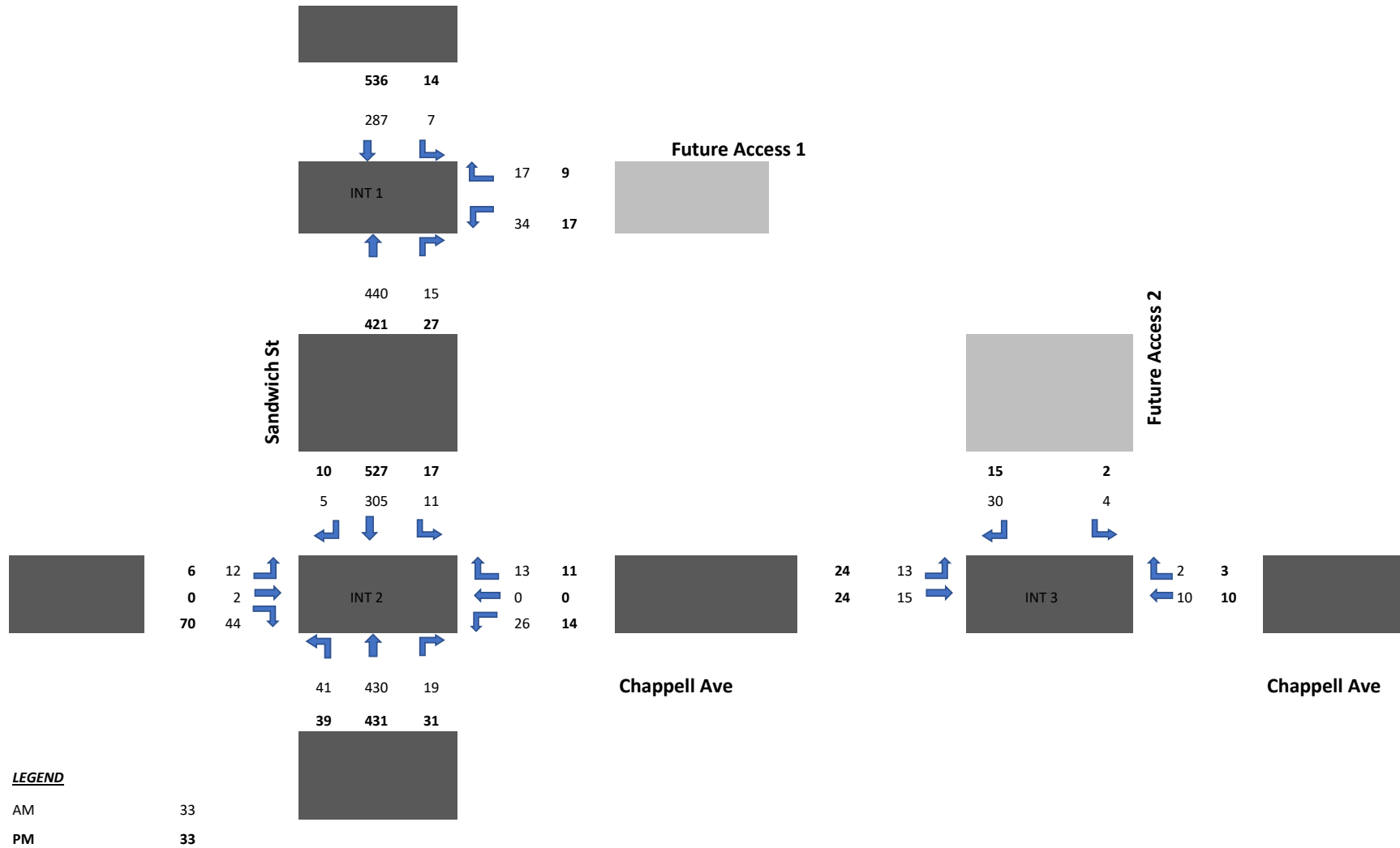


**DATE:** 26-May-22

**PROJECT NO:** 20-028

**FIGURE NO:** Figure 2.2

# MIXED USE HIGH-RISE BUILDING



**PROJECT NAME** 3885 Sandwich Street, Windsor

**SHEET TITLE** 2027 TOTAL TRAFFIC



**DATE:** 26-May-22

**PROJECT NO:** 20-028

**FIGURE NO:** Figure 2.3

**MIXED USE HIGH-RISE BUILDING**

**Legend:**  
 AM  
 PM

**Diagram Data:**

- INT 1 (Top):**
  - Top: 592 (AM), 0 (PM)
  - Left: 311 (AM), 0 (PM)
  - Right: 0 (AM), 0 (PM)
  - Bottom: 481 (AM), 0 (PM)
  - Bottom: 466 (AM), 0 (PM)
- INT 2 (Middle):**
  - Top: 11 (AM), 573 (AM), 8 (PM)
  - Top: 5 (AM), 305 (AM), 1 (PM)
  - Left: 7 (AM), 14 (AM), 0 (PM), 3 (PM), 79 (PM), 49 (PM)
  - Right: 1 (AM), 5 (AM), 0 (PM), 0 (PM), 10 (PM), 5 (PM)
  - Bottom: 46 (AM), 466 (AM), 12 (PM)
  - Bottom: 44 (AM), 454 (AM), 19 (PM)
- INT 3 (Right):**
  - Top: 0 (AM), 0 (PM)
  - Top: 0 (AM), 0 (PM)
  - Left: 0 (AM), 0 (PM)
  - Right: 0 (AM), 0 (PM)
  - Bottom: 11 (AM), 11 (PM)

**Streets and Access Points:**

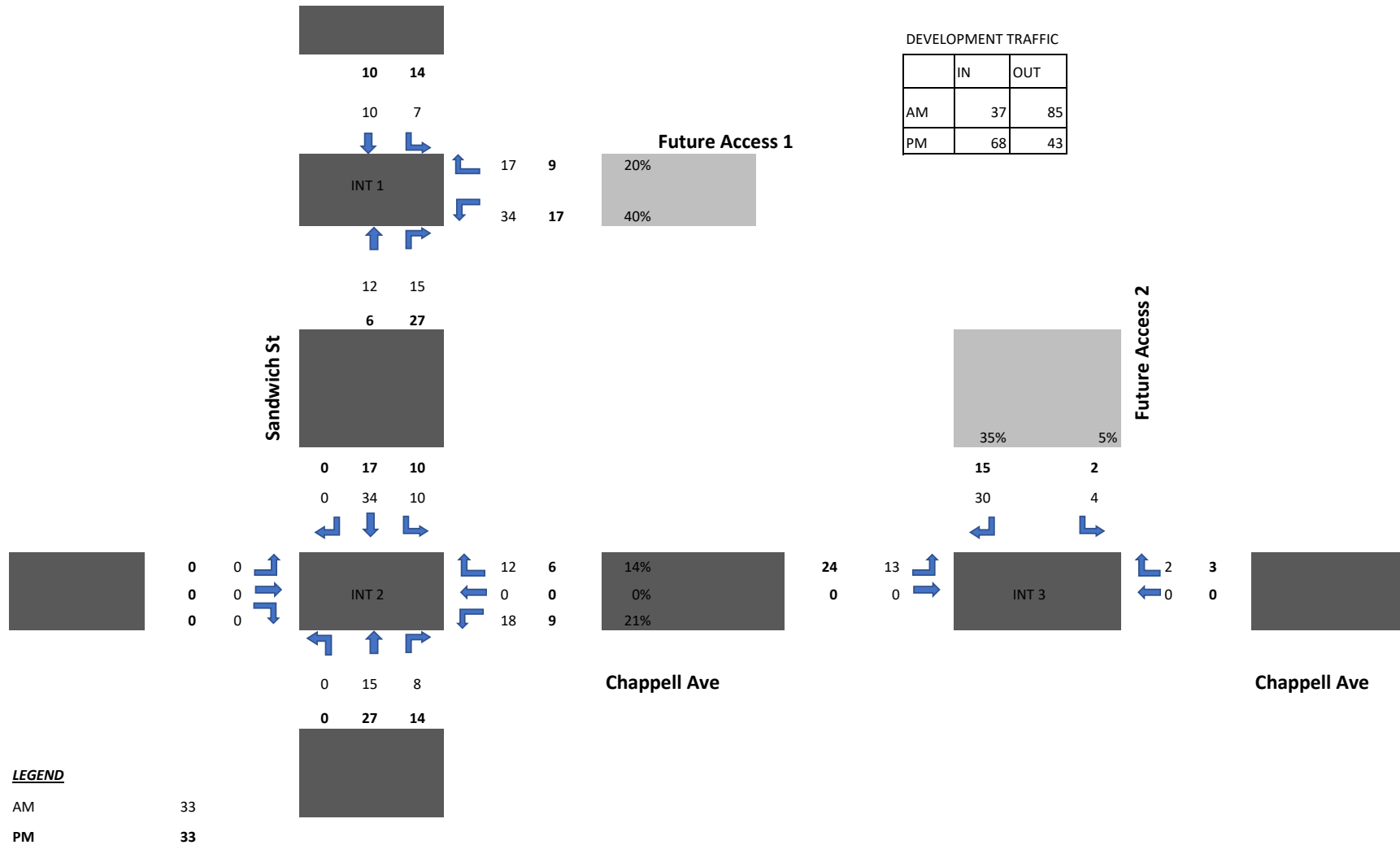
- Sandwich St:** Vertical street on the left.
- Chappell Ave:** Horizontal street at the bottom.
- Future Access 1:** Located to the right of INT 1.
- Future Access 2:** Located to the right of INT 3.

FIGURE NO: Figure 3.1

# MIXED USE HIGH-RISE BUILDING

DEVELOPMENT TRAFFIC

	IN	OUT
AM	37	85
PM	68	43



**PROJECT NAME** 3885 Sandwich Street, Windsor

**SHEET TITLE** 2032 DEVELOPMENT TRAFFIC



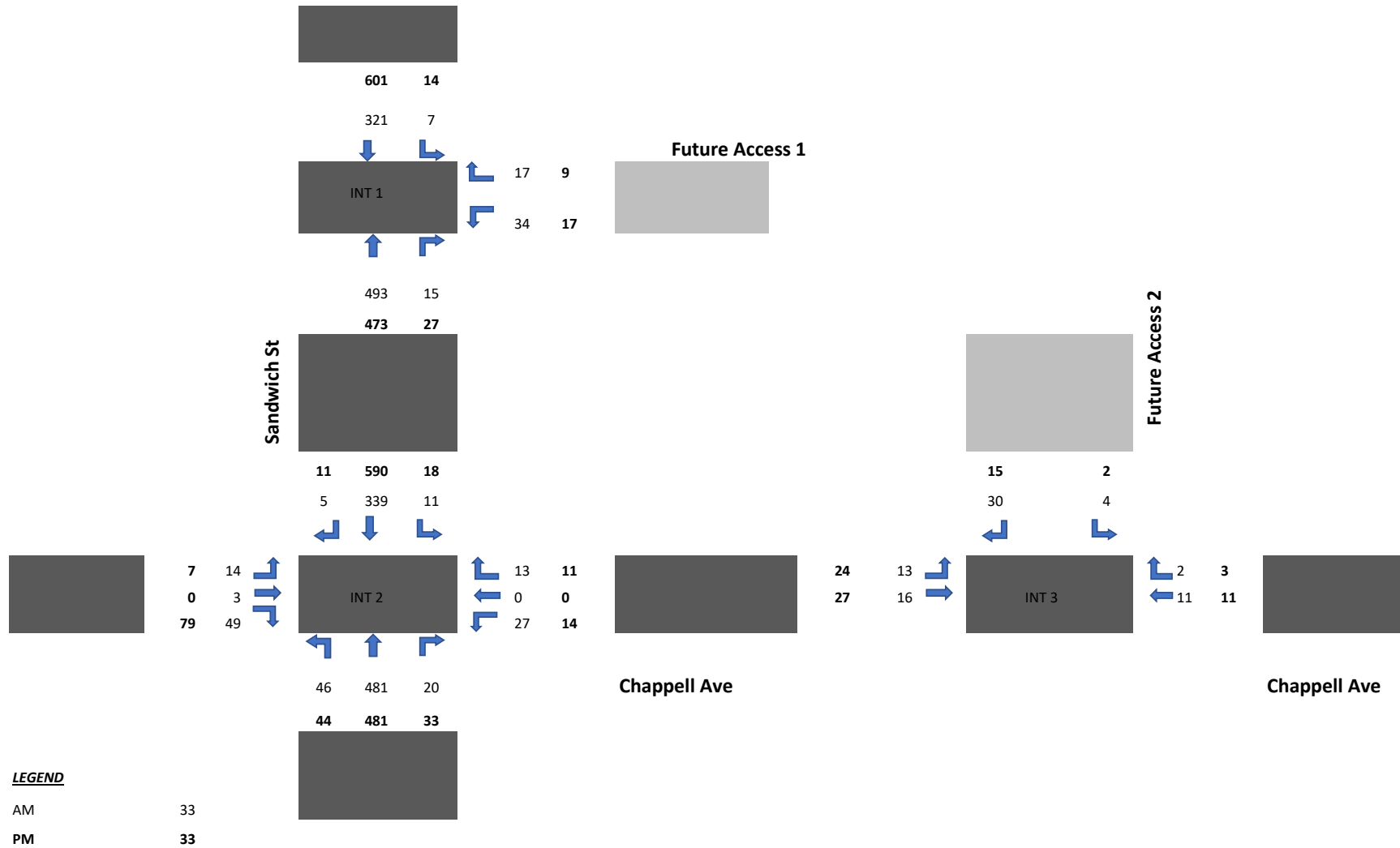
**DATE:** 26-May-22

**PROJECT NO:** 20-028

**FIGURE NO:** Figure 3.2



# MIXED USE HIGH-RISE BUILDING



**PROJECT NAME** 3885 Sandwich Street, Windsor

**SHEET TITLE** 2032 TOTAL TRAFFIC



**DATE:** 26-May-22

**PROJECT NO:** 20-028

**FIGURE NO:** Figure 3.3

## Appendix C

---
















### CAPACITY ANALYSIS

# HCM Unsignalized Intersection Capacity Analysis

2022 BKGD AM TRAFFIC

1: Chappell Ave & Sandwich St

10-05-2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	11	2	38	7	0	1	36	364	10	1	237	4
Future Volume (Veh/h)	11	2	38	7	0	1	36	364	10	1	237	4
Sign Control		Stop			Stop			Free			Free	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	12	2	41	8	0	1	39	396	11	1	258	4
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type								None			None	
Median storage (veh)												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	742	747	260	784	744	402	262			407		
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	742	747	260	784	744	402	262			407		
tC, single (s)	7.1	6.5	6.7	7.1	6.5	6.5	4.9			4.3		
tC, 2 stage (s)												
tF (s)	3.5	4.0	3.8	3.5	4.0	3.5	2.9			2.3		
p0 queue free %	96	99	94	97	100	100	96			100		
cM capacity (veh/h)	323	330	675	284	331	602	965			1080		
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	55	9	446	263								
Volume Left	12	8	39	1								
Volume Right	41	1	11	4								
cSH	529	301	965	1080								
Volume to Capacity	0.10	0.03	0.04	0.00								
Queue Length 95th (m)	2.8	0.7	1.0	0.0								
Control Delay (s)	12.6	17.3	1.2	0.0								
Lane LOS	B	C	A	A								
Approach Delay (s)	12.6	17.3	1.2	0.0								
Approach LOS	B	C										
Intersection Summary												
Average Delay			1.8									
Intersection Capacity Utilization			47.9%		ICU Level of Service				A			
Analysis Period (min)			15									

3885 SANDWICH STREET  
BAIRDAE

















Synchro 10 Report  
Page 1

# HCM Unsignalized Intersection Capacity Analysis

2022 TOTAL AM TRAFFIC

1: Chappell Ave & Sandwich St

10-02-2020










												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	11	2	38	22	0	11	36	373	15	10	265	4
Future Volume (Veh/h)	11	2	38	22	0	11	36	373	15	10	265	4
Sign Control		Stop			Stop			Free			Free	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	12	2	41	24	0	12	39	405	16	11	288	4
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type								None			None	
Median storage (veh)												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	815	811	290	845	805	413	292			421		
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	815	811	290	845	805	413	292			421		
tC, single (s)	7.4	6.5	6.8	7.2	6.5	6.2	4.7			4.1		
tC, 2 stage (s)												
tF (s)	3.8	4.0	3.8	3.6	4.0	3.3	2.8			2.2		
p0 queue free %	95	99	93	90	100	98	96			99		
cM capacity (veh/h)	251	300	628	241	303	643	993			1149		
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	55	36	460	303								
Volume Left	12	24	39	11								
Volume Right	41	12	16	4								
cSH	460	305	993	1149								
Volume to Capacity	0.12	0.12	0.04	0.01								
Queue Length 95th (m)	3.2	3.2	1.0	0.2								
Control Delay (s)	13.9	18.4	1.2	0.4								
Lane LOS	B	C	A	A								
Approach Delay (s)	13.9	18.4	1.2	0.4								
Approach LOS	B	C										
Intersection Summary												
Average Delay			2.4									
Intersection Capacity Utilization			47.1%	ICU Level of Service					A			
Analysis Period (min)			15									

3885 SANDWICH STREET  
BAIRDAE

Synchro 10 Report  
Page 1

HCM Unsignalized Intersection Capacity Analysis  
7: Sandwich St & Fut Acc 1

2022 TOTAL AM TRAFFIC  
10-02-2020

						
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Volume (veh/h)	28	14	385	10	5	252
Future Volume (Veh/h)	28	14	385	10	5	252
Sign Control	Stop		Free			Free
Grade	0%		0%			0%
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	30	15	418	11	5	274
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type			None			None
Median storage (veh)						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	708	424			429	
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	708	424			429	
tC, single (s)	6.4	6.2			4.1	
tC, 2 stage (s)						
tF (s)	3.5	3.3			2.2	
p0 queue free %	92	98			100	
cM capacity (veh/h)	400	630			1141	
Direction, Lane #	WB 1	NB 1	SB 1			
Volume Total	45	429	279			
Volume Left	30	0	5			
Volume Right	15	11	0			
cSH	455	1700	1141			
Volume to Capacity	0.10	0.25	0.00			
Queue Length 95th (m)	2.6	0.0	0.1			
Control Delay (s)	13.8	0.0	0.2			
Lane LOS	B		A			
Approach Delay (s)	13.8	0.0	0.2			
Approach LOS	B					
<b>Intersection Summary</b>						
Average Delay		0.9				
Intersection Capacity Utilization		30.9%		ICU Level of Service		A
Analysis Period (min)		15				

3885 SANDWICH STREET  
BAIRDAE

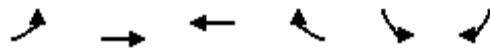
Synchro 10 Report  
Page 2






# HCM Unsignalized Intersection Capacity Analysis

## 16: Chappell Ave & Fut Acc 2

2022 TOTAL AM TRAFFIC  
10-02-2020



Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Traffic Volume (veh/h)	8	13	8	1	4	25
Future Volume (Veh/h)	8	13	8	1	4	25
Sign Control		Free	Free		Stop	
Grade		0%	0%		0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	9	14	9	1	4	27
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type		None	None			
Median storage veh						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	10				42	10
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	10				42	10
tC, single (s)	4.1				6.5	6.5
tC, 2 stage (s)						
tF (s)	2.2				3.6	3.5
p0 queue free %	99				100	97
cM capacity (veh/h)	1610				954	1009
Direction, Lane #	EB 1	WB 1	SB 1			
Volume Total	23	10	31			
Volume Left	9	0	4			
Volume Right	0	1	27			
cSH	1610	1700	1001			
Volume to Capacity	0.01	0.01	0.03			
Queue Length 95th (m)	0.1	0.0	0.8			
Control Delay (s)	2.9	0.0	8.7			
Lane LOS	A		A			
Approach Delay (s)	2.9	0.0	8.7			
Approach LOS			A			
Intersection Summary						
Average Delay			5.2			
Intersection Capacity Utilization			17.8%	ICU Level of Service		A
Analysis Period (min)			15			

3885 SANDWICH STREET  
BAIRDAE

















Synchro 10 Report  
Page 3

# HCM Unsignalized Intersection Capacity Analysis

2022 BKGD PM TRAFFIC

1: Chappell Ave & Sandwich St

10-05-2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	5	0	61	4	0	4	34	354	15	6	446	8
Future Volume (Veh/h)	5	0	61	4	0	4	34	354	15	6	446	8
Sign Control	Stop			Stop			Free			Free		
Grade	0%			0%			0%			0%		
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	5	0	66	4	0	4	37	385	16	7	485	9
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type	None						None					
Median storage (veh)												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	974	978	490	1036	975	393	494			401		
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	974	978	490	1036	975	393	494			401		
tC, single (s)	7.1	6.5	6.7	7.1	6.5	6.5	4.9			4.3		
tC, 2 stage (s)												
tF (s)	3.5	4.0	3.8	3.5	4.0	3.5	2.9			2.3		
p0 queue free %	98	100	87	98	100	99	95			99		
cM capacity (veh/h)	222	239	493	175	240	608	770			1086		
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	71	8	438	501								
Volume Left	5	4	37	7								
Volume Right	66	4	16	9								
cSH	454	272	770	1086								
Volume to Capacity	0.16	0.03	0.05	0.01								
Queue Length 95th (m)	4.4	0.7	1.2	0.2								
Control Delay (s)	14.4	18.6	1.4	0.2								
Lane LOS	B	C	A	A								
Approach Delay (s)	14.4	18.6	1.4	0.2								
Approach LOS	B	C										
Intersection Summary												
Average Delay				1.9								
Intersection Capacity Utilization				51.1%	ICU Level of Service				A			
Analysis Period (min)				15								

3885 SANDWICH STREET  
BAIRDAE

















Synchro 10 Report  
Page 1

# HCM Unsignalized Intersection Capacity Analysis

2022 TOTAL PM TRAFFIC

1: Chappell Ave & Sandwich St

10-02-2020










												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	5	0	61	12	0	9	34	379	28	15	461	8
Future Volume (Veh/h)	5	0	61	12	0	9	34	379	28	15	461	8
Sign Control	Stop			Stop			Free			Free		
Grade	0%			0%			0%			0%		
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	5	0	66	13	0	10	37	412	30	16	501	9
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type	None						None					
Median storage veh												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	1048	1054	506	1104	1043	427	510				442	
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	1048	1054	506	1104	1043	427	510				442	
tC, single (s)	7.1	6.5	6.7	7.1	6.5	6.5	4.9				4.3	
tC, 2 stage (s)												
tF (s)	3.5	4.0	3.8	3.5	4.0	3.5	2.9				2.3	
p0 queue free %	97	100	86	92	100	98	95				98	
cM capacity (veh/h)	194	213	482	156	217	581	758				1048	
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	71	23	479	526								
Volume Left	5	13	37	16								
Volume Right	66	10	30	9								
cSH	436	229	758	1048								
Volume to Capacity	0.16	0.10	0.05	0.02								
Queue Length 95th (m)	4.6	2.6	1.2	0.4								
Control Delay (s)	14.8	22.5	1.4	0.4								
Lane LOS	B	C	A	A								
Approach Delay (s)	14.8	22.5	1.4	0.4								
Approach LOS	B	C										
Intersection Summary												
Average Delay				2.2								
Intersection Capacity Utilization				48.7%	ICU Level of Service				A			
Analysis Period (min)				15								

3885 SANDWICH STREET  
BAIRDAE

Synchro 10 Report  
Page 1

HCM Unsignalized Intersection Capacity Analysis  
7: Sandwich St & Fut Acc 1

2022 TOTAL PM TRAFFIC  
10-02-2020

						
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Volume (veh/h)	15	7	369	25	13	470
Future Volume (Veh/h)	15	7	369	25	13	470
Sign Control	Stop		Free			Free
Grade	0%		0%			0%
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	16	8	401	27	14	511
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type			None			None
Median storage (veh)						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	954	414			428	
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	954	414			428	
tC, single (s)	6.4	6.2			4.1	
tC, 2 stage (s)						
tF (s)	3.5	3.3			2.2	
p0 queue free %	94	99			99	
cM capacity (veh/h)	284	638			1142	
Direction, Lane #	WB 1	NB 1	SB 1			
Volume Total	24	428	525			
Volume Left	16	0	14			
Volume Right	8	27	0			
cSH	348	1700	1142			
Volume to Capacity	0.07	0.25	0.01			
Queue Length 95th (m)	1.8	0.0	0.3			
Control Delay (s)	16.1	0.0	0.4			
Lane LOS	C		A			
Approach Delay (s)	16.1	0.0	0.4			
Approach LOS	C					
Intersection Summary						
Average Delay		0.6				
Intersection Capacity Utilization		45.2%		ICU Level of Service		A
Analysis Period (min)		15				

3885 SANDWICH STREET  
BAIRDAE

Synchro 10 Report  
Page 2




# HCM Unsignalized Intersection Capacity Analysis

## 16: Chappell Ave & Fut Acc 2

2022 TOTAL PM TRAFFIC

10-02-2020



Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Traffic Volume (veh/h)	22	21	8	3	2	13
Future Volume (Veh/h)	22	21	8	3	2	13
Sign Control		Free	Free		Stop	
Grade		0%	0%		0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	24	23	9	3	2	14
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type		None	None			
Median storage veh						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	12				82	10
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	12				82	10
tC, single (s)	4.1				6.5	6.5
tC, 2 stage (s)						
tF (s)	2.2				3.6	3.5
p0 queue free %	99				100	99
cM capacity (veh/h)	1607				897	1007
Direction, Lane #	EB 1	WB 1	SB 1			
Volume Total	47	12	16			
Volume Left	24	0	2			
Volume Right	0	3	14			
cSH	1607	1700	992			
Volume to Capacity	0.01	0.01	0.02			
Queue Length 95th (m)	0.4	0.0	0.4			
Control Delay (s)	3.8	0.0	8.7			
Lane LOS	A		A			
Approach Delay (s)	3.8	0.0	8.7			
Approach LOS			A			
Intersection Summary						
Average Delay			4.2			
Intersection Capacity Utilization			19.0%	ICU Level of Service		A
Analysis Period (min)			15			

3885 SANDWICH STREET  
BAIRDAE

Synchro 10 Report  
Page 3



















# HCM Unsignalized Intersection Capacity Analysis

2027 BKGD AM TRAFFIC

1: Chappell Ave & Sandwich St

10-05-2020

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	12	2	44	8	0	1	41	415	11	1	271	5
Future Volume (Veh/h)	12	2	44	8	0	1	41	415	11	1	271	5
Sign Control		Stop			Stop			Free			Free	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	13	2	48	9	0	1	45	451	12	1	295	5
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type	None								None			
Median storage (veh)												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	848	852	298	896	849	457	300	463				
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	848	852	298	896	849	457	300	463				
tC, single (s)	7.1	6.5	6.7	7.1	6.5	6.5	4.9	4.3				
tC, 2 stage (s)												
tF (s)	3.5	4.0	3.8	3.5	4.0	3.5	2.9	2.3				
p0 queue free %	95	99	93	96	100	100	95	100				
cM capacity (veh/h)	273	284	642	233	285	559	930	1029				
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	63	10	508	301								
Volume Left	13	9	45	1								
Volume Right	48	1	12	5								
cSH	486	248	930	1029								
Volume to Capacity	0.13	0.04	0.05	0.00								
Queue Length 95th (m)	3.5	1.0	1.2	0.0								
Control Delay (s)	13.5	20.1	1.4	0.0								
Lane LOS	B	C	A	A								
Approach Delay (s)	13.5	20.1	1.4	0.0								
Approach LOS	B	C										
Intersection Summary												
Average Delay	2.0											
Intersection Capacity Utilization	52.7%			ICU Level of Service					A			
Analysis Period (min)	15											

3885 SANDWICH STREET  
BAIRDAE

















Synchro 10 Report  
Page 1

# HCM Unsignalized Intersection Capacity Analysis

2022 TOTAL AM TRAFFIC

1: Chappell Ave & Sandwich St

05-26-2022










												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	11	2	38	25	0	13	36	378	17	11	271	4
Future Volume (Veh/h)	11	2	38	25	0	13	36	378	17	11	271	4
Sign Control		Stop			Stop			Free			Free	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	12	2	41	27	0	14	39	411	18	12	295	4
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type								None			None	
Median storage (veh)												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	833	828	297	861	821	420	299			429		
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	833	828	297	861	821	420	299			429		
tC, single (s)	7.4	6.5	6.8	7.2	6.5	6.2	4.7			4.1		
tC, 2 stage (s)												
tF (s)	3.8	4.0	3.8	3.6	4.0	3.3	2.8			2.2		
p0 queue free %	95	99	93	89	100	98	96			99		
cM capacity (veh/h)	243	293	622	235	296	638	987			1141		
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	55	41	468	311								
Volume Left	12	27	39	12								
Volume Right	41	14	18	4								
cSH	451	300	987	1141								
Volume to Capacity	0.12	0.14	0.04	0.01								
Queue Length 95th (m)	3.3	3.8	1.0	0.3								
Control Delay (s)	14.1	18.9	1.2	0.4								
Lane LOS	B	C	A	A								
Approach Delay (s)	14.1	18.9	1.2	0.4								
Approach LOS	B	C										
Intersection Summary												
Average Delay			2.5									
Intersection Capacity Utilization			47.7%	ICU Level of Service					A			
Analysis Period (min)			15									

3885 SANDWICH STREET  
BAIRDAE

Synchro 10 Report  
Page 1

HCM Unsignalized Intersection Capacity Analysis  
7: Sandwich St & Fut Acc 1

2022 TOTAL AM TRAFFIC  
05-26-2022

						
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Volume (veh/h)	34	17	387	15	7	252
Future Volume (Veh/h)	34	17	387	15	7	252
Sign Control	Stop		Free			Free
Grade	0%		0%			0%
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	37	18	421	16	8	274
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type			None			None
Median storage (veh)						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	719	429			437	
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	719	429			437	
tC, single (s)	6.4	6.2			4.1	
tC, 2 stage (s)						
tF (s)	3.5	3.3			2.2	
p0 queue free %	91	97			99	
cM capacity (veh/h)	392	626			1134	
Direction, Lane #	WB 1	NB 1	SB 1			
Volume Total	55	437	282			
Volume Left	37	0	8			
Volume Right	18	16	0			
cSH	447	1700	1134			
Volume to Capacity	0.12	0.26	0.01			
Queue Length 95th (m)	3.3	0.0	0.2			
Control Delay (s)	14.2	0.0	0.3			
Lane LOS	B		A			
Approach Delay (s)	14.2	0.0	0.3			
Approach LOS	B					
Intersection Summary						
Average Delay			1.1			
Intersection Capacity Utilization			31.3%	ICU Level of Service		A
Analysis Period (min)			15			




3885 SANDWICH STREET  
BAIRDAE

Synchro 10 Report  
Page 2

# HCM Unsignalized Intersection Capacity Analysis 16: Chappell Ave & Fut Acc 2

2022 TOTAL AM TRAFFIC  
05-26-2022



Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Traffic Volume (veh/h)	13	13	8	2	4	30
Future Volume (Veh/h)	13	13	8	2	4	30
Sign Control		Free	Free		Stop	
Grade		0%	0%		0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	14	14	9	2	4	33
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type		None	None			
Median storage veh						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	11				52	10
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	11				52	10
tC, single (s)	4.1				6.5	6.5
tC, 2 stage (s)						
tF (s)	2.2				3.6	3.5
p0 queue free %	99				100	97
cM capacity (veh/h)	1608				938	1008
Direction, Lane #	EB 1	WB 1	SB 1			
Volume Total	28	11	37			
Volume Left	14	0	4			
Volume Right	0	2	33			
cSH	1608	1700	1000			
Volume to Capacity	0.01	0.01	0.04			
Queue Length 95th (m)	0.2	0.0	0.9			
Control Delay (s)	3.7	0.0	8.7			
Lane LOS	A		A			
Approach Delay (s)	3.7	0.0	8.7			
Approach LOS			A			
Intersection Summary						
Average Delay			5.6			
Intersection Capacity Utilization			18.1%	ICU Level of Service	A	
Analysis Period (min)			15			

3885 SANDWICH STREET  
BAIRDAE

















Synchro 10 Report  
Page 3

# HCM Unsignalized Intersection Capacity Analysis

2022 TOTAL PM TRAFFIC

1: Chappell Ave & Sandwich St

05-26-2022

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	5	0	61	13	0	10	34	381	29	16	463	8
Future Volume (Veh/h)	5	0	61	13	0	10	34	381	29	16	463	8
Sign Control		Stop			Stop			Free			Free	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	5	0	66	14	0	11	37	414	32	17	503	9
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type								None			None	
Median storage (veh)												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	1056	1062	508	1112	1050	430	512			446		
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	1056	1062	508	1112	1050	430	512			446		
tC, single (s)	7.1	6.5	6.7	7.1	6.5	6.5	4.9			4.3		
tC, 2 stage (s)												
tF (s)	3.5	4.0	3.8	3.5	4.0	3.5	2.9			2.3		
p0 queue free %	97	100	86	91	100	98	95			98		
cM capacity (veh/h)	191	211	481	154	214	579	756			1044		
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	71	25	483	529								
Volume Left	5	14	37	17								
Volume Right	66	11	32	9								
cSH	434	228	756	1044								
Volume to Capacity	0.16	0.11	0.05	0.02								
Queue Length 95th (m)	4.6	2.9	1.2	0.4								
Control Delay (s)	14.9	22.8	1.4	0.5								
Lane LOS	B	C	A	A								
Approach Delay (s)	14.9	22.8	1.4	0.5								
Approach LOS	B	C										
Intersection Summary												
Average Delay			2.3									
Intersection Capacity Utilization			48.9%	ICU Level of Service					A			
Analysis Period (min)			15									










3885 SANDWICH STREET  
BAIRDAE

Synchro 10 Report  
Page 1



HCM Unsignalized Intersection Capacity Analysis  
7: Sandwich St & Fut Acc 1

2022 TOTAL PM TRAFFIC  
05-26-2022

						
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Volume (veh/h)	17	9	370	27	14	471
Future Volume (Veh/h)	17	9	370	27	14	471
Sign Control	Stop		Free			Free
Grade	0%		0%			0%
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	18	10	402	29	15	512
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type			None			None
Median storage (veh)						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	958	416			431	
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	958	416			431	
tC, single (s)	6.4	6.2			4.1	
tC, 2 stage (s)						
tF (s)	3.5	3.3			2.2	
p0 queue free %	94	98			99	
cM capacity (veh/h)	282	636			1139	
Direction, Lane #	WB 1	NB 1	SB 1			
Volume Total	28	431	527			
Volume Left	18	0	15			
Volume Right	10	29	0			
cSH	352	1700	1139			
Volume to Capacity	0.08	0.25	0.01			
Queue Length 95th (m)	2.1	0.0	0.3			
Control Delay (s)	16.1	0.0	0.4			
Lane LOS	C		A			
Approach Delay (s)	16.1	0.0	0.4			
Approach LOS	C					
Intersection Summary						
Average Delay			0.7			
Intersection Capacity Utilization			46.1%	ICU Level of Service		A
Analysis Period (min)			15			

3885 SANDWICH STREET  
BAIRDAE

Synchro 10 Report  
Page 2




# HCM Unsignalized Intersection Capacity Analysis

## 16: Chappell Ave & Fut Acc 2

2022 TOTAL PM TRAFFIC

05-26-2022



















Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Traffic Volume (veh/h)	24	21	8	3	2	15
Future Volume (Veh/h)	24	21	8	3	2	15
Sign Control		Free	Free		Stop	
Grade		0%	0%		0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	26	23	9	3	2	16
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type		None	None			
Median storage veh						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	12				86	10
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	12				86	10
tC, single (s)	4.1				6.5	6.5
tC, 2 stage (s)						
tF (s)	2.2				3.6	3.5
p0 queue free %	98				100	98
cM capacity (veh/h)	1607				891	1007
Direction, Lane #	EB 1	WB 1	SB 1			
Volume Total	49	12	18			
Volume Left	26	0	2			
Volume Right	0	3	16			
cSH	1607	1700	993			
Volume to Capacity	0.02	0.01	0.02			
Queue Length 95th (m)	0.4	0.0	0.4			
Control Delay (s)	3.9	0.0	8.7			
Lane LOS	A		A			
Approach Delay (s)	3.9	0.0	8.7			
Approach LOS			A			
Intersection Summary						
Average Delay		4.4				
Intersection Capacity Utilization		19.1%		ICU Level of Service		A
Analysis Period (min)		15				

# HCM Unsignalized Intersection Capacity Analysis

2027 TOTAL AM TRAFFIC

1: Chappell Ave & Sandwich St

05-26-2022










												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	12	2	44	26	0	13	41	430	19	11	305	5
Future Volume (Veh/h)	12	2	44	26	0	13	41	430	19	11	305	5
Sign Control		Stop			Stop			Free			Free	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	13	2	48	28	0	14	45	467	21	12	332	5
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type								None			None	
Median storage veh												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	940	936	334	975	928	478	337			488		
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	940	936	334	975	928	478	337			488		
tC, single (s)	7.4	6.5	6.8	7.2	6.5	6.2	4.7			4.1		
tC, 2 stage (s)												
tF (s)	3.8	4.0	3.8	3.6	4.0	3.3	2.8			2.2		
p0 queue free %	94	99	92	85	100	98	95			99		
cM capacity (veh/h)	203	252	591	192	254	592	952			1086		
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	63	42	533	349								
Volume Left	13	28	45	12								
Volume Right	48	14	21	5								
cSH	411	247	952	1086								
Volume to Capacity	0.15	0.17	0.05	0.01								
Queue Length 95th (m)	4.3	4.8	1.2	0.3								
Control Delay (s)	15.3	22.5	1.3	0.4								
Lane LOS	C	C	A	A								
Approach Delay (s)	15.3	22.5	1.3	0.4								
Approach LOS	C	C										
Intersection Summary												
Average Delay			2.8									
Intersection Capacity Utilization			53.9%	ICU Level of Service					A			
Analysis Period (min)			15									

3885 SANDWICH STREET  
BAIRDAE

Synchro 10 Report  
Page 1

HCM Unsignalized Intersection Capacity Analysis  
7: Sandwich St & Fut Acc 1

2027 TOTAL AM TRAFFIC  
05-26-2022

						
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Volume (veh/h)	34	17	440	15	7	287
Future Volume (Veh/h)	34	17	440	15	7	287
Sign Control	Stop		Free			Free
Grade	0%		0%			0%
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	37	18	478	16	8	312
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type			None			None
Median storage (veh)						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	814	486			494	
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	814	486			494	
tC, single (s)	6.4	6.2			4.1	
tC, 2 stage (s)						
tF (s)	3.5	3.3			2.2	
p0 queue free %	89	97			99	
cM capacity (veh/h)	345	581			1080	
Direction, Lane #	WB 1	NB 1	SB 1			
Volume Total	55	494	320			
Volume Left	37	0	8			
Volume Right	18	16	0			
cSH	398	1700	1080			
Volume to Capacity	0.14	0.29	0.01			
Queue Length 95th (m)	3.8	0.0	0.2			
Control Delay (s)	15.5	0.0	0.3			
Lane LOS	C		A			
Approach Delay (s)	15.5	0.0	0.3			
Approach LOS	C					
Intersection Summary						
Average Delay			1.1			
Intersection Capacity Utilization			34.1%	ICU Level of Service		A
Analysis Period (min)			15			

3885 SANDWICH STREET  
BAIRDAE

Synchro 10 Report  
Page 2

# HCM Unsignalized Intersection Capacity Analysis

## 16: Chappell Ave & Fut Acc 2

2027 TOTAL AM TRAFFIC

05-26-2022



Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↕	↕		↕	
Traffic Volume (veh/h)	13	15	10	2	4	30
Future Volume (Veh/h)	13	15	10	2	4	30
Sign Control		Free	Free		Stop	
Grade		0%	0%		0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	14	16	11	2	4	33
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type		None	None			
Median storage veh						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	13				56	12
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	13				56	12
tC, single (s)	4.1				6.5	6.5
tC, 2 stage (s)						
tF (s)	2.2				3.6	3.5
p0 queue free %	99				100	97
cM capacity (veh/h)	1606				933	1005
Direction, Lane #	EB 1	WB 1	SB 1			
Volume Total	30	13	37			
Volume Left	14	0	4			
Volume Right	0	2	33			
cSH	1606	1700	997			
Volume to Capacity	0.01	0.01	0.04			
Queue Length 95th (m)	0.2	0.0	0.9			
Control Delay (s)	3.4	0.0	8.7			
Lane LOS	A		A			
Approach Delay (s)	3.4	0.0	8.7			
Approach LOS			A			
Intersection Summary						
Average Delay			5.3			
Intersection Capacity Utilization			18.2%	ICU Level of Service		A
Analysis Period (min)			15			

3885 SANDWICH STREET  
BAIRDAE

Synchro 10 Report  
Page 3



















# HCM Unsignalized Intersection Capacity Analysis

2027 TOTAL PM TRAFFIC

1: Chappell Ave & Sandwich St

05-26-2022

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	6	0	70	14	0	11	39	431	31	17	527	10
Future Volume (Veh/h)	6	0	70	14	0	11	39	431	31	17	527	10
Sign Control	Stop				Stop				Free			
Grade	0%				0%				0%			
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	7	0	76	15	0	12	42	468	34	18	573	11
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type							None			None		
Median storage veh												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	1196	1200	578	1260	1189	485	584				502	
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	1196	1200	578	1260	1189	485	584				502	
tC, single (s)	7.1	6.5	6.7	7.1	6.5	6.5	4.9				4.3	
tC, 2 stage (s)												
tF (s)	3.5	4.0	3.8	3.5	4.0	3.5	2.9				2.3	
p0 queue free %	95	100	83	87	100	98	94				98	
cM capacity (veh/h)	152	172	436	116	175	538	705				994	
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	83	27	544	602								
Volume Left	7	15	42	18								
Volume Right	76	12	34	11								
cSH	376	178	705	994								
Volume to Capacity	0.22	0.15	0.06	0.02								
Queue Length 95th (m)	6.6	4.2	1.5	0.4								
Control Delay (s)	17.3	28.9	1.6	0.5								
Lane LOS	C	D	A	A								
Approach Delay (s)	17.3	28.9	1.6	0.5								
Approach LOS	C	D										
Intersection Summary												
Average Delay			2.7									
Intersection Capacity Utilization			54.7%		ICU Level of Service				A			
Analysis Period (min)			15									

3885 SANDWICH STREET  
BAIRDAE










Synchro 10 Report  
Page 1

# HCM Unsignalized Intersection Capacity Analysis

## 7: Sandwich St & Fut Acc 1

2027 TOTAL PM TRAFFIC

05-26-2022

						
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Volume (veh/h)	17	9	421	27	14	536
Future Volume (Veh/h)	17	9	421	27	14	536
Sign Control	Stop		Free			Free
Grade	0%		0%			0%
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	18	10	458	29	15	583
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type			None			None
Median storage veh)						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	1086	472			487	
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	1086	472			487	
tC, single (s)	6.4	6.2			4.1	
tC, 2 stage (s)						
tF (s)	3.5	3.3			2.2	
p0 queue free %	92	98			99	
cM capacity (veh/h)	236	592			1086	
Direction, Lane #	WB 1	NB 1	SB 1			
Volume Total	28	487	598			
Volume Left	18	0	15			
Volume Right	10	29	0			
cSH	301	1700	1086			
Volume to Capacity	0.09	0.29	0.01			
Queue Length 95th (m)	2.4	0.0	0.3			
Control Delay (s)	18.2	0.0	0.4			
Lane LOS	C		A			
Approach Delay (s)	18.2	0.0	0.4			
Approach LOS	C					
Intersection Summary						
Average Delay			0.7			
Intersection Capacity Utilization			49.5%	ICU Level of Service		A
Analysis Period (min)			15			

3885 SANDWICH STREET  
BAIRDAE

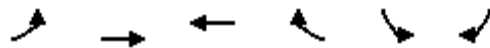
Synchro 10 Report  
Page 2




# HCM Unsignalized Intersection Capacity Analysis

## 16: Chappell Ave & Fut Acc 2

2027 TOTAL PM TRAFFIC

05-26-2022



















Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Traffic Volume (veh/h)	24	24	10	3	2	15
Future Volume (Veh/h)	24	24	10	3	2	15
Sign Control		Free	Free		Stop	
Grade		0%	0%		0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	26	26	11	3	2	16
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type		None	None			
Median storage veh)						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	14				90	12
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	14				90	12
tC, single (s)	4.1				6.5	6.5
tC, 2 stage (s)						
tF (s)	2.2				3.6	3.5
p0 queue free %	98				100	98
cM capacity (veh/h)	1604				886	1005
Direction, Lane #	EB 1	WB 1	SB 1			
Volume Total	52	14	18			
Volume Left	26	0	2			
Volume Right	0	3	16			
cSH	1604	1700	990			
Volume to Capacity	0.02	0.01	0.02			
Queue Length 95th (m)	0.4	0.0	0.4			
Control Delay (s)	3.7	0.0	8.7			
Lane LOS	A		A			
Approach Delay (s)	3.7	0.0	8.7			
Approach LOS			A			
Intersection Summary						
Average Delay			4.2			
Intersection Capacity Utilization			19.3%	ICU Level of Service		A
Analysis Period (min)			15			

# HCM Unsignalized Intersection Capacity Analysis

2032 TOTAL AM TRAFFIC

1: Chappell Ave & Sandwich St

05-26-2022










												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	14	3	49	27	0	13	46	481	20	11	339	5
Future Volume (Veh/h)	14	3	49	27	0	13	46	481	20	11	339	5
Sign Control		Stop			Stop			Free			Free	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	15	3	53	29	0	14	50	523	22	12	368	5
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type								None			None	
Median storage (veh)												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	1042	1040	370	1083	1031	534	373			545		
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	1042	1040	370	1083	1031	534	373			545		
tC, single (s)	7.4	6.5	6.8	7.2	6.5	6.2	4.7			4.1		
tC, 2 stage (s)												
tF (s)	3.8	4.0	3.8	3.6	4.0	3.3	2.8			2.2		
p0 queue free %	91	99	91	82	100	97	95			99		
cM capacity (veh/h)	170	217	562	157	220	550	920			1034		
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	71	43	595	385								
Volume Left	15	29	50	12								
Volume Right	53	14	22	5								
cSH	362	205	920	1034								
Volume to Capacity	0.20	0.21	0.05	0.01								
Queue Length 95th (m)	5.7	6.1	1.4	0.3								
Control Delay (s)	17.4	27.2	1.4	0.4								
Lane LOS	C	D	A	A								
Approach Delay (s)	17.4	27.2	1.4	0.4								
Approach LOS	C	D										
Intersection Summary												
Average Delay			3.1									
Intersection Capacity Utilization			59.8%		ICU Level of Service				B			
Analysis Period (min)			15									

3885 SANDWICH STREET  
BAIRDAE

Synchro 10 Report  
Page 1

HCM Unsignalized Intersection Capacity Analysis  
7: Sandwich St & Fut Acc 1

2032 TOTAL AM TRAFFIC  
05-26-2022

						
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Volume (veh/h)	34	17	493	15	7	321
Future Volume (Veh/h)	34	17	493	15	7	321
Sign Control	Stop		Free			Free
Grade	0%		0%			0%
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	37	18	536	16	8	349
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type			None			None
Median storage (veh)						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	909	544			552	
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	909	544			552	
tC, single (s)	6.4	6.2			4.1	
tC, 2 stage (s)						
tF (s)	3.5	3.3			2.2	
p0 queue free %	88	97			99	
cM capacity (veh/h)	303	539			1028	
Direction, Lane #	WB 1	NB 1	SB 1			
Volume Total	55	552	357			
Volume Left	37	0	8			
Volume Right	18	16	0			
cSH	354	1700	1028			
Volume to Capacity	0.16	0.32	0.01			
Queue Length 95th (m)	4.4	0.0	0.2			
Control Delay (s)	17.0	0.0	0.3			
Lane LOS	C		A			
Approach Delay (s)	17.0	0.0	0.3			
Approach LOS	C					
Intersection Summary						
Average Delay			1.1			
Intersection Capacity Utilization			36.9%	ICU Level of Service		A
Analysis Period (min)			15			

3885 SANDWICH STREET  
BAIRDAE

Synchro 10 Report  
Page 2



# HCM Unsignalized Intersection Capacity Analysis

## 16: Chappell Ave & Fut Acc 2

2032 TOTAL AM TRAFFIC

05-26-2022



Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↶	↶		↶	
Traffic Volume (veh/h)	13	16	11	2	4	30
Future Volume (Veh/h)	13	16	11	2	4	30
Sign Control		Free	Free		Stop	
Grade		0%	0%		0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	14	17	12	2	4	33
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type		None	None			
Median storage (veh)						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	14				58	13
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	14				58	13
tC, single (s)	4.1				6.5	6.5
tC, 2 stage (s)						
tF (s)	2.2				3.6	3.5
p0 queue free %	99				100	97
cM capacity (veh/h)	1604				931	1004
Direction, Lane #	EB 1	WB 1	SB 1			
Volume Total	31	14	37			
Volume Left	14	0	4			
Volume Right	0	2	33			
cSH	1604	1700	996			
Volume to Capacity	0.01	0.01	0.04			
Queue Length 95th (m)	0.2	0.0	0.9			
Control Delay (s)	3.3	0.0	8.8			
Lane LOS	A		A			
Approach Delay (s)	3.3	0.0	8.8			
Approach LOS			A			
Intersection Summary						
Average Delay			5.2			
Intersection Capacity Utilization			18.2%	ICU Level of Service		A
Analysis Period (min)			15			

3885 SANDWICH STREET  
BAIRDAE

















Synchro 10 Report  
Page 3

# HCM Unsignalized Intersection Capacity Analysis

2032 TOTAL PM TRAFFIC

05-26-2022

## 1: Chappell Ave & Sandwich St










												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	7	0	79	14	0	11	44	481	33	18	590	11
Future Volume (Veh/h)	7	0	79	14	0	11	44	481	33	18	590	11
Sign Control	Stop			Stop			Free			Free		
Grade	0%			0%			0%			0%		
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	8	0	86	15	0	12	48	523	36	20	641	12
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type							None			None		
Median storage (veh)												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	1336	1342	647	1410	1330	541	653				559	
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	1336	1342	647	1410	1330	541	653				559	
tC, single (s)	7.1	6.5	6.7	7.1	6.5	6.5	4.9				4.3	
tC, 2 stage (s)												
tF (s)	3.5	4.0	3.8	3.5	4.0	3.5	2.9				2.3	
p0 queue free %	93	100	78	82	100	98	93				98	
cM capacity (veh/h)	120	139	396	85	142	499	658				946	
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	94	27	607	673								
Volume Left	8	15	48	20								
Volume Right	86	12	36	12								
cSH	331	135	658	946								
Volume to Capacity	0.28	0.20	0.07	0.02								
Queue Length 95th (m)	9.2	5.7	1.9	0.5								
Control Delay (s)	20.2	38.2	2.0	0.6								
Lane LOS	C	E	A	A								
Approach Delay (s)	20.2	38.2	2.0	0.6								
Approach LOS	C	E										
Intersection Summary												
Average Delay			3.2									
Intersection Capacity Utilization			60.1%	ICU Level of Service					B			
Analysis Period (min)			15									

# HCM Unsignalized Intersection Capacity Analysis

## 7: Sandwich St & Fut Acc 1

2032 TOTAL PM TRAFFIC

05-26-2022

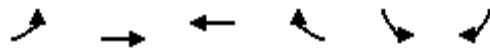
						
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Volume (veh/h)	17	9	473	27	14	601
Future Volume (Veh/h)	17	9	473	27	14	601
Sign Control	Stop		Free			Free
Grade	0%		0%			0%
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	18	10	514	29	15	653
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type			None			None
Median storage (veh)						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	1212	528			543	
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	1212	528			543	
tC, single (s)	6.4	6.2			4.1	
tC, 2 stage (s)						
tF (s)	3.5	3.3			2.2	
p0 queue free %	91	98			99	
cM capacity (veh/h)	198	550			1036	
Direction, Lane #	WB 1	NB 1	SB 1			
Volume Total	28	543	668			
Volume Left	18	0	15			
Volume Right	10	29	0			
cSH	257	1700	1036			
Volume to Capacity	0.11	0.32	0.01			
Queue Length 95th (m)	2.9	0.0	0.4			
Control Delay (s)	20.7	0.0	0.4			
Lane LOS	C		A			
Approach Delay (s)	20.7	0.0	0.4			
Approach LOS	C					
Intersection Summary						
Average Delay		0.7				
Intersection Capacity Utilization		52.9%	ICU Level of Service	A		
Analysis Period (min)		15				




3885 SANDWICH STREET  
BAIRDAE

Synchro 10 Report  
Page 2

# HCM Unsignalized Intersection Capacity Analysis 16: Chappell Ave & Fut Acc 2

2032 TOTAL PM TRAFFIC  
05-26-2022



Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Traffic Volume (veh/h)	24	27	11	3	2	15
Future Volume (Veh/h)	24	27	11	3	2	15
Sign Control		Free	Free		Stop	
Grade		0%	0%		0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	26	29	12	3	2	16
Pedestrians						
Lane Width (m)						
Walking Speed (m/s)						
Percent Blockage						
Right turn flare (veh)						
Median type		None	None			
Median storage veh						
Upstream signal (m)						
pX, platoon unblocked						
vC, conflicting volume	15				94	14
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	15				94	14
tC, single (s)	4.1				6.5	6.5
tC, 2 stage (s)						
tF (s)	2.2				3.6	3.5
p0 queue free %	98				100	98
cM capacity (veh/h)	1603				881	1003
Direction, Lane #	EB 1	WB 1	SB 1			
Volume Total	55	15	18			
Volume Left	26	0	2			
Volume Right	0	3	16			
cSH	1603	1700	988			
Volume to Capacity	0.02	0.01	0.02			
Queue Length 95th (m)	0.4	0.0	0.4			
Control Delay (s)	3.5	0.0	8.7			
Lane LOS	A		A			
Approach Delay (s)	3.5	0.0	8.7			
Approach LOS			A			
Intersection Summary						
Average Delay		4.0				
Intersection Capacity Utilization		19.4%		ICU Level of Service		A
Analysis Period (min)		15				

3885 SANDWICH STREET  
BAIRDAE

Synchro 10 Report  
Page 3

## Appendix D

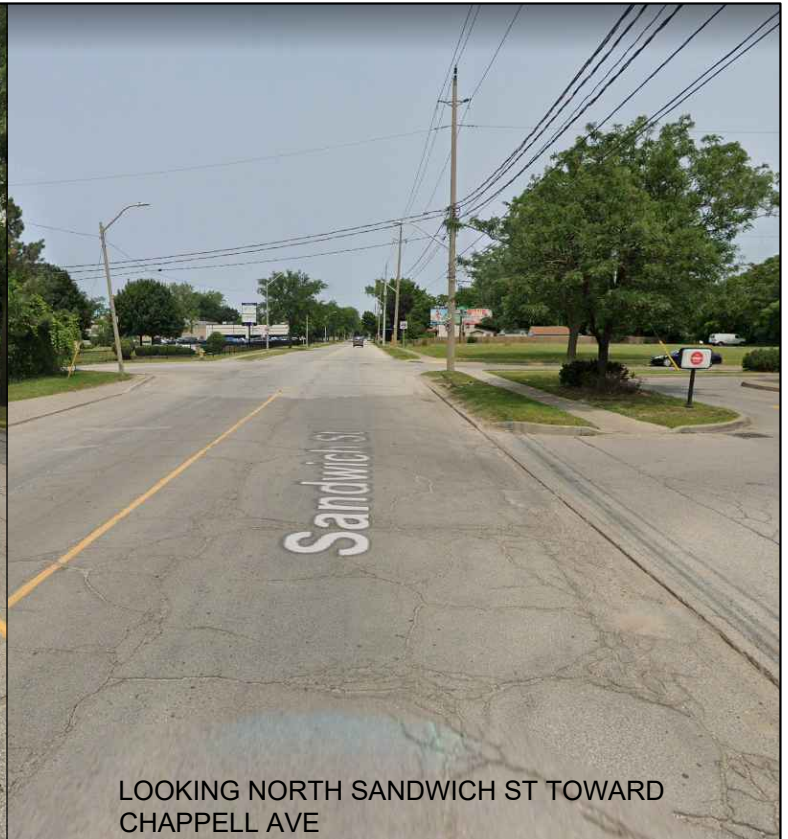
---

### SITE PLANS, WARRANTS AND PHOTOS





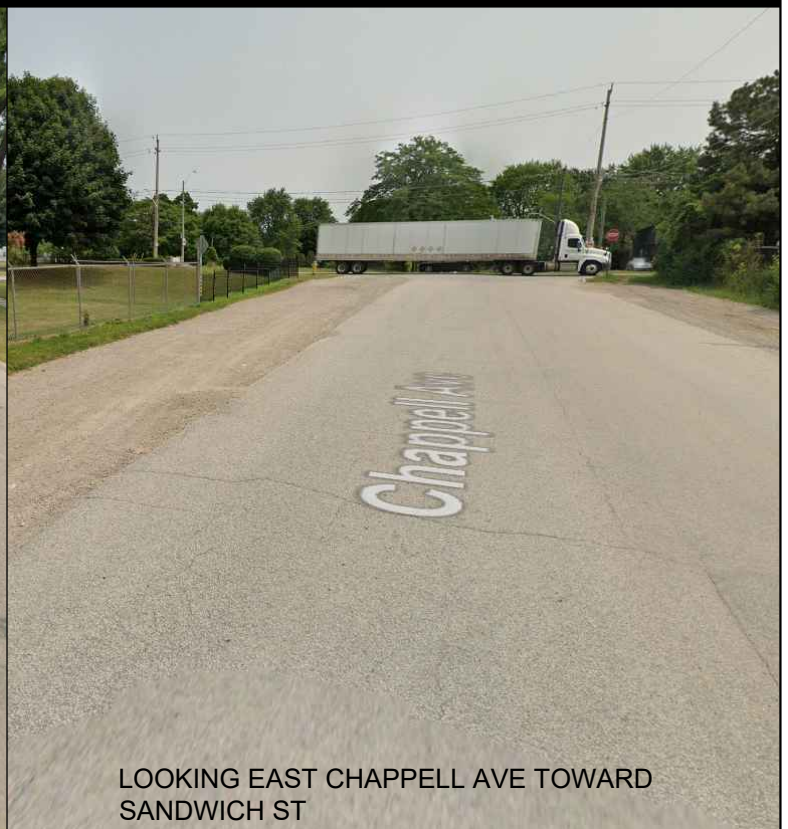
LOOKING SOUTH SANDWICH ST TOWARD  
CHAPPELL AVE



LOOKING NORTH SANDWICH ST TOWARD  
CHAPPELL AVE



LOOKING WEST CHAPPELL AVE TOWARD  
SANDWICH ST



LOOKING EAST CHAPPELL AVE TOWARD  
SANDWICH ST

<p>PROJECT TITLE:</p> <p><b>HIGH RISE DEVELOPMENT 3885 SANDWICH STREET, WINDSOR ON</b></p>	<p>DATE:</p> <p>OCT 5, 2020</p>	 <p>27 PRINCESS STREET, SUITE #102 LEAMINGTON, ONTARIO N8H 2X8</p> <p>1000 - 267 PELLISSIER STREET, WINDSOR, ONTARIO N9A 4E4</p>
<p>SHEET TITLE:</p> <p><b>PHOTOS</b></p>	<p>SCALE:</p> <p>NTS</p>	
	<p>PROJECT NO:</p> <p>20-028</p> <p>FIGURE NO:</p> <p>EXHIBIT 3</p>	



# Canadian Traffic Signal Warrant Analysis

## Main Street Side Street

MainStreet1Lanes  
MainStreet2Lanes  
MainStreet.LT.Lanes  
SideStreet1Lanes  
SideStreet2Lanes  
MainStreetSpeedLimit  
MainStreetTrucks/Buses  
Refuge Width on Median

(#)  
(#)  
(#)  
(#)  
(#)  
(km/h)  
(%)  
(m)

1
1
0
1
1
50
5.0%
0.0

←  
→  
↓  
↑

Distance to next signal (m)  
Elementary School (y/n)  
Senior's Complex (y/n)  
Pathway to School (y/n)  
Metro Area Population (#)  
Side Street Bus Route (y/n)  
Side Street Trucks (%)  
T or 1-Way Intersection (y/n)  
Central Business District (y/n)

550
n
n
n
1,000
n
5.0%
n
n

Date: May 26, 2022

City: City of Windsor

Vm = 1,040 (MainSt Vol Total)  
Vs = 76 (SideSt Vol Highest)  
Pc = 0 Peds Crossing Main  
K1 = 1,100 veh/veh const  
K2 = 2,000 veh/ped const  
L = 2.0 TotalMainStLanes  
F = 1.000 (PedDemoFactor)  
Vm1 = 1,040 (MainStVeh-Veh#)  
Cvp = 1.206 (product of Cs,Cmt,Cv,Cp)  
Ct1 = 1.000 T Int / one way Factor

Cs = 1.005 (Int SpacingFactor)  
Cmt = 1.000 (MainStTruckFactor)  
Cv = 1.000 (SpeedFactor)  
Cp = 1.200 (PopDemoFactor)  
Csb = 1.000 (SideStBusFactor)  
Cst = 1.000 (SideStTruckFactor)  
Vmx = 553 (MainStHighest)  
Vm2 = 1,040 (MainStVeh-Ped#)  
Cbt = 1.000 (maximum of Csb,Cst)

	←			→			↓			↑				
	MS1LT	MS1TH	MS1RT	MS2LT	MS2TH	MS2RT	SS1LT	SS1TH	SS1RT	SS2LT	SS2TH	SS2RT	PedC1	PedC2
7:00 - 8:00	46	481	20	11	339	5	27	0	13	14	2	49	0	0
8:00 - 9:00	46	481	20	11	339	5	27	0	13	14	2	49	0	0
11:00 - 12:00	46	481	20	11	339	5	27	0	13	14	2	49	0	0
12:00 - 13:00	44	481	33	18	590	11	14	0	11	7	0	79	0	0
16:00 - 17:00	44	481	33	18	590	11	14	0	11	7	0	79	0	0
17:00 - 18:00	44	481	33	18	590	11	14	0	11	7	0	79	0	0
Average	45	481	27	15	465	8	21	0	12	11	1	64	0	0

\*\*\* Enter the hourly turning movement counts averaged over the peak six hours of a typical week day

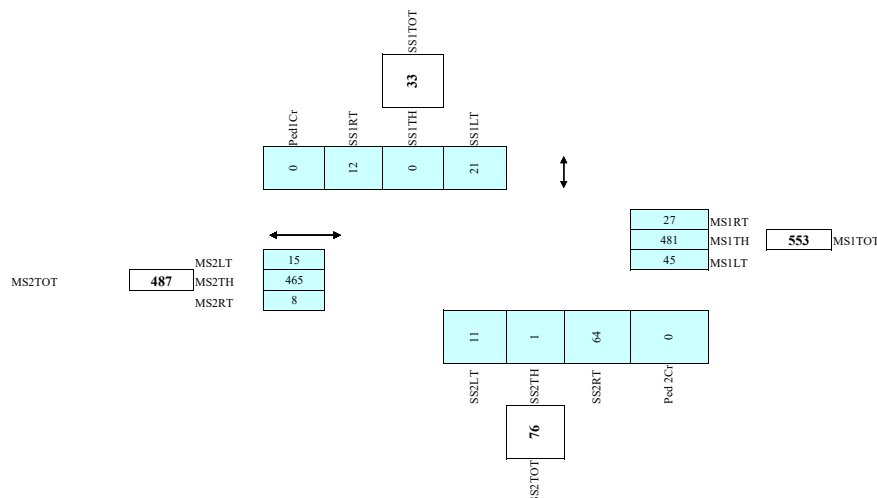
\*\*\* Enter the peak pedestrian volume crossing the main street averaged over the same hours

$$W = [Ct1xCbt(Vm1 \times Vs)/K1 + (F(Vm2 \times Pc)L)/K2] \times Cvp$$

$$W = 86 \quad 86 \quad 0$$

NOT Warranted

Veh Ped



Roadway, Vehicle and Pedestrian Factors		Range			
		Min	@	Max	@
Cs =	(Int SpacingFactor)	0.90	<200 m	1.10	isolated
Cmt =	(MainStTruckFactor)	1.00	<5%	1.15	>20%
Cv =	(SpeedFactor)	1.00	<60 km/h	1.10	>80 km/h
Cp =	(PopDemoFactor)	1.00	>250,000	1.20	<10,000
Csb =	(SideStBusFactor)	1.00	no	1.05	yes
Cst =	(SideStTruckFactor)	1.00	<10%	1.05	>10%
F =	(Ped DemoFactor)				
	(max of )				
	Elementary School	1.20			
	Seniors Complex	1.10			
	Path to School	1.10			

### Explanation of Factors:

**Cbt** = 1.05 if the side street either is a bus route, or has more than 10% trucks, otherwise = 1.00.  
(it is assumed that these two factors only affect the side street vehicles trying to cross the main street, not the pedestrians)

**Ci** = the product of the other 4 geographic factors  
(Cs = intersection spacing, Cmt = main street truck, Cv = Speed, Cp = Population)

**Vm1** = the main street volume - either the total of the two approaches or the highest single approach  
(if the median is >=10.0 metres) (averaged over 6 peak hours)

**Vm2** = the main street volume - either the total of the two approaches or the highest single approach  
(if the median is >=6.0 metres) (averaged over 6 peak hours)

**Vs** = the highest side street approach volume (averaged over 6 peak hours)

\*\*\* note: it has been determined that Vs must be > 75 for signals to be considered \*\*\*

**F** = Pedestrian demographic factor - the maximum of the 3 individual pedestrian demographic factors

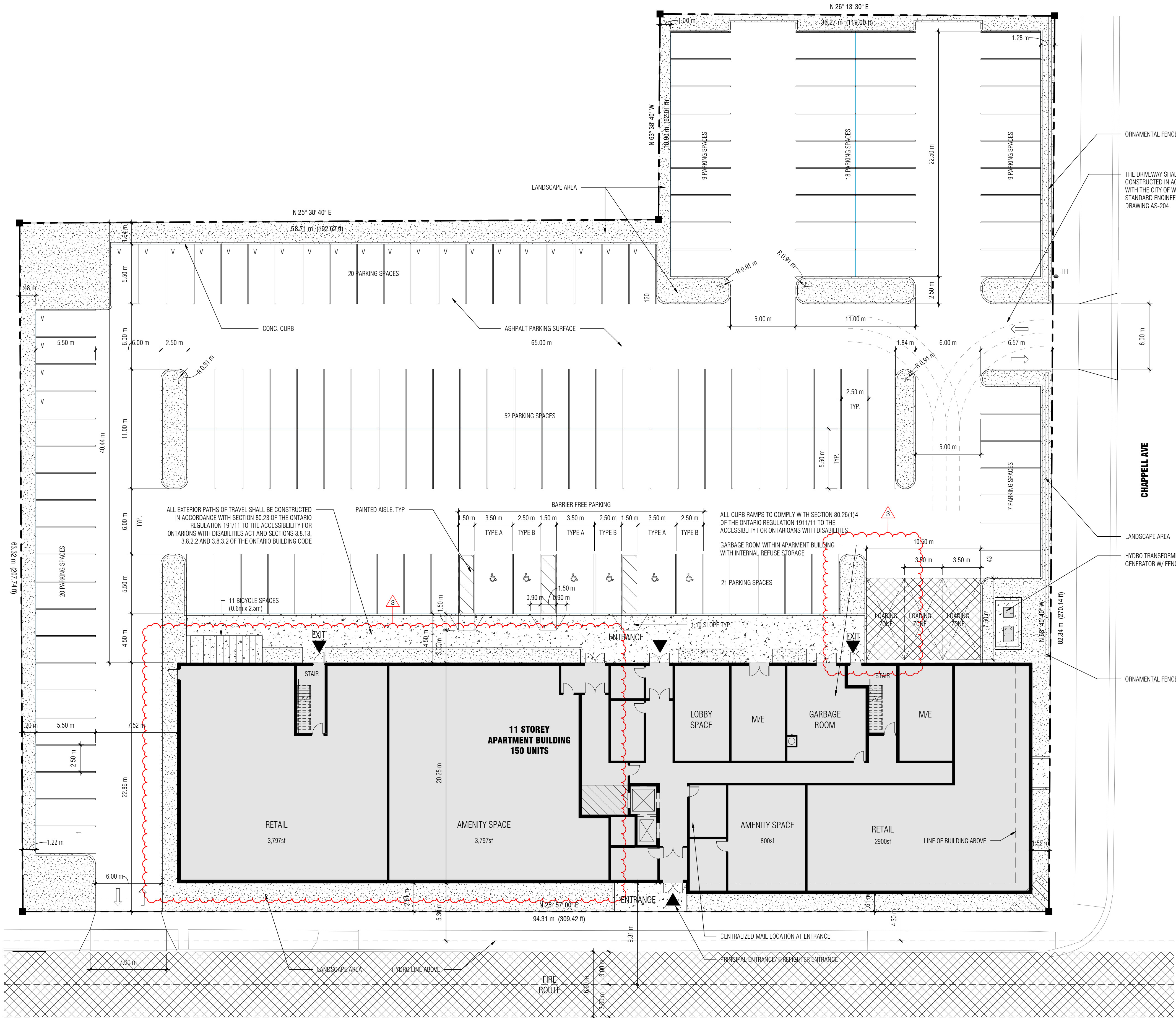
**Pc** = the total pedestrian volume crossing the mainstreet  
(averaged over 6 peak hours)

**L** = number of lanes that the pedestrians have to cross  
(only half the street if the median is >=5.0 metres)

**Kv** = Vehicle - Vehicle denominator constant  
(Kv = 1,100 if L<=3, Kv = 1,400 if L >3)

**Kp** = Vehicle - Pedestrian denominator constant  
(Kp = 2,000 if L<=3, Kp = 5,000 if L >3)





SITE DATA MATRIX			OBC REFERENCE
PROJECT DESCRIPTION: 11 STOREY MULTI UNIT RESIDENTIAL BUILDING 150 RESIDENTIAL UNITS			<input checked="" type="checkbox"/> NEW <input type="checkbox"/> ADDITION <input type="checkbox"/> ALTERATION <input type="checkbox"/> CHANGE OF USE
ZONING DESIGNATION: CD2.1			<input checked="" type="checkbox"/> PART 3 <input type="checkbox"/> PART 9 <input type="checkbox"/> PART 11
MAJOR OCCUPANCY: RESIDENTIAL			
BUILDING CLASSIFICATION: GROUP C			
SITE AREA		BUILDING AREA	GROSS AREA
EXISTING:	6,694m <sup>2</sup>	EXISTING:	N/A
PROPOSED:	6,694m <sup>2</sup>	PROPOSED:	1,622m <sup>2</sup>
TOTAL:	6,694m <sup>2</sup>	TOTAL:	19,122.04m <sup>2</sup>
LOT COVERAGE		MINIMUM LOT WIDTH	BUILDING HEIGHT
MAXIMUM:	N/A	REQUIRED:	N/A
PROVIDED:	1,623m <sup>2</sup>	PROVIDED:	94.8m
MINIMUM FRONT YARD DEPTH		MINIMUM REAR YARD DEPTH	MINIMUM SIDE YARD DEPTH
MAXIMUM:	N/A	REQUIRED:	N/A
PROVIDED:	1.61m	PROVIDED:	40.44m
PARKING		BICYCLE SPACES	LOADING SPACES
USE CLASSIFICATION		EXISTING:	0
EXISTING:	0	PROPOSED:	11
PROPOSED:	156 (6 BF, 3 TYPE A & 3 TYPE B) 1.04 RATIO	REQUIRED:	11
REQUIRED:	188 (6 BF, 3 TYPE A & 3 TYPE B) 1.25 RATIO	TOTAL:	11
TOTAL:	156 (6 BF, 3 TYPE A & 3 TYPE B) 1.04 RATIO	PROPOSED:	3
LANDSCAPE AREA		AMENITY SPACE	SCREENING FENCE LENGTH
EXISTING:	N/A	REQUIRED:	900m <sup>2</sup>
PROPOSED:	868m <sup>2</sup>	PROPOSED:	1294m <sup>2</sup>
TOTAL:	868.56m <sup>2</sup>	TOTAL:	N/A
NOTE: 1. ALL EXTERIOR PATHS OF TRAVEL SHALL BE CONSTRUCTED IN ACCORDANCE WITH SECTION 80.23 OF ONTARIO REGULATION 191/11 TO THE ACCESSIBILITY FOR ONTARIANS WITH DISABILITIES ACT AND SECTIONS 3.8.1.3 AND 3.8.3.2 OF THE ONTARIO BUILDING CODE. 2. ALL CURB RAMP SHALL BE CONSTRUCTED IN ACCORDANCE WITH SECTION 80.26(1) OF THE ONTARIO REGULATION 191/11 TO THE ACCESSIBILITY FOR ONTARIANS WITH DISABILITIES ACT AND SECTION 3.8.3.1.8 OF THE ONTARIO BUILDING CODE.			

102 - 27 Princess Street  
Leamington ON N8H 2X8

BAIRDIAE

architecture + engineering

1000 - 267 Pelissier Street  
Windsor ON N9A 4K4

A DIVISION OF MATTHEW J. BAIRD CONSULTING SERVICES INC.

3 SITE REVISIONS 2 22.05.25

2 SITE REVISIONS 21.06.06

1 ISSUED FOR OPA/ZBA/SPC 20.11.09

No. Description Date

Revision Schedule

SEAL

ONTARIO ASSOCIATION  
OF  
ARCHITECTS  
PAC. WEED  
LICENCE  
7289

MIXED-USE DEVELOPMENT

SANDWICH ST., WINDSOR ON

SHEET TITLE

20-028

SP-1

2022-05-25 3:23:07 PM

GENERAL NOTES:  
1. This drawing is NOT to be scaled.  
2. This drawing, as an instrument of service, is provided by and is the property of Baird AE.  
3. The contractor must verify and accept responsibility for all dimensions and conditions on site and must notify Baird AE of any variation from the supplied information.  
4. This disclaimer is not responsible for the accuracy of survey, and the other disciplines information shown on this drawing. Refer to the appropriate consultant's drawings before proceeding with the work.  
5. Construction must conform to all applicable codes and requirements of authorities having jurisdiction.  
6. The contractor working from drawings not specifically marked For Construction must assume full responsibility and bear costs for any corrections or damages resulting from his work.

Consolidated Council Agenda - June 13, 2022  
Page 743 of 1025

**Originally submitted at June 6, 2022  
Development & Heritage Standing Committee  
Revisions from Applicant**

**From:** Alexander, Kevin <kalexander@citywindsor.ca>  
**Sent:** June 2, 2022 9:39 AM  
**To:** Ciacelli, Anna <aciacelli@citywindsor.ca>; Toldo, Beth <toldob@citywindsor.ca>  
**Subject:** FW: S 65/2022\_3885 and 0 Sandwich St.

On May 31<sup>st</sup>, 2022, the applicant provided a revised Parking Justification Report and Traffic Impact Study to further support a reduction in required parking for Official Plan and Zoning By-law amendments regarding the proposed Combined Use Building located at 3885 and 0 Sandwich Street. The attached studies include revisions to the reports identified as Appendix 'N' and 'O' of Report S 65/2022.

Generally, the changes include the following which **do not have any bearing on the recommendations** of Report S 65/2022.

Parking Justification Report

- On page 4 under Proposed Condition the amount of commercial area is corrected from 2900 ft<sup>2</sup> to 6697 ft<sup>2</sup>.
- Revisions are included on page 5 regarding addressing Zoning By-law 8600 related to required parking spaces. The additions discuss how the site is serviced by Transit and other alternative modes of transportation (biking and walking), which encourages resident's and visitors to use alternative modes of transportation other than automobile use.
- Revisions also discuss how car share spaces will be provided and that there is a reduction in the demand for commercial spaces in the evening, which will allow residents to use their spaces upon mutual agreement between residents and commercial units.
- The last paragraph of Pages 5 to 7 discuss Parking demand based on the Institution of Transportation Engineering (ITE) and how the number of parking spaces is calculated based on the ITE manual and how if this methodology is considered that actually deficient number of parking spaces is only 15.

Traffic Impact Study

- On page 2 under Proposed Development the amount of commercial area is corrected from 2900 ft<sup>2</sup> to 6697 ft<sup>2</sup>.
- The traffic analysis was revised to include the trips from the new proposal; there is a relatively small increase in trip generation due to the changes from the original proposal. The conclusions remain that no off-site changes are required to accommodate this development.

**Originally submitted at June 6, 2022  
Development & Heritage Standing Committee  
Written Submission**

**From:** Cheryl Jordan  
**Sent:** May 24, 2022 10:50 AM  
**To:** clerks <[clerks@citywindsor.ca](mailto:clerks@citywindsor.ca)>  
**Subject:** June 6 2022 Hearing - Development & Heritage Standing Committee

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear City of Windsor Development & Heritage Standing Committee,

We are property owners located across the street from the proposed development located at 0 & 3885 Sandwich St which is requesting a site specific amendment to the City of Windsor Official Plan and Site Specific Amendment to Zoning By-law 8600 to allow for a mixed-use residential development. We object to and disagree with placing a residential development in this industrial area. There is truck traffic and industrial noise in this area, which is not favorable to residential use.

Cheryl Verran Jordan



**Originally submitted at June 6, 2022  
Development & Heritage Standing Committee Meeting  
Written Submission**

**From:** Kevin Kelly  
**Sent:** May 25, 2022 1:07 PM  
**To:** clerks <[clerks@citywindsor.ca](mailto:clerks@citywindsor.ca)>  
**Cc:** Alexander, Kevin <[kalexander@citywindsor.ca](mailto:kalexander@citywindsor.ca)>; Grant Bourdeau ; S Kelly ; Cheryl Kelly ; Roxanne Boow ; Kevin Kelly  
**Subject:** File Numbers OPA/6504 and ZNG/6503

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

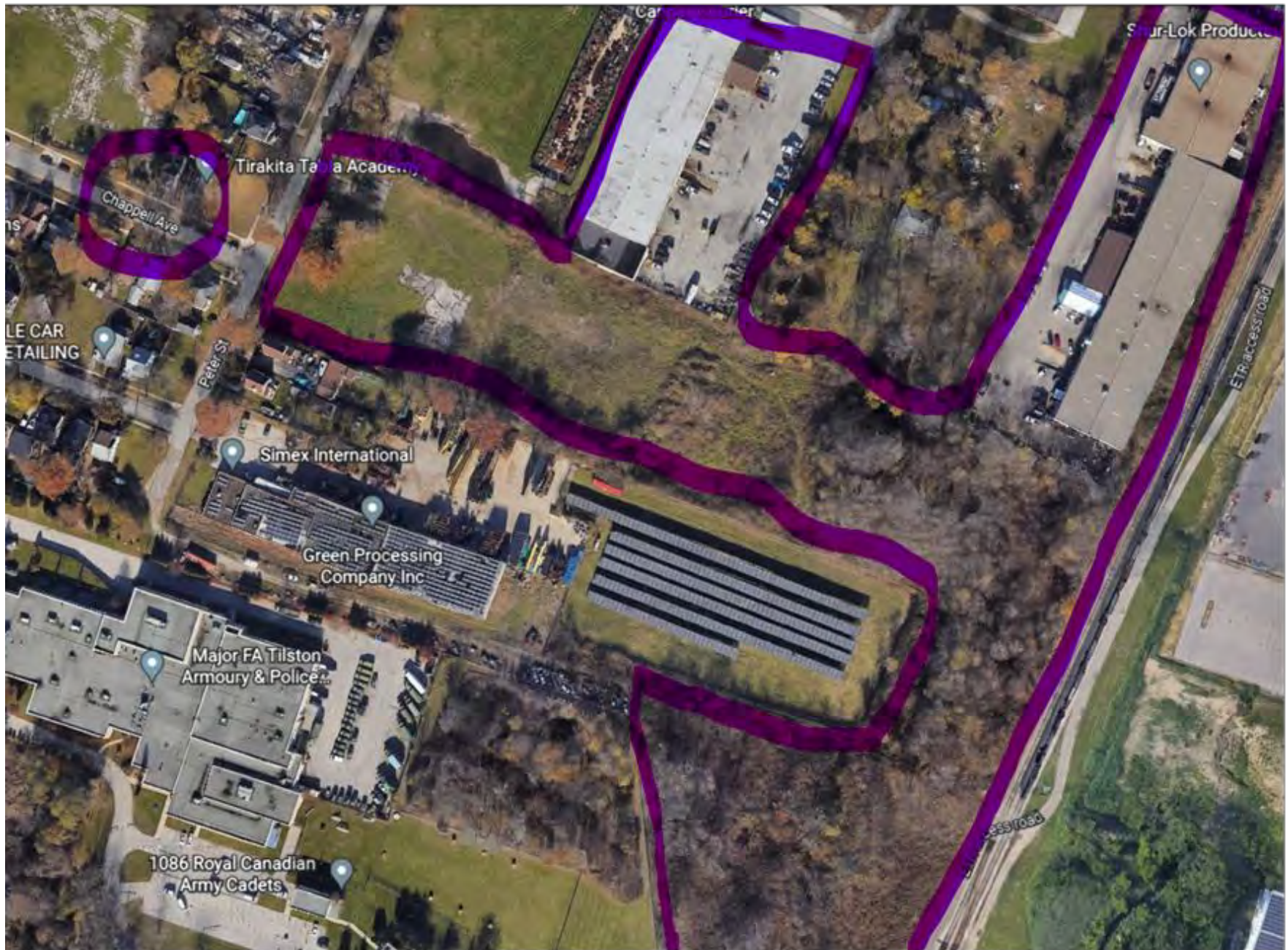
City Clerk

As president of Kelbour managements I wish to express my concerns to the zoning bylaw as per Subject and attached changes.

The area outlined on the below is our current industrial properties owned by Kelbour Management.

Our major concern is that the addition of residential traffic will be an issue for trucks entering our 3901 Peter St property, that is zoned industrial and is currently up for sale as an industrial usage property. If Chappell rd becomes closed to Truck traffic due to this influx of residential usage it will severely hamper, if not make it untenable, the ability to do an industrial business at this location and we want assurances this will not happen at any time, now or in the future due to the Tunnio Development proposal or any other reason which may be a future consideration.

Kevin Kelly  
Shurlok Products LTD  
735 Prince Rd. Windsor, On. Canada  
N9C 2Z2







**Originally submitted at June 6, 2022**  
**Development & Heritage Standing Committee**  
**Response to Kevin Kelly's email**

**From:** Toufeili, Rania <RToufeili@citywindsor.ca>  
**Sent:** June 3, 2022 12:50 PM  
**To:** Alexander, Kevin <kalexander@citywindsor.ca>  
**Cc:** Hagan, Jeff <jhagan@citywindsor.ca>  
**Subject:** 3885 Sandwich - Development Truck Question

Hi Kevin,

I wanted to follow up on our conversation earlier today for the property at 3901 Peter Street that is zoned industrial and currently up for sale.

There will be no changes to the roadway in terms of classification and it would not prohibit trucks. We will not be closing Chappell Street or limiting access to the road, the property at 3901 Peter St can still use this segment of Chappell Ave to get to and from their property. There is no parking allowed on one side of the road so the remaining available width of the roadway will still be able to accommodate trucks.

The intersection of Chappell Ave and Sandwich St will still operate at an acceptable level of service and there will be no significant traffic impact as a result of this development in the area.

Thank you

**Rania Toufeili, EIT | Policy Analyst**



Transportation Planning Services  
350 City Hall Square | Suite 210 | Windsor, ON | N9A 7K6  
(519)-255-6543 ext. 6830  
[www.citywindsor.ca](http://www.citywindsor.ca)



**Committee Matters: SCM 152/2022**

**Subject: Draft Plan of Condominium with Exemption under Section 9(3) of the Condominium Act – St. Clair Rhodes Development – 233 Watson Avenue – Ward 6**

Moved by: Member Gyemi  
Seconded by: Councillor Morrison

Decision Number: **DHSC 400**

THAT the application of St. Clair Rhodes Development for an exemption under Section 9(3) of the *Condominium Act* for approval of a plan of condominium (Standard Condominium), comprised of a total of 24 dwelling units, as shown on the attached Map No. CDM-001/22-1 and CDM-001/22-2 on a parcel legally described as; Part of Lot 129, Concession 1, and Part of Closed Alley, Registered Plan 895; more particularly described as Parts 1 to 4, 12R-25008; City of Windsor; located at 233 Watson Avenue **BE APPROVED** for a period of three (3) years.

Carried.

Report Number: S 55/2022  
Clerk's File: Z/14381

**Clerk's Note:**

1. The recommendation of the Standing Committee and Administration are the same.
2. Please refer to Item 7.4. from the Development & Heritage Standing Committee Meeting held June 6, 2022.
3. To view the stream of this Standing Committee meeting, please refer to:  
<http://csg001-harmony.sliq.net/00310/Harmony/en/PowerBrowser/PowerBrowserV2/20220608/-1/7334>



**Subject: Draft Plan of Condominium with Exemption under Section 9(3) of the Condominium Act – St. Clair Rhodes Development – 233 Watson Avenue – Ward 6**

**Reference:**

Date to Council: June 6, 2022  
Author: Jim Abbs  
Senior Planner - Subdivisions  
519 255 6543 x6317  
jabbs@citywindsor.ca

Planning & Building Services  
Report Date: May 5, 2022  
Clerk's File #: Z/14381

**To:** Mayor and Members of City Council

**Recommendation:**

**THAT** the application of St. Clair Rhodes Development for an exemption under Section 9(3) of the Condominium Act for approval of a plan of condominium (Standard Condominium), comprised of a total of 24 dwelling units, as shown on the attached Map No. CDM-001/22-1 and CDM-001/22-2 on a parcel legally described as; Part of Lot 129, Concession 1, and Part of Closed Alley, Registered Plan 895; more particularly described as Parts 1 to 4, 12R-25008; City of Windsor; located at 233 Watson Avenue **BE APPROVED** for a period of three (3) years.

**Executive Summary:**

N/A

**Background:**

**Application Information:**

**Location:** West side of Watson Avenue, South of Riverside Drive

**Ward:** 6                                      **Planning District:** 19- Riverside                                      **ZDM:** 10

**Applicant:** St. Clair Rhodes Development (Sheila Luno)

## Proposal:

The applicant is applying for an exemption under Section 9(3) of the *Condominium Act* for approval of a plan of condominium for in an existing Building to create 24 dwelling units.

The subject site has received Site Plan Approval (File AMT-008/20) registered as instrument CE966039, on September 29, 2020 which permitted a total of 24 dwelling units within newly constructed building. The Site Plan agreement covers a range of municipal and agency requirements to be completed by the owner including items such as the provision of landscaping, parking, parkland conveyance, levies and lighting, all required prior to the issuance of a construction permit.

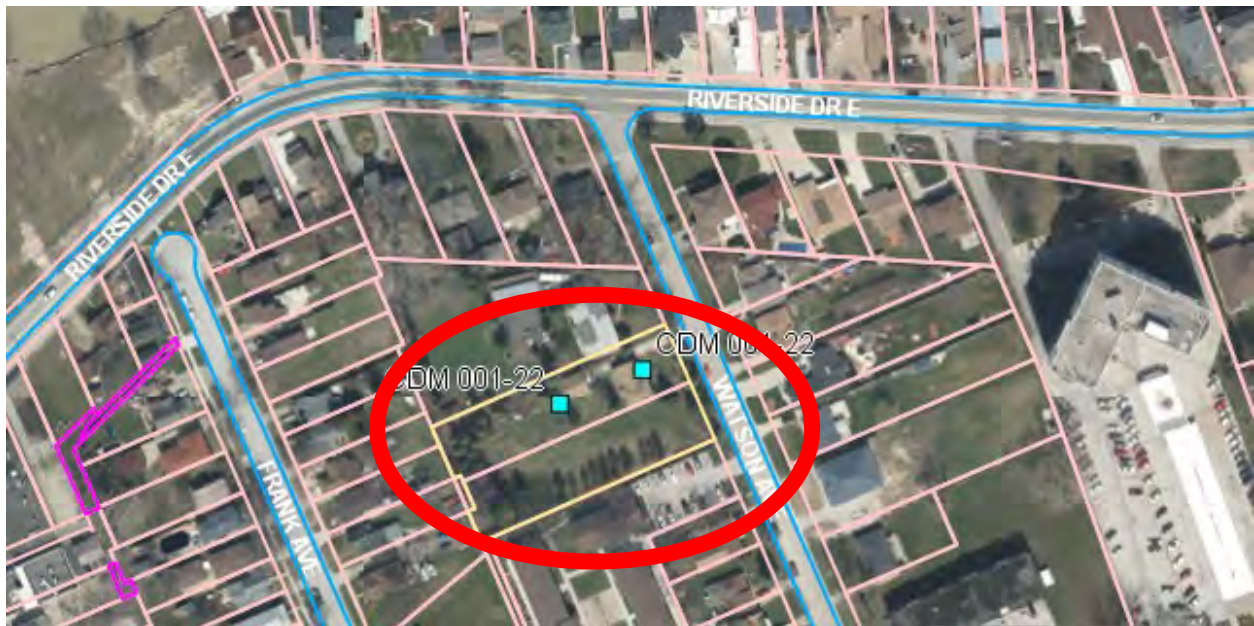
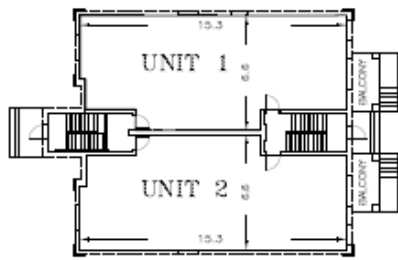


Figure 1: Location Map

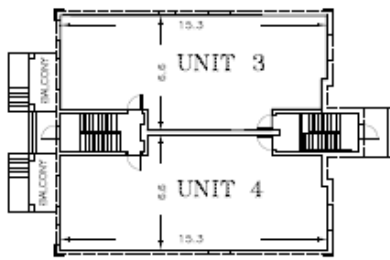
## Plan of Condominium:



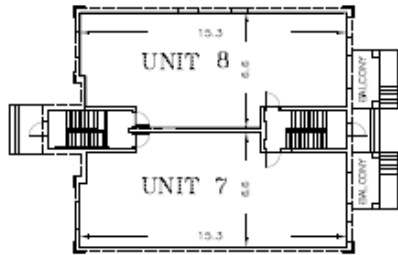
CDM-006/21-1



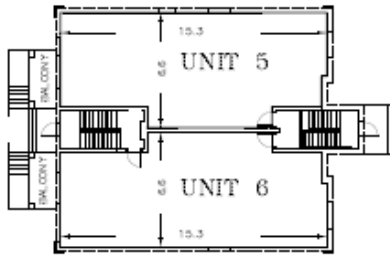
UNITS 1 and 2  
LEVEL 2  
BUILDING 1



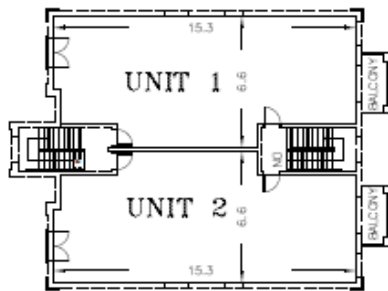
UNITS 3 and 4  
LEVEL 2  
BUILDING 2



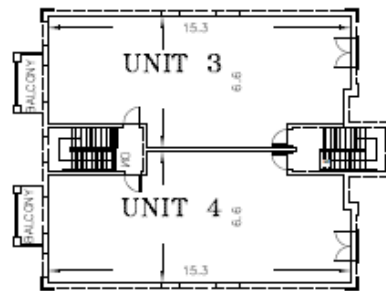
UNITS 7 & 8  
LEVEL 2  
BUILDING 4



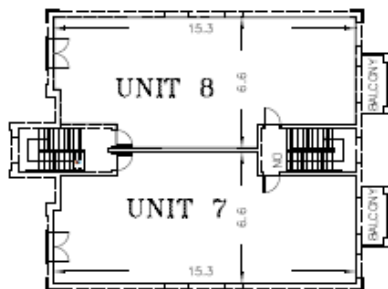
UNITS 5 and 6  
LEVEL 2  
BUILDING 3



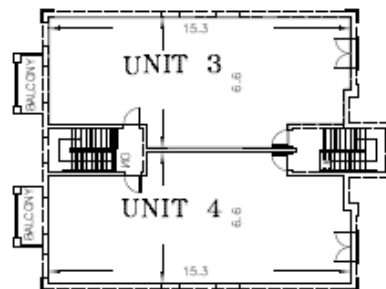
UNITS 1 and 2  
LEVEL 3  
BUILDING 1



UNITS 3 and 4  
LEVEL 3  
BUILDING 2



UNITS 7 & 8  
LEVEL 3  
BUILDING 4



UNITS 5 and 6  
LEVEL 3  
BUILDING 3

CDM-001/22-2

**Site Information:**

OFFICIAL PLAN	ZONING	CURRENT USE	PREVIOUS USE
Residential (Schedule D-Land Use)	Residential District 3.1 (RD3.1)	Multiple Dwelling (Under Construction)	Single Detached Dwelling/Vacant Parcel
LOT WIDTH	LOT DEPTH	AREA	SHAPE
47.2m	86.7m	3686 m <sup>2</sup>	rectangular
All measurements are for the entire parcel and are approximate.			

**Neighbourhood Characteristics:**

The surrounding land uses consist of a mix of residential uses including single detached, double duplex and townhome dwellings.

Watson Avenue is a Local Road. Public transit is available via the Crosstown 2, on Riverside Drive.

**Discussion:****Planning Analysis:****Statutory Regulations:**

Under Section 9 of the Condominium Act, an owner may request approval of a plan of condominium subject to Section 51 of the Planning Act (subdivisions). As such, the usual approval process for plans of subdivision is invoked, i.e. review by municipal and provincial agencies, public notification, draft plan approval, a condominium agreement and final registration.

The Condominium Act also provides that owners can be exempted from the above mentioned Planning Act provision if the approval authority (i.e. the City of Windsor) is of the opinion that "such exemption is appropriate in the circumstances". The reasons for exemptions are not specified, but usually applicants can be exempted if the following conditions are satisfied:

- (i) that all municipal requirements and conditions have been addressed (for example by an approved application for rezoning and/or site plan control); and
- (ii) that the building(s) is suitable for a condominium by virtue of design and amenities.

The proposed condominium complies with the above conditions.



**Official Plan:**

The subject property is designated “Residential” on Schedule D of the City of Windsor Official Plan. The designation is intended to accommodate residential uses. The proposed development conforms with this policy.

The City of Windsor has policies in the Official Plan for approval of a plan of condominium with exemption under Section 9 of the Condominium Act. Applications for exemptions may be considered by Council if:

- (a) a residential building is constructed or a building permit for its construction has been issued;
- (b) the development has received site plan control approval; and
- (c) the development (new proposed units) do not contain any occupied residential rental units.

The proposed condominium complies with the above conditions.

**Zoning By-Law**

The property is zoned Residential District (RD) 3.1. The proposed development is permitted under the RD3.1 category. Building permits have been issued and construction of the building is nearing completion.

**Risk Analysis:**

N/A

**Climate Change Risks****Climate Change Mitigation:**

N/A

**Climate Change Adaptation:**

N/A

**Financial Matters:**

N/A

**Consultations:**

Municipal and agency requirements have been addressed and implemented through the site plan control approval process, and the registration of a site plan agreement. (File AMT-008/20) registered as instrument CE966039, on September 29, 2020)

**Public Notice:**

No public notification is required where exemptions are requested and the development is properly zoned for the permitted use. Nevertheless, notice was mailed to all property owners within 120 metres (400 feet) and notice was also given in the Windsor Star.

**Conclusion:**

The application has been processed and evaluated with regard to both the Planning Act and the Condominium Act, as well as the City of Windsor Official Plan. It conforms to the City of Windsor Official Plan and complies with the zoning regulations contained in Bylaw 8600. Municipal requirements regarding this development have been addressed in the site plan control agreement. The draft plan of condominium is consistent with the approved site plan (File AMT-008/20 registered as instrument CE966039, on September 29, 2020)

It is recommended that this application for approval be exempted from Section 51 of the Planning Act (per Section 9(3) of the Condominium Act). This means that the owner can proceed directly to registration following submission of an approved final plan of condominium.

**Planning Act Matters:**

I concur with the above comments and opinion of the Registered Professional Planner.

*Michael Cooke MCIP RPP, Manager of Planning Policy/Deputy City Planner*

*Thom Hunt, City Planner*

I am not a registered Planner and have reviewed as a Corporate Team Leader

*JP SAH*

**Approvals:**

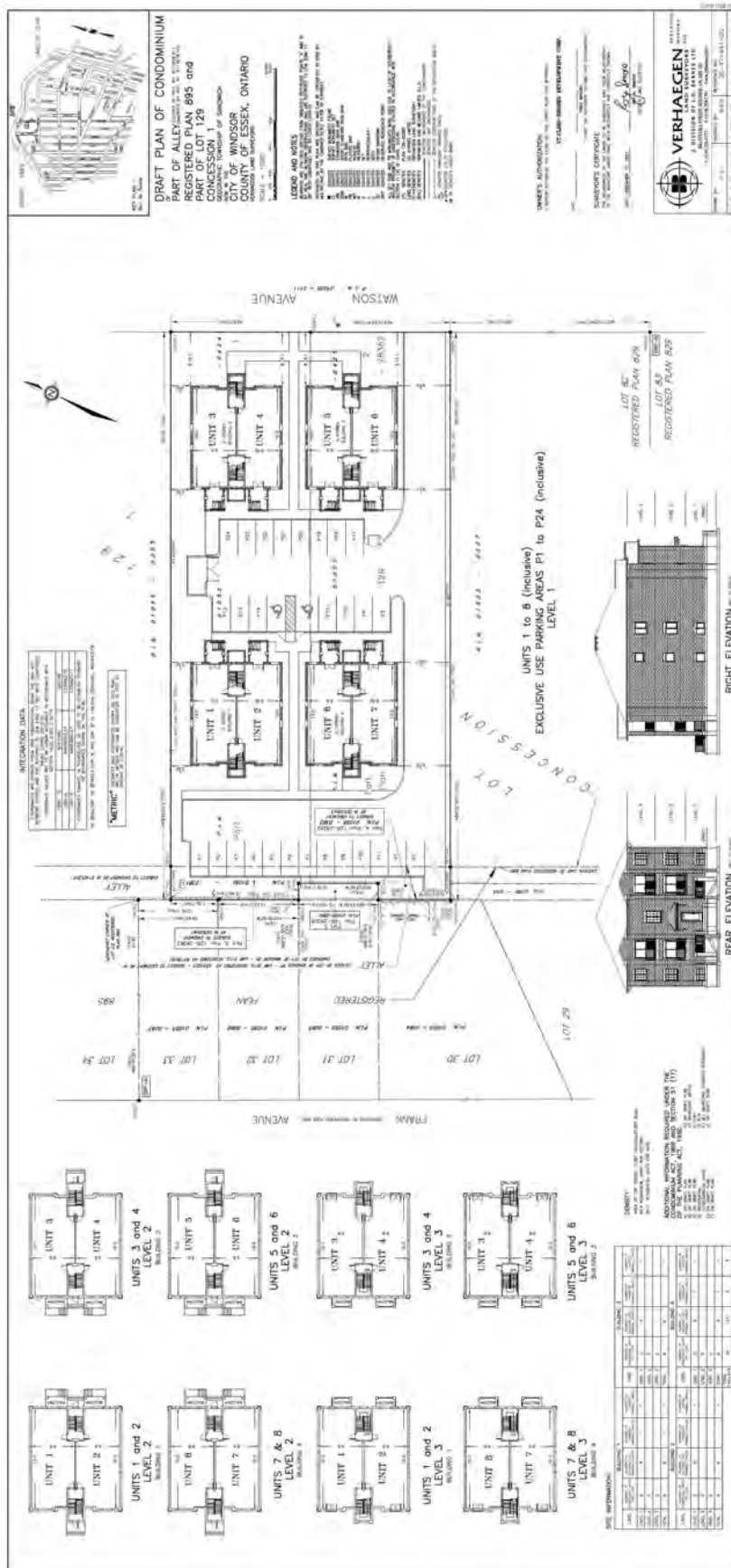
Name	Title
Michael Cooke	Manager of Planning Policy/Deputy City Planner
Thom Hunt	City Planner / Executive Director, Planning & Development Services
Dana Paladino	Acting Commissioner, Legal & Legislative Services
Jelena Payne	Commissioner of Economic Development and Innovation
Shelby Askin Hager	Acting Chief Administrative Officer

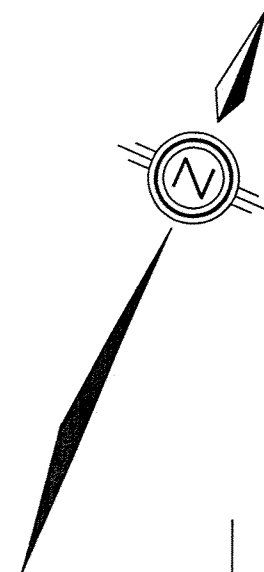
**Notifications:**

<b>Name</b>	<b>Address</b>	<b>Email</b>
St. Clair-Rhodes Development Corp. (Sheila Luno)	3235 Electricity Drive Windsor ON N8W 5J1	sheila@midsouth.ca
McTague Law Firm LLP (Brian Chillman)	455 Pelissier Street Windsor ON N9A 6Z9	bchillman@mctaguelaw.com

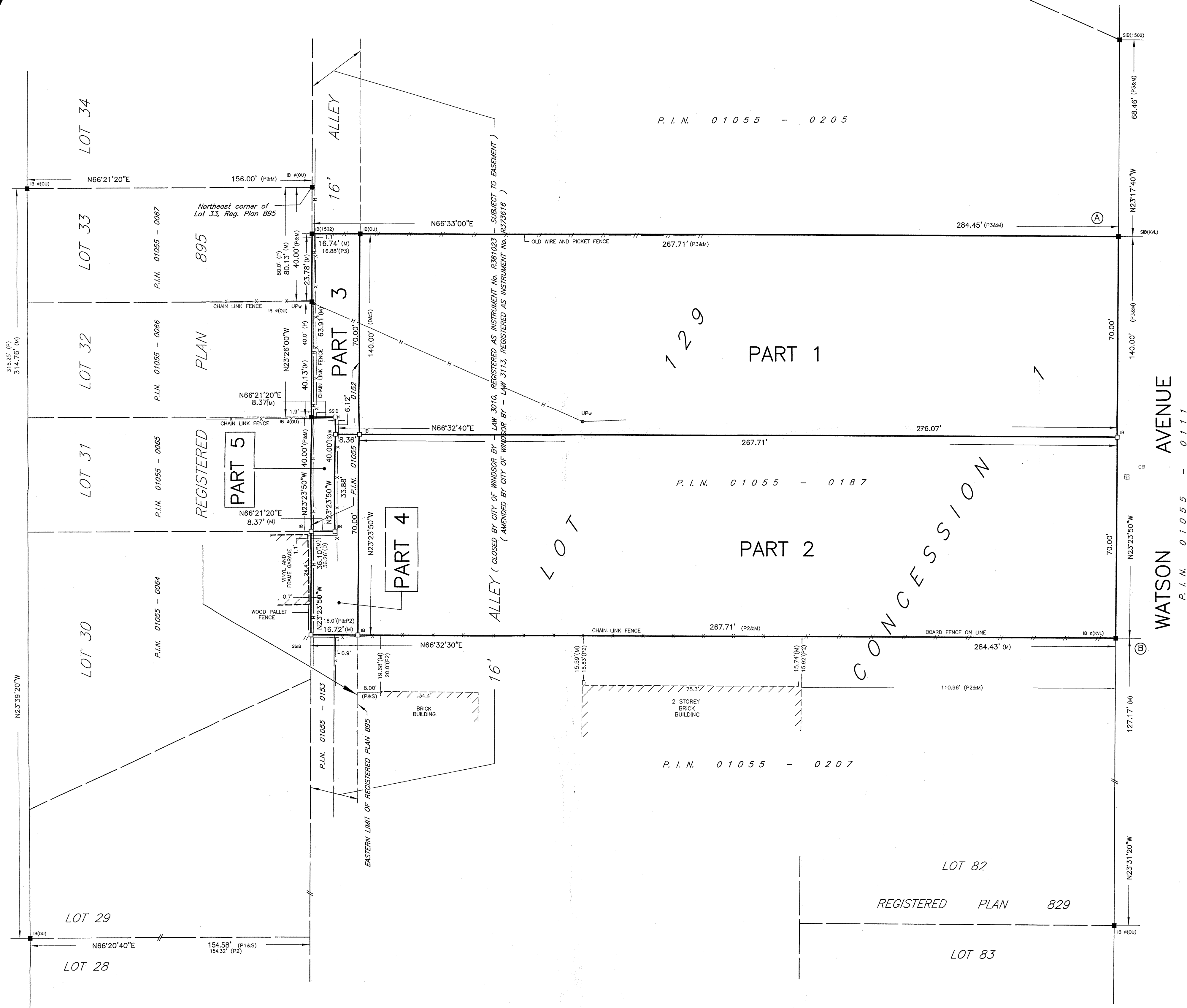
**Appendices:**

- 1 Proposed Draft Plan of Condominium
- 2 12R-25008





FRANK ( 66' WIDE PER REGISTERED PLAN 895 ) AVENUE



PARTS 1 AND 2 - COMPRISE ALL OF P.I.N. 01055-0187  
PARTS 3, 4 AND 5 - COMPRISE ALL OF P.I.N. 01055-0152

PARTS SCHEDULE			
PART	AREA	DESCRIPTION	P.I.N.
1	0.430 Acres	PART OF LOT 129, CONCESSION 1	ALL OF 01055-0187
2	0.430 Acres		
3	0.026 Acres	PART OF ALLEY, REGISTERED PLAN 895 (CLOSED BY INST. No. R361023) (AMENDED BY INST. No. R373616)	ALL OF 01055-0152
4	0.020 Acres		
5	0.008 Acres		

I REQUIRE THIS PLAN TO BE DEPOSITED UNDER THE LAND TITLES ACT.  
DATE FEBRUARY 16, 2012  
ROY A. SIMONE  
ONTARIO LAND SURVEYOR

PLAN 12R-25008  
RECEIVED AND DEPOSITED  
DATE 2012 FEB 21  
BARBARA CAIRA  
REPRESENTATIVE FOR LAND REGISTRAR FOR THE LAND TITLES DIVISION OF ESSEX (12)

PLAN OF SURVEY  
OF  
PART OF ALLEY (CLOSED BY INST. No. R361023)  
REGISTERED PLAN 895 AND  
PART OF LOT 129  
CONCESSION 1  
GEOGRAPHIC TOWNSHIP OF SANDWICH  
NOW IN THE  
CITY OF WINDSOR  
COUNTY OF ESSEX, ONTARIO  
VERHAEGEN • STUBBERFIELD • HARTLEY • BREWER • BEZAIRE INC.  
SCALE : 1"=20'  
0 10.0 20.0 40.0 60.0 80.0 100.0  
FEET

"IMPERIAL" DISTANCES AND COORDINATES SHOWN ON THIS PLAN ARE IN FEET AND CAN BE CONVERTED TO METRES BY MULTIPLYING BY 0.3048

LEGEND AND NOTES  
BEARINGS ARE UTM GRID, DERIVED FROM SIMULTANEOUS GPS OBSERVATIONS ON MONUMENTS "A" AND "B", SHOWN HEREON, HAVING A GRID BEARING OF N23°23'50"W NAD83 (CSRS) (1997.0) AND ARE REFERRED TO THE CENTRAL MERIDIAN OF UTM ZONE 17 (81° WEST LONGITUDE).

DISTANCES ON THIS PLAN ARE GROUND AND CAN BE CONVERTED TO GRID BY MULTIPLYING BY THE COMBINED SCALE FACTOR OF 0.99989349

SIB DENOTES 1" X 1" X 4'-0" STANDARD IRON BAR  
SSIB DENOTES 1" X 1" X 2'-0" SHORT STANDARD IRON BAR  
IB DENOTES 5/8" X 5/8" X 2'-0" IRON BAR  
IB # DENOTES 3/4" diameter X 2'-0" ROUND IRON BAR  
CC DENOTES CUT-CROSS  
CP DENOTES 5mm X 50mm STEEL PIN  
■ DENOTES SURVEY MONUMENT FOUND  
■ DENOTES SURVEY MONUMENT SET AND MARKED 1528  
WIT. DENOTES WITNESS  
(S) DENOTES SET (M) DENOTES MEASURED (O) DENOTES DEED  
SSIB'S SHOWN ON THIS PLAN HAVE BEEN SET IN LIEU OF SIB'S WHERE THE POSSIBILITY THAT UNDERGROUND UTILITIES EXIST.  
(S/P) DENOTES SET PROPORTIONALLY (OU) DENOTES ORIGIN UNKNOWN  
(P) DENOTES REGISTERED PLAN 895  
(P1) DENOTES PLAN BY THE CITY OF WINDSOR PER ALLEY CLOSING BY-LAW 3010  
(P2) DENOTES PLAN OF SURVEY PER C.G.R. ARMSTRONG, OLS, Dated May 30, 1967, 3K-518  
(P3) DENOTES PLAN OF SURVEY PER 1528, Dated Jan. 25, 1996, Plan File B-2364  
(P4) DENOTES PLAN OF SURVEY PER (KVL), Dated Feb. 8, 1977, Plan File W77-L235  
(1502) DENOTES VERHAEGEN STUBBERFIELD HARTLEY BREWER BEZAIRE INC., O.L.S.  
(KVL) DENOTES VERHAEGEN AND BEZAIRE LIMITED, O.L.S.

INTEGRATION DATA		
COORDINATES ARE DERIVED FROM GPS OBSERVATIONS USING THE CAN-NET NETWORK SERVICE AND ARE REFERRED TO UTM ZONE 17 (81° WEST LONGITUDE) NAD83 (CSRS) (1997.0)		
COORDINATE VALUES ARE TO AN URBAN ACCURACY IN ACCORDANCE WITH SECTION 14(2) O. REG 216/10		
POINT ID	NORTHING	EASTING
A	N15385316.180	E1114789.357
B	N15385187.738	E1114844.933
COORDINATES CANNOT, IN THEMSELVES, BE USED TO RE-ESTABLISH CORNERS OR BOUNDARIES SHOWN ON THIS PLAN.		

SURVEYOR'S CERTIFICATE  
I CERTIFY THAT:  
1. THIS SURVEY AND PLAN ARE CORRECT AND IN ACCORDANCE WITH THE SURVEYS ACT, THE SURVEYORS ACT, THE LAND TITLES ACT AND THE REGULATIONS MADE UNDER THEM.  
2. THIS SURVEY WAS COMPLETED ON THE 18th DAY OF JANUARY, 2012

DATE FEBRUARY 16, 2012  
ROY A. SIMONE  
ONTARIO LAND SURVEYOR  
for VERHAEGEN • STUBBERFIELD • HARTLEY  
BREWER • BEZAIRE INC.

WINDSOR 475 Devonshire Road, Suite 200 N8Y 2L5 Ph: (519)258-1772 Fax: (519)258-1791

VERHAEGEN STUBBERFIELD HARTLEY BREWER BEZAIRE INC.

LEAMINGTON 187 Talbot Street East N8H 1L8 Ph: (519)322-2375 Fax: (519)322-2675

ONTARIO LAND SURVEYORS www.vshbbsurveys.com

DRAWN BY: NMG CAD Date: February 16, 2012 4:29:05 PM  
CHECKED BY: RAS CAD File: 42596000.dwg

WORK ORDER: 4-25960 FILE NO.: E-WINDSOR-1-129W PLAN FILE NO.: C-3723





**Committee Matters: SCM 157/2022**

**Subject: Approval of a Draft Plan of Subdivision for lands located on the south side of North Talbot Rd, between Southwood Lakes Blvd and HWY 401; File No. SDN-001/21[SDN/6575]; Applicant – Bellocorp Inc.; Ward 1**

Moved by: Councillor Holt  
Seconded by: Councillor Sleiman

Decision Number: **DHSC 402**

- I. THAT the application of Bellocorp Inc. for Draft Plan of Subdivision approval for Part Lot 306 and Part Lot 307, Concession N Talbot Rd., Sandwich East, Windsor (PIN 01558-0962 LT, PIN 01558-0544 LT and PIN 01558-0964 LT), **BE APPROVED** subject to the following conditions:
  - A. That the Draft Plan Approval shall lapse on \_\_\_\_\_ (3 years from the date of approval);
  - B. That this approval applies to the draft plan of subdivision identified on attached Map No. SDN-001/21-1, prepared by VERHAEGEN Land Surveyors for Bellocorp Inc., Reference No. 21-47-019-01, dated May 20, 2022, showing 33 Lots for single unit dwellings, 1 block for Stormwater Management Facility (SWM), 1 block for Parkland and 1 block for Reserve, plus proposed road allowances (Streets A, B & C);
  - C. That the Owner(s) shall enter into a subdivision agreement with the Corporation of the City of Windsor for the proposed development on the subject land;
  - D. That prior to the execution and registration of the subdivision agreement between the Owner(s) and the Corporation of the City of Windsor, the Owner(s) shall submit for approval of the City Planner/Executive Director of Planning & Building a final M plan; and
  - E. That the subdivision agreement between the Owner(s) and the Corporation of the City of Windsor be registered on title and shall contain, among other matters, the following provisions:
    1. The Owner(s) shall include all items as set out in the Results of Circulation (Appendix C, attached) with further amendments as required and other relevant matters set out in CR233/98 (Standard Subdivision Agreement).

2. That the Owner(s) shall create, prior to the issuance of a building permit, a 20m right-of-way for the new Street A, Street B and Street C, in accordance with the approved Plan of Subdivision.
3. The Owner(s) shall, prior to the issuance of a construction permit,
  - (i) Finalize the engineering analysis to identify stormwater quality and quantity measures as necessary to control any increase in flows in downstream watercourses, up to and including the 1:100 year design storm and in accordance with the Windsor-Essex Stormwater Management Standards Manual, to the satisfaction of the City Engineer and the Essex Region Conservation Authority;
  - (ii) install the stormwater management measures identified in the engineering analysis completed as part of the development for the site and undertake to implement the recommendations contained therein, to the satisfaction of the City Engineer and the Essex Region Conservation Authority;
  - (iii) obtain the necessary development review clearances from the Essex Region Conservation Authority prior to undertaking site alterations and/or construction activities of any kind; and
  - (iv) provide, to the Essex Region Conservation Authority, a copy of the fully executed subdivision agreement between the Owner(s) and the City of Windsor, containing provisions to carry out the recommendations of the final plans, reports and requirements noted in Recommendations I.E.4 (i) & (ii) above;
4. Servicing Study - Prior to the issuance of a construction permit, the Owner(s) shall at its own expense, retain a Consulting Engineer to provide a detailed servicing study report on the impact of the increased flow to the existing municipal sewer systems, satisfactory in content to the City Engineer. The Servicing study shall
  - (i) review the proposed impact and recommend solutions to address the problems and ultimate implementation of solutions should there be a negative impact to the municipal sewer system, and
  - (ii) be finalized in agreement with the City Engineer.
5. Site Servicing Plans – The Owner(s) shall submit a site servicing plan for the subject lands to the satisfaction of the Chief Building Official, the City Engineer, and ERCA in regulated areas, prior to the issuance of any construction permits for the subject lands.
6. Sidewalks -The Owner(s) shall pay to the Corporation
  - (i) the sum of \$33,500.00 being the Owner's contribution towards the future construction of a concrete sidewalk on the North Talbot Road frontage of the subject lands, prior to the issuance of a Building Permit; and

- (ii) the sum of \$16,500.00, being the Owner's contribution towards the future construction of a 3.0m wide multi-use trail connection from the Southwood Lakes Multi-use Trail, in Stoneybrook Park, to the proposed cul-de-sac at the western end of the proposed Street 'A', prior to the execution of a Subdivision Agreement.
- 7. Curbs and Gutters – The Owner(s) shall also pay to the Corporation, prior to the issuance of a construction permit, the sum of \$17,750.00 being the Owner's contribution towards the construction of concrete curb and gutter on the North Talbot Road frontage of the subject lands.
- 8. Corner Cut-Off - The Owner(s) shall, prior to the issuance of a construction permit, gratuitously convey a 4.6m x 4.6m (15' x 15'), corner cut-off at the intersection of North Talbot Road and Street 'B' as well as North Talbot Road and Street 'C' on Map No. SDN-001/21-1, in accordance with City of Windsor Standard Drawing AS-230.
- 9. Cul-De-Sac: The Owner(s) shall, prior to the issuance of construction permit, construct a cul-de-sac with a minimum radius of 9.5m at the west and east limits of Street A.
- 10. Stormwater Management Facility: The Owner(s) shall, prior to the issuance of a construction permit, gratuitously convey Block 34 on Map No. SDN-001/21-1 to the Corporation of the City of Windsor for Storm Water Management (SWM) Facility SWM purposes, to the satisfaction of the City Engineer and City Solicitor.
- 11. Berm Requirement - Prior to the issuance of a construction permit, the Owner(s) shall install a berm along the west, south and east limits of the plan of subdivision, as proposed in Appendix A attached to the Stormwater and Functional Servicing Report dated May 14, 2021, prepared by Bill Fuerth of BAIRD AE Inc., to the satisfaction of the City Engineer and the Essex Region Conservation Authority.
- 12. Fencing Requirement – Prior to the issuance of a construction permit, the Owner(s) shall install a 1.8m (6ft) chain link fence on Block 34, along the mutual boundary line with Lots 9, 10 and 11, to the satisfaction of the City Engineer, the Executive Director of Parks and the Chief of Police.
- 13. Video Inspection (Mainline) - The applicant shall, prior to the issuance of a construction permit, conduct at its entire expense a video inspection, or pay the cost of similar inspection, of ALL EXISTING sanitary/storm sewers on North Talbot Road which will be tapped to service the development, all to the satisfaction of the City Engineer.

14. MTO Corridor Management (MTO Requirement) - The Owner(s) shall, prior to the issuance of a construction permit, contact the Ministry of Transportation (MTO) Corridor Management to obtain the necessary permits, clearances and/or approvals in accordance with the *Public Transportation & Highway Improvement Act*.
15. Noise Control Measures for Lots 4, 5 & 6 (inclusive): The Owner(s) shall at its entire expense
  - i) install a noise barrier fence along North Talbot Road as recommended in the Acoustical Report, prepared by Shurjeel Tunio of Baird AE Inc. (Baird AE Acoustical Report) dated March 16, 2021; and
  - ii) ensure that ducting is provided for the installation of Central Air Conditioning for all the affected lots (Lots 4, 5 & 6), to the satisfaction of the Chief Building Official.
16. Noise Control Measures for all Lots on Map No. SDN-001/21-1: The Owner(s) shall at its entire expense
  - i) ensure that all windows and doors leading to sensitive living areas have a minimum sound transmission class (STC) to meet the Ministry of Environment and Climate Change (MOECC) indoor noise level criteria, as specified in the March 16, 2021 Baird AE Acoustical Report;
  - ii) engage an acoustical consultant for review of the sound transmission class (STC) for walls, windows and doors, after being installed, to ensure they conform to the recommendations outlined in the March 16, 2021 BAIRD AE Acoustical Report.
17. Warning Clause(s) for Lots 4, 5 & 6 (inclusive): The Owner(s) shall place the following warning clauses in all Offers to Purchase, and Agreements of Purchase or Sale or lease between the Owner(s) and all prospective home buyers, and in the title of each dwelling unit within Lots 4, 5 & 6 on Map No. SDN-001/21-1:
  - i) "Purchasers/tenants are advised that despite the inclusion of noise control features in the development and within the building units, sound levels due to increasing road traffic may on occasions interfere with some activities of the dwelling occupants as the sound levels exceed the sound level limits of the Municipality and the Ministry of Environment."
  - ii) "This dwelling unit has been designed with the provision for adding central air conditioning at the occupant's discretion. Installation of central air conditioning by the occupant in low and medium density developments will allow windows and exterior doors to remain closed, thereby ensuring that the indoor sound levels are within the sound level limits of the Municipality and the Ministry of Environment."

18. Species at Risk/Habitat Protection: The Owner(s) shall comply with, and implement, mitigation measures for construction adjacent to habitat for SARS Reptiles (which include awareness training, strategic vegetation clearing, wildlife exclusion and erosion control fencing, equipment inspection, proper site maintenance and management, and implementation of encounter and reporting protocols) as detailed in Appendix B attached to the report (Preliminary Screening For Species at Risk) by MTE Consultants Inc., dated April 29, 2021, for the subject development.
19. Additional *Endangered Species Act* measures: To reduce the potential for impacts to maternity roosting habitat for Protected Species of bats, the Owner(s) shall
  - i) ensure that vegetation removal will occur between October 1 and March 31, outside of the active season for bats; and
  - ii) install replacement bat roosting habitat (two rocket boxes) under the direction of a qualified professional, prior to the issuance of a construction permit.
20. Parkland Conveyance - The Owner(s) shall, prior to the issuance of any construction permit, convey to the Corporation Block 35 on Map No. SDN-001/21-1 which represents 2.7% Parkland conveyance and the 2.3% remaining balance of the Parkland conveyance in the form of cash payment.
21. Tree Preservation –
  - i) Prior to the final Subdivision Plan approval, the Owner(s) shall provide a Landscape and Tree Preservation Plan identifying the proposed locations of all existing trees removed from the development and those to be retained in Block 35, to the satisfaction of the Executive Director of Parks and the City Planner/Executive Director of Planning and Development Services.
  - ii) Prior to the issuance of the construction permit the Owner(s) shall pay to the Corporation the sum of \$130,000.00 which represents replacement tree compensation, at a rate of one (1) street tree for every 70mm caliper (dbh) of desirable trees removed. Costs for the replacement trees is in accordance with the Corporation's Fees and Charges By-law 392-2002.
22. Performance Security - for preservation of the existing desirable trees in Block 35:
  - i) Prior to issuance of the construction permit, the Owner(s) shall provide a Performance Security in the amount of \$25,000.00, in the form of cash or a certified cheque, to the Corporation to ensure that the nine (9) desirable trees located on Block 35 are preserved during the construction process;



- ii) Prior to release of the Performance Security, the Owner(s) shall request inspection by the Corporation's City Forester to ensure that the proposed tree protection and appropriate method of protection has been completed to the satisfaction of the Executive Director of Parks;
- iii) Where trees proposed to be preserved have been removed, at the time of inspection by the Corporation's City Forester, replacement tree compensation will be deducted from the Performance Security at a rate of one tree for every 70mm of tree caliper (dbh) or portion thereof missing. Costs for the replacement trees will be in accordance with the Corporation's Fees and Charges By-law 392-2002.

23. Climate Change Considerations:

- i) Replacement trees will be planted at locations deemed appropriate by the Corporation's City Forester within and near the subject development, including the parkland allowance (Block 35), the Stormwater facility (Block 34), Stoneybrook Park and nearby Public Park(s), to compensate for the removal of existing trees on the subject lands.
- ii) The Corporation's City Forester shall incorporate shade trees, among the new trees, to minimize the urban heat island impacts, and incorporate native, drought resistant plants to limit watering requirements.

24. Bell Canada - The Owner(s) shall, prior to the issuance of a construction permit,

- i) relocate any existing Bell Canada facilities or easements found to be in conflict with the proposed development;
- ii) contact Bell Canada during detailed design to confirm the provision of communication/telecommunication infrastructure needed to service the development;
- iii) provide entrance/service duct(s) from Bell Canada's existing network infrastructure to service the subject development, and, where the required infrastructure is unavailable, the Owner(s) shall agree to pay for the connection to and/or extension of such network infrastructure.

25. Schools - The Owner(s) shall place the following warnings in all Offers to Purchase, Agreements of Purchase and Sale or lease between the Owner(s) and all prospective home buyers, and in the title:

- i) "Students from this area may not be able to attend the closest neighbourhood school due to insufficient capacity and may have to be bussed to a distant school with available capacity or could be accommodate in temporary portable space."

26. Archaeological Warning Clause(s) - The Owner(s) agrees to insert, the following warning clauses in all construction documents concerning the subject lands:

1. Should archaeological resources be found during grading, construction or soil removal activities, all work in the area must stop immediately and the City's Planning & Building Department, the City's Manager of Culture and Events, and the Ontario Ministry of Heritage, Sport, Tourism and Culture Industries must be notified and confirm satisfaction of any archaeological requirements before work can recommence.
2. In the event that human remains are encountered during grading, construction or soil removal activities, all work in that area must be stopped immediately and the site secured. The local police or coroner must be contacted to determine whether or not the skeletal remains are human, and whether the remains constitute a part of a crime scene. The Local police or coroner will then notify the Ontario Ministry of Heritage, Sport, Tourism and Culture Industries and the Registrar at the Ministry of Government and Consumer Services if needed, and notification and satisfactory confirmation be given by the Ministry of Heritage, Sport, Tourism and Culture Industries.

#### **NOTES TO DRAFT APPROVAL (File # SDN-001/21)**

1. *The applicant is directed to Section 51(39) of The Planning Act 1990 regarding appeal of any imposed conditions to the Local Planning Appeal Tribunal. Appeals are to be directed to the City Clerk and Licence Commissioner of the City of Windsor.*
2. *It is the applicant's responsibility to fulfil the conditions of draft approval and to ensure that the required clearance letters are forwarded by the appropriate agencies to the City of Windsor, to the attention of the Executive Director/City Planner, quoting the above-noted file number.*
3. *Required agreements with the Municipality will be prepared by the City Solicitor.*
4. *The applicant should consult with an Ontario Land Surveyor for this proposed plan concerning registration requirements relative to the Certification of Titles Act.*
5. *The final plan approved by the Corporation of the City of Windsor must be registered within thirty (30) days or the Corporation may withdraw its approval under Section 51(59) of The Planning Act 1990.*
6. *All plans of subdivision/condominium are to be prepared and presented in metric units and certified by the Ontario Land Surveyor that the final plan is in conformity to the approved zoning requirements.*

7. *Where agency conditions are included in the City's Subdivision Agreement, the Applicant is required to forward a copy of the agreement to the agencies in order to facilitate their clearance of conditions for final approval of this plan.*
- II. THAT the City Clerk **BE AUTHORIZED** to issue the required notice respecting approval of the draft plan of subdivision under Section 51(37) of *The Planning Act*.
- III. THAT the subdivision agreement shall **BE REGISTERED** against lands to which it applies prior to the final registration of the Plan of Subdivision.
- IV. THAT prior to the final approval by the Corporation of the City of Windsor, the City Planner/Executive Director of Planning and Development Services shall **BE ADVISED**, in writing, by the appropriate agencies that conditions have been satisfied.
- V. THAT the Chief Administrative Officer and City Clerk **BE AUTHORIZED** to sign all necessary agreements and documents approved as to form and content satisfactory to the City Solicitor.
- VI. THAT an amendment to City of Windsor Zoning By-law 8600 **BE APPROVED**, changing the zoning of Block 34 on the Draft Plan of Subdivision, identified as SDN001/21-1 in this report from HRD1.4 to GD1.5 for the purposes of a stormwater management facility, and Block 35 on the Draft Plan of Subdivision, identified as SDN001/21-1 in this report from HRD1.4 to GD1.1 for Parkland.

Carried.

Councillor Gill discloses an interest and abstains from voting on this matter.

Report Number: S 59/2022  
Clerk's File: Z/14278

**Clerk's Note:**

1. The recommendation of the Standing Committee and Administration are **not** the same.
2. Please refer to Item 7.6. from the Development & Heritage Standing Committee Meeting held June 6, 2022.
3. To view the stream of this Standing Committee meeting, please refer to:  
<http://csg001-harmony.sliq.net/00310/Harmony/en/PowerBrowser/PowerBrowserV2/20220608/-1/7334>

**Subject: Approval of a Draft Plan of Subdivision for lands located on the south side of North Talbot Rd, between Southwood Lakes Blvd and HWY 401; File No. SDN-001/21[SDN/6575]; Applicant – Bellocorp Inc.; Ward 1**

**Reference:**

Date to Council: June 6, 2022  
Author: Justina Nwaesei, MCIP, RPP  
Senior Planner - Subdivisions  
519-255-6543, ext. 6165  
jnwaesei@citywindsor.ca

Planning & Building Services  
Report Date: May 17, 2022  
Clerk's File #: Z/14278

**To:** Mayor and Members of City Council

**Recommendation:**

I THAT the application of Bellocorp Inc. for Draft Plan of Subdivision approval for Part Lot 306 and Part Lot 307, Concession N Talbot Rd., Sandwich East, Windsor (PIN 01558-0962 LT, PIN 01558-0544 LT and PIN 01558-0964 LT), **BE APPROVED** subject to the following conditions:

- A. That the Draft Plan Approval shall lapse on \_\_\_\_\_ (3 years from the date of approval);
- B. That this approval applies to the draft plan of subdivision identified on attached Map No. SDN-001/21-1, prepared by VERHAEGEN Land Surveyors for Bellocorp Inc., Reference No. 21-47-019-01, dated May 20, 2022, showing 33 Lots for single unit dwellings, 1 block for Stormwater Management Facility (SWM), 1 block for Parkland and 1 block for Reserve, plus proposed road allowances (Streets A, B & C);
- C. That the Owner(s) shall enter into a subdivision agreement with the Corporation of the City of Windsor for the proposed development on the subject land;
- D. That prior to the execution and registration of the subdivision agreement between the Owner(s) and the Corporation of the City of Windsor, the Owner(s) shall submit for approval of the City Planner/Executive Director of Planning & Building a final M plan; and

- E. That the subdivision agreement between the Owner(s) and the Corporation of the City of Windsor be registered on title and shall contain, among other matters, the following provisions:
1. The Owner(s) shall include all items as set out in the Results of Circulation (Appendix C, attached) with further amendments as required and other relevant matters set out in CR 233/98 (Standard Subdivision Agreement).
  2. That the Owner(s) shall create, prior to the issuance of a building permit, a 20m right-of-way for the new Street A, Street B and Street C, in accordance with the approved Plan of Subdivision.
  3. The Owner(s) shall, prior to the issuance of a construction permit,
    - (i) Finalize the engineering analysis to identify stormwater quality and quantity measures as necessary to control any increase in flows in downstream watercourses, up to and including the 1:100 year design storm and in accordance with the Windsor-Essex Stormwater Management Standards Manual, to the satisfaction of the City Engineer and the Essex Region Conservation Authority;
    - (ii) install the stormwater management measures identified in the engineering analysis completed as part of the development for the site and undertake to implement the recommendations contained therein, to the satisfaction of the City Engineer and the Essex Region Conservation Authority;
    - (iii) obtain the necessary development review clearances from the Essex Region Conservation Authority prior to undertaking site alterations and/or construction activities of any kind; and
    - (iv) provide, to the Essex Region Conservation Authority, a copy of the fully executed subdivision agreement between the Owner(s) and the City of Windsor, containing provisions to carry out the recommendations of the final plans, reports and requirements noted in Recommendations I.E.4 (i) & (ii) above;
  4. Servicing Study - Prior to the issuance of a construction permit, the Owner(s) shall at its own expense, retain a Consulting Engineer to provide a detailed servicing study report on the impact of the increased flow to the existing municipal sewer systems, satisfactory in content to the City Engineer. The Servicing study shall
    - (i) review the proposed impact and recommend solutions to address the problems and ultimate implementation of solutions should there be a negative impact to the municipal sewer system, and
    - (ii) be finalized in agreement with the City Engineer.
  5. Site Servicing Plans – The Owner(s) shall submit a site servicing plan for the subject lands to the satisfaction of the Chief Building Official, the City



Engineer, and ERCA in regulated areas, prior to the issuance of any construction permits for the subject lands.

6. Sidewalks -The Owner(s) shall pay to the Corporation
  - (i) the sum of \$33,500.00 being the Owner's contribution towards the future construction of a concrete sidewalk on the North Talbot Road frontage of the subject lands, prior to the issuance of a Building Permit; and
  - (ii) the sum of \$16,500.00, being the Owner's contribution towards the future construction of a 3.0m wide multi-use trail connection from the Southwood Lakes Multi-use Trail, in Stoneybrook Park, to the proposed cul-de-sac at the western end of the proposed Street 'A', prior to the execution of a Subdivision Agreement.
7. Curbs and Gutters – The Owner(s) shall also pay to the Corporation, prior to the issuance of a construction permit, the sum of \$17,750.00 being the Owner's contribution towards the construction of concrete curb and gutter on the North Talbot Road frontage of the subject lands.
8. Corner Cut-Off - The Owner(s) shall, prior to the issuance of a construction permit, gratuitously convey a 4.6m x 4.6m (15' x 15'), corner cut-off at the intersection of North Talbot Road and Street 'B' as well as North Talbot Road and Street 'C' on Map No. SDN-001/21-1, in accordance with City of Windsor Standard Drawing AS-230.
9. Cul-De-Sac: The Owner(s) shall, prior to the issuance of construction permit, construct a cul-de-sac with a minimum radius of 9.5m at the west and east limits of Street A.
10. Stormwater Management Facility: The Owner(s) shall, prior to the issuance of a construction permit, gratuitously convey Block 34 on Map No. SDN-001/21-1 to the Corporation of the City of Windsor for Storm Water Management (SWM) Facility SWM purposes, to the satisfaction of the City Engineer and City Solicitor
11. Berm Requirement - Prior to the issuance of a construction permit, the Owner(s) shall install a berm along the west, south and east limits of the plan of subdivision, as proposed in Appendix A attached to the Stormwater and Functional Servicing Report dated May 14, 2021, prepared by Bill Fuerth of BAIRD AE Inc., to the satisfaction of the City Engineer and the Essex Region Conservation Authority.
12. Fencing Requirement – Prior to the issuance of a construction permit, the Owner(s) shall install a 1.8m (6ft) chain link fence on Block 34, along the mutual boundary line with Lots 9, 10 and 11, to the satisfaction of the City Engineer, the Executive Director of Parks and the Commissioner of Police.
13. Video Inspection (Mainline) - The applicant shall, prior to the issuance of a construction permit, conduct at its entire expense a video inspection, or pay

the cost of similar inspection, of ALL EXISTING sanitary/storm sewers on North Talbot Road which will be tapped to service the development, all to the satisfaction of the City Engineer.

14. MTO Corridor Management (MTO Requirement) - The Owner(s) shall, prior to the issuance of a construction permit, contact the Ministry of Transportation (MTO) Corridor Management to obtain the necessary permits, clearances and/or approvals in accordance with the Public Transportation & Highway Improvement Act.
15. Noise Control Measures for Lots 4, 5 & 6 (inclusive): The Owner(s) shall at its entire expense
  - i) install a noise barrier fence along North Talbot Road as recommended in the Acoustical Report, prepared by Shurjeel Tunio of Baird AE Inc. (Baird AE Acoustical Report) dated March 16, 2021; and
  - ii) ensure that ducting is provided for the installation of Central Air Conditioning for all the affected lots (Lots 4, 5 & 6), to the satisfaction of the Chief Building Official.
16. Noise Control Measures for all Lots on Map No. SDN-001/21-1: The Owner(s) shall at its entire expense
  - i) ensure that all windows and doors leading to sensitive living areas have a minimum sound transmission class (STC) to meet the Ministry of Environment and Climate Change (MOECC) indoor noise level criteria, as specified in the March 16, 2021 Baird AE Acoustical Report;
  - ii) engage an acoustical consultant for review of the sound transmission class (STC) for walls, windows and doors, after being installed, to ensure they conform to the recommendations outlined in the March 16, 2021 BAIRD AE Acoustical Report.
17. Warning Clause(s) for Lots 4, 5 & 6 (inclusive): The Owner(s) shall place the following warning clauses in all Offers to Purchase, and Agreements of Purchase or Sale or lease between the Owner(s) and all prospective home buyers, and in the title of each dwelling unit within Lots 4, 5 & 6 on Map No. SDN-001/21-1:
  - i) "Purchasers/tenants are advised that despite the inclusion of noise control features in the development and within the building units, sound levels due to increasing road traffic may on occasions interfere with some activities of the dwelling occupants as the sound levels exceed the sound level limits of the Municipality and the Ministry of Environment."
  - ii) "This dwelling unit has been designed with the provision for adding central air conditioning at the occupant's discretion. Installation of central air conditioning by the occupant in low and medium density

developments will allow windows and exterior doors to remain closed, thereby ensuring that the indoor sound levels are within the sound level limits of the Municipality and the Ministry of Environment.”

18. Species at Risk/ Habitat Protection: The Owner(s) shall comply with, and implement, mitigation measures for construction adjacent to habitat for SARS Reptiles (which include awareness training, strategic vegetation clearing, wildlife exclusion and erosion control fencing, equipment inspection, proper site maintenance and management, and implementation of encounter and reporting protocols) as detailed in Appendix B attached to the report (Preliminary Screening For Species at Risk) by MTE Consultants Inc., dated April 29, 2021, for the subject development.
19. Additional Endangered Species Act measures: To reduce the potential for impacts to maternity roosting habitat for Protected Species of bats, the Owner(s) shall
  - i) ensure that vegetation removal will occur between October 1 and March 31, outside of the active season for bats; and
  - ii) install replacement bat roosting habitat (two rocket boxes) under the direction of a qualified professional, prior to the issuance of a construction permit.
20. Parkland Conveyance - The Owner(s) shall, prior to the issuance of any construction permit, convey to the Corporation Block 35 on Map No. SDN-001/21-1 which represents 2.7% Parkland conveyance and the 2.3% remaining balance of the Parkland conveyance in the form of cash payment.
21. Tree Preservation –
  - i) Prior to the final Subdivision Plan approval, the Owner(s) shall provide a Landscape and Tree Preservation Plan identifying the proposed locations of all existing trees removed from the development and those to be retained in Block 35, to the satisfaction of the Executive Director of Parks and the City Planner/Executive Director of Planning and Development Services.
  - ii) Prior to the issuance of the construction permit the Owner(s) shall pay to the Corporation the sum of \$130,000.00 which represents replacement tree compensation, at a rate of one (1) street tree for every 70mm caliper (dbh) of desirable trees removed. Costs for the replacement trees is in accordance with the Corporation's Fees and Charges By-law 392-2002.
22. Performance Security - for preservation of the existing desirable trees in Block 35:
  - i) Prior to issuance of the construction permit, the Owner(s) shall provide a Performance Security in the amount of \$25,000.00, in the form of cash or a certified cheque, to the Corporation to ensure that the nine

(9) desirable trees located on Block 35 are preserved during the construction process;

ii) Prior to release of the Performance Security, the Owner(s) shall request inspection by the Corporation's City Forester to ensure that the proposed tree protection and appropriate method of protection has been completed to the satisfaction of the Executive Director of Parks;

iii) Where trees proposed to be preserved have been removed, at the time of inspection by the Corporation's City Forester, replacement tree compensation will be deducted from the Performance Security at a rate of one tree for every 70mm of tree caliper (dbh) or portion thereof missing. Costs for the replacement trees will be in accordance with the Corporation's Fees and Charges By-law 392-2002.

23. Climate Change Considerations:

i) Replacement trees will be planted at locations deemed appropriate by the Corporation's City Forester within and near the subject development, including the parkland allowance (Block 35), the Stormwater facility (Block 34), Stoneybrook Park and nearby Public Park(s), to compensate for the removal of existing trees on the subject lands.

ii) The Corporation's City Forester shall incorporate shade trees, among the new trees, to minimize the urban heat island impacts, and incorporate native, drought resistant plants to limit watering requirements.

24. Bell Canada - The Owner(s) shall, prior to the issuance of a construction permit,

i) relocate any existing Bell Canada facilities or easements found to be in conflict with the proposed development;

ii) contact Bell Canada during detailed design to confirm the provision of communication/telecommunication infrastructure needed to service the development;

iii) provide entrance/service duct(s) from Bell Canada's existing network infrastructure to service the subject development, and, where the required infrastructure is unavailable, the Owner(s) shall agree to pay for the connection to and/or extension of such network infrastructure.

25. Schools - The Owner(s) shall place the following warnings in all Offers to Purchase, Agreements of Purchase and Sale or lease between the Owner(s) and all prospective home buyers, and in the title:

i) "Students from this area may not be able to attend the closest neighbourhood school due to insufficient capacity and may have to be bussed to a distant school with available capacity or could be accommodate in temporary portable space."

26. Archaeological Warning Clause(s) - The Owner(s) agrees to insert, the following warning clauses in all construction documents concerning the subject lands:

1. Should archaeological resources be found during grading, construction or soil removal activities, all work in the area must stop immediately and the City's Planning & Building Department, the City's Manager of Culture and Events, and the Ontario Ministry of Heritage, Sport, Tourism and Culture Industries must be notified and confirm satisfaction of any archaeological requirements before work can recommence.

2. In the event that human remains are encountered during grading, construction or soil removal activities, all work in that area must be stopped immediately and the site secured. The local police or coroner must be contacted to determine whether or not the skeletal remains are human, and whether the remains constitute a part of a crime scene. The Local police or coroner will then notify the Ontario Ministry of Heritage, Sport, Tourism and Culture Industries and the Registrar at the Ministry of Government and Consumer Services if needed, and notification and satisfactory confirmation be given by the Ministry of Heritage, Sport, Tourism and Culture Industries.

#### **NOTES TO DRAFT APPROVAL (File # SDN-001/21)**

- 1. The applicant is directed to Section 51(39) of The Planning Act 1990 regarding appeal of any imposed conditions to the Local Planning Appeal Tribunal. Appeals are to be directed to the City Clerk and Licence Commissioner of the City of Windsor.*
- 2. It is the applicant's responsibility to fulfil the conditions of draft approval and to ensure that the required clearance letters are forwarded by the appropriate agencies to the City of Windsor, to the attention of the Executive Director/City Planner, quoting the above-noted file number.*
- 3. Required agreements with the Municipality will be prepared by the City Solicitor.*
- 4. The applicant should consult with an Ontario Land Surveyor for this proposed plan concerning registration requirements relative to the Certification of Titles Act.*
- 5. The final plan approved by the Corporation of the City of Windsor must be registered within thirty (30) days or the Corporation may withdraw its approval under Section 51(59) of The Planning Act 1990.*
- 6. All plans of subdivision/condominium are to be prepared and presented in metric units and certified by the Ontario Land Surveyor that the final plan is in conformity to the approved zoning requirements.*



*7. Where agency conditions are included in the City's Subdivision Agreement, the Applicant is required to forward a copy of the agreement to the agencies in order to facilitate their clearance of conditions for final approval of this plan.*

**II** THAT the City Clerk **BE AUTHORIZED** to issue the required notice respecting approval of the draft plan of subdivision under Section 51(37) of The Planning Act.

**III** THAT the subdivision agreement shall **BE REGISTERED** against lands to which it applies prior to the final registration of the Plan of Subdivision.

**IV** THAT prior to the final approval by the Corporation of the City of Windsor, the City Planner/Executive Director of Planning and Development Services shall **BE ADVISED**, in writing, by the appropriate agencies that conditions have been satisfied.

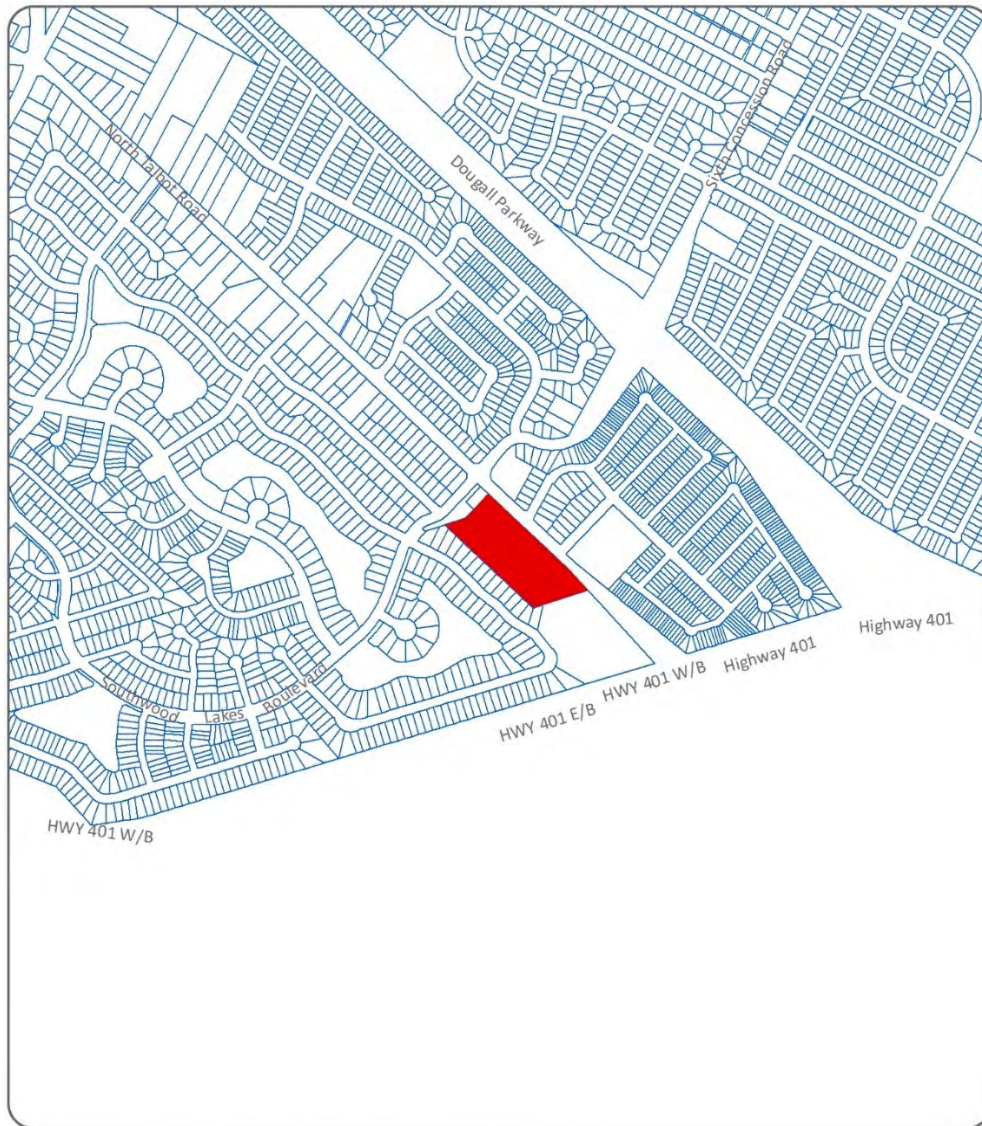
**V** THAT the Chief Administrative Officer and City Clerk **BE AUTHORIZED** to sign all necessary agreements and documents approved as to form and content satisfactory to the City Solicitor.

**VI** THAT an amendment to City of Windsor Zoning By-law 8600 **BE APPROVED**, changing the zoning of Block 34 on the Draft Plan of Subdivision, identified as SDN001/21-1 in this report from HRD1.4 to GD1.5 for the purposes of a stormwater management facility, and Block 35 on the Draft Plan of Subdivision, identified as SDN001/21-1 in this report from HRD1.4 to GD1.1 for Parkland.

**Executive Summary:** N/A

Background:

## 1. KEYMAP



KEY MAP - SDN-001/21, SDN-6575



● SUBJECT LANDS

## 2. APPLICATION INFORMATION:

**LOCATION:** South side of North Talbot Road, between Southwood Lakes Blvd and HWY 401, described as Part Lot 306 and Part Lot 307, Concession N Talbot Rd., Sandwich East, Windsor; municipally known as 0, 1095 & 1185 North Talbot Road.

<b>ADDRESS:</b>	0 North Talbot	1095 North Talbot	1185 North Talbot
<b>ROLL No.:</b>	070-140-04101	070-140-04100	070-140-04000
<b>PIN</b>	01558-0962 LT	01558-0544 LT	01558-0964 LT

**WARD: 1**

**PLANNING DISTRICT: 08 - ROSELAND**

**ZDM: 13**

**APPLICANT:** BELLOCORP INC., c/o Tosin Bello

**AUTHORIZED AGENT:** PILLON-ABBS INC., (c/o Tracey Pillon-Abbs)

**PROPOSAL:** The applicant is requesting approval of a draft plan of Subdivision for the development of 33 lots for single unit detached dwellings, 1 block for SWM facility (shown as Block 34), 1 block for Parkland (shown as Block 35), 1 block for land Reserve (shown as Block 36), and three new road allowances (shown as Streets A, B and C.) Two of the proposed roads provide direct vehicular access to/from North Talbot Road.

The subject lands are designated Residential on Schedule D – Land Use, of the Official Plan and zoned Residential District 1.4 with a holding provision (HRD1.4) by Bylaw 8600. Subsection 20(1)85 of By-law 8600 applies to the subject lands and requires a minimum front yard depth of 9m for any lot fronting on North Talbot Rd between Southwood Lakes Blvd and HWY 401.

**SUBMISSIONS BY APPLICANT:**

- Plan of Subdivision Application form, signed and commissioned;
- Draft Plan of Subdivision plus Area & Lot width Table;
- Geo warehouse Property Report, dated Sep. 19, 2021;
- Service Ontario - Property Identification from Land Registry Office, Dec. 16, 2021;
- Service Ontario - Property Index Maps;
- Planning Rationale Report, dated Dec. 3, 2021, prepared by Pillon-Abbs Inc.;
- Stormwater Management (SWM) & Functional Servicing Report (FSR), dated May 14, 2021, prepared by Baird AE;
- Tree Condition Report, dated Aug. 13, 2021, prepared by Bezaire Partners;
- Topographic Survey, completed Feb. 4, 2021; prepared by Verhaegen Land Surveyors;
- Phase 1 Environmental Site Assessment (ESA), Mar. 2021, prepared by Coulson & Associates Ltd;
- Acoustical Report, dated Mar. 16, 2021, prepared by Baird AE; and
- Preliminary Screening Report for Species at Risk, dated Apr. 29, 2021, prepared by MTE.
- Continued Endangered Species Act (ESAct) Studies, dated Sep. 16, 2021, by MTE.

**3. SITE INFORMATION:**

OFFICIAL PLAN	ZONING & ZDM	CURRENT USE(S)	PREVIOUS USE(S)
<ul style="list-style-type: none"><li>• ROSELAND Planning District</li><li>• RESIDENTIAL Land Use</li></ul>	<ul style="list-style-type: none"><li>- Residential District 1.4 with a holding symbol (HRD1.4)</li><li>- ZDM13</li></ul>	<ul style="list-style-type: none"><li>- VACANT (most of the land);</li><li>- Single Unitdwelling N/E corner of site.</li></ul>	Unknown
FRONTAGE	DEPTH	AREA	SHAPE
291.07m	irregular	2.932 ha	Irregular
Note: All measurements are as shown on the draft plan (Map No. SDN-001/21-1)			

**PROPOSED DRAFT PLAN OF SUBDIVISION:**

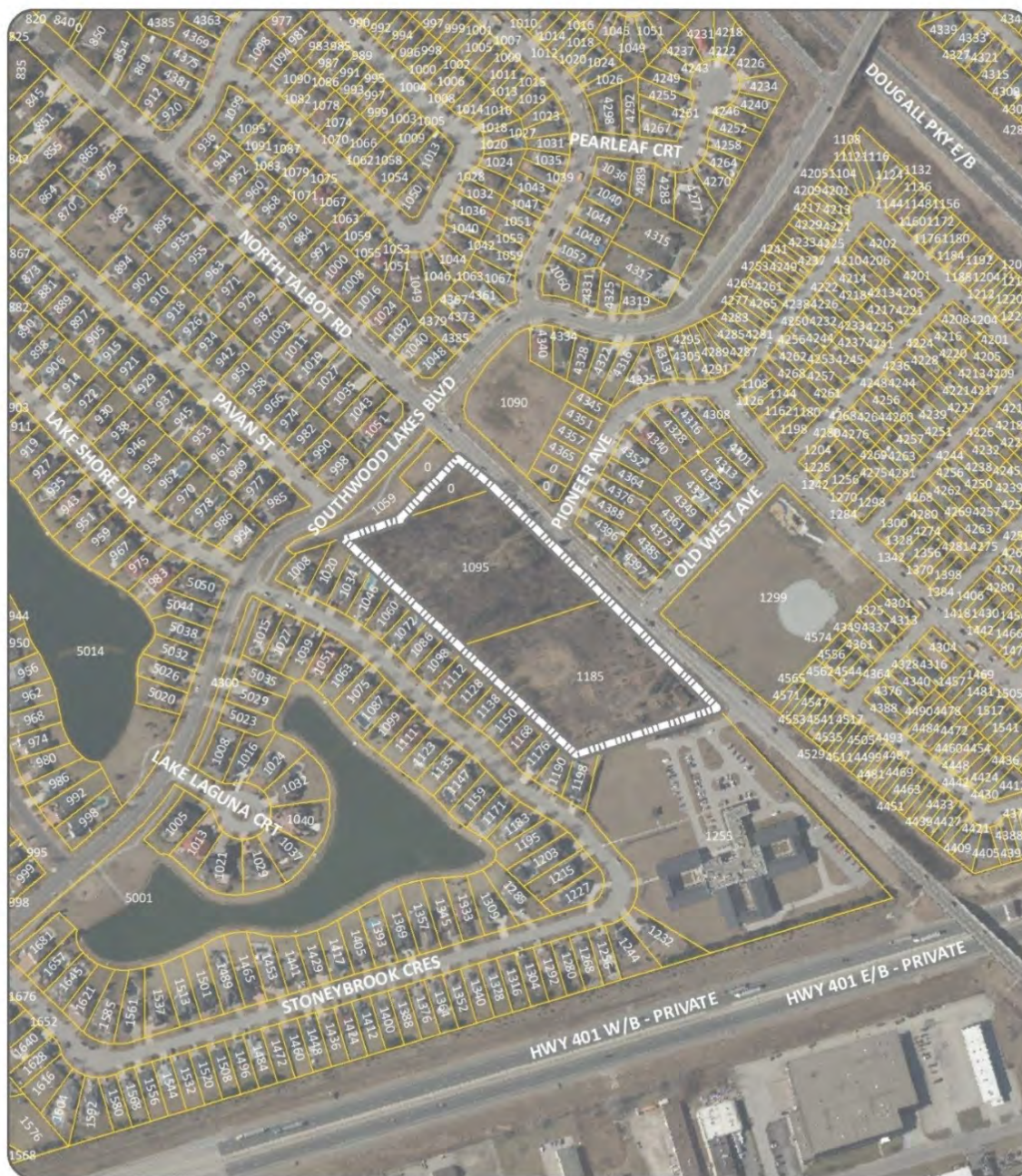
*[See also Appendices B-1 and B-2, herein attached, for more details]*



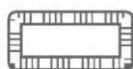


Attached to this report as **Appendix A** are site photos taken on May 12, 2022. The site photos, along with the neighbourhood map below, show some of the surrounding land uses and the character of the subject neighbourhood.

## NEIGHBOURHOOD MAP



NEIGHBOURHOOD MAP - SDN-001/21, SDN-6575



SUBJECT LANDS



## MUNICIPAL INFRASTRUCTURE

- The City's records show that there are municipal storm and sanitary sewers within the abutting road way, available to service the subject lands as follows:
  - North Talbot Road
    - Storm Sewer: 450mm diameter Corrugated Steel Sewer and 1200mm diameter Reinforced Concrete Pipe
    - Sanitary Sewer: 600mm diameter Concrete Pipe and Reinforced Concrete
- Municipal watermain, fire hydrants and LED streetlights are available within the north side of North Talbot R.O.W. across from the subject lands.
- There are overhead hydro wires and poles on north side of North Talbot R.O.W. across from the frontage of the subject lands.
- There are curbs and gutter along Southwood Lakes boulevard, but none on North Talbot Road.
- There is an existing ditch along the frontage of the subject lands.
- Concrete sidewalks are available on the north side of North Talbot Road, terminating at Old West Avenue.
- Multi-use trail exists along the east side of Southwood Lakes Blvd from the Sixth Concession intersection heading south, through Stoney Park.
- Transit Windsor Bus services (North and Southbound Walkerville 8 Buses) are available in the subject area, along Sixth Concession and North Talbot Roads.
- The closest existing bus stops are located on North Talbot at Sixth Concession, Pioneer, and Oldwest. All of the proposed subdivision would be within a 400m walking distance.
- Bike lanes are available on both sides of the pavement along North Talbot Road.
- North Talbot Road is classified as Class 1 collector road.

## Discussion:

### 1. PROVINCIAL POLICY STATEMENT (PPS) 2020

In making the determination whether the requested draft Plan of Subdivision approval is consistent with PPS 2020, a number of policies of the PPS 2020 are relevant for the discussion and have been considered in the Planning Rationale Report (PRR) dated December 3, 2021, prepared by the applicant's planning consultant (Pillon-Abbs Inc.) I have reviewed the planning consultant's summary of the key policy considerations of the PPS as it relates to the proposed development on the subject lands. I am providing complementary planning analysis to what the planning consultant has already stated in the PRR.

Policy 1.1.1 outlines specific land use planning activities and values that are known to continuously keep communities healthy, liveable and safe.

*1.1.1 Healthy, liveable and safe communities are sustained by:*

- a) promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term;*
- b) accommodating an appropriate affordable and market-based range and mix of residential types (including single-detached, additional residential units, multi-unit housing, affordable housing and housing for older persons), employment (including industrial and commercial), institutional (including places of worship, cemeteries and long-term care homes), recreation, park and open space, and other uses to meet long-term needs;*
- c) avoiding development and land use patterns which may cause environmental or public health and safety concerns;*

- d) *avoiding development and land use patterns that would prevent the efficient expansion of settlement areas in those areas which are adjacent or close to settlement areas;*
- e) *promoting the integration of land use planning, growth management, transit-supportive development, intensification and infrastructure planning to achieve cost-effective development patterns, optimization of transit investments, and standards to minimize land consumption and servicing costs;*
- f) *improving accessibility for persons with disabilities and older persons by addressing land use barriers which restrict their full participation in society;*
- g) *ensuring that necessary infrastructure and public service facilities are or will be available to meet current and projected needs;*
- h) *promoting development and land use patterns that conserve biodiversity; and*
- i) *preparing for the regional and local impacts of a changing climate.*

With respect to 1.1.1(a) – The proposed draft plan of subdivision for development of 33 lots for single detached dwellings, promotes a land use pattern that efficiently utilizes the vacant, underutilized subject lands.

With respect to 1.1.1(b) –The surrounding land uses in the subject area comprise a mix of low-density residential dwelling (mostly single unit dwellings), vacant/undeveloped lands, and open space lands with multiuse trails and storm water facilities. The proposed low density, small scale, low profile residential development is an appropriate within the subject area.

With respect to 1.1.1(c) – The proponent submitted an Acoustic Assessment for the proposed residential development. BAIRD AE prepared the Acoustic Assessment dated March 16, 2021, which recommended noise barrier fence along some sections of the North Talbot Road frontage of the subject land, windows and doors requirements, air conditioning installation requirements and Noise warning clauses to mitigate impact on public health and achieve a healthy and efficient living environment.

With respect to 1.1.1(d) – The proposed residential subdivision is within the inner part of the City of Windsor settlement area and is surrounded by existing developments and other land holdings. Therefore, the proposed development does NOT prevent the efficient expansion of settlement areas.

With respect to 1.1.1(e) – The proposed infill development on the subject land minimizes land consumption and is more cost effective. Allowing the proposed Draft Plan of Subdivision in this location contributes to minimizing land consumption and servicing costs by using a site that already has available trunk infrastructure in the immediate area.

With respect to 1.1.1(f) – Sidewalks improve *accessibility for persons with disabilities and older persons*. As noted in the recommendation section of this report, concrete sidewalk will be provided within the subject draft plan of subdivision, as well as along the North Talbot frontage of the subject lands, to connect to existing sidewalks and multi-use trail in the area.

With respect to 1.1.1(g) – As noted in this report under “NEIGHBOURHOOD CHARACTERISTICS”, the subject neighbourhood contains existing municipal infrastructure, existing local park(s) with multi-use trail, nearby place of worship and nearby schools.

With respect to 1.1.1(h) – The tree conditioning reports and the Endangered Species Act Report aim to promote a development and land use pattern that conserve biodiversity. This report contains recommendations that will help save existing desirable trees and protect habitats for endangered species on the subject lands; thereby conserving as much biodiversity as possible on the subject lands.

With respect to 1.1.1(i) – Consideration for climate change is addressed through various methods including lot-grading plans, stormwater management measures, tree planting requirements, landscaping requirements and more.

In summary, the proposed draft plan of subdivision represents an efficient development and land use pattern that will have no adverse impact on the financial well-being of the City of Windsor, land consumption and servicing costs. The noise control measures, amongst other recommendations in this planning report, will help the subdivision to be developed in a manner that does not cause any environmental or public health and safety concerns. The proposed draft plan of subdivision is consistent with Policy 1.1.1 of the PPS.

*1.1.3.1 Settlement areas shall be the focus of growth and development.*

The subject parcel is located within the settlement area of the City of Windsor. The proposed development creates growth and development within the City of Windsor settlement area; thereby, promoting the vitality of the settlement area. The proposed development will positively impact the existing nearby facilities (parks, schools, and places of worship) in the subject area.

*1.1.3.2 Land use patterns within settlement areas shall be based on densities and a mix of land uses which:*

- a) efficiently use land and resources;*
- b) are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion;*
- c) minimize negative impacts to air quality and climate change, and promote energy efficiency;*
- d) prepare for the impacts of a changing climate;*
- e) support active transportation;*
- f) are transit-supportive, where transit is planned, exists or may be developed; and*

*Land use patterns within settlement areas shall also be based on a range of uses and opportunities for intensification and redevelopment in accordance with the criteria in policy 1.1.3.3, where this can be accommodated.*

The proposed draft plan of subdivision, in the City of Windsor settlement area, promotes a land use pattern that is based on density that makes efficient use of land and existing infrastructure, including existing and planned active transportation options such as sidewalks, bike lanes, and multiuse trails. The recommendations provided in this report will help minimize negative impacts to air quality and climate change.

*1.1.3.3 Planning authorities shall identify appropriate locations and promote opportunities for transit-supportive development, accommodating a significant supply and range of housing options through intensification and redevelopment where this can be accommodated taking into account existing building stock or areas, including brownfield sites, and the availability of suitable existing or planned infrastructure and public service facilities required to accommodate projected needs.*

The subject lands are located in an area that is appropriate for residential intensification. The proposed Draft Plan of Subdivision is a residential intensification that takes into account existing building stock (mostly small scale, low-profile developments of the single detached type of housing), infrastructure (existing and planned) and public service facilities in the subject area. The draft plan of subdivision is consistent with Policies 1.1.3.1, 1.1.3.2 & 1.1.3.3 of the PPS.

*1.4.3 Planning authorities shall provide for an appropriate range and mix of housing options and densities to meet projected market-based and affordable housing needs of current and future residents of the regional market area by:*

*b) permitting and facilitating:*

- 1. all housing options required to meet the social, health, economic and well-being requirements of current and future residents, including special needs requirements and needs arising from demographic changes and employment opportunities; and*

- 2. *all types of residential intensification, including additional residential units, and redevelopment in accordance with policy 1.1.3.3;*
- c) *directing the development of new housing towards locations where appropriate levels of infrastructure and public service facilities are or will be available to support current and projected needs;*
- d) *promoting densities for new housing which efficiently use land, resources, infrastructure and public service facilities, and support the use of active transportation and transit in areas where it exists or is to be developed; and*
- f) *establishing development standards for residential intensification, redevelopment and new residential development which minimize the cost of housing and facilitate compact form, while maintaining appropriate levels of public health and safety.*

The proposed housing type (single detached dwellings) is use anticipated by municipal zoning by-law and official plan for the subject lands. The proposed residential intensification is appropriate for the subject lands in the subject area. The proposal is for development of new housing in a location where appropriate levels of infrastructure and public service facilities are or will be available.

Approving the Draft Plan of Subdivision would support residential development using the infrastructure that is already in place, instead of requiring more expenditure on new infrastructure in an agricultural setting. In terms of supporting active transportation and transit, the proposed Plan of Subdivision is in close proximity to Transit Windsor service. Also, there are existing and planned sidewalks, multi-use trails and bike lanes in the immediate area. The proposed draft plan will support the use of active transportation and public transit. The proposed draft plan of subdivision is consistent with Policy 1.4.3 of the PPS.

*1.6.6.2 Municipal sewage services and municipal water services are the preferred form of servicing for settlement areas to support protection of the environment and minimize potential risks to human health and safety. Within settlement areas with existing municipal sewage services and municipal water services, intensification and redevelopment shall be promoted wherever feasible to optimize the use of the services.*

The subject lands are within an area that is serviced by municipal sewage services and municipal water services. Additional municipal services are planned for the subject area through the 2016 Municipal Class EA for the subject corridor. Therefore, the draft plan of subdivision is consistent with policy 1.6.6.2 of the PPS.

*1.6.6.7 Planning for stormwater management shall:*

- a) *be integrated with planning for sewage and water services and ensure that systems are optimized, feasible and financially viable over the long term;*
- b) *minimize, or, where possible, prevent increases in contaminant loads;*
- c) *minimize erosion and changes in water balance, and prepare for the impacts of a changing climate through the effective management of stormwater, including the use of green infrastructure;*
- d) *mitigate risks to human health, safety, property and the environment;*
- e) *maximize the extent and function of vegetative and pervious surfaces; and*
- f) *promote stormwater management best practices, including stormwater attenuation and re-use, water conservation and efficiency, and low impact development.*

The applicant submitted a Stormwater Management and Functional Servicing Report that addressed the servicing requirements for the proposed development. Stormwater management is incorporated in the general provisions of the City's Subdivision Agreement. The proposed SWM Facility will be a dry pond. There SWM area will have a Low Impact Design (LID) and provide a stronger resilience to flooding through vegetation to slow runoff, as well as absorb storm water before it reaches the storm water management basin. As a result, the draft plan is consistent with the stormwater management policy (1.6.6.7) of the PPS.

*2.1.1 Natural features and areas shall be protected for the long term.*

*2.1.7 Development and site alteration shall not be permitted in habitat of endangered species and threatened species, except in accordance with provincial and federal requirements.*

This planning report contains mitigation measures recommended by MTE Consultants in their April 29, 2021 and September 16, 2021 ESAct Study Reports. According to the report dated September 16, 2021, submitted by MTE Consultants, the Endangered Species Act concerns for the proposed development are limited to potential impacts to maternity roosting habitat for Protected Species of bats. The report concludes that *“to reduce the potential for impacts, vegetation removal will occur between October 1 and March 31, outside of the active season for bats, and replacement bat roosting habitat (two rocket boxes) will be installed under the direction of a qualified professional”*. Therefore, the proposed draft plan of subdivision is consistent with policy 2.1.7 of the PPS.

In summary, the above planning analysis and the planning analysis provided in the Planning Rationale Report prepared by Pillon-Abbs Inc. (applicant's Planning Consultant) and dated December 3, 2021, confirm that the proposed draft plan of subdivision is consistent with the relevant Policies of the PPS 2020.

### 3. OFFICIAL PLAN:

The site is designated “Residential” in the Land Use Schedule D of City of Windsor Official Plan. The objectives and policies of the Residential land use designation establish the framework for development decisions in Residential areas within the City of Windsor.

The Official Plan supports a complementary range of housing forms, promotes compact residential form for new developments and also promotes selective residential infill and intensification initiative in the City of Windsor. (Sections 6.3.1.1, 6.3.1.2 & 6.3.1.3 of OP Vol.1.)

The above noted objectives of the OP are satisfied by the proposed single detached dwelling development (low density and complementary range of housing form) on the subject land. The proposed draft plan of subdivision is an infill development, which by its very nature promotes a compact neighbourhood.

The Residential land use designation permits *“Low, Medium and High Profile dwelling units.”* (See section 6.3.2 of OP Vol.1). Based on the OP classification of *“types of low profile housing”* (s. 6.3.2.3), the proposed single detached dwellings are deemed small scale form of low profile housing development and are, therefore, permitted in the Residential land use designation.

The proposed development is on lands with access to a collector road (North Talbot road). As noted already in this report, there are existing full municipal physical services available to service the subject land. Existing community services, open spaces and public transportation are already in, and near, the neighbourhood and can service the new development. The proposed development satisfies the locational criteria (s.6.3.2.4) of OP Vol. 1.

With respect to the evaluation criteria set out under s. 6.3.2.5 of OP Vol. 1, the proponent has, in their consultant's Planning Rationale Report, demonstrated to the satisfaction of the City Planner that the proposed development is

- compatible with the surrounding area in terms of scale, massing, height, siting, orientation, setbacks, parking and amenity areas;
- capable of being provided with full municipal physical services and emergency services; and
- provided with adequate off street parking.

In addition, the proposed plan of subdivision is for low density low profile residential development much like the existing Southwood Lakes subdivision.



Section 7.2.3.2 – Pedestrian Network, OP Vol. 1: The Official Plan requires the installation of sidewalks on at least one side of proposed local roads (Streets A, B and C) and installation of sidewalks on both sides of North Talbot Road. Recommendation I (E) 6 of this report ensures compliance with the sidewalk requirements under s.7.2.3.2 (a)(i) of OP Vol. 1. General provision G-2(25) of the City's Subdivision Agreement addresses the sidewalk requirement under 7.2.3.2(a)(ii).

The subject land is adjacent to bicycle lanes, bus stop plus sidewalks on North Talbot Road, Sixth Concession Road and Southwood Lakes Boulevard; therefore, active transportation is supported in the subject neighbourhood.

The proposed development meets the requirements for noise control measures and off-street parking requirements as found in Section 7.2.6.18(b) – Residential Areas, OP Vol. 1.

Traffic Calming measures per section 7.2.6.20 of OP Vol. 1, are incorporated in the General Provisions under G-2(21) of the Subdivision Agreement.

#### 4. ZONING BY-LAW

The subject lands are zoned Residential District 1.4 with a holding provision (HRD1.4) in By-law 8600. The holding provision is meant to ensure the property is developed to municipal standards by way of a plan of subdivision. The RD1.4 category permits the construction of single unit dwellings on lots with minimum lot width of 18m and minimum lot area of 540m<sup>2</sup>. The application proposes to create parcels that will comply with and exceed the zone regulations of the existing RD 1.4 zoning category.

Removal of the holding provision requires compliance with section 5.4.20 of By-law 8600. Execution of a Subdivision Agreement for the subject lands will fulfill two of the three applicable requirements (s.5.4.20 (ii) & (iii)). Registration of a Subdivision Agreement is required in order to satisfy the requirement in s.5.4.20(i), so the Owner(s) can apply to remove the hold provision once the Plan of Subdivision has Final Registration.

Block 34 will be conveyed to the City to be used as a dry pond for storm water management of the subject residential development. The GD1.5 zone is specifically designed for storm water management facilities. Therefore, it is recommended that the zoning for Block 34 be changed from HRD1.4 to GD1.5.

Block 35 will be conveyed to the City for parkland dedication. Bylaw 8600 shows that Green District 1.1 (GD1.1) permits Public Park, child care centre and accessory uses to the two permitted uses. Consequently, it is more appropriate to change the zoning of Block 35 from HRD1.4 to GD1.1. In addition, Block 35 will be consolidated into the existing Stoneybrook Park, which is already zoned GD1.1.

This planning report contains recommendation to amend the zoning of the land parcels shown as Block 34 and Block 35 as suggested in the two paragraphs above.

#### 5. SUPPORTING STUDIES

Support studies were received for the proposed development on the subject lands. Stormwater Management (SWM) & Functional Servicing Report (FSR), Tree Conditioning Report, Phase 1 Environmental Site Assessment, Acoustic Report, SARS screening.

- *Planning Rationale Report (PRR)*, by Pilon-Abbs Inc. summarized the recommendations contained in the support studies. The PRR also reviewed the relevant planning policies and guidelines and concluded that *“the proposed development is suitable for residential use, is consistent with the PPS, conforms with the intent and purpose of the City of Windsor Official Plan and represents good planning.”*
- Stormwater Management (SWM) & Functional Servicing Report (FSR) – Baird AE Inc., concluded that
  - The proposed development is modelled using new ERCA Stormwater Management Manual (SWMM) guideline and meets all standard criteria specified therein;
  - Water elevations for 5, 100 and urban storm events satisfies the new ERCA SWMM guidelines; and
  - The proposed subdivision did not have any adverse impacts on existing downstream developments.
- *Phase 1 Environmental Site Assessment (ESA)* – Coulson & Associates Ltd. concludes that *“the Phase 1 ESA has revealed no significant evidence of actual environmental contamination on the subject properties”* and recommended that *“no further investigation or remedial action is required at this time”*.
- *Acoustical Report* - Baird AE determined that the traffic noise from North Talbot Road and Southwood Lakes Blvd will impact on the proposed development as some of the proposed buildings are within 60dBA daytime noise limits which will trigger for noise attenuation wall. The Acoustical Report shows that mitigation measures such as warning clause, air conditioning and forced air heating are required. No traffic impact was observed from Highway 401 traffic.

The Acoustical Report demonstrated that mitigation measures are required to bring residential units within the development into compliance with MOECC criteria. This planning report includes the recommended mitigation measures found in section 5.0 of the Acoustical Report as conditions of the draft plan approval; therefore, MOECC criteria will be satisfied. BAIRD AE concluded that the development can, with the implementation of the mitigation measures described in section 5 of the Acoustical report, be designed to address impacts from the surrounding noise sources.

- *Endangered Species Act Study (Preliminary Screening Report for Species at Risk)*, dated Apr. 29, 2021, prepared by MTE contained mitigation measures, which were later updated in an addendum submitted in September 2021. This report contains recommendations that address the mitigation measures contained in both April 29, 2021 and September 16, 2021 ESAct Studies.
- *Tree Conditioning Reports 1 & 2, along with their Addendum*, are helpful in determining which existing trees can be saved or removed. Based on the information provided by Bezaire Partners, a tree preservation and landscape plan is required as a condition of approval for this subdivision.

## 6. MUNICIPAL CLASS ENVIRONMENTAL ASSESSMENT STUDY (CLASS EA):

North Talbot Road between Sixth Concession Road and the City limits is a municipal roadway classified as a Class I Collector. In 2016, the City of Windsor completed a Municipal Class Environmental Assessment study (Class EA) to provide an improved Sixth Concession Road / North Talbot Road corridor that will serve the needs of the transportation system and area growth for a 20-year period. These improvements will provide enhanced traffic safety and

efficiency, improved transit, pedestrian and cycling facilities within study area, enclosed drainage system for maintenance, safety and aesthetic improvements.

The section of North Talbot Road abutting the subject lands falls within the boundary of the 6th Concession Road/North Talbot Environmental Study Report (ESR) prepared by Dillon Consulting, dated April 2016. The ESR does not identify any property requirements across the frontage of the subject lands.

Construction has begun for Phase 1 of the North Talbot Road improvements from Howard Avenue to Southwood Lakes Boulevard and includes, the installation of new storm sewers, new watermain services connected to the existing watermain, new widened asphalt pavement with on-road bike lanes, new curb and gutters, new concrete sidewalks on both sides of North Talbot, and new streetlights.

The infrastructure improvements at the Sixth Concession Road / North Talbot Road intersection are scheduled for in the next few years in accordance with the 10-year Capital Budget, and will include a roundabout at North Talbot and Southwood lakes Blvd intersection and new multiuse trail on the north side of North Talbot Road across from the subject lands.

### **Risk Analysis:**

**Mitigation & Adaptation:** The General Provision for the City of Windsor Subdivision Agreements includes items such as stormwater management, landscaping (tree-planting and tree preservation) and lot-grading requirements. These items mitigate climate change risks, while also serving as adaptation tools. Stormwater, tree-planting and preservation, and lot grading requirements are implemented and enforced through the Building Permit process. The use of low-impact development practice and design should also be encouraged at the building permit stage.

### **Financial Matters: N/A**

### **Consultations:**

#### **1. DEPARTMENT AND AGENCIES**

Comments received from municipal departments, service units and external agencies are included in **Appendix C**. The applicant/owner shall satisfy all items as set out in the Results of Circulation (Appendix C) attached.

The requirements of Engineering & Geomatics, Transportation Planning, City's Landscape Architect, ERCA, Canada Post, Utility companies and other agencies and departments, as found in Appendix C, have all been addressed under Recommendation I of this report and will be included in the subdivision agreement as special provisions or are already included in the general provisions of the subdivision agreement. It should be noted that in my discussion with the City's Engineering staff on May 16, 2022, it was confirmed that ditch enclosure is not required and culverts for driveways along North Talbot shall be addressed at the time of processing Street Opening Permit application. The following points are worth noting with respect to some municipal and agency comments found in Appendix C attached:

**Canada Post** will provide mail delivery service to the subdivision through centralized Community Mail Boxes (CMBs). See Appendix C of this report for Canada Post "Additional Developer Requirements". These Canada Post requirements are included under provisions contained in General Provisions [G-2 (11)] of the City's Subdivision Agreement.

**Essex Region Conservation Authority (ERCA):** The subject site is not located within a regulated area that is under the jurisdiction of ERCA. The Essex Region Conservation Authority has expressed some concerns regarding the proposed development. ERCA requested deferral of the proposed development pending completion of an Environmental Evaluation Report /Environmental Impact Assessment (EER/EIA). In addition, ERCA had a number of questions regarding stormwater management report submitted by BAIRD AE Inc. ERCA concluded by recommending several draft conditions for inclusion in the Subdivision Agreement. The recommendations in this planning report include all, but one, of the conditions recommended by ERCA. Baird AE Inc. provided an addendum with response to ERCA's questions. See Appendix D attached.

With respect to ERCA's request for an EER/EIA my comments are noted below:

- Environmental Evaluation Report /Environmental Impact Assessment (EER/EIA) is not required for the subject site.
- Section 10.2.5.1 of the OP states that the purpose of an Environmental Evaluation Report is to demonstrate that a proposed development or infrastructure undertaking may proceed in or adjacent to lands designated as Natural Heritage, Environmental Policy Area A or B and/or Candidate Natural Heritage Site.
- Section 5.3.3.1 of the OP states that Lands designated as Natural Heritage appear on Schedules B: Greenway System, C: Development Constraints and D: Land Use.
- The proposed development is not in or adjacent to lands designated as Natural Heritage, Environmental Policy Area A or B and/or Candidate Natural Heritage Site.
- Policy 2.1.8 of the PPS does not apply to the subject site for the reasons noted above, and for the fact that the subject lands and the adjacent lands are not identified as significant woodlands. Consequently, policy 2.1.8 is not a relevant policy for discussions pertaining to the proposed development on the subject land.
- Notwithstanding the above comment, it is good planning to demonstrate that the development has regard for the ecological function of the subject lands. This has been achieved through the applicant's submission of a tree conditioning report containing tree inventory, preservation and compensation plan as well as submission of Endangered Species Act Report containing mitigation measures for the proposed development.
- This planning report shows that the proposed development addresses Endangered Species and desirable trees on the subject lands to the satisfaction of the municipality.

**City's Landscape Architect:** A number of conditions are stipulated for the purpose of preserving existing desirable trees and ensuring that compensation plan is implemented for those desirable trees on the subject lands that could not be saved. See Appendix C, herein attached, for detailed comments. This report incorporates the requirements in the Landscape Architect's comments.

## 2. PUBLIC NOTICE

The official notice of the statutory public meeting will be advertised in the local newspaper, the Windsor Star.

Courtesy notice will be mailed to all properties within 120m (400 feet) of the subject parcel prior to the Development & Heritage Standing Committee (DHSC) meeting.

## Planner's Conclusion and Opinion:

The information provided by the applicant's Land Surveyor and Planning Consultant confirm that the proposed Draft Plan of Subdivision contain 33 residential building lots that comply with the lot width and lot area requirements for the development of single unit dwellings on lands zoned

RD1.4 by Zoning By-law 8600. The proposed development will provide housing opportunities in the subject area and City.

In my professional opinion, the Draft Plan of Subdivision is consistent with the relevant policies of the Provincial Policy Statement 2020 cited in this report. The Draft Plan of Subdivision also conforms with the City of Windsor Official Plan and constitutes good planning.

Administration is recommending that the Draft Plan of Subdivision be approved subject to the conditions found in the Recommendation section of this planning report.

Furthermore, Administration is requesting and recommending approval of a Zoning change from HRD1.4 to GD1.5 for Block 34 and from HRD1.4 to GD1.1 for Block 35.

### **Planning Act Matters:**

I concur with the above comments and opinion of the Registered Professional Planner.

**Michael Cooke, MCIP, RPP**  
Manager, Planning Policy / Deputy City Planner

**Thom Hunt, MCIP, RPP**  
City Planner/ Executive Director

I am not a registered Planner and have reviewed as a Corporate Team Leader

**JP, Commissioner, Economic Development & Innovation**      **SAH, Chief Administrative Officer(A)**

### **Approvals:**

<b>Name</b>	<b>Title</b>
Michael Cooke	Manager of Planning Policy/Deputy City Planner
Thom Hunt	City Planner / Executive Director, Planning & Development Services
Wira Vendrasco	Deputy City Solicitor, Legal Services & Real Estate
Jelena Payne	Commissioner, Economic Development & Innovation
Shelby Askin Hager	Chief Administration Officer (A)

### **Notifications:**

<b>Name</b>	<b>Address</b>	<b>Email</b>
Abutting property owners and tenants within 120 meter (400 feet) radius of the subject land		
Applicant/Registered Owner: Bellocorp Inc., (c/o Tosin Bello)	1185 North Talbot Rd., Windsor ON N9A 6J3	bellocorpdevelopments@gmail.com
Agent: Pillion-Abbs Inc. (c/o Tracey Pillion-Abbs)	23669 Prince Albert Rd, Chatham, ON N7M 5J7	tpillionabbs@gmail.com
Ontario Land Surveyor: VERHAEGEN Land Surveyors (c/o Roy Simone)	944 Ottawa St., Windsor ON N8X 2E1	rsimone@vshbbsurveys.com
Councillor Fred Francis	350 City Hall Sq. W., Suite 220, Windsor, ON N9A 6S1	ffrancis@citywindsor.ca

### **Appendices:**

- 1 Appendix A, Site Photos
- 2 Appendix B-1, Map No. SDN-001/21-1, dated May 20, 2022
- 3 Appendix B-2, Lot Width and Lot Area for Map No. SDN-001/21-1
- 4 Appendix C, Consultations - Comments from Departments & External Agencies
- 5 Appendix D, BAIRD AE's response to ERCA's Stormwater questions





PARTIAL VIEW OF SUBJECT LAND, LOOKING WEST FROM STONEYBROOK  
CRESCENT, AT 1255 NORTH TALBOT RD

## APPENDIX A – SITE PHOTOS taken on May 12, 2021

File SDN-001/21



**VIEW OF HOMES ON STONEYBROOK CRESCENT  
ABUTTING SOUTH OF SUBJECT LANDS**



**VIEW OF RESIDENTIAL CARE FACILITY  
ABUTTING EAST OF SUBJECT LANDS**





LOOKING SOUTH FROM SOUTHWOOD LAKES &  
NORTH TALBOT INTERSECTION



LOOKING EAST FROM SOUTHWOOD LAKES &  
NORTH TALBOT INTERSECTION

## VIEWS OF ABUTTING STONEY PARK AND MULTI-USE TRAIL ON THE WEST SIDE OF SUBJECT LANDS





VIEWS OF THE SUBJECT LANDS AND  
SURROUNDING FEATURES, LOOKING EAST  
FROM SOUTHWOOD LAKES BLVD  
INTERSECTION WITH NORTH TALBOT RD





VIEW OF NORTH TALBOT ROAD, LOOKING WEST FROM PIONEER AVENUE INTERSECTION, SHOWING FRONTAGE OF SUBJECT LANDS ON THE SOUTH SIDE





VIEW OF NORTH  
TALBOT, LOOKING  
EAST AT OLD WEST  
AVENUE, SHOWING  
FRONTAGE OF 1085  
and 1255 NORTH  
TALBOT RD



VIEW OF NORTH  
TALBOT, LOOKING  
WEST AT OLD  
WEST AVENUE,  
SHOWING  
FRONTAGE OF  
SUBJECT LANDS



VIEW OF NORTH TALBOT, LOOKING SOUTH AT OLD WEST AVENUE  
INTERSECTION, SHOWING FRONTAGE OF SUBJECT LANDS





PARTIAL VIEW OF EXISTING HOMES  
AND MUNICIPAL PARK AT OLD  
WEST AVENUE & NORTH TALBOT  
INTERSECTION, ACROSS FROM THE  
SUBJECT LANDS

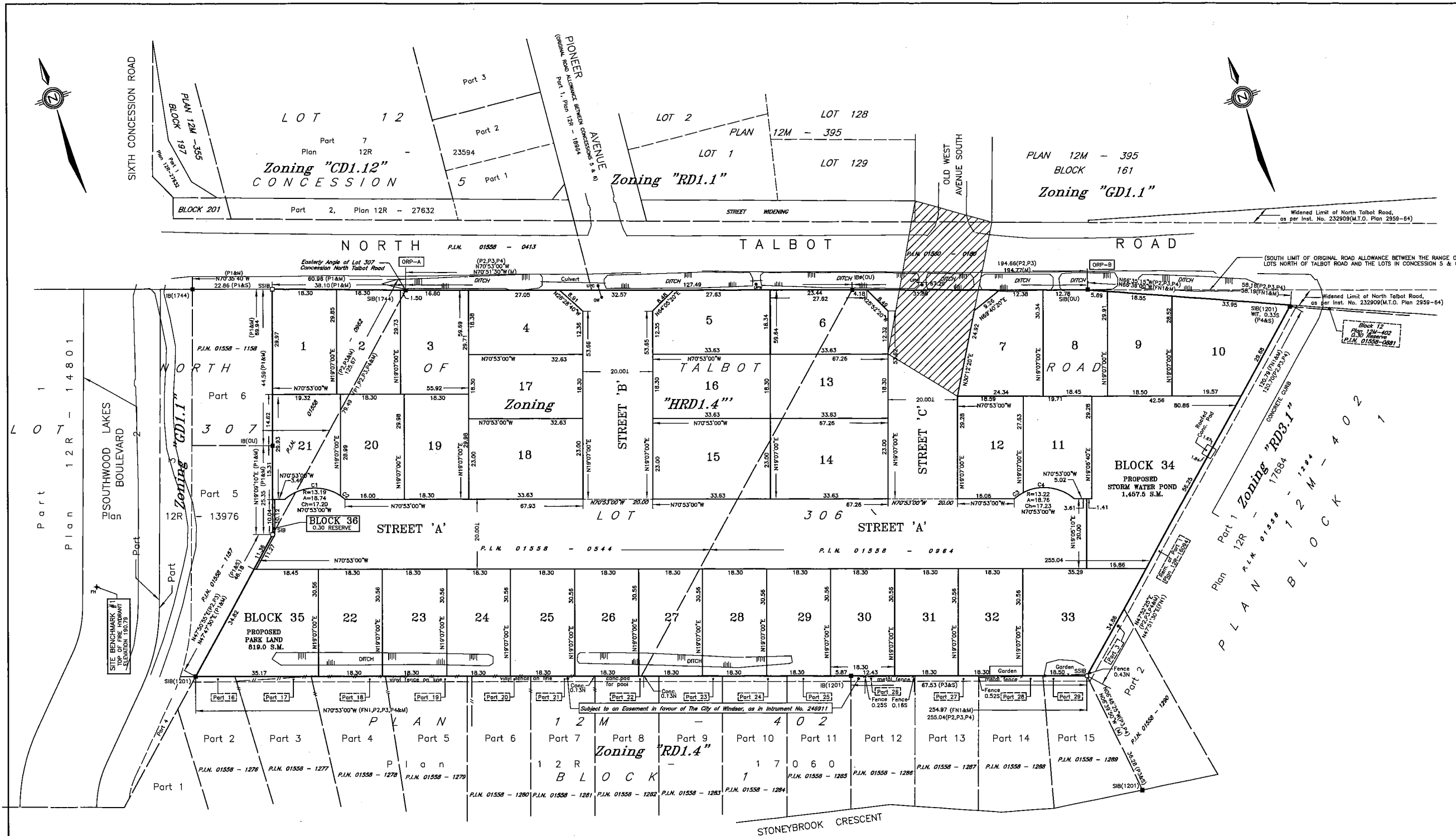
PARTIAL VIEW OF EXISTING  
MUNICIPAL PARK (NORTH  
TALBOT PARK) AT OLD WEST  
AVENUE ACROSS FROM THE  
SUBJECT LANDS







PARTIAL VIEW OF EXISTING SINGLE UNIT DWELLING AT 1085 NORTH  
TALBOT, LOOKING SOUTHWEST FROM OLD WEST AVENUE



KEY MAP  
NOT TO SCALE

**DRAFT PLAN OF SUBDIVISION**  
OF  
PART OF LOTS 306 AND 307  
CONCESSION NORTH OF TALBOT ROAD  
GEOGRAPHIC TOWNSHIP OF SANDWICH SOUTH  
NOW IN THE  
CITY OF WINDSOR  
COUNTY OF ESSEX, ONTARIO  
© VERHAEGEN LAND SURVEYORS

SCALE = 1:500

**LEGEND AND NOTES**  
BEARINGS ARE UTM GRID DERIVED FROM OBSERVED REFERENCE POINTS "A" AND "B" BY  
REAL TIME NETWORK OBSERVATIONS AND ARE REFERRED TO UTM ZONE 17 (81° WEST  
LONGITUDE) NAD 83 (CSRS) (2010.0).

DISTANCES ON THIS PLAN ARE GROUND AND CAN BE CONVERTED TO GRID BY MULTIPLYING  
BY THE COMBINED SCALE FACTOR OF 0.99990326.

- LEGEND**
- DENOTES SURVEY MONUMENT FOUND
  - DENOTES SURVEY MONUMENT SET
  - SIB DENOTES STANDARD IRON BAR
  - SSIB DENOTES SHORT STANDARD IRON BAR
  - IB DENOTES IRON BAR
  - PS DENOTES PLASTIC BAR
  - CP DENOTES STEEL PIN
  - CC DENOTES CUT-CROSS
  - WT DENOTES WITNESS
  - M DENOTES MEASURED
  - L DENOTES PERPENDICULAR
  - S DENOTES SET
  - ORP DENOTES OBSERVED REFERENCE POINT
- ALL SET SIB AND PS MONUMENTS WERE USED DUE TO LACK OF OVERBURDEN  
AND PROXIMITY OF UNDERGROUND UTILITIES IN ACCORDANCE WITH  
SECTION 11 (4) OF OREG. 325/91.
- (OU) DENOTES ORIGIN UNKNOWN
  - (P1) DENOTES PLAN 12R-13976
  - (P2) DENOTES PLAN 12M-402
  - (P3) DENOTES PLAN 12R-17060
  - (P4) DENOTES PLAN 12R-17684
  - (FN1) DENOTES FIELD NOTE BY
  - (1744) DENOTES VERHAEGEN LAND SURVEYORS OLS
  - (1201) DENOTES CLARKE SURVEYORS INC., O.L.S.
  - ✚ FH DENOTES FIRE HYDRANT
  - UPC DENOTES UTILITY POLE CONCRETE
  - UPW DENOTES UTILITY POLE WOOD
  - GW DENOTES GUT WIRE

**OWNER'S CERTIFICATE**  
I HEREBY AUTHORIZE THE FILING OF THIS DRAFT PLAN FOR APPROVAL.

DATE: \_\_\_\_\_  
"I HAVE THE AUTHORITY TO BIND THE CORPORATION"

**SURVEYOR'S CERTIFICATE**  
I CERTIFY THAT:  
THE BOUNDARIES OF THE LANDS TO BE SUBDIVIDED AND THEIR RELATIONSHIP  
TO THE ADJACENT LANDS HAVE BEEN ACCURATELY AND CORRECTLY SHOWN.

DATE: MAY 19, 2022  
*Roy Simone*  
ROY A. SIMONE  
ONARIO LAND SURVEYOR

**INTEGRATION DATA**

COORDINATES ARE DERIVED FROM GRID OBSERVATIONS USING THE CAN-NET  
NETWORK SERVICE AND ARE REFERRED TO UTM ZONE 17 (81° WEST LONGITUDE)  
NAD83 (CSRS) (2010.0).  
COORDINATE VALUES ARE TO AN URBAN ACCURACY IN ACCORDANCE WITH  
SECTION 14(2) OREG. 216/10

POINT ID	NORTHING	EASTING
ORP-A	N4678646.40	E336574.75
ORP-B	N4678562.53	E336758.76

COORDINATES CANNOT, IN THEMSELVES, BE USED TO RE-ESTABLISH CORNERS  
OR BOUNDARIES SHOWN ON THIS PLAN.

FOR BEARING COMPARISON, A ROTATION 01°00'25" CLOCKWISE WAS APPLIED TO (P2), (P3)  
& (P4) TO CONVERT TO GRID BEARINGS.  
FOR BEARING COMPARISON, A ROTATION 00°59'20" CLOCKWISE WAS APPLIED TO (P1)  
TO CONVERT TO GRID BEARINGS.  
FOR BEARING COMPARISON, A ROTATION 01°02'30" CLOCKWISE WAS APPLIED TO (FN1)  
TO CONVERT TO GRID BEARINGS.

**ADDITIONAL INFORMATION**  
REQUIRED UNDER SECTION 51(17) OF  
THE PLANNING ACT, (R.S.O. 1990)

(A) AS SHOWN ON PLAN	(F) AS SHOWN ON PLAN
(B) AS SHOWN ON PLAN	(G) AS SHOWN ON PLAN
(C) AS SHOWN ON PLAN	(H) PIPED MUNICIPAL
(D) SINGLE FAMILY RESIDENTIAL	(I) CLAY LOAM
(E) NORTH - EXISTING RESIDENCE	(J) AS SHOWN ON PLAN
WEST - EXISTING RESIDENTIAL	(K) ALL SERVICES TO BE PROVIDED
EAST - EXISTING RESIDENTIAL	(L) AS SHOWN ON PLAN
SOUTH - EXISTING RESIDENTIAL	

**SUMMARY**  
TOTAL NUMBER OF LOTS 33  
TOTAL NUMBER OF BLOCKS 3  
TOTAL AREA OF SITE 2.932 Hectares  
DENSITY 11.3 Lots per Hectares

**CURVE SCHEDULE**

CURVE	RADIUS	ARC	CHORD	CHORD BEARING
C1	13.19	17.10	15.93	N74°26'30"W
C2	13.19	1.84	1.84	N33°44'40"W
C3	13.22	3.08	3.08	N75°05'30"E
C4	13.22	15.70	14.80	N64°14'50"W

"METRIC" DISTANCES AND COORDINATES SHOWN ON THIS PLAN  
ARE IN METRES AND CAN BE CONVERTED TO FEET BY  
DIVIDING BY 0.3048

**MAP No. SDN-001/21-1**

**VERHAEGEN** SURVEYING  
LAND SURVEYORS  
A DIVISION OF J.D. BARNES LTD.  
944 OTTAWA STREET, WINDSOR, ON, N6X 2E1  
T: (519) 258-1772 F: (519) 258-1791 www.jdbarnes.com

DRAWN BY: D.J.	CHECKED BY: RAS	REFERENCE NO.: 21-47-019-01
CAD File: 21-47-019-02.dwg	E-WIND-NTR-308	CAD Date: May 20, 2022 10:23 AM



## Draft Plan of Subdivision Dated May 19, 2022

Area and Lot Width Table (May 19, 2022)

J.D.Barnes Reference No. 21-47-019-02

Lot/Block	Lot Width	Area (Square Metres)
LOT 1	18.30	548.3
LOT 2	18.30	545.4
LOT 3	18.30	573.6
LOT 4	18.38	579.7
LOT 5	18.35	598.8
LOT 6	18.34	598.4
LOT 7	24.34	630.3
LOT 8	18.45	558.6
LOT 9	18.50	540.6
LOT 10	19.57	737.1
LOT 11	19.71	541.9
LOT 12	18.59	542.1
LOT 13	18.30	615.5
LOT 14	23.00	773.5
LOT 15	23.00	773.8
LOT 16	18.30	615.5
LOT 17	18.30	597.1
LOT 18	23.00	750.9
LOT 19	18.30	548.7
LOT 20	18.30	548.1
LOT 21	19.32	542.7
LOT 22	18.30	559.2
LOT 23	18.30	559.2
LOT 24	18.30	559.2
LOT 25	18.30	559.2
LOT 26	18.30	559.2
LOT 27	18.30	559.2
LOT 28	18.30	559.2
LOT 29	18.30	559.2
LOT 30	18.30	559.2
LOT 31	18.30	559.2
LOT 32	18.30	559.2
LOT 33	26.90	821.4
BLOCK 34		1457.5
BLOCK 35		819.0
BLOCK 36		6.8
STREET A		4757.6
STREET B		1230.8
STREET C		1230.1



## **APPENDIX C - COMMENTS**

### **ENBRIDGE – Kelly Buchanan**

It is Enbridge Gas Inc.'s (formerly Union Gas Ltd) request that as a condition of final approval that the owner/developer provide to Union the necessary easements and/or agreements required by Union for the provision of gas services for this project, in a form satisfactory to Enbridge.

### **CANADA POST – Bruno DeSando**

Thank you for contacting Canada Post regarding plans for a new subdivision in the City of Windsor. Please see Canada Post's feedback regarding the proposal, below.

#### **Service type and location**

1. Canada Post will provide mail delivery service to the subdivision through centralized Community Mail Boxes (CMBs).
2. If the development includes plans for (a) multi-unit building(s) with a common indoor entrance, the developer must supply, install and maintain the mail delivery equipment within these buildings to Canada Post's specifications.

#### **Municipal requirements**

1. Please update our office if the project description changes so that we may determine the impact (if any).
2. Should this subdivision application be approved, please provide notification of the new civic addresses as soon as possible.

#### **Developer timeline and installation**

Please provide Canada Post with the excavation date for the first foundation/first phase as well as the date development work is scheduled to begin. Finally, please provide the expected installation date(s) for the CMB(s).

**Please see Appendix A for any additional requirements for this developer.**

### **Appendix A**

#### **Additional Developer Requirements:**

- The developer will consult with Canada Post to determine suitable permanent locations for the Community Mail Boxes. The developer will then indicate these locations on the appropriate servicing plans.
- The developer agrees, prior to offering any units for sale, to display a map on the wall of the sales office in a place readily accessible to potential homeowners that indicates the location of all Community Mail Boxes within the development, as approved by Canada Post.
- The developer agrees to include in all offers of purchase and sale a statement which advises the purchaser that mail will be delivered via Community Mail Box. The developer also agrees to note the locations of all Community Mail Boxes within the development, and to notify affected homeowners of any established easements granted to Canada Post to permit access to the Community Mail Box.

- The developer will provide a suitable and safe temporary site for a Community Mail Box until curbs, sidewalks and final grading are completed at the permanent Community Mail Box locations. Canada Post will provide mail delivery to new residents as soon as the homes are occupied.
- The developer agrees to provide the following for each Community Mail Box site and to include these requirements on the appropriate servicing plans:
  - Any required walkway across the boulevard, per municipal standards
  - Any required curb depressions for wheelchair access, with an opening of at least two metres (consult Canada Post for detailed specifications)
  - A Community Mailbox concrete base pad per Canada Post specifications.

**ASSESSMENT MANAGEMENT OFFICER - Jose Mejalli**

No objection to the 34 lot subdivision and related roadways and change in zoning.

**TRANSIT WINDSOR - Jason Scott**

Transit Windsor has no objections to this development. The closest existing transit route to this proposed subdivision is with the Walkerville 8. The closest existing bus stops are located on North Talbot at Sixth Concession, Pioneer, and Old West. All of the proposed subdivision would be within our walking distance guidelines of 400 metres. The transit service will be changing in this area with our Council approved Transit Master Plan as the area will be getting a new 2 way conventional transit route along Southwood Lakes to replace the existing 1 way loop that is currently present along North Talbot. All of the proposed subdivision would still be within the walking distance guideline with this change.

**ENVIRONMENTAL & SUSTAINABILITY COORDINATOR - Jennifer Nantais**

In response to the application for a Plan of Subdivision there are no objections. Please also note the following comments for consideration:

**Energy Conservation, Air Quality and Climate Change:**

Please note PPS 2020 energy conservation and efficiency policies as they relate to long-term economic prosperity (1.7.1 (j)), as well as improved air quality and reduced greenhouse gas emissions (1.8.1). In addition, the City of Windsor Community Energy Plan (approved July 17 2017) aims to improve energy efficiency; modifying land use planning; reducing energy consumption and greenhouse gas emissions; and fostering green energy solutions throughout Windsor, while supporting local economic development.

As per these policies the developer should consider energy efficiency in the building design. This may include but not be limited to increased insulation, energy efficient appliances and fixtures, high efficiency windows and doors. In addition, consideration for EV charging infrastructure and opportunities to increase resiliency such as providing strategic back-up power capacity is warranted.

In addition, the large scale paving of natural space will increase the urban heat island in the area. It is recommended that the developer consider shade trees, white colour roofs or green roofs to mitigate this impact. For more suggestions please consult the following resources: LEED, Built Green Canada, and EnerGuide.

To promote the use of active transportation, bike racks should be included.

#### Stormwater Management:

Consideration should be given, as per PPS 2020 Section 1.6.6.7 to maximize the extent and function of vegetative and pervious surfaces; and promote stormwater management best practices, including stormwater attenuation and reuse, water conservation and efficiency, and low impact development.

#### Landscaping

Consideration for shade trees are recommended to minimize the urban heat island impacts. Consideration of native, drought resistant plants is encouraged to limit watering requirements.

The Environmental Sustainability & Climate Change team supports the comments submitted by the Landscape Architect on this application.

In addition we encourage the developer to consider community gardening space for residents. Local food production is very popular in Windsor and considering the size of this development a space for community garden boxes is warranted.

#### **HYDRO ONE – Dolly Shetty**

We are in receipt of Application SDN-001/21 [SDN/6575] - BelloCorp Inc., dated January 11, 2022. We have reviewed the documents concerning the noted Plan and have no comments or concerns at this time. *Our preliminary review considers issues affecting Hydro One's 'High Voltage Facilities and Corridor Lands' only.*

For proposals affecting 'Low Voltage Distribution Facilities' please consult your local area Distribution Supplier.

To confirm if Hydro One is your local distributor please follow the following link:

<http://www.hydroone.com/StormCenter3/>

Please select “ Search” and locate address in question by entering the address or by zooming in and out of the map

If Hydro One is your local area Distribution Supplier, please contact Customer Service at 1-888-664-9376 or e-mail [CustomerCommunications@HydroOne.com](mailto:CustomerCommunications@HydroOne.com) to be connected to your Local Operations Centre.

#### **BUILDING DEPARTMENT- Barbara Rusan**

The Building Code Act, Section 8.(1) requires that a building permit be issued by the Chief Building Official for any construction or demolition of a building. It is strongly recommended that the owner

and/or applicant contact the Building Division to determine building permit needs for the proposed project. The City of Windsor Building Division can be reached by phone at 519-255-6267 or through email at [buildingdept@citywindsor.ca](mailto:buildingdept@citywindsor.ca)

### **WINDSOR MAPPING, ENBRIDGE - Sandro Aversa C.E.T.**

After reviewing the provided drawing and consulting our mapping system, please note that Enbridge Gas has active infrastructure in the proposed area. A PDF drawing has been attached for reference.

Please NOTE:

1. The shown piping locations are approximate and for information purposes only
2. The drawings are not to scale
3. This drawing does not replace field locates. Please contact Ontario One Call for onsite locates prior to excavating, digging, etc

Enbridge Gas requires a minimum separation of 0.6m horizontal and 0.3m vertical from all of our plant less than NPS 16 and a minimum separation 1.0m horizontal and 0.6m vertical between any CER-regulated and vital pipelines. For all pipelines (including vital pipelines), when drilling parallel to the pipeline, a minimum horizontal clearance measured from the edge of the pipeline to the edge of the final bore hole of 1 m (3.3 ft) is required. Please ensure that this minimum separation requirement is maintained, and that the contractor obtains locates prior to performing any work and utilizes safe excavation practices while performing any work in the vicinity.

Also, please note the following should you find any abandoned infrastructure in the area:

- Any pipe that is excavated, please assume that it is live
- If during the course of any job, any pipe is found that is not on the locate sheet and is in conflict with your work, please call our emergency number (1-877-969-0999), and one of our Union Gas representatives will respond to determine if that plant is in fact live or dead
- Please note that our Enbridge Gas representative will respond to the live or dead call within 1-4 hours, so please plan your work accordingly

### **HERITAGE PLANNER - Kristina Tang**

No supporting information required.

There is no apparent built heritage concern with this property and it is located on an area of low archaeological potential.

Nevertheless, the Applicant should be notified of the following archaeological precaution.

1. Should archaeological resources be found during grading, construction or soil removal activities, all work in the area must stop immediately and the City's Planning & Building Department, the City's Manager of Culture and Events, and the Ontario Ministry of Heritage, Sport, Tourism and Culture Industries must be notified and confirm satisfaction of any archaeological requirements before work can recommence.
2. In the event that human remains are encountered during grading, construction or soil removal activities, all work in that area must be stopped immediately and the site secured. The local police or coroner must be contacted to determine whether or not the skeletal remains are

human, and whether the remains constitute a part of a crime scene. The Local police or coroner will then notify the Ontario Ministry of Heritage, Sport, Tourism and Culture Industries and the Registrar at the Ministry of Government and Consumer Services if needed, and notification and satisfactory confirmation be given by the Ministry of Heritage, Sport, Tourism and Culture Industries.

Contacts:

Windsor Planning & Building Department:

519-255-6543 x6179, [ktang@citywindsor.ca](mailto:ktang@citywindsor.ca), [planningdept@citywindsor.ca](mailto:planningdept@citywindsor.ca)

Windsor Manager of Culture and Events (A):

Michelle Staaedegaard, (O) 519-253-2300x2726, (C) 519-816-0711,  
[mstaaedegaard@citywindsor.ca](mailto:mstaaedegaard@citywindsor.ca)

Ontario Ministry of Heritage, Sport, Tourism and Culture Industries

Archaeology Programs Unit, 1-416-212-8886, [Archaeology@ontario.ca](mailto:Archaeology@ontario.ca)

Windsor Police: 911

Ontario Ministry of Government & Consumer Services

A/Registrar of Burial Sites, War Graves, Abandoned Cemeteries and Cemetery Closures,  
1-416-212-7499, [Crystal.Forrest@ontario.ca](mailto:Crystal.Forrest@ontario.ca)

**PARKS DEVELOPMENT - Sherif Barsom**

We have reviewed the submitted application and noticed that there was a Tree Condition Report submitted for this site. At this point Parks Development has no comments pertaining this submission.

Noting that the City Forestry team has to review the report and comment on it if any.

**ERCA**

The following is provided, as a result of our review, to consider, Draft Plan of Subdivision (SDN-001-21 / SDN- 6575).

The applicant requests approval of a draft plan of Subdivision for the development of 34 residential building lots for single unit detached dwellings, on the parcels described, as, Part Lot 306 and Part Lot 307, Concession N Talbot Rd., Sandwich East, Windsor. The draft plan includes 3 new roadways (Streets A, B & C), and 3 Blocks (Block 35 for SWM facility; Block 36 & 37 for Reserves), identified as part of the proposed residential subdivision development. Two of the proposed new roads provide direct vehicular access from/to North Talbot Road.

The subject lands are designated Residential on Schedule D – Land Use, of the Official Plan and zoned Residential District 1.4 with a holding provision (HRD1.4) by Bylaw 8600. Subsection 20(1)85 of By-law 8600 applies to the subject lands and requires a minimum front yard depth of 9m for any lot fronting on North Talbot Rd between Southwood Lakes Blvd and HWY 401.

It is the ERCA understanding that the existing holding symbol maybe removed, when a Final Plan of Subdivision has been registered, for the site and when the applicant submits an application to remove the holding symbol.



## DELEGATED RESPONSIBILITY TO REPRESENT THE PROVINCIAL INTEREST IN NATURAL HAZARDS AND REGULATORY RESPONSIBILITIES ASSOCIATED WITH THE CONSERVATION AUTHORITIES ACT

The following comments reflect our role as representing the provincial interest in natural hazards as outlined by Section 3.1 of the *Provincial Policy Statement of the Planning Act*, as well as our regulatory role as defined by Section 28 of the *Conservation Authorities Act*.

We have reviewed our floodline mapping for this area and it has been determined this site is not located within a regulated area that is under the jurisdiction of the ERCA (Section 28 of the *Conservation Authorities Act*).

As a result, a permit is not required from ERCA for issues related to Section 28 of the *Conservation Authorities Act*, Development, Interference with Wetlands and Alteration to Shorelines and Watercourses Regulation under the *Conservations Authorities Act*, (Ontario Regulation No. 158/06).

Please note the ERCA will still require a *Development Review Clearance* for this proposal, given the size and scale of the proposed residential development.

## PLANNING ADVISORY SERVICE TO PLANNING AUTHORITIES - NATURAL HERITAGE POLICIES OF THE PPS, 2020

The following comments are provided from our perspective, as an advisory service provider to the Planning Authority, on matters related to natural heritage and natural heritage systems, as outlined in Section 2.1 of the *Provincial Policy Statement (PPS)* of the *Planning Act*. The comments in this section do not necessarily represent the provincial position and are advisory in nature for the consideration of the Planning Authority.

The subject property contains a natural heritage feature that may meet criteria for significant woodland, significant wildlife habitat and/or habitat of endangered species and threatened species. A 'species at risk study' and a "tree inventory" is not sufficient, in order to meet the requirements of PPS natural heritage policies. The proposal is to develop the entire Natural Heritage feature. This would not meet the requirements of PPS policies to demonstrate no negative impact.

Therefore, this application should be *Deferred*, pending completion of a *Environmental Evaluation Report (EER / EIA)*, documenting ALL species on the site and evaluating the site in accordance with EIA guidelines.

## WATERSHED BASED RESOURCE MANAGEMENT AGENCY

The following comments are provided in an advisory capacity, as a public commenting body on matters related to watershed management.

### SECTION 1.6.6.7 Stormwater Management (PPS, 2020)

ERCA has concerns with the potential impact of the quality and quantity of runoff in the downstream watercourse due to the proposed development on this site.

ERCA recommends that stormwater quality and stormwater quantity will need to be addressed, up to and including the 1:100 year storm event and be in accordance with the guidance provided by the "*Stormwater Management Planning and Guidance Manual, prepared by the Ministry of the*

*Environment (MOE, March 2003)" and the "Windsor-Essex Region Stormwater Management Standards Manual".*

#### FINAL RECOMMENDATIONS

This application should be "*Deferred*", pending completion of an *Environmental Evaluation Report (EER / EIA)*, documenting ALL species on the site and evaluating the site in accordance with EIA guidelines.

In addition, we therefore request inclusion of the following draft conditions, in the Subdivision Agreement:

1. That the subdivision agreement between the Owner and the Municipality contain provisions, to the satisfaction of the "Municipality" and the "Essex Region Conservation Authority", that stipulate that the Owner will finalize the engineering analysis to identify stormwater quality and quantity measures as necessary to control any increase in flows in downstream watercourses, up to and including the 1:100 year design storm and in accordance with the *Windsor-Essex Stormwater Management Standards Manual*.
2. That the subdivision agreement between the Owner and the Municipality contain provisions that requires that the Owner installs the stormwater management measures identified in the engineering analysis completed as part of the development for the site and undertake to implement the recommendations contained therein, to the satisfaction of the "Municipality" and the "Essex Region Conservation Authority".
3. That prior to undertaking construction or site alteration activities, any necessary permits or clearances be received from the Essex Region Conservation Authority (ERCA), in accordance with Section 28 of the *Conservation Authorities Act*. Alternatively, if not regulated, by the ERCA, any required development review clearances.
4. That the subdivision agreement between the Owner and the Municipality contain provisions that require the Owner to implement any and / or all recommendations of a final *Environmental Evaluation Report (EER / EIA)*, to the satisfaction of the Municipality and the ERCA. Recommendations from this report shall be implemented in the design and construction phases of the development (*note: this draft condition is subject to the ERCA request for a Deferral of this application at this time*).
5. That prior to final approval, the Essex Region Conservation Authority shall require a copy of the fully executed subdivision agreement between the Owner and the Municipality, in wording acceptable to the Essex Region Conservation Authority, containing provisions to carry out the recommendations of the final plans, reports and requirements noted above.

We have no objections to the application for Draft Plan of Subdivision at this time, subject to the draft conditions noted above and the consideration of a *deferral* at this time, pending submission of an *Environmental Evaluation Report (EER / EIA)*, documenting ALL species on the site and evaluating the site in accordance with EIA guidelines.

We ask the County of Essex or the approval authority, in this case, to forward a copy of the Notice of Decision, including a copy of the Draft Approved Plan for our records.

We also request to be notified, on any future circulations for this application or notifications, regarding this proposed plan of subdivision.

The ERCA has also provided some preliminary comments on the Stormwater Management Plan to facilitate your review (see attached).

Reviewed the following for SWM considerations:

- 1095 North Talbot Residential SWM and FS Report (Project No. 21-021)

Provide the following SWM comments:

- Please confirm is the 100 year SCS event modelled?
- Why is the UST HWL lower than the 100 yr HWL?
- Discharge is to be restricted via a 377 mm diameter plate. Does this size exist? Or will it be custom made?
- Is there an overland spill route to the pond or the ROW? Proposed Street A – Has a HP on the west boundary edge of 190.35 m and on the east edge of 190.08 m. The dry pond has a TOB of 190.27 m. It appears that the overland flow route will spill into the existing adjacent property (1255 North Talbot Rd) for events exceeding the stress test.
- Is an easement required behind blocks 7, 9, 9, 35 for the 250 mm storm sewer. If this is the case, please provide confirmation that the easement is satisfactory to the City.
- Sewer design sheet is missing A-7, Tc is selected as 10 mins but inlet time shows 20 mins. Please clarify.
- What is the purpose of the ditches on the North and South end of the property? Are these for storage or conveyance? Where is the water directed to?
- Since the proposed development is adjacent to existing developed blocks on the southern edge, can you please confirm if the existing grading will outlet into the proposed rear yard cbs? If so, will the provided SWM need to account for additional drainage requirements posed by these lots?

Please NOTE: At this time, ERCA is requesting a **deferral** of the planning application, pending completion of an *Environmental Evaluation Report (EER/EIA)*, documenting ALL species on the site and evaluating the site in accordance with EIA guidelines.

### **Rania Toufeili – Transportation**

- North Talbot Road is classified as a Class I Collector with a required right-of-way width of 25.3 meters per Schedule X of the Official Plan. The current right-of-way is sufficient per the Sixth Concession Road/North Talbot Road Environmental Assessment and therefore no conveyance is required along the roadway.
- Corner cut-offs of 4.6 meters are required on North Talbot Road and Streets B and C (lots 4, 5, 6 and 7). The submitted plan shows corner cut-offs.
- The new proposed Street B and Street C should align with the intersections to the north of Pioneer Avenue and Old West Avenue South respectively.
- Driveways for lots 4, 5, 6 and 7 should be set as far back as possible from the intersections. Furthermore, lots that are abutting two streets should have driveways on the lower classification street (local road).
- Sidewalk contributions are to be made as required and outlined by Engineering Right-of-Way.
- Detailed and dimensioned drawings showing the proposed driveways, curb cuts and cul-de-sac design are required to provide further comments on conveyances and additional

requirements. All roadways should be constructed to City of Windsor Standards Engineering Drawings.

- A pedestrian connection should be provided from Street A to the trail on Southwood Lakes Boulevard. A standard cul-de-sac bulb will need to be provided for Street A and therefore a sidewalk connection should be accommodated with sufficient space at this cul-de-sac.
- Parking restrictions and required by-law amendments will be reviewed at the engineering drawings review stage.
- All accesses shall conform to the TAC Geometric Design Guide for Canadian Roads and the City of Windsor Standard Engineering Drawings.
- All exterior paths of travel must meet the requirements of the *Accessibility for Ontarians with Disabilities Act* (AODA).

### **Bell Canada – Circulations**

We have reviewed the circulation regarding the above noted application and have no objections to the application as this time. However, we hereby advise the Owner to contact Bell Canada at [planninganddevelopment@bell.ca](mailto:planninganddevelopment@bell.ca) during detailed design to confirm the provisioning of communication/telecommunication infrastructure needed to service the development. We would also ask that the following paragraph be included as a condition of approval:

“The Owner agrees that should any conflict arise with existing Bell Canada facilities where a current and valid easement exists within the subject area, the Owner shall be responsible for the relocation of any such facilities or easements at their own cost.”

It shall also be noted that it is the responsibility of the Owner to provide entrance/service duct(s) from Bell Canada’s existing network infrastructure to service this development. In the event that no such network infrastructure exists, in accordance with the Bell Canada Act, the Owner may be required to pay for the extension of such network infrastructure.

If the Owner elects not to pay for the above noted connection, Bell Canada may decide not to provide service to this development.

To ensure that we are able to continue to actively participate in the planning process and provide detailed provisioning comments, we note that we would be pleased to receive circulations on all applications received by the Municipality and/or recirculations.

Please note that WSP operates Bell’s development tracking system, which includes the intake of municipal circulations. WSP is mandated to notify Bell when a municipal request for comments or for information, such as a request for clearance, has been received. All responses to these municipal circulations are generated by Bell, but submitted by WSP on Bell’s behalf. WSP is not responsible for Bell’s responses and for any of the content herein.

If you believe that these comments have been sent to you in error or have questions regarding Bell’s protocols for responding to municipal circulations and enquiries, please contact [planninganddevelopment@bell.ca](mailto:planninganddevelopment@bell.ca).

### **Enwin**

**Hydro Engineering:** No Objection, however, a hydro easement may be required to accommodate the existing hydro anchor on the northwest corner of Street B on the draft plan. See attached sketch.

**Water Engineering:** Water Engineering has no objections.

### **Patrick Winters – Engineering**

The applicant is requesting approval for a Draft Plan of Subdivision consisting of 34 lots for single unit detached dwellings. The draft plan includes 3 new roadways (Streets A, B & C), and 3 blocks (Block 35 for SWM facility; Block 36 & 37 for Reserves) identified as part of the proposed residential subdivision development. The subject land is currently zoned (H) RD1.4 under By-law 8600 and designated as residential use under the City of Windsor Official Plan.

After reviewing the servicing requirements of the subject lands pertinent to the subject application, we have the following comments:

#### **Roads and Right-of Way**

North Talbot Road between Sixth Concession Road and the City limits is a municipal roadway classified as a Class I Collector. The section of road falls within the boundary of the 6<sup>th</sup> Concession Road/North Talbot Environmental Study Report [by Dillon Consulting dated April 2016]. The ESR does not identify any property requirements across the frontage of the subject lands

The new proposed Street B and Street C should align with the intersections to the north of North Talbot Road, Pioneer Avenue and Old West Avenue, respectively. Proper cul-de-sacs bulbs would be required at both ends of Street A. The driveways for lots 4, 5, 6 and 7 should be set as far back as possible from the intersections. Furthermore, lots that are abutting two streets should have driveways on the lower classification street (local road). This section of North Talbot Road currently has a rural cross section, and as such contributions are requested in the amounts of \$33,500.00 and \$17,750.00 towards future construction of concrete sidewalks as well as curbs and gutters, respectively, on the North Talbot Road frontage of the subject lands.

There are City trees and hydro poles in the right-of-way; therefore, the City Forester and respective utility companies should be contacted to see what measures need to be taken to resolve these impediments in the right-of-way prior permit issuance.

The subject lands fall within the Ministry of Transportation (MTO) permit control area. The owner will be required, prior to the issuance of a construction permit, to contact MTO and obtain any necessary permits and approvals.

Additionally, a geotechnical report may be required to determine the capacity of the soil below the road bed and building envelopes.

#### **Sewers**

There are municipal storm and sanitary sewers within the abutting road way, available to service the subject property as follows:

- North Talbot Road
  - Storm Sewer: 450mm Corrugated Steel Sewer, 1200mm Reinforced Concrete Pipe
  - Sanitary Sewer: 600mm Concrete Pipe and Reinforced Concrete



Dillon Consulting Ltd. is currently doing detailed engineering design for the North Talbot Road Corridor. The post-development land use parameters used within the model to consider future condition peakflow and volume through the system are as follows:

- East half of the property
  - Area = 1.39 ha
  - Max. Allowable Release Rate = 201 L/s
- West half of the property
  - Area = 1.42 ha
  - Max. Allowable Release Rate = 206 L/s

A servicing study is required for this development and a stormwater strategy supported by the City. Detailed civil servicing drawings are required to provide further comments on additional requirements.

We have no objection to the Subdivision Application, subject to the following conditions:

Subdivision Agreement - The applicant agrees to enter into a Development Agreement with the Corporation of the City of Windsor with the General Provisions of Council Resolutions 233/98 and any other specific requirements.

Corner Cut-Off – The owner(s) agrees, prior to the issuance of a construction permit, to gratuitously convey a 4.6m x 4.6m (15' x 15'), corner cut-off at the intersection of North Talbot Road and Street 'B' as well as North Talbot Road and Street 'C' in the Draft Plan, in accordance with City of Windsor Standard Drawing AS-230.

Servicing Study - The owner agrees, at its own expense, to retain a Consulting Engineer to provide a detailed servicing study report on the impact of the increased flow to the existing municipal sewer systems, satisfactory in content to the City Engineer and prior to the issuance of a construction permit. The study shall review the proposed impact and recommend solutions to addressing the problems and ultimate implementation of solutions should there be a negative impact to the system. The study shall be finalized in agreement with the City Engineer.

Site Servicing Plans – The owner agrees to submit a site servicing plan for the subject lands to the satisfaction of the Chief Building Official, the City Engineer, and ERCA in regulated areas, prior to the issuance of any construction permits for the subject lands.

Video Inspection (Mainline) - The applicant shall agree to conduct at its entire expense a video inspection, or pay the cost of similar inspection, of ALL EXISTING sanitary/storm sewers on North Talbot Road which will be tapped to service the development, all to the satisfaction of the City Engineer.

MTO Corridor Management (MTO Requirement) - The owner will be required, prior to the issuance of a construction permit, to contact the Ministry of Transportation (MTO) Corridor Management Section at 1-800-265-6072 to obtain the necessary permits, clearances and/or approvals in accordance with the Public Transportation & Highway Improvement Act.

Sidewalks -The owner(s) agrees to pay to the Corporation, prior to the issuance of a Building Permit, the sum of \$33,500.00 being the Owner's contribution towards the future construction of a concrete sidewalk on the North Talbot Road frontage of the subject lands.

Curbs and Gutters – The owner further agrees to pay to the Corporation, prior to the issuance of a construction permit, the sum of \$17,750.00 being the Owner's contribution towards the construction of concrete curb and gutter on the North Talbot Road frontage of the subject lands.

### **LANDSCAPE ARCHITECT & URBAN DESIGN - Stefan Fediuk**

Pursuant to the application for a Plan of Subdivision (**SDN-001/21**) for the properties situated on the south side of North Talbot Road, between Southwood Lakes Blvd and HWY 401 (0 North Talbot Rd; 1095 North Talbot Rd and 1185 North Talbot Rd), please note no objections from a landscape architectural or urban design perspective.

The applicant has provided several documents required through the Pre-submission process as requested by the Landscape Architect, including; Species at Risk Study, Topographic Plan of Survey, Tree inventory and Preservation Plan, and Planning Rationale.

The Applicant's landscape architect has also provided an updated Tree Preservation Report No 2, dated 18 April 2022, and addendum to that report dated May 17, 2022, as well as a revised Subdivision plan and Landscape/Tree Preservation Plan. Through consultation with the Executive Director of Parks, please also note the following comments:

#### Urban Design:

The proposal's Planning Rationale highlights these options through its response to the required studies as well as the revised subdivision plan to include parkland where the existing trees are the densest, abutting the existing Stoneybrook Park. This preservation and enhance of the natural wooded lots positively respond to the following Provincial and Civic policies related to development:

- Policy of PPS Section 1.1.1 (h) recommends Healthy, livable and safe communities are sustained by promoting development and land use patterns that conserve biodiversity.
- Section 8 Urban Design of the OP, recommends that development within the city provide a memorable, attractive and liveable city, and that development is to maintain and improve the quality of life for present and future generations by integrating the principles of sustainability and place making.

#### Species at Risk:

The Planning Rationale outlines the mitigation measures for Species at Risk (SAR) Section 4.2.2 including:

- Mitigation measures to avoid potential impact to the Yellow-breasted Chat and protected reptiles should be followed to prevent against potential contraventions of the Endangered Species Act, 2007 or Migratory Bird Convention Act, 2010
- To reduce the potential for impacts, vegetation removal will occur between October 1 and March 31, outside of the active season for bats, and replacement bat roosting habitat (two rocket boxes) will be installed under the direction of a qualified professional.

These are supported by the Landscape Architect.

#### Parks and Parkland Dedication:

Per By-law 12780 and the Planning Act, Residential subdivisions require the provision of parkland dedication representing 5% of the subject lands in the form of land and/or cash-in-lieu, to the satisfaction of the Commissioner of Parks.

Through consultation with the Executive Director of Parks, and Planning Staff and the developer's consultants, opportunities for parkland development; physically connecting the proposed development to the Stoneybrook Park multiuse trail, and help preserve a large portion of the existing natural environment, as per Section 6.3.2.4 of the OP are now being proposed.

Block 35 at the western end of the Street "A", consists of 819.1 m<sup>2</sup>, will represent a Parkland conveyance of 2.7% of the overall site development. The difference from the required Parkland of 5% shall be through a provision of cash-in-lieu. In addition, the developer is aware that a Parkland Development contribution is also to be provided.

The proposal found in Section 3.1 of the Planning Rationale identifies a stormwater management facility to be located on Block 35 and further detailed in Section 4.2.5. The addendum and subdivision landscape plan identify that the SWM facility will be 1,498.2 m<sup>2</sup> in area and surrounded by trees that will help to absorb stormwater before entering the municipal stormwater infrastructure. The location and solution is supportable and will a potential attraction for the development, but the developer is to be aware that Parks does not accept stormwater management facilities as Parkland Dedication.

#### **Required**

- Special Provision of the Subdivision Agreement pertaining to G-5(5) Parkland Conveyance identifying Block 35 to be conveyed to the City of Windsor as 2.7% Parkland along with cash-in-lieu representing the 2.3% remaining Parkland Conveyance.
- Special Provision of the Subdivision Agreement pertaining to G-2(25) Sidewalks, that the owner(s) shall pay to the Corporation, prior to the issuance of a Subdivision Agreement, the sum of \$16,500.00, being the Owner's contribution towards the future construction of a 3.0m wide multi-use trail connection from the Southwood Lakes Multi-use Trail, in Stoneybrook Park, to the proposed cul-de-sac at the western end of the proposed Street 'A'.

#### Climate Change & Tree Preservation:

As identified in the Planning Rationale provided, *"The Site is grassed and has areas of clustered mature trees predominantly near the western half of the Site and scattered near the eastern portion of 1185 North Talbot Road. The two vacant parcels also have areas of clustered mature trees throughout the Site."* The report also recognizes PPS policy 2.1.1 which requires natural features and area to be protected for the long term.

The development recognizes that most of the vegetation will be removed from the site for development. Removal of vegetation and developing with paved roads and buildings will reduce the stormwater resiliency of the property and increase the heat island effect in the area. Preservation of as many trees as possible has been highly recommended. Where unable to preserve trees, the developer is to provide measures to improve and replace the tree canopy loss that will help mitigate climate change ( Section 1.1.3.2 clauses c) and d), as well as 1.6.1 of the PPS).

The Revised Tree Inventory and Conditions Report No 2 prepared by Bezair Partners, identified 44 instances (including 3 on adjacent municipal land) of desirable species of trees of

significant size and health, to be impacted, by the development (representing a total of 21,160mm of total tree caliper). The western half of the development consists of the most notable number of the trees inventoried consisting of desirable native species providing suitable habitat for urban wildlife (such as squirrels, bats and song birds).

An amendment to that Conditions Report No2, itemizes the trees species, size (dbh in cm), and which could possibly be saved through preservation methods and through the conveyance of Block 35 to the Corporation as parkland. Proposed is the removal of 50 tree (13,700mm of tree caliper) and 16 trees plus one cluster of trees to be preserved (7460mm of tree caliper).

While the developer has suggested planting new trees within the development as compensation for tree loss, residential building permits are completed on a lot-by-lot basis by individual homebuilders. This process would make this difficult to accomplish by the developer and for city staff to administer, once the developer has relinquished ownership to the individual builders. Five Lots (2, 3, 4, 20 & 21) would require special permits, individual agreements and site-specific securities, and inspections by the Parks and Planning staff on private single-family residential properties. This process is generally conducted through Site Plan Control; however, the Planning Act precludes private single-family residential properties from that process.

Any new trees to be planted to repopulate the urban tree canopy, is to be completed by City Parks Forestry staff on public lands to ensure that the trees planted are the same as those desirable species removed from the site, and that the trees are more readily maintained by Forestry for the long-term. Block 35 require to be preserved. This will unfortunately, increase the number of trees lost to 57 (17,530mm caliper). It will however, ensure additional new trees to be planted, and maintained through the City Forestry staff, instead of risking expending a great deal of time and resources of the developer and the corporation, only to have the home owner potential remove the preserved tree once the builder sells the home.

#### **Required**

- Special Provisions of the Subdivision Agreement pertaining to G-3(2) Preservation of Existing Trees,
  - The owner shall provide a Landscape and Tree Preservation Plan identifying the proposed locations of all existing trees removed from the development and those to be retained in Block 35, to the satisfaction of the Executive Director of Parks and the Executive Director/City Planner, prior to the final subdivision plan approval.
  - The owner shall pay to the Corporation prior to the issuance of a Subdivision Agreement, the sum of \$130,000.00 towards compensation, at a rate of one (1) street tree for every 70mm caliper (dbh) of desirable tree removed, in accordance with the Corporation's Fees and Changes By-law 392-2002.
  - Individual lot owner(s) shall also provide cash-in-lieu for one (1) new 70mm caliper native tree per each lot for planting a boulevard tree (per CR 332/79) , in accordance with the Corporation's Fees and Changes By-law 392-2002, prior to the issuance of any construction permit.

In addition to Special Provisions for preservation of the existing desirable trees in Lot22:

- Special Provisions of the Subdivision Agreement pertaining to G-6 the owner shall:
  - Prior to the final subdivision plan approval, provide a Performance Security in the amount of \$25,000.00 in the form of cash or a certified cheque to be liquid to ensure that the nine desirable trees located on Block 35 are preserved during the construction process are preserved.

- Request inspection by Corporation's City Forester to ensure that the proposed tree protection and appropriate method of protection has been completed to the satisfaction of the Executive Director of Parks, prior to release of the Security.
- Where trees, proposed to be preserved, have been removed from development, at the time of inspection by the Corporation's City Forester, compensation will be drawn from the Performance Security at a rate of one tree for every 70mm of tree caliper (dbh) or portion thereof missing, in accordance with the Corporation's Fees and Charges By-law 392-2002.

### **Barry Horrobin – Police Services**

I have reviewed the drawings associated with this plan of subdivision application and hereby offer my revised comments for inclusion into the application with other reviewers:

The following comments on the proposed draft plan of subdivision are provided with a particular focus on public safety impact. These comments are in two general categories as follows:

1. The importance of establishing and maintaining proper emergency vehicular access/response capability
2. All other important public safety and security implications relating to the application to ensure a development that is safe for all

### **EMERGENCY VEHICULAR ACCESS**

- The Windsor Police Service generally supports the roadway layout and access connections proposed for this land development to existing roadway infrastructure, most notably the connections back to North Talbot Road. The positioning of Streets 'A' and 'B', 'C' properly connect to ensure overall ease of police incident response and general police patrolling activities for this newly developed neighbourhood.
- One issue that may arise as relates to road safety however is that Street 'C' appears to be offset slightly from its alignment with Old West Avenue on the other side of North Talbot Road. Once connected, this could lead to difficulties for drivers making left hand turns onto North Talbot. The offset is not substantial, yet it is enough to divert driver sightlines that could make turns more challenging. As a result, safety could be reduced for all users of the immediate roadway area. If there was a way to better align Street 'C' with Old West, that would seem to present a safer layout

### **SPECIFIC SAFETY ISSUES & CONSIDERATIONS**

The following issues, in no particular order, are hereby raised for consideration, with the goal being to optimize public safety in a practical manner:

- There is a parcel of land proposed for the storm water management facility (block 35). Given this lot will not be a regularly occupied space within the broader neighbourhood, it is important that it be properly constructed/modified and maintained to help discourage any risks to its use in an unlawful or undesirable way (such as trespassing, loitering, etc.). The orientation of this lot as shown on the draft plan is capable of leveraging adequate ongoing natural surveillance, most notably on its southern boundary near the eastern terminus of Street 'A', and this should be maintained as the minimum standard going forward. It would also be prudent to ensure some measure of access control is considered



(by way of a fence for example) on the sides of this lot that abut, residential lots #8, #9, #10, and #11 to provide a functional degree of separation between public and private spaces. Ideally, such fencing should allow for some degree of observation capacity – a steel picket or chain link style fence of 1.8 m would be preferred over an opaque, wood privacy style fence. Fencing along any other lot line/section of lot #35 would not be recommended from a safety and security perspective.

- Ensuring prompt and effective response capability by police responders is directly correlated to accurately locating the right address where an emergency call for assistance is required. Therefore, it is very important that each separate dwelling have a prominently displayed address number that is at least 5" high, is of a contrasting colour to the backdrop onto which it is mounted, and can be easily seen from the adjacent roadway by police without obstruction. This will optimize the address identification by Police/Fire/Ambulance during an emergency response.
- Pedestrian safety is very important in all residential neighbourhoods. This includes appropriate sidewalk infrastructure to connect to adjacent areas and proper street lighting as well. Lighting provided should be LED, in keeping with the current municipal standard, which helps in promoting public safety

In summary, a clause(s), if possible or appropriate, would ideally help as one of the conditions of approval for this application to address the important issues raised here to ensure they are incorporated.

April 22, 2022

Essex Region Conservation Authority  
360 Fairview Avenue West  
Suit 311, Essex, ON N8M 1Y6

**ATTENTION: Essex Region Conservation Authority**

**RE: 1<sup>st</sup> Submission Review Comments  
1095 North Talbot Residential  
Development – City of Windsor**

---

We have completed design changes as per the memorandum dated February 01, 2022. The drawings, report and response letter are dated April 21, 2022.

The following are our point-to-point responses (our responses are shown with listed in bold and italic font) to Town's comments.

- Please confirm is the 100 year SCS event modelled?

***The 100 Year SCS event was modelled please refer to Appendix E.***

- Why is the UST HWL lower than the 100 yr HWL?

***HWL depends on the hydraulics of each site.***

- Discharge is to be restricted via a 377 mm diameter plate. Does this size exist? Or will it be custom made?

***Discharge is restricted via a 362mm diameter Tempest Device; details in Appendix A.***

- Is there an overland spill route to the pond or the ROW? Proposed Street A - Has a HP on the west boundary edge of 190.35 m and on the east edge of 190.08 m. The dry pond has a TOB of 190.27m. It appears that the overland flow route will spill into the existing adjacent property (1255 NORTH TALBOT RD) for events exceeding the stress test.

***A berm has been added to prevent the overland flow route from spilling into the neighboring property please refer to Appendix A for more detail.***

- Is an easement required behind blocks 7,8,9,35 for the 250 mm storm sewer. If this is the case please provide confirmation that the easement is satisfactory to the City.

***Baird AE followed up with City of Windsor regarding the easement.***

- Sewer design sheet is missing A-7 , Tc is selected as 10 mins but inlet time shows 20 mins. Please clarify.

***Revised.***

- What is the purpose of the ditches on the North and South end of the property. Are these for storage or conveyance? Where is the water directed to?

***The ditches have been removed.***

- Since the proposed development is adjacent to existing developed blocks on the southern edge. Can you please confirm if the existing grading will outlet into the proposed rear yard cbs? If so, will the provided SWM need to account for additional drainage requirements posed by these lots?

***Yes, the existing grades will outlet into the rear yard CBs and to ensure that we have provided a berm all around the site which can be seen in Appendix A.***



Bill Fuerth, P.Eng.

**BAIRD AE**

**Originally received at June 6, 2022  
Development & Heritage Standing Committee  
Meeting Written Submission**

-----Original Message-----

From: Scott Dube

Sent: May 23, 2022 7:57 PM

To: clerks <[clerks@citywindsor.ca](mailto:clerks@citywindsor.ca)>

Subject: SDN/6575

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

I am in support of the proposed development. I do not like the 2 streets coming out to meet North Talbot, listed as "Street B" and "Street C" . I think they will create traffic issues and be dangerous to turn left out onto North Talbot. I would like to see them closed off on the ends and only have vehicle access to the development on Street A.

At a minimum street C should be aligned with Old West on the opposite side of North Talbot.

I would also like to see as many of the mature trees as possible saved.

Scott Dube



**Committee Matters: SCM 153/2022**

**Subject: Request for Heritage Permit – 3036 Sandwich Street, McKee Park (Ward 2)**

Moved by: Councillor Sleiman

Seconded by: Member Baker

Decision Number: **DHSC 404**

- I. THAT a Heritage Permit at 3036 Sandwich Street, McKee Park, for the alterations and addition of the gazebo, plaza, boardwalk, benches, decorative light standards, changes to address drainage, and playground, **BE APPROVED**, subject to the following condition(s):
  - a. Submission of satisfactory product details and samples (including material and colour selections as necessary) to the City Planner or designate; and further,
- II. THAT the City Planner or designate **BE DELEGATED** the authority to approve any further changes to the heritage alteration permit associated with the current phase of proposed scope for McKee Park including but not limited to the following items:
  - a. Receptacles
  - b. Widened paths

Carried.

Report Number: S 61/2022  
Clerk's File: MBA2022

**Clerk's Note:**

1. The recommendation of the Standing Committee and Administration are the same.
2. Please refer to Item 10.2. from the Development & Heritage Standing Committee Meeting held June 6, 2022.
3. To view the stream of this Standing Committee meeting, please refer to:  
<http://csg001-harmony.sliq.net/00310/Harmony/en/PowerBrowser/PowerBrowserV2/20220608/-1/7334>



**Subject: Request for Heritage Permit – 3036 Sandwich Street, McKee Park (Ward 2)**

**Reference:**

Date to Council: June 6, 2022  
Author: Kristina Tang, MCIP, RPP  
Heritage Planner  
ktang@citywindsor.ca  
519-255-6543 X 6179

Tracy Tang  
Planner II- Revitalization & Policy Initiatives  
ttang@citywindsor.ca  
519-255-6543 x 6449  
Planning & Building Services  
Report Date: May 18, 2022  
Clerk's File #: MBA2022

**To:** Mayor and Members of City Council

**Recommendation:**

- I. **THAT** a Heritage Permit at 3036 Sandwich Street, McKee Park, for the alterations and addition of the gazebo, plaza, boardwalk, benches, decorative light standards, changes to address drainage, and playground, **BE APPROVED**, subject to the following condition(s):
  - a. Submission of satisfactory product details and samples (including material and colour selections as necessary) to the City Planner or designate; and further,
- II. **THAT** the City Planner or designate **BE DELEGATED** the authority to approve any further changes to the heritage alteration permit associated with the current phase of proposed scope for McKee Park including but not limited to the following items:
  - a. Receptacles
  - b. Widened paths

## **Executive Summary: N/A**

### **Background:**

3036 Sandwich Street, known as McKee Park, is a heritage designated property under Part V of the *Ontario Heritage Act* as it is located within the boundaries of the Sandwich Heritage Conservation District (HCD). City Council passed the Sandwich HCD Designation By-law No. 22-2009 in January 2009, along with related by-laws. McKee Park became part of the Sandwich HCD when it came into effect in October 2012. The Sandwich HCD Plan requires certain proposed modifications on properties within the HCD, including changes or improvements to public parks, to apply for Heritage Permits.



Photographs of McKee Park in May 2022

In May 2022, the City of Windsor's Parks Department submitted a Heritage Permit application as part of the proposed redevelopment plans for McKee Park, which is significantly supported through funding from Bridging North America. Due to construction season scheduling and the deadline for fund usage, the Parks Department has advised that some phases of the construction would begin in late May 2022. The Heritage Permit application is outlined in Appendix 'A' – Heritage Permit Application.

### **Discussion:**

#### **Property description:**

The subject property is a municipal public park located on the west side of Sandwich Street, and also borders on Chewett Street, Russell Street, and the Detroit River. Facilities in the park include a boat ramp, picnic tables, a playground area, a boardwalk, and a parking lot for about 40 cars.



2017 aerial photograph of the property

Parks Staff reported in the Heritage Permit application that the existing boardwalk, pathways, and playground are nearing the end of their lifecycles, and that due to their age, maintenance of the amenities is becoming increasingly challenging. There is also evidence of ponding and drainage issues at the park. Bridging North America has provided the opportunity to redevelop McKee Park and make improvements for the community.



Photographs of McKee Park in May 2022 showing current state of the boardwalk, drainage issues, pathways, and benches.





Area planned for plaza leading to gazebo and the playground.

### **Proposal:**

The Parks Department is proposing the following works at McKee Park:

- Remove existing wooden boardwalk and replace with new composite recycled plastic boardwalk
- New gazebo
- New plaza adjacent to boardwalk and gazebo
- Remove existing asphalt pathways and replace with new widened asphalt pathways
- Remove existing lights and benches and replace with new decorative lights and benches matching the standardized Sandwich HCD public furniture
- Improve heritage appearance of existing silo receptacles and make plans to install new heritage appropriate receptacles
- Make elevation changes and add catch basins to address drainage problems
- Replace the existing playground equipment with new

As part of the Heritage Permit application, Parks staff have submitted a redevelopment plan and specifications for individual components of the proposed works. These supplementary materials are included in Appendix 'A' – Heritage Permit Application.

The proposed works will improve the condition of the park and create an enhanced experience for park users.

### **Legal provisions:**

The subject property is located within the boundaries of the Sandwich Heritage Conservation District (HCD) and is designated under Part V of the *Ontario Heritage Act* (OHA), which in Section 41.1(5) requires the HCD Plan to contain (c) “a description of the heritage attributes of the heritage conservation district and of properties in the district”. Changes to the properties within the district are to be considered according to (d) “policy statements, guidelines and procedures for achieving the stated objectives and managing change in the heritage conservation district”. The Sandwich HCD Plan outlines some changes to be approved by City Council after review by the Committee; and some minor changes to be approved by staff.

The Sandwich HCD Plan requires Heritage Committee review and Council approval for changes or improvements to public parks.

### **Official Plan Policy:**

The Windsor Official Plan states “Council will enhance heritage resources by (a) Ensuring that within any Heritage Area or Heritage Conservation District that: (i) Infrastructure undertakings respect and enhance the historic character of the area; (ii) Development be of compatible height, massing, scale, setback and architectural style.” (9.3.5.1)

Volume II Section 1.26 on the Sandwich Heritage Conservation District of the City’s Official Plan includes more detailed policies, in particular that “All applications for heritage permits will be required to conform to the design guidelines in the Sandwich Heritage Conservation District Plan, adopted by By-law 22-2009.” (Volume II, Section 1.26.9)

### **Sandwich Heritage Conservation District (HCD):**

#### **Review of Sandwich HCD policies**

Section 4.5 on Public Realm emphasizes the important role of the public realm in defining the overall heritage character of the neighbourhood through its mature trees and public furniture. The policies include:

- (a) Mature street trees are to be protected and preserved to the extent possible unless they present a public safety hazard or are in a serious state of decline due to age or disease. When removal of street trees is required, they should be replaced with new trees of an appropriate size and species as determined by the Community Services Department;*
- (e) Existing road right-of-ways and paved surfaces should not be increased;*
- (f) Street furnishings, including benches, garbage cans, bicycle racks and other components, will be consistent throughout the neighbourhood and be of a style and material that complements the heritage attributes of the District;*

Section 5. 6. 2 on Approvals for Public Property and Infrastructure obligates the municipality to be consistent with the policies and guidelines of the Plan through Council review and approval of such works and items:

- Replacement of street lighting, street signs;
- Street furnishings, including benches, trash receptacles, bicycle racks, planters and similar items;
- Alterations, reconstruction or removal of grassed boulevards;
- Changes to sidewalks or roadway pavement widths;
- Changes or improvements to public parks and open space features.



Section 8.2 on Mature Trees provides guidelines for municipal authority to protect mature trees and to be concerned about the protection of root zones of trees, and any replacements. Section 8.6 on Parks and Open Space provides detailed guidelines and recommendations for Paterson Park and Mackenzie Hall Parkette (now Mary Bibb Park) while not specifically mentioning McKee Park. The same principles emphasizing the presence of trees is repeated.

Section 8.10.2 on Lighting Style discusses establishing lighting that is sensitive to the heritage character of the district, recommending the use of King Luminaire ornamental style. Section 8.10.3 on Street Furniture discusses the coordination of elements such as lighting, benches, and trash receptacles to create a sense of place and to set it apart from other areas. It discusses the installation of unifying ornamental furniture such as the Ultra Plus bench series.

### **Review of Proposal:**

**Look-out (Boardwalk, Gazebo & Plaza):** The Heritage Permit identifies the boardwalk and gazebo that would serve as a look-out area towards the views of the Detroit River and Ambassador Bridge, supporting the HCD element of preserving views and vistas. The gazebo proposed is intended to have a louvered cupola and be without railings (slightly different from the application drawing submitted), which would be acceptable in design. The frame of the gazebo would be painted in charcoal grey and the roofing would be an 18" standing seam style in dark green. The finish chosen is within the Heritage Colour palettes of the HCD. Grass near the proposed gazebo location has been inevitably damaged by foot traffic between the parking lot and the boardwalk. Therefore, as a supplemental destination feature to the gazebo, a plaza paved with exposed aggregate with acid wash finish (then sealed to ensure its condition) will be installed. The recycled plastic material chosen for the boardwalk has been used by the Parks Department at other city parks (Ojibway), though from a purely heritage colour perspective, the brown footpath planks would have been preferred over the grey. The grey colour, however, has been determined to be an acceptable choice in this instance primarily because it absorbs less heat which helps to accommodate visitors to the park should they remove their shoes to enjoy the beach and adjacent boardwalk.



On left, example of grey boardwalk at Ojibway Park. Photo on right shows view from Sandwich Street looking towards Detroit River across McKee Park.

**Light & Benches:** The eight lights proposed will be the same as the Sandwich Street Roundabout light standards, and the benches will be selected from the same Ultra Bench series used at Patterson Park. This ornamental furniture will fit well with Sandwich HCD and demonstrate continuity with ongoing City investment in consistent streetscaping with other public furniture elements along Sandwich Street and at other parks located in Sandwich.

**Receptacles:** As for receptacles, there is currently an existing garbage silo within McKee Park. These types of underground silos have an operational advantage due to storage capacity, but have a utilitarian appearance that does not necessarily enhance the HCD. Parks staff is investigating potential finishing details that would “mask” the existing garbage silo, and will consult and discuss further with Planning staff when more options and product information is made available.

**Paths, Trees & Drainage:** The existing asphalt pathways are currently 5 feet wide but replacement with new 3m wide asphalt pathways is proposed to facilitate operational maintenance, accessibility needs and COVID distancing concerns. Planning staff will continue to work with Parks staff in adjusting the design of the path and incorporating landscaping near Sandwich Street to visually “hide” the appearance of the widened paths. These paths are also designed in a way to minimize tree loss by limiting the root damage to the drip line of the trees and construction plans include tree protection instructions. However, forestry staff have indicated that some/most of the mature maple trees are showing signs of stress already with tip dieback. A review also concluded that the removal of three juvenile trees and one mature tree is required due to the poor health of the trees, and not the construction design. The preservation of the remaining trees has been taken into consideration through the use of woodchips (instead of topsoil), to reduce stress on the root systems with the elevation changes that are being introduced with the catch basins to address the park’s drainage and ponding issues.

**Playground:** The playground set for McKee Park has been ordered as part of the planned 35 new playgrounds that the Parks Department will be installing across the city. As McKee Park’s existing playground has been categorized as medium-sized, its design is standardized with other similar sized playgrounds. Heritage appropriate colours have been chosen for McKee’s playground, and the play units and swings will be erected in the same location as existing.



On left, view of existing playground. Photo on right shows concept design of new playground.

**Archaeology:** Since McKee Park is located within an area of High Archeological Potential within the former Huron Village area, the Parks Department has engaged licensed archaeological consultants to conduct archaeological assessments on the property. At the time of writing of this report, the assessments have not resulted in any archaeological finds, but will be further reviewed by the Ministry of Heritage, Sport, Tourism & Culture Industries.

**Summary of Review:** The proposal considers the Sandwich HCD Plan policies. Further refinement of details is needed on the visual screening of the receptacles, and widened paths, which is recommended to be delegated to staff for approval.

### **Risk Analysis:**

As outlined in the Council approved Sandwich HCD Plan, the heritage permit process and Council review and approval is required to obligate the municipality and other property owners to be consistent with the policies and guidelines of the Sandwich HCD Plan. The proposal would improve the McKee Park experience and enhance the Sandwich Heritage Conservation District.

### **Climate Change Risks**

**Climate Change Mitigation:** N/A

**Climate Change Adaptation:** N/A

### **Financial Matters:**

The funding for the redevelopment of McKee Park comes from a partnership between Bridging North America (BNA) and funding from the 2021 capital budget that was approved for use as per CR453/2021.

<b>Description of Funding</b>	<b>Funding Source</b>	<b>Funding Amount</b>
Capital Budget	<b>PFO-007-12- Neighbourhood Parks Initiatives</b>  McKee Park Capital Project 7221003	\$150,000
Grant Funding	<b>Bridging North America (BNA) Agreement</b>	\$200,000
<b>TOTAL</b>		<b>\$350,000</b>

### **Consultations:**

Heritage Planning staff have been in discussion with Parks Department staff. Discussions also took place between Planning Department Heritage Planning staff and Urban Design staff.

**Conclusion:**

The heritage permit requests for the alterations and addition of the gazebo, boardwalk, benches, decorative light standards, improvements to drainage, and playground at 3036 Sandwich Street, McKee Park, is recommended for approval, subject to conditions. Further changes or verifications to the proposal including, but not limited to, the receptacles and widened paths, are recommended to be delegated to Planning staff to expedite the implementation of McKee Park improvements.

**Planning Act Matters:** N/A

**Approvals:**

Name	Title
Michael Cooke	Manager of Planning Policy/ Deputy City Planner
Thom Hunt	City Planner / Executive Director Planning & Building
Dana Paladino	Acting Commissioner, Legal & Legislative Services
Jelena Payne	Commissioner, Economic Development & Innovation
Shelby Askin Hager	Chief Administrative Officer (Acting)

**Notifications:**

Name	Address	Email
Supervisor, Parks Projects: Darron Ahlstedt		<a href="mailto:dahlstedt@citywindsor.ca">dahlstedt@citywindsor.ca</a>
Manager, Parks Development: Wadah Al-Yassiri		<a href="mailto:walyassiri@citywindsor.ca">walyassiri@citywindsor.ca</a>
Executive Director, Parks & Facilities: James Chacko		<a href="mailto:jchacko@citywindsor.ca">jchacko@citywindsor.ca</a>

**Appendices:**

- 1 Appendix A- Heritage Permit Application





# HERITAGE PERMIT APPLICATION

Revised 12/2021

## CORPORATION OF THE CITY OF WINDSOR

Planning Dept., Suite 320-350 City Hall Sq W, Windsor ON N9A 6S1  
519-255-6543 | 519-255-6544 (fax) | [planningdept@citywindsor.ca](mailto:planningdept@citywindsor.ca)

### 1. Applicant, Agent and Registered Owner Information

Provide in full the name of the applicant, registered owner and agent, the name of the contact person, and address, postal code, phone number, fax number and email address. If the applicant or registered owner is a numbered company, provide the name of the principals of the company. If there is more than one applicant or registered owner, copy this page, complete in full and submit with this application.

#### APPLICANT

Contact Name(s) Darron Ahlstedt/Wadah Al-Yassiri  
Company or Organization City of Windsor, Parks Design and Development  
Mailing Address 2450 McDougall Ave. Windsor, ON  
  
Postal Code N8X 3N6  
Email dahlstedt@citywindsor.ca Phone(s) \_\_\_\_\_

#### REGISTERED OWNER IF NOT APPLICANT

Contact Name(s) \_\_\_\_\_  
Company or Organization \_\_\_\_\_  
Mailing Address \_\_\_\_\_  
  
Postal Code \_\_\_\_\_  
Email \_\_\_\_\_ Phone(s) \_\_\_\_\_

#### AGENT AUTHORIZED BY REGISTERED OWNER TO FILE THE APPLICATION

Contact Name(s) \_\_\_\_\_  
Company or Organization \_\_\_\_\_  
Mailing Address \_\_\_\_\_  
  
Postal Code \_\_\_\_\_  
Email \_\_\_\_\_ Phone(s) \_\_\_\_\_

#### Who is the primary contact?

☒ Applicant ☐ Registered Owner ☐ Agent



## 2. SUBJECT PROPERTY

Municipal Address: 3036 Sandwich Street

Legal Description (if known): Plan 410; lot 13

Building/Structure Type:

☐ Residential ☐ Commercial ☐ Industrial ☐ Institutional

Heritage Designation:

☐ Part IV (Individual) ☒ Part V (Heritage Conservation District)

By-law #: 22-2009 District: Sandwich HCD

Is the property subject to a Heritage Easement or Agreement?

☐ Yes ☒ No

## 3. TYPE OF APPLICATION

Check all that apply:

☐ Demolition/Removal of heritage attributes ☒ Addition ☒ Erection ☒ Alteration\*  
☐ Demolition/Removal of building or structure ☐ Signage ☒ Lighting

\*The *Ontario Heritage Act's* definition of "alter" means to change in any manner and includes to restore, renovate, repair or disturb.

## 4. HERITAGE DESCRIPTION OF BUILDING

*Describe the existing design or appearance of buildings, structures, and heritage attributes where work is requested. Include site layout, history, architectural description, number of storeys, style, features, etc..*

McKee Park, located on the city's riverfront, is due for redevelopment. The existing boardwalk, pathways and playground are all at the end of their useful life. Parks Design and Development has a plan and limited funding to remove and replace/add the following to the park:  
New recycled plastic boardwalk, gazebo, pathways (3m width), decorative lights and benches .  
Additional catch basins will be added as well to improve the drainage issues that are present in the park now.

## 5. PROPOSED WORK

*Provide a detailed written description of work to be done, including any conservation methods you plan to use. Provide details, drawings, and written specifications such as building materials, measurements, window sizes and configurations, decorative details, etc.. Attach site plans, elevations, product spec sheets, etc. to illustrate, if necessary.*

Parks Design and Development has a plan and limited funding to remove and replace/add the following to the park:

New recycled plastic boardwalk, gazebo, pathways (3m width), decorative lights and benches, and in the next year a new playground. Additional catch basins will be added as well to improve the drainage issues that are present in the park now. See the attached Site plan.

## 6. HERITAGE PERMIT RATIONALE

*Explain the reasons for undertaking the proposed work and why it is necessary.*

The existing park amenities are aging to the point that maintaining them as they are, is becoming increasing difficult and time consuming. One time funding through Bridging North America (BNA) was provided to McKee Park to upgrade the park. These new amenities will bring the park up to current park standards with respect to pathways and lighting. The other features to be added will create an improved park experience for all who use the park.

*Describe the potential impacts to the heritage attributes of the property.*

## 7. CHECKLIST OF MATERIALS SUBMITTED Check all that apply:

Required:

- ☒ Photographs (showing the current condition and context of existing buildings, structures, and heritage attributes that are affected by the application)
- ☒ Site plan/ Sketch (showing buildings on the property and location of proposed work)
- ☒ Drawings of proposed work (e.g. existing and proposed elevations, floor plans, roof plans, etc., as determined by Heritage Planning staff)
- ☒ Specifications of proposed work (e.g. construction specification details)

Potentially required (to be determined by Heritage Planning staff):

- ☐ Registered survey
- ☐ Material samples, brochures, product data sheets etc.
- ☐ Cultural Heritage Evaluation Report
- ☐ Heritage Impact Assessment (HIA)
- ☐ Heritage Conservation Plan
- ☐ Building Condition Assessment



## 8. NOTES FOR DECLARATION

*The applicant hereby declares that the statements made herein and information provided are, to the best of their belief and knowledge, a true and complete representation of the purpose and intent of this application.*

*The applicant agrees that the proposed work shall be done in accordance with this application, including attachments, and understands that the issuance of the Heritage Alteration Permit under the Ontario Heritage Act shall not be a waiver of any of the provisions of any By-Law of the Corporation of the City of Windsor, or the requirements of the Building Code Act, RSO 1980, c51.*

*The applicant acknowledges that in the event a permit is issued, any departure from the conditions imposed by the Council of the Corporation of the City of Windsor, or plans and specifications approved is prohibited and could result in the permit being revoked. The applicant further agrees that if the Heritage Alteration Permit is revoked for any cause of irregularity, in the relation to non-conformance with the said agreements, By-Laws, acts or regulations that, in consideration of the issuance of the permit, all claims against the City for any resultant loss or damage are hereby expressly waived.*

**APPLICANT** Signature(s) \_\_\_\_\_

Date \_\_\_\_\_

Date \_\_\_\_\_

## SCHEDULE A

### A. Authorization of Registered Owner for Agent to Make the Application

If the applicant is not the registered owner of the land that is the subject of this application, the written authorization of the registered owner that the applicant is authorized to make the application must be included with this application form or the authorization below must be completed.

I, \_\_\_\_\_, am the registered owner of the land that is  
*name of registered owner*  
subject of this application for a Heritage Alteration Permit and I authorize  
\_\_\_\_\_ to make this application on my behalf.  
*name of agent*

\_\_\_\_\_  
Signature of Registered Owner

\_\_\_\_\_  
Date

If Corporation – I have authority to bind the corporation.

### B. Consent to Enter Upon the Subject Lands and Premises

I, \_\_\_\_\_, hereby authorize the members of the Windsor Heritage Committee and City Council and staff of the Corporation of the City of Windsor to enter upon the subject lands and premises described in Section 3 of the application form for the purpose of evaluating the merits of this application and subsequently to conduct any inspections on the subject lands that may be required as condition of approval. This is their authority for doing so.

\_\_\_\_\_  
Signature of Registered Owner

\_\_\_\_\_  
Date

If Corporation – I have authority to bind the corporation.

### C. Acknowledgement of Applicant

I understand that receipt of this application by the City of Windsor Planning Department does not guarantee it to be a complete application. Further review of the application will occur and I may be contacted to provide additional information and/or resolve any discrepancies or issues with the application as submitted.

I further understand that pursuant to the provisions of the Ontario Heritage Act and the Municipal Freedom of Information and Protection of Privacy Act, this application and all material and information provided with this application are made available to the public.

\_\_\_\_\_  
Signature of Applicant

\_\_\_\_\_  
Date

# HERITAGE PERMIT APPLICATION

Revised 12/2021

## DO NOT COMPLETE BELOW – STAFF USE ONLY

### Approval Record

Date Received by Heritage Planner: \_\_\_\_\_

Building Permit Application Date, if needed: \_\_\_\_\_

☐ Application Approval (City Council):

Development & Heritage Standing Committee: \_\_\_\_\_

City Council: \_\_\_\_\_

☐ Application Approval (City Planner):

Heritage Planner: \_\_\_\_\_

Staff Decision Appealed to City Council: \_\_\_\_\_

If so, Date to City Council: \_\_\_\_\_

Council Decision Appealed: \_\_\_\_\_

Additional Notes / Conditions:

---

---

---

---

### DECISION

Heritage Permit No.: \_\_\_\_\_ Date: \_\_\_\_\_

Council Motion or City Planner's Signature: \_\_\_\_\_

Please contact Heritage Planning to request inspections at [ktang@citywindsor.ca](mailto:ktang@citywindsor.ca)

### CONTACT INFORMATION

Planning Department - Planning Policy  
Corporation of the City of Windsor  
Suite 320 - 350 City Hall Square West  
Windsor ON N9A 6S1  
[planningdept@citywindsor.ca](mailto:planningdept@citywindsor.ca)  
519-255-6543 x 6179  
519-255-6544 (fax)  
<http://www.citywindsor.ca>



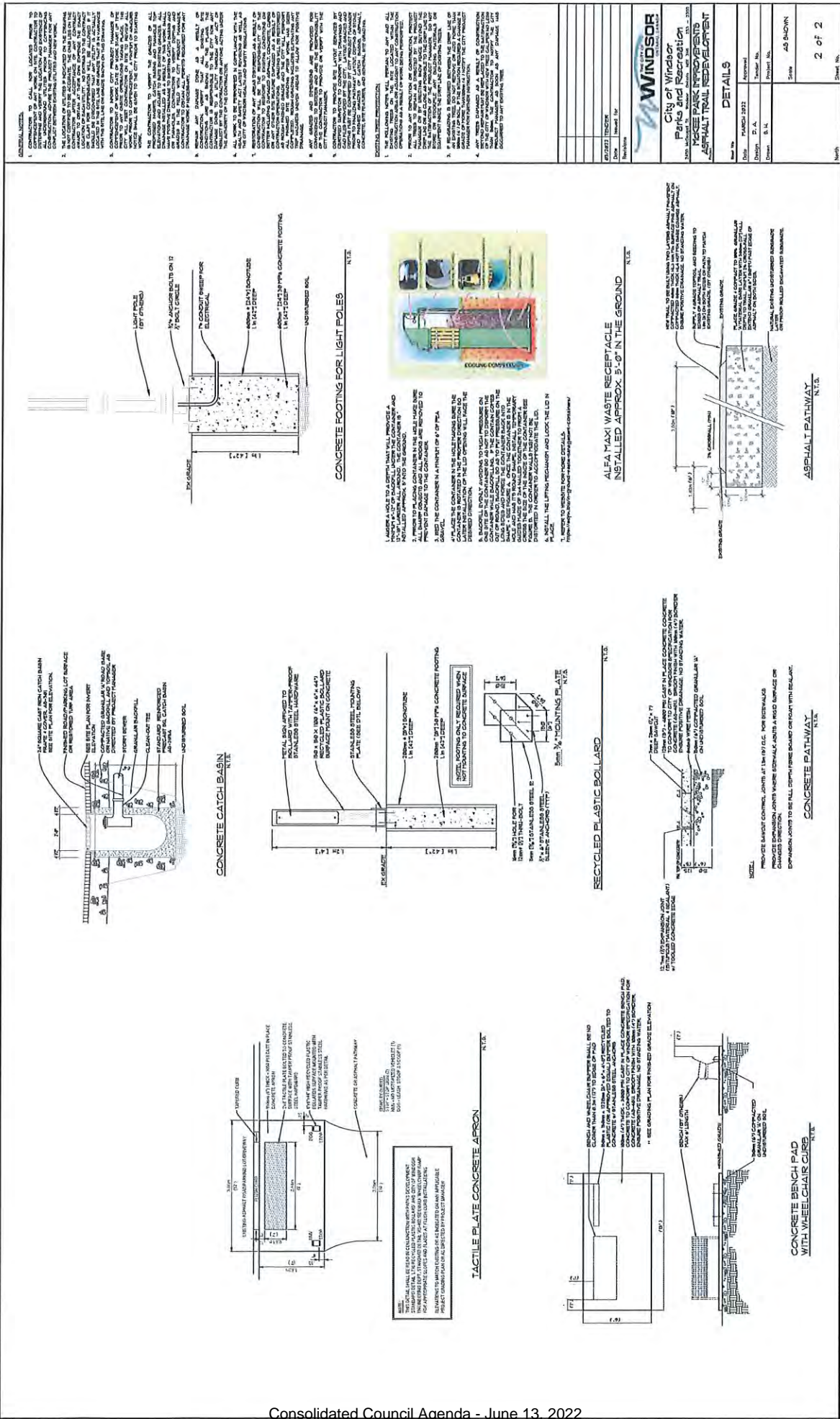


# McKEE PARK - EXISTING CONDITIONS

1 : 1000  
SEPTEMBER 2020











## The hanit® Product



## AND WHAT'S THE CATCH? THERE IS NONE.

### WHAT MAKES HANIT® UNIQUE?

Each material has its advantages. Unfortunately, it also has its drawbacks.

Wood, for example, is visually attractive, but it cannot assert itself against rain and cold. Only time-consuming maintenance helps against decay. Concrete, however, is very durable, but it is also difficult to transport. It is simply too heavy for many applications. Besides, concrete gets water marks over the years, and begins to flake. Steel, in turn, is flexible, and can be used for various things, if not for the corrosion! And stainless steel has its price. This also applies to new plastics, where the ecological balance sheet is also devastating.

And then there's hanit®.

Unlike wood or steel, the weather cannot harm it. hanit® is also easy on the back and the purse, in other words: it is lighter than concrete and cheaper than stainless steel. But above all, virtually any product can be made out of this high-tech material. Due to its individual composition, hanit can be adapted to any application.

### DURABILITY

- Weather-resistant
- Rot-resistant
- Splinter-free, therefore low risk of injury Can be used year round
- Moisture-repellent, does not absorb water, therefore dries fast

### LIGHT-WEIGHT

- Installation does not require heavy equipment
- Higher load capacity
- Transport cost savings
- Faster installation
- Reduced workload

### ECONOMICAL

- Long service life
- Resistant to oils, brines, acids, and salt water
- Low maintenance and service costs



- Made of high-quality processed secondary plastics (polyolefins)
- Excellent price-quality ratio
- Perfect construction material, especially for robust profiles and finished parts

### ECO-FRIENDLY

- Produced without preservatives
- Reduces the strain on landfills, is sustainably environmentally friendly
- Recyclable in the material cycle
- Awarded the "BLUE ANGEL" eco-label
- Water neutral
- Non-toxic (safe according to DIN 71, Section 3 Playground Regulation)

### SIMPLE PROCESSING

- Easy to process mechanically (drilling, sawing, screwing, nailing)
- Simple adjustments can be made on the spot

#### Contact Us

Telephone support and enquiries:

+44 (0)161 850 1965

Monday to Friday, 8am to 5pm

[info@hahnplastics.com](mailto:info@hahnplastics.com)

#### Service/Help

[Order Catalogue](#)

[UK Office Contact](#)

[North America Office Contact](#)

[Privacy Policy](#)

[Terms and Conditions](#)

[Warranty](#)

[FAQ](#)

#### Member of



#### Awards

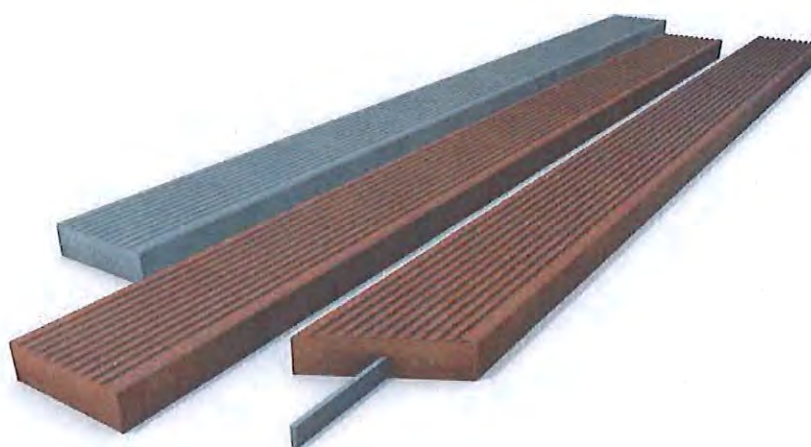


WINNER



[Products](#) > [Ground Reinforcement and Surfaces](#) > [Footpath planks](#)

## Footpath plank



### Colour

Brown

Grey

### Detail selection

4.0 x 19.7 cm

4.0 x 19.7 cm, with reinforcement

4.8 x 16.5 cm

4.8 x 16.5 cm, with reinforcement

6.0 x 19.7 cm

6.0 x 19.7 cm, with reinforcement

### Length

100 cm

150 cm

200 cm

250 cm

300 cm

1

Add to wishlist

Order No.:RGU048250

[Description](#) [Additional Information](#) [Downloads](#)

### Product information "Footpath plank"

Whether used for stairs, bridges, paths or jetties, our footpath planks have proved their mettle over and over again. The grooved, anti-slip surface means they're especially useful wherever water or moisture could be an issue. And you can rest assured that our footpath planks will last a long, long time.

- » Lengths: 100 to 300 cm
- » Profile measures: 4.8 x 16.5 cm, 4.0 x 19.7 cm, 6.0 x 19.7 cm
- » Reinforced version also available
- » Slip resistant
- » Splinter-free and rot-resistant
- » Fast drying
- » No painting, lacquering or sanding

Products exceeding 200 cm in length, are packed on double pallets. Packaging units are for guidance only and are no order requirements. Please note our installation recommendation when doing the installation.

## Specification

Detail selection	4.8 x 16.5 cm
Quality (material)	hanit®
Colour	Grey
Length	250 cm
Width	16.5 cm
Thickness	4.8 cm
Weight	18.4 kg
Weight per m <sup>2</sup>	44.4 kg
Specification	Standard
Piece per pallet	75

» Do you have any questions concerning this product?

### Contact Us

Telephone support and enquiries:

+44 (0)161 850 1965

Monday to Friday, 8am to 5pm

✉ [info@hahnplastics.com](mailto:info@hahnplastics.com)

### Service/Help

[Order Catalogue](#)

[UK Office Contact](#)

[North America Office Contact](#)

[Privacy Policy](#)

[Terms and Conditions](#)

[Warranty](#)

[FAQ](#)

### Member of



### Awards

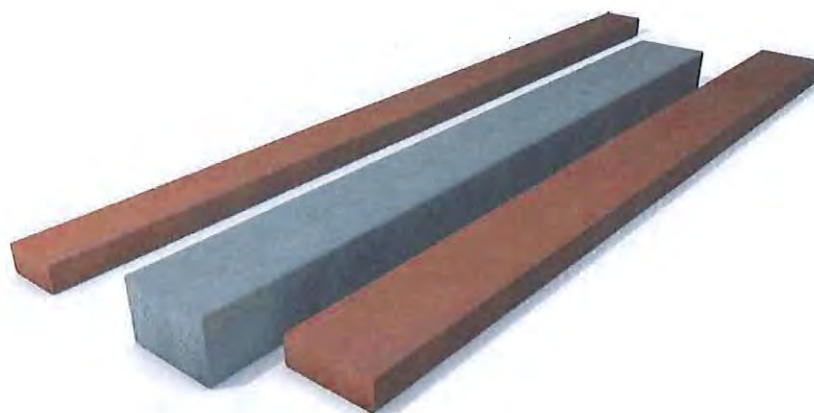






[Products](#) > [Building and Construction](#) > [Beams / Sleepers / Ties](#)

## Beams / Sleepers



### Colour



### Detail selection






### Length








Order No.: RG0823250

[Description](#) [Downloads](#)

### Product information "Beams / Sleepers"

Our beams and sleepers made of recycled plastic are basic elements for building edgings, sandboxes or stairs. The recycled products are also ideal for edging playgrounds, as foundations or substructures. Our material has recycled plastic is robust and allows easy handling. The beams us recycled plastic are used wherever material strength, durability and safety are important.



Our 16.0 x 24.0 cm sleepers are produced with an internal plastic reinforcement. This ensures a higher density and thus a better screw connection.

- » Internal plastic reinforcement
- » Less shrinkage
- » Better assembly

Packaging units are for orientation only. They are not order specifications. Product length over 200 cm is packed on double pallets.

## Specification

Detail selection	8 x 23 cm
Material	hanit®
Colour	Grey
Weight	42.5 kg
Length	250 cm
Packaging unit	30 Pieces

› Do you have any questions concerning this product?

## Contact Us

Telephone support and enquiries:

+44 (0)161 850 1965

Monday to Friday, 8am to 5pm

[info@hahnplastics.com](mailto:info@hahnplastics.com)

## Service/Help

[Order Catalogue](#)

[UK Office Contact](#)

[North America Office Contact](#)

[Privacy Policy](#)

[Terms and Conditions](#)

[Warranty](#)

[FAQ](#)

## Member of



## Awards



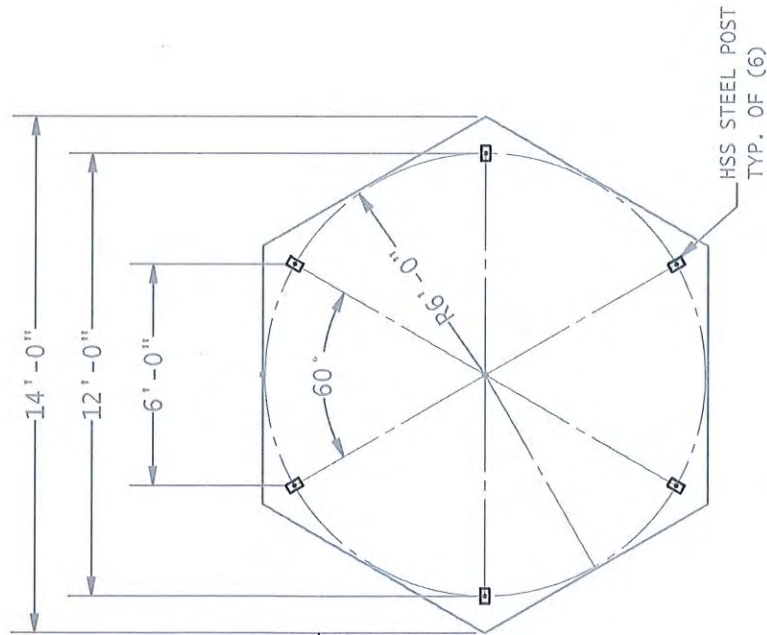


DOCUMENT NOT FOR CONSTRUCTION:

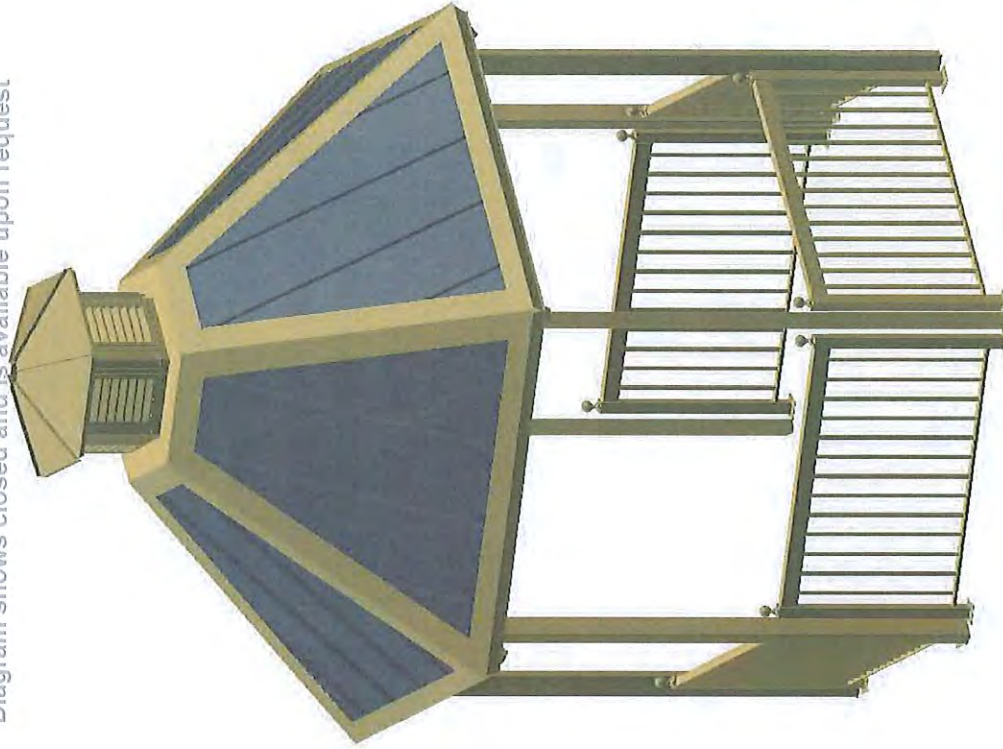
- A FOUNDATION DESIGN HAS NOT BEEN PERFORMED BY AMERICANA OUTDOORS
- A LICENSED ENGINEER FAMILIAR WITH SOIL CONDITIONS AT CONSTRUCTION SITE MUST PERFORM A FOUNDATION DESIGN

This would be an open coupola

Diagram shows closed and is available upon request



FOUNDATION PLAN



TITLE

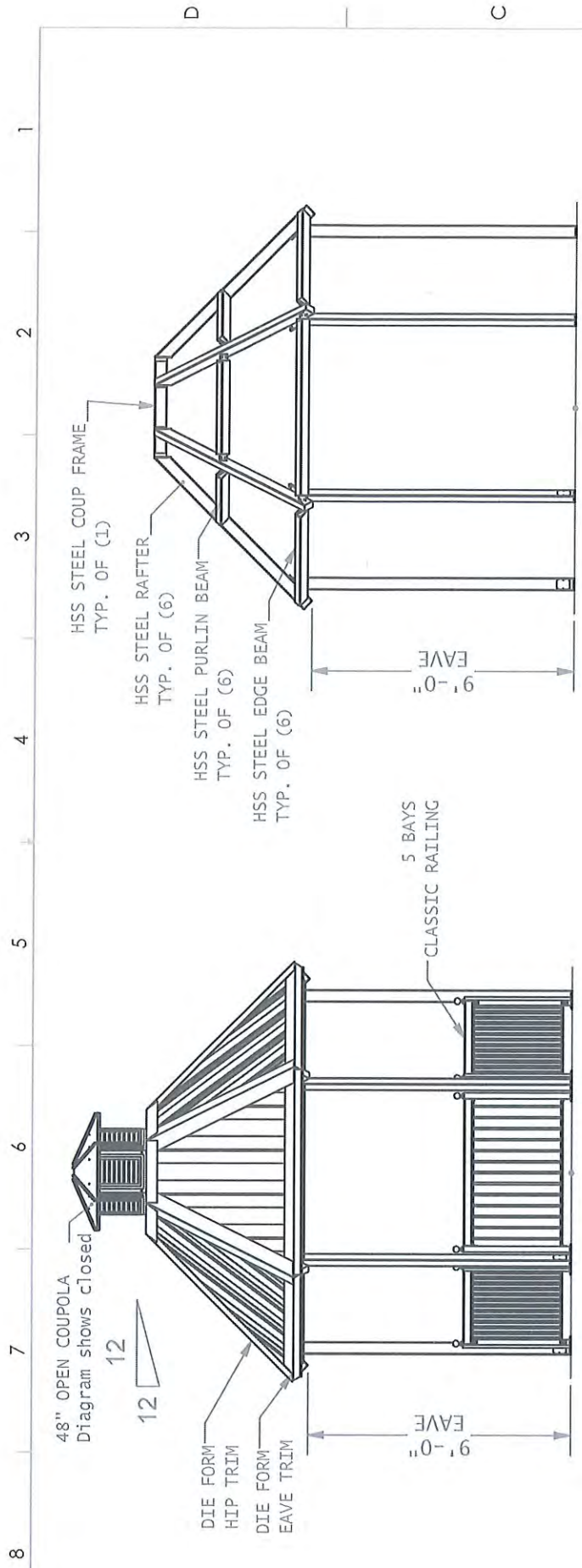
14'  
APACHE SHELTER

SIZE	DATE	DWG NO	REV
A	2/22/2022	98461	

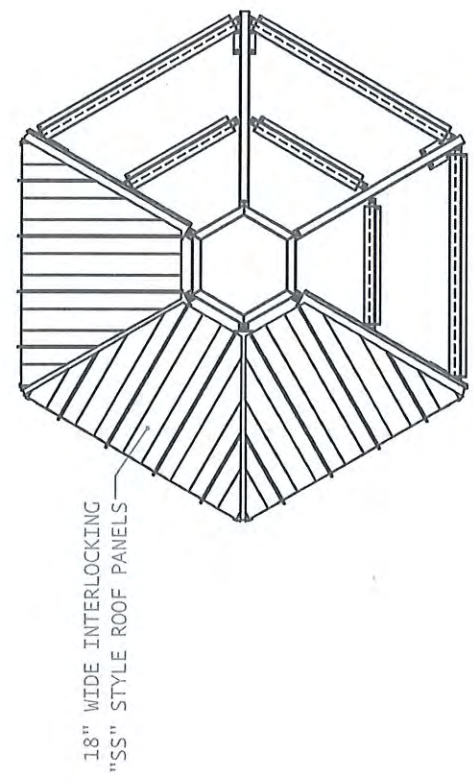
SCALE: AS NOTED DRAWN: MT SHEET 1 of 2

(800) 851-0865  
www.americana.com

NOT FOR CONSTRUCTION



FRONT VIEW



ROOF PLAN

ELEVATION PLAN  
FRAME ONLY



TITLE		14' APACHE SHELTER	
SIZE	DATE	DWG NO	REV
A	2/22/2022	98461	
SCALE: AS NOTED		DRAWN: MT	SHEET 2 of 2

(800) 851-0865  
www.americana.com

NOT FOR CONSTRUCTION



# TRYSTAN

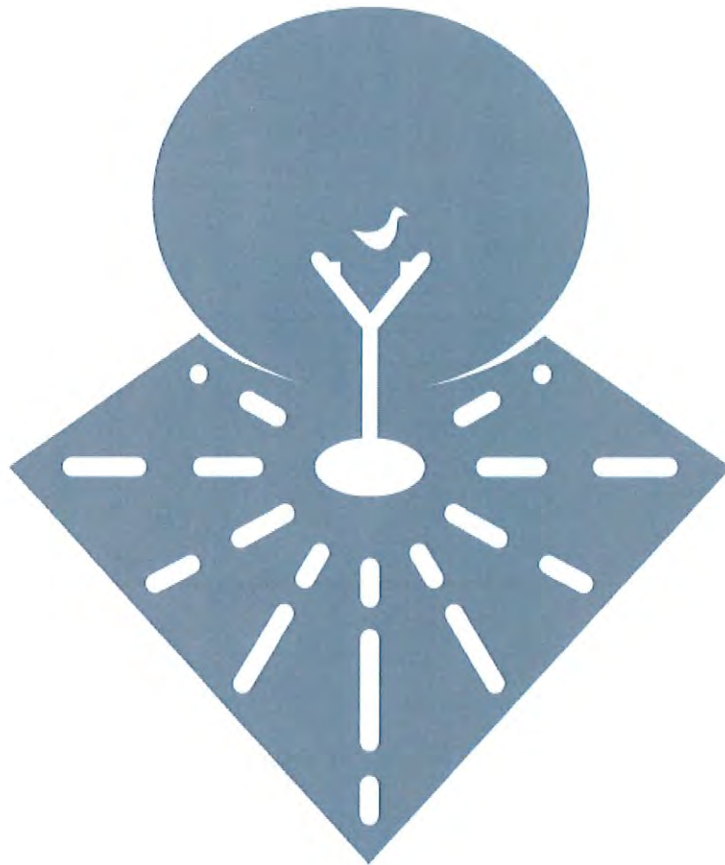
## Site Furnishings



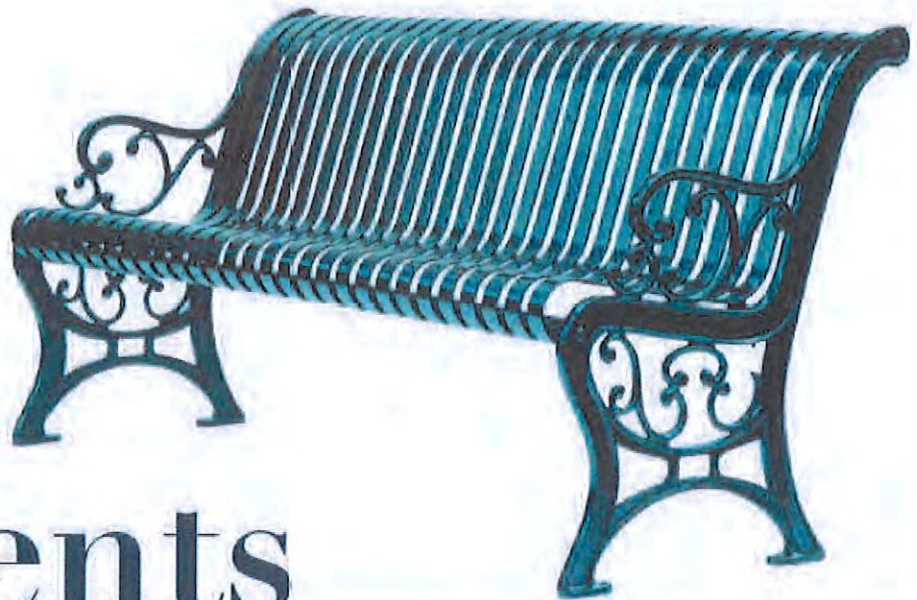


# TRYSTAN

## Site Furnishings



Trystan has been a manufacturer of quality site furnishings for the past 26 years with 46 years of foundry experience. Our experience gives us the unique ability to not only provide you with an extensive line of attractive and durable site furnishings, but also enables us to modify any of our designs, or create a totally new and unique product to your specifications. We currently sell products across North America dealing directly with our customers or through one of our highly qualified representatives.



# Contents

<b>Benches</b>	<b>4</b>
Ultimate Series	4
Heritage Series	6
Omega Series	8
Grand Series	9
Rustic Series	10
Harbour Series	12
<b>Bollards</b>	<b>14</b>
<b>Bell Bollards</b>	<b>18</b>
<b>Custom Work</b>	<b>20</b>
<b>Bike Racks &amp; Drinking Fountains</b>	<b>24</b>
<b>Litter Units</b>	<b>28</b>
<b>Tree Grates</b>	<b>32</b>
<b>Tree Guards</b>	<b>35</b>
<b>Standard Specifications</b>	<b>36</b>





**Elite Bench**

Overall Height 30<sup>3</sup>/<sub>4</sub>"  
Seat Height 16"  
*2 by 4 slats*



**Select Bench**

Overall Height 30<sup>3</sup>/<sub>4</sub>"  
Seat Height 16"  
*2 by 4 slats*



**BB - 1**

Seat Height 16"  
*2 by 3 slats*



**BB - 2**

Seat Height 16"  
*2 by 3 slats*



**Ultra Bench**

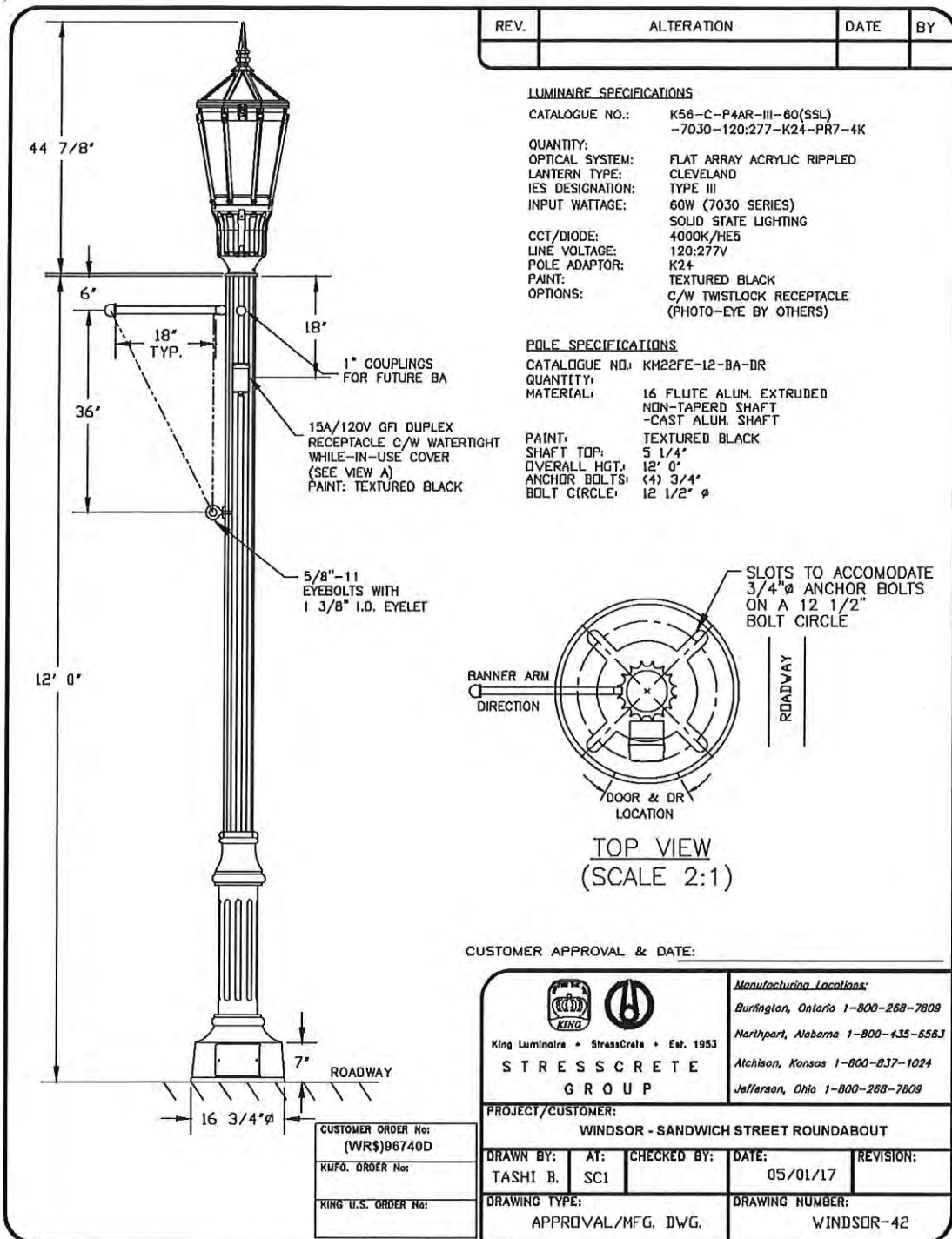
Overall Height 30<sup>3</sup>/<sub>4</sub>"  
Seat Height 16"  
*2 by 3 slats*



**TUPD - 1**

*Ultra Plus Bench double base*

Overall Height 30<sup>3</sup>/<sub>4</sub>"  
Seat Height 16"  
*Also available as elite double, ultra double.*









**Committee Matters: SCM 154/2022**

**Subject: Request for Partial Demolition of a Heritage Listed Property- 2038 Willistead Crescent, C.E. Platt House (Ward 4)**

Moved by: Councillor Holt  
Seconded by: Member Foot

Decision Number: **DHSC 405**

THAT Council **BE INFORMED** of the proposed removal of the cement and flagstone walkway, front stone porch and front door landing, and stone walls around the house above the collapsing porch, at 2038 Willistead Crescent, C.E. Platt House.  
Carried.

Report Number: S 62/2022  
Clerk's File: MBA2022

**Clerk's Note:**

1. The recommendation of the Standing Committee and Administration are the same.
2. Please refer to Item 10.3. from the Development & Heritage Standing Committee Meeting held June 6, 2022.
3. To view the stream of this Standing Committee meeting, please refer to:  
<http://csg001-harmony.sliq.net/00310/Harmony/en/PowerBrowser/PowerBrowserV2/20220608/-1/7334>



**Subject: Request for Partial Demolition of a Heritage Listed Property -  
2038 Willistead Crescent, C.E. Platt House (Ward 4)**

**Reference:**

Date to Council: June 6, 2022  
Author: Kristina Tang, MCIP, RPP  
Heritage Planner  
ktang@citywindsor.ca  
519-255-6543 X 6179  
Planning & Building Services  
Report Date: May 19, 2022  
Clerk's File #: MBA2022

**To:** Mayor and Members of City Council

**Recommendation:**

THAT Council **BE INFORMED** of the proposed removal of the cement and flagstone walkway, front stone porch and front door landing, and stone walls around the house above the collapsing porch, at 2038 Willistead Crescent, C.E. Platt House.

**Executive Summary:** N/A

**Background:**

The property at 2038 Willistead Crescent was 'listed' on the Windsor Municipal Heritage Register on August 27, 2007, alongside other Willistead Crescent properties. The two-and-half storey house was constructed c.1929 in Arts & Craft/Tudor Revival house.



Front view of house (on left) and view of impacted areas (on right)

The current owners, Jesse Garant and Jessica Lockhart, are proposing to remove the connected cement and flagstone walkway, front stone porch and front door landing, and stone walls around the house above the collapsing porch due to safety hazard



concerns. The proposal is intended to be a temporary removal to address the failures and would be followed by restoration conducted in phases. The demolition application is outlined in Appendix A- Heritage Permit Application.

## **Discussion:**

### **Legal provisions:**

The subject property is listed on the Windsor Municipal Heritage Register, but not designated. Section 27 of Part IV of the *Ontario Heritage Act* states that “the register may include property ... that the council of the municipality believes to be of cultural heritage value or interest”, without being designated. Also, “[T]he owner of the property shall not demolish or remove a building or structure on the property or permit the demolition or removal of the building or structure unless the owner gives the council of the municipality at least 60 days notice in writing of the owner’s intention to demolish or remove the building or structure or to permit the demolition or removal of the building or structure.” The 60 days only begins after notice is received accompanying plans and information as Council may require. City of Windsor Council approved “Requirements and Procedures, Application for Demolition of Heritage-Listed Properties” (Council Decision # M163-2015) which outlines the required information for demolition, and notes that Administration has 30 days to evaluate if the information submitted is sufficient. Only after determination has been made that the required information has been submitted, does the 60 day count begin.

During the 60 days after notice, City Council (with Committee consultation) may initiate designation, or decide to take no action. If a property is proposed for designation, a notice of intent to designate must include a statement explaining the cultural heritage value or interest of the property and a description of the heritage attributes of the property, which are those features that are considered important to retain if any alterations to the property are proposed after designation. “Cultural heritage value or interest” is to be considered according to Ontario Regulation 9/06.

There is no explicit provision for the Committee or Council to comment on additions to or remodelling a heritage-listed, non-designated property, other than removal/demolition of structures from the Register under the *Ontario Heritage Act* unless designation is initiated. The Owners are interested in designation but is not a formal recommendation of this report. Designation of the property will be the subject of a separate report if so pursued by the Owner.

### **Property description and proposal:**

The subject property is located on the far-east end close to the loop on Willistead Crescent, whereby the houses to its east were constructed in the 1920s and are recognized on the Windsor Municipal Heritage Register while the rest of the houses on the loop were constructed a few decades later in the middle of the 20<sup>th</sup> century.

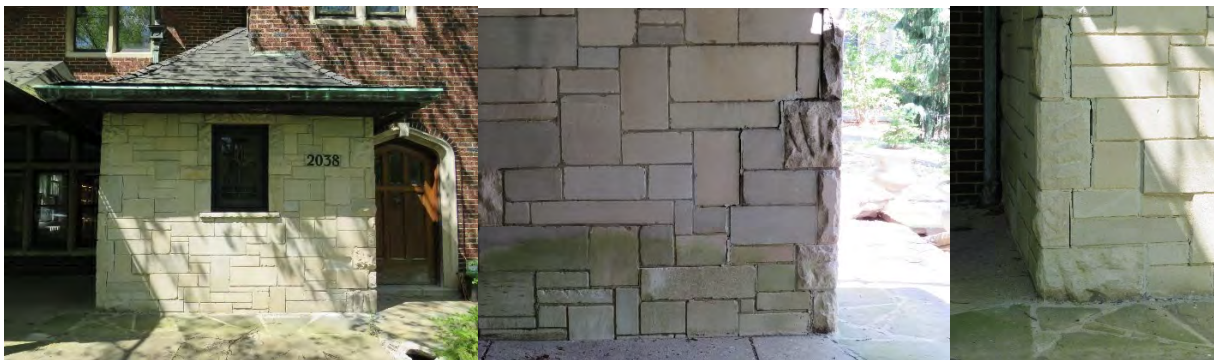
The front porch consists of a low stone wall with stone coping and surface stone pavers, and spans from the landing step to the canopy at the west wall of the building. The porch appears to be original to the construction of the building. A center section of the house west of the front entrance protrudes out. That portion serves as the coat room to

the house and is thought to be dressed with veneer stones that are sitting on the porch. Also associated with the porch are the cement and flagstone walkways.



November 2020 Google Streetview photo of property indicating porch failure (left) and indications of deterioration even as early as May 2012 (right) that has since fallen into the cavity below grade.

From Google Streetview, it appears that the porch started to show signs of visible distress even a decade ago and has deteriorated much since. The current owners rented the property in 2014 and later purchased the property around 4.5 years ago. The owner was not aware of the issues then but informed staff that around 2-3 years ago, part of the front porch wall started to collapse. There had been poor waterproofing work done on the house as it appears no backfilling was done. Over the COVID pandemic, no work was conducted on the property to address the issue. Then in January 2022, the Owner recounts that the foundation for the porch appeared to have moved and the 10" thick top slab of the porch cracked and collapsed. The west side wall of the porch has also destabilized and the Owner is concerned that it would be at risk of collapse next. The stone walls on the coat room for the house (the center portion with stones similar to the porch wall) also shows very visible angled cracks as a result of the movements from the porch.



Cracks developed from porch movements.



Fallen section of the porch wall sunken into the cavity, and associated walkway.



View of hole at slab and view underneath porch with no backfill (Source: Jesse Garant)



View of porch wall still standing with stone coping and veneer stone pieces on the interior and exterior sides.



On left, back steps leading to front porch. On right, the exterior southwest corner of the porch wall.

The Owner has attempted to reach contractors to address the issues but expressed that it has been challenging to find interested contractors and that the work would need to be phased out for financial purposes. The Owner's current plan is to have the porch removed first and salvage materials, to follow up with proper waterproofing work in the fall or summer 2023, then in 2024 to reconstruct. The Owner indicated the wish to have removal of the remaining stone walls at the porch and walkways in 2024, and rebuild the stone porch with salvaged material and similar stones. Also in 2024, there would be a plan to install black paving stones for the porch and walkways reflective of the original period.

A building permit has been noted to be required for the work to review structural and Building Code compliance issues. As a heritage listed property, the building permit application would be circulated to the Heritage Planner for courtesy review though comments are not obligatory unless the property was designated. The phasing of work may still be subject to change depending on construction sequence and building permit requirements.



Reporting of the porch removal is brought forward to Heritage Committee/ Council as per Heritage Processes although the Heritage Planner has conveyed to the Owner that there would be no holdback on the timing of the removals given the safety concerns of the landing/porch (which is the primary access to the building).

However, the Owner has been encouraged to find a contractor that would be able to document before removals. Then to conduct the removals in a careful manner to salvage materials for the rebuild, and to rebuild according to the original proportions and likeness using salvaged materials as a first preference, then combined with matching materials. The Owner is being asked to consider the following extracts from the *Standards & Guidelines for Conservation of Historic Places in Canada*:

9. Make any *intervention* needed to preserve *character-defining elements* physically and visually compatible with the *historic place* and identifiable on close inspection. Document any intervention for future reference.
10. Repair rather than replace *character-defining elements*. Where character-defining elements are too severely deteriorated to repair, and where sufficient physical evidence exists, replace them with new elements that match the forms, materials and detailing of sound versions of the same elements. Where there is insufficient physical evidence, make the form, material and detailing of the new elements compatible with the character of the *historic place*.

	Recommended	Not Recommended
15	<b>Replacing</b> in kind an irreparable entrance, porch or balcony based on physical and documentary evidence. If using the same materials and design details is not technically or economically feasible, then compatible substitute materials or details may be considered.	Removing an irreparable entrance, porch or balcony and not replacing it, or replacing it with a new one that does not convey the same appearance or serve the same function.
15	<b>Using</b> mortars that ensure the long-term preservation of the masonry assembly, and are compatible in strength, porosity, absorption and vapour permeability with the existing masonry units. Pointing mortars should be weaker than the masonry units; bedding mortars should meet structural requirements; and the joint profile should be visually compatible with the masonry in colour, texture and width.	Repointing with mortar of a higher Portland cement content than in the original mortar. This can create a bond stronger than the historic material (brick or stone) and cause damage as a result of the differing expansion coefficients and porosity of the materials.  Repointing with a synthetic caulking compound.  Using a 'scrub' coating technique to repoint instead of using traditional repointing methods.
16	<b>Duplicating</b> original mortar joints in colour, texture, width and joint profile.	

18	<p><b>Repairing</b> masonry by patching, piecing-in or consolidating, using recognized conservation methods. Repair might include the limited replacement in kind, or replacement with a compatible substitute material, of extensively deteriorated or missing masonry units, where there are surviving prototypes. Repairs might also include dismantling and rebuilding a masonry wall or structure, if an evaluation of its overall condition determines that more than limited repair or replacement in kind is required.</p>
----	--

## Official Plan Policy:

The Windsor Official Plan includes (9.0) “A community’s identity and civic pride is rooted in physical and cultural links to its past. In order to celebrate Windsor’s rich history, Council is committed to recognizing, conserving and enhancing heritage resources.”

Objectives include (9.3.2.1) “Council will identify Windsor’s heritage resources by: ... (c) Researching and documenting the history and architectural and contextual merit of potential heritage resources on an individual property basis; ... 9.3.3.4.(a) maintaining and updating the list of built heritage resources known as the Windsor Municipal Heritage Register.” and 9.3.5.1 (b) “Providing technical information on the preservation of heritage resources.”

## Risk Analysis:

The demolition proposed is requested as the front porch is collapsing and a safety concern. Removals need to take place first to address any structural issues before the entrance to the property can be reinstated.

## Climate Change Risks

**Climate Change Mitigation:** N/A

**Climate Change Adaptation:** N/A

## Financial Matters:

There is no financial request from this report. If there is request for financial support from the City for the restoration of the porch and stone walls, the property will first need to be designated and undergo all of the necessary processes before consideration of eligibility for municipal heritage incentives for conservation works on the property.

## Consultations:

Discussion took place between Heritage Planning staff, the Owner, and City Building Department staff. Adam Coates, Senior Urban Designer was also consulted.

## Conclusion:

Council is to be informed of the proposed removal of the cement and flagstone walkway, front stone porch and front door landing, and stone walls around the house above the collapsing porch, at 2038 Willistead Crescent, C.E. Platt House and of the intended rebuild.



**Planning Act Matters:** N/A

**Approvals:**

<b>Name</b>	<b>Title</b>
Michael Cooke	Manager of Planning Policy/ Deputy City Planner
Thom Hunt	City Planner / Executive Director Planning & Building
Dana Paladino	Acting Commissioner, Legal & Legislative Services
Jelena Payne	Commissioner, Economic Development & Innovation
Shelby Askin Hager	Chief Administrative Officer (Acting)

**Notifications:**

<b>Name</b>	<b>Address</b>	<b>Email</b>
Jesse Garant		jesseg@jgarantmc.com

**Appendices:**

- 1 Appendix A- Heritage Permit Application

## CORPORATION OF THE CITY OF WINDSOR

Planning Dept., Suite 320-350 City Hall Sq W, Windsor ON N9A 6S1  
519-255-6543 | 519-255-6544 (fax) | [planningdept@citywindsor.ca](mailto:planningdept@citywindsor.ca)

### 1. Applicant, Agent and Registered Owner Information

Provide in full the name of the applicant, registered owner and agent, the name of the contact person, and address, postal code, phone number, fax number and email address. If the applicant or registered owner is a numbered company, provide the name of the principals of the company. If there is more than one applicant or registered owner, copy this page, complete in full and submit with this application.

#### APPLICANT

Contact Name(s) Jesse Garant  
Company or Organization N/A  
Mailing Address 2038 Willistead Crescent



#### REGISTERED OWNER IF NOT APPLICANT

Contact Name(s) \_\_\_\_\_  
Company or Organization \_\_\_\_\_  
Mailing Address \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
Email \_\_\_\_\_ Postal Code \_\_\_\_\_  
Phone(s) \_\_\_\_\_

#### AGENT AUTHORIZED BY REGISTERED OWNER TO FILE THE APPLICATION

Contact Name(s) \_\_\_\_\_  
Company or Organization \_\_\_\_\_  
Mailing Address \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
Email \_\_\_\_\_ Postal Code \_\_\_\_\_  
Phone(s) \_\_\_\_\_

#### Who is the primary contact?

☒ Applicant ☒ Registered Owner ☐ Agent





## 5. PROPOSED WORK

*Provide a detailed written description of work to be done, including any conservation methods you plan to use. Provide details, drawings, and written specifications such as building materials, measurements, window sizes and configurations, decorative details, etc.. Attach site plans, elevations, product spec sheets, etc. to illustrate, if necessary.*

2022 - Removal of collapsed front porch. Salvage of coping stones from porch

2023 - Waterproofing, waterline replacement

2024 - Removal of stone wall that is sitting on the remains of the porch and walkways.

salvage of window and key corner stones, rebuild wall with stone similar to the period and salvaged stones, rebuild stone porch with stone similar to period and reuse existing coping stones, installation of black paving stones for porch and walkways reflective of period

## 6. HERITAGE PERMIT RATIONALE

*Explain the reasons for undertaking the proposed work and why it is necessary.*

Safety Hazard, foundation collapse

*Describe the potential impacts to the heritage attributes of the property.*

Temporary removal of front porch and front door landing, sidewalks (flagstone) and top of porch (flagstone & cement) switched to black paving stones reflective of period.

## 7. CHECKLIST OF MATERIALS SUBMITTED Check all that apply:

Required:

- ☒ Photographs (showing the current condition and context of existing buildings, structures, and heritage attributes that are affected by the application)
- ☐ Site plan/ Sketch (showing buildings on the property and location of proposed work)
- ☐ Drawings of proposed work (e.g. existing and proposed elevations, floor plans, roof plans, etc., as determined by Heritage Planning staff)
- ☐ Specifications of proposed work (e.g. construction specification details)

Potentially required (to be determined by Heritage Planning staff):

- ☐ Registered survey
- ☐ Material samples, brochures, product data sheets etc.
- ☐ Cultural Heritage Evaluation Report
- ☐ Heritage Impact Assessment (HIA)
- ☐ Heritage Conservation Plan
- ☐ Building Condition Assessment

## 8. NOTES FOR DECLARATION

*The applicant hereby declares that the statements made herein and information provided are, to the best of their belief and knowledge, a true and complete representation of the purpose and intent of this application.*

*The applicant agrees that the proposed work shall be done in accordance with this application, including attachments, and understands that the issuance of the Heritage Alteration Permit under the Ontario Heritage Act shall not be a waiver of any of the provisions of any By-Law of the Corporation of the City of Windsor, or the requirements of the Building Code Act, RSO 1980, c51.*

*The applicant acknowledges that in the event a permit is issued, any departure from the conditions imposed by the Council of the Corporation of the City of Windsor, or plans and specifications approved is prohibited and could result in the permit being revoked. The applicant further agrees that if the Heritage Alteration Permit is revoked for any cause of irregularity, in the relation to non-conformance with the said agreements, By-Laws, acts or regulations that, in consideration of the issuance of the permit, all claims against the City for any resultant loss or damage are hereby expressly waived.*

**APPLICANT** Signature(s) Jesse Garant Date May 11, 2022  
\_\_\_\_\_  
Date \_\_\_\_\_



## SCHEDULE A

### A. Authorization of Registered Owner for Agent to Make the Application

If the applicant is not the registered owner of the land that is the subject of this application, the written authorization of the registered owner that the applicant is authorized to make the application must be included with this application form or the authorization below must be completed.

I, \_\_\_\_\_, am the registered owner of the land that is  
*name of registered owner*  
subject of this application for a Heritage Alteration Permit and I authorize  
\_\_\_\_\_ to make this application on my behalf.  
*name of agent*

\_\_\_\_\_  
Signature of Registered Owner

\_\_\_\_\_  
Date

If Corporation – I have authority to bind the corporation.

### B. Consent to Enter Upon the Subject Lands and Premises

I, \_\_\_\_\_, hereby authorize the members of the Windsor Heritage Committee and City Council and staff of the Corporation of the City of Windsor to enter upon the subject lands and premises described in Section 3 of the application form for the purpose of evaluating the merits of this application and subsequently to conduct any inspections on the subject lands that may be required as condition of approval. This is their authority for doing so.

\_\_\_\_\_  
Signature of Registered Owner

\_\_\_\_\_  
Date

If Corporation – I have authority to bind the corporation.

### C. Acknowledgement of Applicant

I understand that receipt of this application by the City of Windsor Planning Department does not guarantee it to be a complete application. Further review of the application will occur and I may be contacted to provide additional information and/or resolve any discrepancies or issues with the application as submitted.

I further understand that pursuant to the provisions of the Ontario Heritage Act and the Municipal Freedom of Information and Protection of Privacy Act, this application and all material and information provided with this application are made available to the public.

\_\_\_\_\_  
Signature of Applicant

\_\_\_\_\_  
Date

## DO NOT COMPLETE BELOW – STAFF USE ONLY

### Approval Record

Date Received by Heritage Planner: \_\_\_\_\_

Building Permit Application Date, if needed: \_\_\_\_\_

☐ Application Approval (City Council):

Development & Heritage Standing Committee: \_\_\_\_\_

City Council: \_\_\_\_\_

☐ Application Approval (City Planner):

Heritage Planner: \_\_\_\_\_

Staff Decision Appealed to City Council: \_\_\_\_\_

If so, Date to City Council: \_\_\_\_\_

Council Decision Appealed: \_\_\_\_\_

Additional Notes / Conditions:

---

---

---

---

### DECISION

Heritage Permit No.: \_\_\_\_\_ Date: \_\_\_\_\_

Council Motion or City Planner's Signature: \_\_\_\_\_

Please contact Heritage Planning to request inspections at [ktang@citywindsor.ca](mailto:ktang@citywindsor.ca)

### CONTACT INFORMATION

Planning Department - Planning Policy  
Corporation of the City of Windsor  
Suite 320 - 350 City Hall Square West  
Windsor ON N9A 6S1  
[planningdept@citywindsor.ca](mailto:planningdept@citywindsor.ca)  
519-255-6543 x 6179  
519-255-6544 (fax)  
<http://www.citywindsor.ca>



**Committee Matters: SCM 155/2022**

**Subject: Bill 109, More Homes for Everyone Act, 2022 – Changes to the Planning Act Affecting Site Plan Control Approval, City Wide**

Moved by: Councillor Sleiman

Seconded by: Councillor Holt

Decision Number: **DHSC 406**

- I. THAT Council **REPEAL** Bylaw Number 11275: A by-law to delegate authority to the City Planner or designate, to approve plans and drawings and to impose conditions of the approval.
- II. THAT Council **AMEND** Bylaw 139-2013 to delegate site plan control approval authority to the City Planner, allow the City Planner to determine the completeness of site plan control applications before accepting an application, terminate redundant site plan control agreements and remove all references to the Manager of Development Applications in accordance with the requirements of Bill 109, *More Homes for Everyone Act, 2022*.

Carried.

Report Number: S 57/2022

Clerk's File: Z2022

**Clerk's Note:**

1. The recommendation of the Standing Committee and Administration are the same.
2. Please refer to Item 11.1. from the Development & Heritage Standing Committee Meeting held June 6, 2022.
3. To view the stream of this Standing Committee meeting, please refer to:  
<http://csg001-harmony.sliq.net/00310/Harmony/en/PowerBrowser/PowerBrowserV2/20220608/-1/7334>

**Subject: Bill 109, More Homes for Everyone Act, 2022 – Changes to the Planning Act Affecting Site Plan Control Approval, City Wide**

**Reference:**

Date to Council: June 6, 2022  
Author: Neil Robertson  
Manager Urban Design/Deputy City Planner  
519-255-6543, ext. 6461  
nrobertson@citywindsor.ca  
Planning & Building Services  
Report Date: May 6, 2022  
Clerk's File #: Z2022

**To:** Mayor and Members of City Council

**Recommendation:**

- I. THAT Council **REPEAL** Bylaw Number 11275: A by-law to delegate authority to the City Planner or designate, to approve plans and drawings and to impose conditions of the approval.
- II. THAT Council **AMEND** Bylaw 139-2013 to delegate site plan control approval authority to the City Planner, allow the City Planner to determine the completeness of site plan control applications before accepting an application, terminate redundant site plan control agreements and remove all references to the Manager of Development Applications in accordance with the requirements of Bill 109, More Homes for Everyone Act, 2022.

**Executive Summary:**

N/A

**Background:**

**Site Plan Control**

Section 41 of the *Planning Act* permits municipalities to establish a site plan control area (or areas) within the municipality. In 2004, the "Site Plan Control Area" designation was expanded to the entire limits of the city of Windsor by Bylaw 1-2004. Most classes (e.g. commercial, multi-res, etc) of development in Windsor are subject to Site Plan Control.

Site Plan Control refers to the process by which the City reviews and approves development in accordance with the physical planning, built form and operational objectives identified within the Official Plan. This is to ensure that development will, among other things:

- Be compatible with adjacent or nearby properties;
- Have safe and easy access for pedestrians and vehicles;
- Have adequate landscaping, parking and servicing;
- Meet specific standards of quality and appearance; and,
- Be built and maintained in the manner by which the proposal was approved.

The Site Plan Review process takes into account input from various departments and agencies towards the preparation of a Site Plan Approval Agreement. The Site Plan Approval Agreement is a binding contract between the City of Windsor and the applicant/owner. The agreement consists of conditions of development, is registered on title of the property and is a prerequisite to the building permit application process.

### **Bill 109, More Homes for Everyone Act, 2022**

*Bill 109, More Homes for Everyone Act, 2022* received Royal Assent on April 14, 2022. This Act amends a number of other statutes, including the *Planning Act*, with respect to housing, development and various other matters.

### **Discussion:**

*Bill 109, More Homes for Everyone Act, 2022* makes a number of changes to section 41 of the *Planning Act* that impact Site Plan Control. These amendments set out the rules respecting consultations with municipalities before plans and drawings are submitted for approval and respecting completeness of applications. New subsection (4.0.1) of the *Planning Act* requires municipalities to appoint an authorized staff person to approval all site plans. Finally, new subsection (11.1) provides for rules respecting when municipalities are required to refund fees when legislated timelines are not met. This report will discuss all of these changes and what they mean for the delivery of the Site Plan Control service in Windsor.

### **Delegating SPC Authority**

The City of Windsor has had delegated approval authority for Site Plan Control since 1998 for all site plans with the following specific exceptions:

- Any development on City-owned lands;
- Any development within the Downtown Business Improvement Area;
- Any development on the north side of Riverside Drive; and,
- Any development on lands identified in a resolution of Council requiring site plan approval by Council.

However, new subsection (4.0.1) of the *Planning Act* requires municipalities to appoint an authorized staff person to approval all site plans. This means that municipal councils



in Ontario no longer have the authority to directly approve site plans. Specifically, Bill 109 states:

*(4.0.1) A council that passes a by-law under subsection (2) shall appoint an officer, employee or agent of the municipality as an authorized person for the purposes of subsection (4).*

The reference to Subsection (4) above is the approval of plans and drawings. **This change is non-discretionary and is required to be implemented by July 1, 2022.**

Windsor's original delegated approval authority came from Bylaw Number 11275. In order to implement this change, Recommendation 1 is to repeal Bylaw 11275 that includes the exceptions listed above that are no longer permitted. Recommendation 2 is to amend Bylaw 139-2013 to add Site Plan Control to the other approvals that are already delegated to the City Planner.

### **Pre-Consultation**

*Bill 109, More Homes for Everyone Act, 2022* also provides for the opportunity to require pre-consultation prior to submission of a formal site plan control application. In order to implement mandatory pre-consultation, municipalities need a bylaw that requires consultation before submitting an application. Windsor has Bylaw 199-2007 that requires pre-consultation prior to submitting any Planning Act application.

Windsor's pre-consultation process results in a letter provided to the applicant that outlines the required studies, documents, and information needed to be submitted as part of a complete application. Since this bylaw has been in place since 2007, there is no further action required to implement mandatory pre-consultation.

### **Complete Application**

The new changes also allow for the City to refuse applications until all of the material identified during the pre-consultation process has been submitted. This process is like the complete application process that already applies to official plan amendment applications and zoning bylaw amendment applications. This ensures that site plan staff have all of the information that they need to address and mitigate the potential impacts of the proposed development. Until there is a complete application the clock doesn't start on the prescribed time period for approving the drawings or plans (see Timeline Changes below).

The *Planning Act* requires policies in the Official Plan to allow to define what is required for a complete application and to refuse acceptance of the application until such time that all of the required materials have been submitted. Windsor's Official Plan already includes the following policy regarding a complete application:

*When the pre-application consultation process for a proposed development approval application identifies the need for one or more support studies, the application shall not be considered complete for processing purposes until the required study or studies is prepared and submitted to the satisfaction of the Municipality. Notification of a complete*

*application shall be given to the applicant and all other parties by the Municipality in accordance with the Planning Act. 10.2.1.12*

The complete application provisions flow directly from the Pre-Consultation process that is made mandatory by Bylaw 199-2007.

The City has 30 days to provide notice to the applicant that the plans and drawings, and the information and material required from the pre-consultation have been provided, or to inform the applicant that additional material is required prior to accepting the application as complete.

### **Timeline Changes**

Prior to April 14, 2022, the *Planning Act* provided the municipality 30 days from when the application was submitted to approve the plans or drawings. *Bill 109, More Homes for Everyone Act, 2022* is changing the time period from 30 days from when the application is submitted to 60 days from when the application is deemed to be complete.

The Planning Department has been issuing the draft approval within the 30 day deadline consistently (+95% of the time) over the last number of years. Even though the time period has increased to 60 days, the Planning Department intends to continue meeting the 30 day deadline to maintain a high standard of service to the development community.

### **Refunding Fees**

Finally, *Bill 109, More Homes for Everyone Act, 2022* makes a change to the *Planning Act* that will require municipalities to refund a portion or all of the SPC fees paid if the 60 day deadline is passed without approving the plans or drawings. The refund amount is based on the following sliding scale:

1. No refund if the municipality approves the plans or drawings within the 60 days;
2. A 50% refund if approval comes between 61 and 89 days;
3. A 75% refund if approval comes between 90 and 119 days; and,
4. A 100% refund if the approval comes after 120 days of the application being deemed complete.

This change takes effect January 1, 2023, and will also apply to amendments to the official plan and zoning bylaw.

### **Terminating Agreements**

This is not a change coming from *Bill 109, More Homes for Everyone Act, 2022*; it is a housekeeping matter that is related to the other changes that are recommended by this report.

Currently, site plan approval agreements are registered on title as soon as the agreements are executed. They also include a defined period of time – usually two years – for the construction of the project to start. There are occasions when the site plan agreement is registered on title but the project does not proceed. There may also

be situations when a new site plan approval supersedes an older one. In these instances, the agreement will remain on title, which can sometimes complicate future real estate transactions. Occasionally, the City will receive a request to authorize the removal of an old site plan agreement from title because of one of these two situations.

The authority to terminate site plan agreements and approve their removal from title currently resides with the City Planner in Bylaw 11275. Since Recommendation 1 is to repeal Bylaw 11275, this component of the bylaw will be moved over to Bylaw 139-2013 to allow the City Planner to continue with the authority to terminate such redundant site plan agreements and approving the removal of old site plan agreements from title.

### **Updating Bylaw 139-2013**

Since this report is recommending changes to Bylaw 139-2013, it is an opportune time to address some other changes to the bylaw. The changes resulting from Recommendation 2 eliminates all references to the Manager of Development Applications position in the Bylaw. This position was eliminated as part of a departmental reorganization in 2018.

### **Implementation Summary**

The following is a summary of the actions required to implement the changes resulting from *Bill 109, More Homes for Everyone Act, 2022* and to effect other housekeeping amendments to site plan related bylaws:

- **Delegating SPC Authority** – This change will be implemented by repealing Bylaw 11275 and amending Bylaw 139-2013 to add Site Plan Control to the approvals that are already delegated to the City Planner (see Appendix B - Subsection 8.1);
- **Pre-Consultation** – No action is required because Windsor has Bylaw 199-2007 in place to require pre-consultation;
- **Complete Application** – No action is required because Windsor has Bylaw 199-2007 in place to require pre-consultation and to allow the City to refuse applications that are not complete. Adding a reference to Section 41 of the Planning Act (site plan) to Subsection 1.1.a of Schedule A from Bylaw 139-2013;
- **Timeline Changes** – This is change to the Planning Act that does not require any action by Council;
- **Refunding Fees** – This is change to the Planning Act that does not require any action by Council;
- **Terminating Agreements** – This is a housekeeping amendment to Bylaw 139-2013 to add the termination of old site plan agreements that are already

delegated to the City Planner by Bylaw 11275, which is being repealed (see Appendix B - Subsection 10.1); and,

- **Updating Bylaw 139-2013** – This is a housekeeping amendment to Bylaw 139-2013 to eliminate all references to the Manager of Development Applications position in the Bylaw.

### **Risk Analysis:**

There is the risk being out of compliance with Provincial legislation (i.e. the *Planning Act*) if all Site Plan Control approvals are not delegated to an officer, employee or agent of the municipality by July 1, 2022.

### **Climate Change Risks**

#### **Climate Change Mitigation:**

N/A

#### **Climate Change Adaptation:**

N/A

### **Financial Matters:**

There are no financial implications associated with any of the changes resulting from this report.

### **Consultations:**

Wira Vendrasco – Deputy City Solicitor - Legal & Real Estate

Alex Hartley – Senior Legal Counsel

### **Conclusion:**

*Bill 109, More Homes for Everyone Act, 2022* resulted in some changes to the *Planning Act* that require changes to existing municipal bylaws in order to comply with the legislative changes. There are also some discretionary changes to the *Planning Act* – many of which the City already does – that require amending some of the site plan related bylaws. Finally, there are some housekeeping amendments that are necessary to update the existing site plan related bylaws.

### **Planning Act Matters:**

N/A

### **Approvals:**

Name	Title
Neil Robertson	Manager of Urban Design / Deputy City Planner

<b>Name</b>	<b>Title</b>
Thom Hunt	City Planner / Executive Director, Planning & Development Services
Wira Vendrasco	Deputy City Solicitor, Legal Services & Real Estate
Jelena Payne	Commissioner, Economic Development & Innovation
Shelby Askin Hager	Acting Chief Administration Officer

**Notifications:**

<b>Name</b>	<b>Address</b>	<b>Email</b>

**Appendices:**

- 1 Appendix A – Bylaw Number 11275 - Site Plan Approval Authority
- 2 Appendix B - Bylaw 199-2007
- 3 Appendix C – Schedule A from Bylaw 139-2013
- 4 Appendix D - Delegation to City Planner



## APPENDIX A

amended B/L 11569, Sept. 7/93  
amended B/L 12772, Dec.16/96  
amended B/L 398-1998, Dec.21/98  
amended B/L 228-2002, Aug.12/2002  
amended B/L 172-2003, June 16/2003  
amended B/L 54-2014, April 7/2014

B I L L  
No. 5  
1993

### BY – L A W N U M B E R 11275

A BY-LAW TO DELEGATE AUTHORITY TO THE CITY PLANNER OR DESIGNATE, TO APPROVE PLANS AND DRAWINGS AND TO IMPOSE CONDITIONS OF THE APPROVAL (**amended B/L 398-1998, Dec.21/98; B/L 228-2002, Aug.12/2002; amended B/L 172-2003, June 16/2003**)

Passed the 4<sup>th</sup> day of January, 1993

WHEREAS by By-law Number 6326, as amended, parts of the City of Windsor have been designated as a site plan control area;

AND WHEREAS Subsection 41 (13)(b) of the Planning Act, R.S.O. 1990, provides that the Council may by by-law delegate to an appointed officer of the municipality identified in a by-law by position occupied by any of the Council's power or authority under Section 41 of the said Planning Act;

AND WHEREAS it is deemed expedient to delegate to the City Planner or designate, the authority to approve plans and drawings and impose conditions to such approval as set out in Subsections 41 (4), (5) and (7) of the Planning Act; (**amended B/L 398-1998, Dec.21/98; B/L 228-2002, Aug.12/2002; amended B/L 172-2003, June 16/2003**)

THEREFORE the Council of the Corporation of the City of Windsor enacts as follows:

1. The City Planner or designate may approve plans and drawings referred to in Subsection 2(2) of by-law 6326 except where the development referred to in Subsection 2(1) of the said By-law 6326 is located on the following lands: (**amended B/L 398-1998, Dec.21/98; B/L 228-2002, Aug.12/2002; amended B/L 172-2003, June 16/2003**)

- (a) land owned by the Corporation of the City of Windsor;
- (b) land described in a resolution of the Council of the Corporation of the City of Windsor as requiring approval by the Council;
- (c) land on the north side of Riverside Drive lying between the easterly limit of the City of Windsor and Chewett Street;
- (d) land within the Downtown Business Improvement Area and bounded by Riverside Drive on the north; Elliott Street on the south; Glengarry Avenue, Chatham Street, McDougall Street, Tuscarora Street and Windsor Avenue on the east; and Bruce Avenue, Pitt Street, Church Street, Park Street, the north/south alley west of Victoria Avenue between Park Street and Elliott Street, Elliott Street and Victoria Avenue on the west; and

further that the City Planner or designate may approve a minor change or changes to any plans and drawings approved by Council on lands set out in clauses (a), (b), (c) and (d) provided that general intent of such plans and drawings is maintained. (**amended B/L 398-1998, Dec.21/98; B/L 228-2002, Aug.12/2002; amended B/L 172-2003, June 16/2003**)

1a. Despite clauses 1. (a), (b), (c) and (d) the City Planner or designate may, within the Downtown Business Improvement Area, approve plans and drawings of minor development of limited scale, which will not significantly alter the current use of the property and/or off-site uses, and/or have any impact on municipal services, or approve a plan or drawing on any land as authorized by a resolution of the Council of the Corporation of the City of Windsor. (**added B/L 12772, Dec. 16/96**) (**amended B/L 398-1998, Dec.21/98; B/L 228-2002, Aug.12/2002; amended B/L 172-2003, June 16/2003**)(**deleted and substituted B/L 54-2014, April 7/14**)

2. The City Planner or designate may as a condition to the approval of plans and drawings referred to in Section 1 and 1a of this by-law, require the owner of the land to: **(amended B/L 12772, Dec.16/96) (amended B/L 398-1998, Dec.21/98; B/L 228-2002, Aug.12/2002; amended B/L 172-2003, June 16/2003)**

- (a) provide to the satisfaction of and at no expense to the Corporation of the City of Windsor any or all of the following:
  - 1. Widenings of highways that abut on the land as provided for in the Official Plan of the City of Windsor;
  - 2. Subject to the Public Transportation and Highway Improvement Act, facilities to provide access to and from the land such as access ramps and curbs and traffic direction signs;
  - 3. Off-street vehicular loading and parking facilities, either covered or uncovered, access driveways, including driveways for emergency vehicles, and the surfacing of such areas and driveways;
  - 4. Walkways and walkway ramps, including the surfacing thereof, and all other means of pedestrian access;
  - 5. Facilities for the lighting, including floodlighting, of the land or of any buildings or structures thereon;
  - 6. Walls, fences, hedges, trees, shrubs or other groundcover or facilities for the landscaping of the lands or the protection of adjoining lands;
  - 7. Vaults, central storage and collection areas and other facilities and enclosures for the storage of garbage and other waste material;
  - 8. Easements conveyed to the municipality for the construction, maintenance or improvement of watercourses, ditches, land drainage works, sanitary sewage facilities and other public utilities of the Corporation of the City of Windsor or local board thereof on the land;
- 9. Grading or alteration in elevation or contour of the land and provision for the disposal of storm, surface and waste water from the land and from any buildings or structures thereon;
- (b) maintain to the satisfaction of the Corporation of the City of Windsor and at the sole risk and expense of the owner any or all of the facilities or works mentioned in paragraphs 2, 3, 4, 5, 6, 7, 8 and 9 of clause (a), including the removal of snow from access ramps and driveways, parking and loading areas and walkways;
- (c) enter into one or more agreements with the Corporation of the City of Windsor dealing with and ensuring the provision of any or all of the facilities, works or matters mentioned in clause (a) and the maintenance thereof as mentioned in clause (b) or with the provision and approval of the plans and drawings referred to in Section 1 of this by-law.

2a. The City Planner or designate may terminate an agreement entered into by the Corporation as provided for in Section 2(c) when the development provided for in the said agreement has not commenced or an alternate development has been approved by Council or the City Planner or designate. **(added B/L 11569, Sept. 7/93) (amended B/L 398-1998, Dec.21/98; B/L 228-2002, Aug.12/2002; amended B/L 172-2003, June 16/2003)**

3. That By-law Number 8025 being “A By-law to Delegate Authority to the Building Commissioner to make minor changes in Site Plans or Drawings” is repealed.

4. This by-law should come into force and take effect on the day of the final passing thereof.

(signed) “M. Hurst”  
M A Y O R

(signed) “T. Lynd”  
C L E R K

First Reading - January 4, 1993  
Second Reading - January 4, 1993  
Third Reading - January 4, 1993

**APPENDIX B**

**BY-LAW NUMBER 199-2007**

**A BY-LAW TO REQUIRE A PERSON OR PUBLIC BODY TO CONSULT WITH THE MUNICIPALITY BEFORE SUBMITTING APPLICATIONS FOR AN AMENDMENT TO THE OFFICIAL PLAN, AN AMENDMENT TO THE ZONING BY-LAW, SITE PLAN APPROVAL, PLAN OF SUBDIVISION APPROVAL OR PLAN OF CONDOMINIUM APPROVAL IN THE CITY OF WINDSOR**

Passed the 29th day of October, 2007.

**WHEREAS** the *Planning and Conservation Land Statute Law Amendment Act, 2006* amending the *Planning Act* came into force and effect on January 1, 2007.

**AND WHEREAS** the amendments to the *Planning Act* modify aspects of the land use planning process including the authority of a municipal council to require a person or public body requesting an amendment to its official plan, an amendment to its zoning by-law, approval of plans or drawings within an area of site plan control and approval of a plan of subdivision, to consult with the municipality before submitting an application.

**AND WHEREAS** Windsor City Council adopted resolution 166/2007 on April 23, 2007 that authorized a by-law requiring applicants to consult with the municipality before submitting applications for an official plan amendment, zoning by-law amendment, site plan approval and plan of subdivision approval.

**THEREFORE** the Council of the Corporation of the City of Windsor enacts as follows:

1. That a person or public body shall consult with administrative staff of The Corporation of the City of Windsor before submitting applications for an amendment to the City of Windsor Official Plan, an amendment to the Zoning By-law, approval of plans and drawings within an area of site plan control, approval of a plan of subdivision or approval of a plan of condominium.
2. This by-law shall come into force and take effect on the day of the final passing thereof.

**EDDIE FRANCIS, MAYOR**

**CLERK**

First Reading - October 29, 2007  
Second Reading - October 29, 2007  
Third Reading - October 29, 2007

## **APPENDIX C**

### **SCHEDULE “A”**

Council hereby delegates to the City Planner the authority:

#### **Complete Application**

- 1.1 a) To determine whether an application submitted to Council pursuant to sections 22, 34, 41 and 51 of the Act is complete or incomplete in accordance with the provisions of the Official Plan for the City of Windsor.
- b) To notify an applicant as to the completeness or incompleteness of an application.
- c) To notify the applicant within 30 days of receipt of the processing fee for a planning application, whether the application is complete or incomplete. An incomplete application notification shall identify the missing or deficient information and material necessary to complete the application.
- 1.2 The provisions of subsection 1.1 apply, with necessary modifications, to each subsequent remedial submission provided to complete the application.
- 1.3 Within 15 days after the City Planner gives notice to the applicant that the application is complete, to:
  - (i) give the prescribed persons and public bodies, in the prescribed manner, notice of the application under section 22, 34 or 51 of the Act, accompanied by the prescribed information; and
  - (ii) make the information and material provided by the applicant available to the public.
- 1.4 Any notice required under this section shall be given to the applicant in writing by regular letter mail and shall be deemed to be given within 7 business days of the date of the notice.

#### **Exemption from Part Lot Control**

- 2.1 To approve applications for exemption from part lot control under s. 50(7) of the Act.



## **Extension of Condominium and Subdivision Approval**

- 3.1 To approve applications to extend the draft approval pursuant to s.51 (33) of the Act, for 3 years, and further extend the draft approval for another 3 years, of the draft subdivision/condominium or condominium conversion approval as the case may be, provided such approval is given before the draft approval lapses.

## **Sandwich Heritage Conservation District Plan –Heritage Alteration Permit**

- 4.1 To process applications for and issue permits for alterations pursuant to s. 42 (1) of the Ontario Heritage Act for the following works:
- (i) Window or door removal without replacement,
  - (ii) Additions of a window or door in a new or altered opening,
  - (iii) Shutter removal (if original),
  - (iv) Addition or removal of front and side fencing,
  - (v) Decorative trim and bracket removal or replacement,
  - (vi) Removal of chimneys, if significant visual feature,
  - (vii) Removal or installation of cladding and siding,
  - (viii) Painting of previously unpainted brick or stone,
  - (ix) Re-roofing with different materials,
  - (x) Window removal where window is a significant feature from street,
  - (xi) Removal of brick or stone piers (if original), and
  - (xii) Wall mounted signage.

## **Neighbourhood Residential Rehabilitation Grant Program**

- 5.1 To approve applications for *Neighbourhood Residential Rehabilitation Grant Program* projects to a maximum of \$15,000.00 and where sufficient budget funding is available.

## **Municipal Development Fees Grant Program**

- 6.1 To approve applications for *Municipal Development Fees Grant Program* when it is requested as part of the *Neighbourhood Residential Rehabilitation Grant* **only**, to a maximum of \$2,000.00 and where sufficient budget funding is available.

## **Building Facade Improvements**

- 7.1 Along with the Chief Financial Officer/City Treasurer, to approve beautification projects under \$5,000.00 identified in Category A-Beautification under the City of Windsor Building Facade Improvement Program and Urban Design Guidelines for Main Street.

### **Site Plan Control Approval**

- 8.1 To approve Site Plan Control plans and drawings as set out in Subsections 41(4) and (5), and to impose conditions to such approval as set out in Subsection 41 (7) of the Planning Act.

### **Execute and Cause to be Registered Site Plan Agreements**

- 9.1 To require and approve the Site Plan Agreement(s) as set out in Subsections 41(7) of the Planning Act, and to execute and cause to be registered against the land as set out in Subsection 41 (10) of the Planning Act.

### **Removal of Site Plan Control Agreements from Title**

- 10.1 To terminate an agreement entered into by the Corporation as provided for in Section 9.1 when the development provided for in the said agreement has not commenced or an alternate development has been approved in accordance with Section 8.1.

## APPENDIX D

### BY-LAW NUMBER 139-2013

#### A BY-LAW TO DELEGATE AUTHORITY TO THE CITY PLANNER TO APPROVE AND PROCESS CERTAIN APPLICATIONS UNDER THE PLANNING ACT AND PERMITS UNDER THE ONTARIO HERITAGE ACT

Passed the 26<sup>th</sup> day of August, 2013.

**WHEREAS** various sections of the *Planning Act*, R.S.O. 1990, c. P.13, as amended and the *Ontario Heritage Act*, S.O. 2005, c.6 authorize councils to delegate the authority of council to an appointed officer identified in the by-law by position with respect to certain applications under the *Planning Act* and ;

**AND WHEREAS** section 23.1 of the *Municipal Act*, 2001, S.O. 2001, c. 25, as amended, provides that councils are authorized to delegate their powers and duties under the *Municipal Act* or any other Act to a person or body subject to any restrictions set out;

**AND WHEREAS** Council has deemed it advisable to delegate by by-law authority to the City Planner with respect to certain applications under the *Planning Act*;

**NOW THEREFORE** the Council of the Corporation of the City of Windsor enacts as follows:

#### 1. Definitions:

“the Act” shall mean the *Planning Act*, R.S.O. 1990, c. P.13, as amended

“applicant” shall Mean:

- (i) a person or public body requesting an amendment to the Official Plan of the City of Windsor under section 22 of the Act;
- (ii) a person or public body requesting an amendment to Zoning By-law 8600 under section 34 of the Act;
- (iii) an owner of land requesting draft approval of a plan of subdivision/condominium or condominium conversion under section 51 of the Act, or an extension of a draft approval of a

plan of subdivision/condominium or condominium conversion;

- (iv) a person or public body requesting approval of Site Plan Control plans or drawings under section 41 of the Act;
- (v) an owner of land requesting an exemption from part lot control under section 50(7) of the Act; or
- (vi) an owner of property requesting a permit to alter property under section 42 of the Ontario Heritage Act.

“application shall mean:

- (i) a request to amend the Official Plan of the City of Windsor under section 22 of the Act;
- (ii) a request to amend the Zoning By-law 8600 under section 34 of the Act;
- (iii) a request for a draft approval of a plan of subdivision/ condominium or condominium conversion under section 51 of the Act, or an extension of a draft approval of a plan of subdivision/condominium or condominium conversion;
- (iv) A request for approval of Site Plan Control plans or drawings under section 41 of the Act;
- (v) a request to exempt land from part lot control under section 50(7) of the Act; or
- (vi) a request for a permit to alter property under section 42 of the Ontario Heritage Act.

“Council” shall mean the Council of The Corporation of the City of Windsor.

“City Planner” shall mean the person who holds the position of City Planner for The Corporation of the City of Windsor.

“Manager of Planning Policy” shall mean the person who holds the position of Manager of Planning Policy for The Corporation of the City of Windsor.

“Manager of Urban Design” shall mean the person who holds the position of Manager of Urban Design for The Corporation of the City of Windsor.

**2. Delegation to City Planner**

- 2.1 Council hereby delegates to the City Planner the authority to approve and process the applications set out in Schedule “A” attached hereto and forming part of this by-law.

**3. Absent City Planner**

- 3.1 When the City Planner is absent or his office is vacant, then anyone of the Manager of Planning Policy, or the Manager of Urban Design shall act in the place and stead of the City Planner under this by-law.

**4. Gender**

- 4.1 In this By-law the words “he”, “him” or “his” shall have the same meaning as the words “she”, “her” or “hers”.

**5. General**

- 5.1 For the purposes of subsection 23.2(4) of the Municipal Act, it is the opinion of the Council that any legislative powers delegated pursuant to this by-law are of a minor nature having regard to the number of people, the size of geographic area and the time period affected by the exercise of each such power.

6. By-law 167-2010 and By-law ~~“28-2010”~~ are hereby repealed.  
“28-2013”

**7. Effective Date**

This by-law shall come into force and take effect on the date of the final passing thereof.



EDDIE FRANCIS, MAYOR

CLERK

First Reading - August 26, 2013  
Second Reading - August 26, 2013  
Third Reading - August 26, 2013

BY-LAW NUMBER 86-2022

A BY-LAW TO FURTHER AMEND BY-LAW NUMBER 8600 CITED AS THE "CITY OF WINDSOR ZONING BY-LAW"

Passed the 13<sup>th</sup> day of June, 2022.

**WHEREAS** it is deemed expedient to further amend By-law Number 8600 of the Council of The Corporation of the City of Windsor, cited as the "City of Windsor Zoning By-law" passed the 31st day of March, 1986, as heretofore amended:

**THEREFORE** the Council of the Corporation of the City of Windsor enacts as follows:

1. By-law Number 8600 is further amended by changing the District Maps or parts thereof of the said by-law and made part thereof, so that the zoning district symbol of the lands therein and hereinafter described shall be changed from that shown in Column 5 hereof to that shown in Column 6 hereof:

1. Item Number	2. Zoning District Map Part	3. Lands Affected	4. Official Plan Amendment Number	5. Zoning Symbol	6. New Zoning Symbol
1	7	Block B, Registered Plan 1475  (known municipally as 2095 Seneca Street; Roll No. 020-240-04500, situated on the south side of Seneca Street, between Durham Place and Turner Road)	N/A	HRD3.3  S.20(1)363	RD3.3  S.20(1)363

DREW DILKENS, MAYOR

CITY CLERK

First Reading - June 13, 2022  
Second Reading - June 13, 2022  
Third Reading - June 13, 2022

BY-LAW NUMBER 87-2022

A BY-LAW TO FURTHER AMEND BY-LAW NUMBER 8600 CITED AS THE  
"CITY OF WINDSOR ZONING BY-LAW"

Passed the 13<sup>th</sup> day of June, 2022.

**WHEREAS** it is deemed expedient to further amend By-law Number 8600 of the Council of The Corporation of the City of Windsor, cited as the "City of Windsor Zoning By-law" passed the 31st day of March, 1986, as heretofore amended:

**THEREFORE** the Council of the Corporation of the City of Windsor enacts as follows:

2. That subsection 1 of Section 20, of said by-law, is amended by adding the following paragraph:

**444. SOUTH SIDE OF WYANDOTTE STREET EAST, BETWEEN  
WATSON AVENUE AND ISACK DRIVE**

For the lands Part of lots 72 to 78, part of lots 106 & 107, part of 30' Lane, part of Parkhill Gate, RP 1627, more particularly described as Part 3, 12R-13644, the provisions of S 20 (1) 102 shall not apply, and the following provisions shall apply:

- a) Building Height – Maximum- 21.5m
- b) Lot Coverage – Maximum- 40%
- c) A minimum separation of 12 metres shall be maintained between a multiple dwelling and an RD1.1 District. (ZDM 14; ZNG/6499)"

3. The said by-law is further amended by changing the Zoning District Maps or parts thereof referred to in Section 1, of said by-law and made part thereof, so that the lands described in Column 3 are delineated by a broken line and further identified by the zoning symbol shown in Column 5:

1. Item Number	2. Zoning District Map Part	3. Lands Affected	4. Official Plan Amendment Number	5. Zoning Symbol
1	14	Part of lots 72 to 78, part of Lots 106 & 107, part of 30' Lane, part of Parkhill Gate, RP 1627, more particularly described as Part 3, 12R-13644	N/A	S.20(1)444

DREW DILKENS, MAYOR

CITY CLERK

First Reading - June 13, 2022  
Second Reading - June 13, 2022  
Third Reading - June 13, 2022

BY-LAW NUMBER 88-2022

A BY-LAW TO FURTHER AMEND BY-LAW NUMBER 8600 CITED AS THE  
"CITY OF WINDSOR ZONING BY-LAW"

Passed the 13<sup>th</sup> day of June, 2022.

**WHEREAS** it is deemed expedient to further amend By-law Number 8600 of the Council of The Corporation of the City of Windsor, cited as the "City of Windsor Zoning By-law" passed the 31st day of March, 1986, as heretofore amended:

**THEREFORE** the Council of the Corporation of the City of Windsor enacts as follows:

1. By-law Number 8600 is further amended by changing the District Maps or parts thereof of the said by-law and made part thereof, so that the zoning district symbol of the lands therein and hereinafter described shall be changed from that shown in Column 5 hereof to that shown in Column 6 hereof:

1. Item Number	2. Zoning District Map Part	3. Lands Affected	4. Official Plan Amendment Number	5. Zoning Symbol	6. New Zoning Symbol
1	15	Lots 125 to 143, Registered Plan 1023, Part of Closed Alley  (Roll No. 080-190-02400, 080-190- 02500, 080-190-02520, 080- 190-02550-0000, 080-190- 02600, 080-190-02700, 080- 190-02800, 080-190-02900, 080-190-03000, 080-190- 03050, 080-190-03100, 080- 190-03150, and 080-190- 03200; situated on the east side of Northway Avenue, between Norfolk Street and Pulford Street)	n/a	HRD1.2	RD1.2

DREW DILKENS, MAYOR

CITY CLERK

First Reading - June 13, 2022  
Second Reading - June 13, 2022  
Third Reading - June 13, 2022



BY-LAW NUMBER 89-2022

A BY-LAW TO FURTHER AMEND BY-LAW NUMBER 85-18 CITED AS THE "TOWNSHIP OF SANDWICH SOUTH COMPREHENSIVE ZONING BY-LAW"

Passed the 13<sup>th</sup> day of June, 2022.

**WHEREAS** it is deemed expedient to further amend By-law Number 85-18 of the Council of The Corporation of the City of Windsor, cited as the "Township of Sandwich South Comprehensive Zoning By-law" passed the 21st day of May, 1985, as heretofore amended:

**THEREFORE** the Council of the Corporation of the City of Windsor enacts as follows:

1. By-law Number 85-18 is further amended by changing the Zoning Maps or parts thereof of the said by-law and made part thereof, so that the zoning symbol of the lands therein and hereinafter described shall be changed from that shown in Column 5 hereof to that shown in Column 6 hereof:

1. Item Number	2. Zoning Map Part	3. Lands Affected	4. Official Plan Amendment Number	5. Zoning Symbol	6. New Zoning Symbol
1	T12	Lot 16 on the south side of Baseline Road, Plan 1519, which is a subdivision of part of farm lot 16, Concession 7, Township of Sandwich South, County of Essex (situated between 7th and 8th Concession)	n/a	HRH-6	RH-6

2. That Schedule 'A', attached hereto, is hereby declared to form part of this amending by-law.

DREW DILKENS, MAYOR

CITY CLERK

First Reading - June 13, 2022  
Second Reading - June 13, 2022  
Third Reading - June 13, 2022



BY-LAW NUMBER 90-2022

A BY-LAW TO FURTHER AMEND BY-LAW NUMBER 8600 CITED AS THE  
"CITY OF WINDSOR ZONING BY-LAW"

Passed the 13<sup>th</sup> day of June, 2022.

**WHEREAS** it is deemed expedient to further amend By-law Number 8600 of the Council of The Corporation of the City of Windsor, cited as the "City of Windsor Zoning By-law" passed the 31st day of March, 1986, as heretofore amended:

**THEREFORE** the Council of the Corporation of the City of Windsor enacts as follows:

1. That subsection 1 of Section 20, of said by-law, is amended by adding the following paragraph:

441. **SOUTH SIDE OF ALEXANDRINE STREET BETWEEN REMINGTON AVENUE AND LILLIAN AVENUE**

For the lands comprising Lots 94 & 95 and Part Closed Alley, Registered Plan 1106 (PIN 01339-0396), a *Townhome Dwelling* shall be an additional permitted use and shall be subject to the following additional provisions:

- a) Lot Width – minimum 20.0 m
- b) Lot Area – per dwelling unit – minimum 191.0 m<sup>2</sup>
- c) Lot Coverage – maximum 45.0%
- d) Main Building Height – maximum 10.0 m
- e) Front Yard Depth – minimum 6.0 m
- f) Rear Yard Depth – minimum 7.50 m
- g) Side Yard Width – minimum 1.20 m

[ZDM 8; ZNG/6634]

2. The said by-law is further amended by changing the Zoning District Maps or parts thereof referred to in Section 1, of said by-law and made part thereof, so that the lands described in Column 3 are delineated by a broken line and further identified by the zoning symbol shown in Column 5:

1. Item Number	2. Zoning District Map Part	3. Lands Affected	4. Official Plan Amendment Number	5. Zoning Symbol
1	8	Lots 94 & 95 and Part Closed Alley, Registered Plan 1106  (known municipally as 659 Alexandrine Street; Roll No. 070- 030-16000; PIN 01339-0396, south side of Alexandrine Street between Remington Avenue and Lillian Avenue	N/A	S.20(1)441

DREW DILKENS, MAYOR

CITY CLERK

First Reading - June 13, 2022  
Second Reading - June 13, 2022  
Third Reading - June 13, 2022

BY-LAW NUMBER 91-2022

A BY-LAW TO FURTHER AMEND BY-LAW NUMBER 8600 CITED AS THE  
"CITY OF WINDSOR ZONING BY-LAW"

Passed the 13<sup>th</sup> day of June, 2022.

**WHEREAS** it is deemed expedient to further amend By-law Number 8600 of the Council of The Corporation of the City of Windsor, cited as the "City of Windsor Zoning By-law" passed the 31st day of March, 1986, as heretofore amended:

**THEREFORE** the Council of the Corporation of the City of Windsor enacts as follows:

1. That subsection 1 of Section 20, of said by-law, is amended by adding the following paragraph:

**422. SOUTH SIDE OF PITT STREET WEST AND WEST SIDE OF JANETTE AVE**

For the lands comprising Registered Plan 274 E PT Lot 8, Corner, a *multiple dwelling* with a maximum of six *dwelling units* shall be an additional permitted use, and that for the additional permitted use no additional *parking spaces* are required and the lot width, lot area, lot coverage, main building height, front yard depth, rear yard depth, and side yard width shall be as existing on the date this amendment comes into force.

[ZDM 3; ZNG/6500]

2. The said by-law is further amended by changing the Zoning District Map identified in Column 2 so that the lands described in Column 3 are delineated by a broken line and further identified by the zoning symbol shown in Column 5:



1. Item Number	2. Zoning District Map	3.  Lands Affected	4. Official Plan Amendment Number	5. Zoning Symbol
1	3	Registered Plan 274 E PT Lot 8, Corner (163 Janette Ave; Roll No. 040-110-03800-0000; PIN 0119-60162; South of Pitt St W, West of Janette Ave)	- -	S.20(1)422

DREW DILKENS, MAYOR

CITY CLERK

First Reading - June 13, 2022  
 Second Reading - June 13, 2022  
 Third Reading - June 13, 2022

BY-LAW NUMBER 92-2022

A BY-LAW TO ADOPT AMENDMENT NO. 143  
TO THE OFFICIAL PLAN OF THE CITY OF  
WINDSOR

Passed the 13<sup>th</sup> day of June, 2022.

**WHEREAS** pursuant to the provisions of Section 17(1) of the *Planning Act*, R.S.O. 1990, c. P.13 as amended, the Minister of Municipal Affairs and Housing (Minister) is the approval authority in respect of the approval of a plan as an official plan.

**AND WHEREAS** Section 17(9) of the said *Planning Act* provides that the Minister may by order exempt a proposed official plan amendment from his approval under Section 17(1) of the said Act.

**AND WHEREAS** pursuant to the provisions of Ontario Regulation 525/97 all amendments to the official plan of the City of Windsor commenced after January 19, 1998 are exempt from the approval of the said Minister.

**THEREFORE** the Council of the Corporation of the City of Windsor in accordance with the provisions of the said *Planning Act* hereby enacts as follows:

1. That Amendment No. **143** to the Official Plan of the City of Windsor, attached hereto, is hereby adopted.

DREW DILKENS, MAYOR

CITY CLERK

First Reading - June 13, 2022  
Second Reading - June 13, 2022  
Third Reading - June 13, 2022

**AMENDMENT NO. 143**

**TO THE**

**OFFICIAL PLAN**

**CITY OF WINDSOR**

Part D (Details of the Amendment) of the following text, and attached map of the City of Windsor Official Plan constitute Amendment No. 143.

Also included, but not constituting part of the Amendment, are explanations of Purpose, Location, Background and Implementation of the Amendment, Appendix I (Results of Public Involvement)

**A. PURPOSE:**

The purpose of this recommended amendment is to allow residential use on the subject land by converting the employment land to a non-employment designation that would permit residential use and allow the development of a multi-storey, multi-unit dwelling on the subject land.

**B. LOCATION:**

The amendment applies to the land generally described as located on the north side of Tecumseh Road E., between Banwell Road and the City's east limit, described as Part of Lot 146, Concession 1, (PIN 010540374) and municipally known as 11646 Tecumseh Road East.

**Ward: 7**

**Planning District: Forest Glade**

**ZDM: 15**

**C. BACKGROUND:**

The subject land is designated Industrial in the land use schedule of the Official Plan Vol. 1. *Residential use* is not listed as a permitted use or permitted ancillary use in the subject land use designation.

The applicant proposes to change the land use designation of the subject land from Industrial to Residential to allow *Residential use* as an additional permitted use on the land. A 7-storey residential building (Condo development) with 90 residential units is proposed on the subject land. However, after a detailed planning review of the supporting documents and relevant policies, it was determined that a Mixed Use designation would be more appropriate for the subject land in the subject area.

The recommended amendment seeks to change the land use designation from Industrial to Mixed Use designation to permit *residential use* as additional permitted use on the subject land. The recommended amendment will allow the development of commercial use(s) at street level, while accommodating residential units above commercial uses or, alternatively, above grade at the rear of the commercial use(s).

**D. DETAILS OF THE AMENDMENT:**

THAT the City of Windsor Official Plan Volume I – Primary Plan **BE AMENDED** by changing the land use designation of the land located on the north side of Tecumseh Road E., between Banwell Road and the City's east limit, described as Part of Lot 146, Concession 1, (PIN 010540374) and municipally known as 11646 Tecumseh Road E. from INDUSTRIAL to MIXED USE.

**E. IMPLEMENTATION:**

- i. Amend Schedule D: Land Use, in Volume 1: The Primary Plan to add OPA #143.

- ii. This amendment shall be implemented through amendment to the Zoning By-law 8600 as recommended in Report Number S xxx/2021 (Z-005/21; ZNG-6323).
- iii. Site Plan Control shall be an additional implementation tool for this Official Plan Amendment (OPA #143).

## **APPENDIX I**

The following are the results of public notification of the amendments and the outcome of public meetings. Comments relate to the Official Plan amendment and the associated rezoning.

A public meeting of the Development & Heritage Standing Committee (DHSC), the statutory meeting, was held on March 7, 2022. Below is an extract from the minutes of the meeting.

### **7. PLANNING ACT MATTERS**

#### **7.1 Z-005/21 [ZNG/6323] & OPA 143 [OPA/6324] – Maple Leaf Homes 11676 Tecumseh Rd E – Rezoning & Official Plan Amendment Ward 7**

Justina Nwaesei (author), Planner III – Subdivisions

Michael Cooke provides the presentation on behalf of Justina Nwaesei.

Ms Tracey Pillon-Abbs briefly reviews the proposed development and changes made to make the proposed development compliant with the zoning.

Moved by: Councillor Gill

Seconded by: Councillor Holt

Decision Number: **DHSC 371**

### **RECOMMENDATIONS**

- I. THAT the City of Windsor Official Plan Volume I – Primary Plan **BE AMENDED** by changing the land use designation of the land located on the north side of Tecumseh Road E., between Banwell Road and the City's east limit, described as Part of Lot 146, Concession 1, (PIN 010540374) and municipally known as 11646 Tecumseh Road E. from Industrial to Mixed Use;



- II. THAT Zoning By-law 8600 **BE AMENDED** by adding the following zoning district to Section 16:

### **16.10 COMMERCIAL DISTRICT 3.10 (CD3.10)**

#### **16.10.1 PERMITTED USES**

<i>Business Office</i>	<i>Personal Service Shop</i>
<i>Child Care Centre</i>	<i>Place of Entertainment and Recreation</i>
<i>Commercial School</i>	<i>Place of Worship</i>
<i>Food Outlet - Take-Out</i>	<i>Professional Studio</i>
<i>Hotel</i>	<i>Public Hall</i>
<i>Medical Office</i>	<i>Repair Shop - Light</i>
<i>Medical Appliance Facility</i>	<i>Restaurant</i>
<i>Micro-Brewery</i>	<i>Retail Store</i>

*9 or more dwelling units in a Combined Use Building with any of the above uses*

*Multiple Dwelling with 9 or more dwelling units*

*Residential Care Facility*

Any use accessory to any of the above uses. An *Outdoor Storage Yard* is prohibited.

#### **16.10.5 PROVISIONS**

- .1 Lot Frontage – minimum 18.0 m
- .2 Lot Area – minimum
  - For a *building* containing only non-residential uses 400.0 m<sup>2</sup>
  - For each *dwelling unit* 85.0 m<sup>2</sup>
- .4 Building Height – maximum 20.0 m
- .8 Landscaped Open Space Yard – minimum 30.0% of *lot area*
- .15 For a *Combined Use Building*, all *dwelling units*, not including entrances thereto, shall be located above the non-residential uses.
- .16 A *Multiple Dwelling* shall be located above grade, at the rear of non-residential use.
- .17 Exposed flat concrete block walls or exposed flat concrete walls, whether painted or unpainted, are prohibited.
- .20 Building Setback – minimum
  - a) From an *exterior lot line* abutting Tecumseh Road East, for that part of the building having a *building height* of 10.0 m or less 0.0 m

- b) From an *exterior lot line* abutting Tecumseh Road East, for that part of the building having a *building height* of more than 10.0 m: 6.0 m
  - c) From an *interior lot line* where a habitable room window faces the *interior lot line* 6.0 m
  - d) From an *interior lot line* where a habitable room window does not face the *interior lot line* 3.0 m
- . 90 *Parking space* is prohibited in the *front yard* and in any *side yard* within 6m of the *exterior lot line*.

III. THAT an amendment to the Zoning By-law 8600 **BE APPROVED** to change the zoning of the land located on the north side of Tecumseh Road E., between Banwell Road and the City's east limit, described as Part of Lot 146, Concession 1, (PIN 010540374) and municipally known as 11646 Tecumseh Road E., from MD1.2 to CD3.10 (as shown in Recommendation II above), subject to the following site specific provision:

**“438 NORTH SIDE OF TECUMSEH ROAD E., BETWEEN BANWELL ROAD AND THE CITY LIMIT TO THE EAST**

For the land comprising Part of Lot 146, Concession 1, (PIN 010540374), a 6-storey *Combined Use Building* containing a maximum of 71 *dwelling units* plus one or more non-residential uses listed in section 16.10.1 of by-law 8600 having 190m<sup>2</sup> minimum gross floor area with minimum parking requirement of 8 spaces as in by-law 8600, shall be permitted subject to the following additional regulations:

- a) Section 16.10.5.15 of by-law 8600 shall not apply;
- b) Non-residential use shall be located at street level along the south wall of the building, fronting Tecumseh Road East;
- c) *Dwelling units*, indoor amenity areas and other indoor accessory uses to dwelling units, within the ground floor area of a Combined Use Building, shall be located above grade and be placed on the north of the non-residential units;
- d) The floor areas occupied by indoor amenity areas and other indoor accessory uses to the dwelling units shall be excluded from the permitted 190 m<sup>2</sup> minimum gross floor area of non-residential use;
- e) A minimum separation of 30.0 m shall be maintained between the railway right-of-way and a residential, commercial, institutional or recreational use;

- f) An earth berm having a minimum height of 2.50 m and slopes of 2.5 to 1 or greater, shall be constructed continuously adjacent to the common boundary line between the lot and the railway right of way and maintained in good practice; and
- g) A chainlink fence having a minimum height of 1.830 m shall be erected continuously along the common boundary line between the lot and the railway right-of-way.  
[ZDM 15; ZNG/6323]

IV. THAT the Site Plan Approval Officer **BE DIRECTED** to incorporate the following requirements and other requirements found in Appendix D of this Report, in the Site Plan Approval process and the Site Plan Agreement for the proposed development on the subject land:

- a) Noise mitigation measures as recommended in the Noise Study, including warning clauses for rail and road traffic impacts;
- b) Safety measures per section 7.2.8.8 (d), OP Vol. 1;
- c) Redundant Curb Cuts, Video inspections, and Existing sewers and connections;
- d) Preservation of some existing trees per Landscape Architect's comment in Appendix D of this report;
- e) Easements and/or agreements required for the provision of gas services for this project, in a form satisfactory to Enbridge;
- f) Enbridge Gas minimum separation requirements;
- g) Adequate clearance from existing ENWIN's pole lines and power lines; and
- h) Canada Post multi-unit policy;
- i) SAR Snake mitigation measures as in the attached Appendix F to this report.

Motion CARRIED UNANIMOUSLY

Report Number: S 2/2022 & AI 4/2022

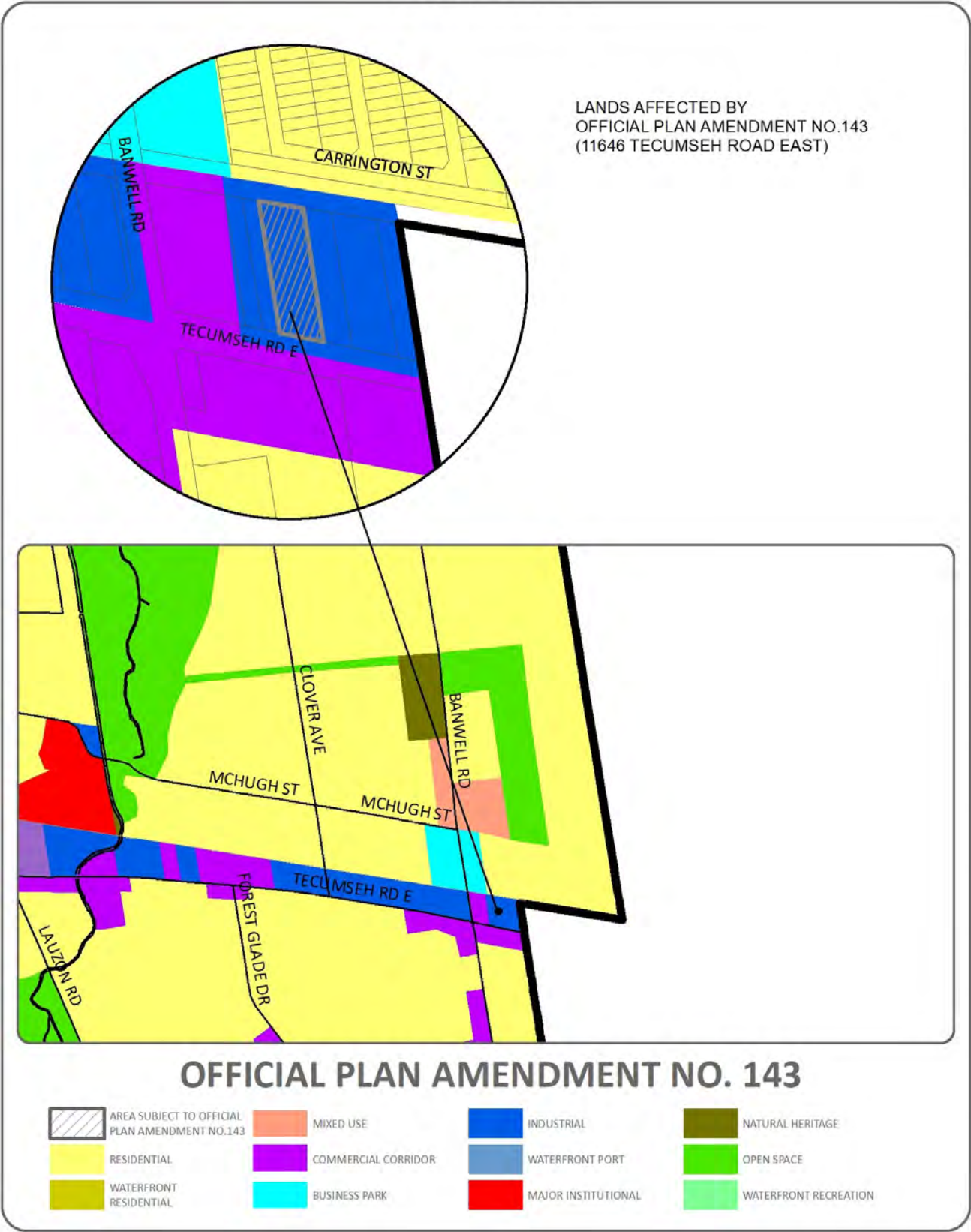
Clerk's File: ZB/14064 & ZO/14063

Following the March 7, 2022 DHSC meeting, another public meeting (Council meeting) was held on March 21, 2022 as noted below.

#### **COUNCIL MEETING: March 21, 2022**

A meeting of City Council was held on March 21, 2022, at which time the subject Official Plan Amendment application was considered along with the accompanying Zoning By-law Amendment application (File No. Z-005/21; ZNG/6232). The recommended OPA #143 was adopted by CR115/2022, and the recommended amendment to the zoning by-law was passed by the same CR115/2022.

# SCHEDULE D



BY-LAW NUMBER 93-2022

A BY-LAW TO FURTHER AMEND BY-LAW NUMBER 8600 CITED AS THE "CITY OF WINDSOR ZONING BY-LAW"

Passed the 13<sup>th</sup> day of June, 2022.

**WHEREAS** it is deemed expedient to further amend By-law Number 8600 of the Council of The Corporation of the City of Windsor, cited as the "City of Windsor Zoning By-law" passed the 31st day of March, 1986, as heretofore amended:

**THEREFORE** the Council of the Corporation of the City of Windsor enacts as follows:

1. THAT Zoning By-law 8600 **BE AMMENDED** by adding the following zoning district to Section 16:

**16.10 COMMERCIAL DISTRICT 3.10 (CD3.10)**

**16.10.1 PERMITTED USES**

- |                                   |  |
|-----------------------------------|--|
| <i>Business Office</i>            | <i>Personal Service Shop</i>                 |
| <i>Child Care Centre</i>          | <i>Place of Entertainment and Recreation</i> |
| <i>Commercial School</i>          | <i>Place of Worship</i>                      |
| <i>Food Outlet - Take-Out</i>     | <i>Professional Studio</i>                   |
| <i>Hotel</i>                      | <i>Public Hall</i>                           |
| <i>Medical Office</i>             | <i>Repair Shop - Light</i>                   |
| <i>Medical Appliance Facility</i> | <i>Restaurant</i>                            |
| <i>Micro-Brewery</i>              | <i>Retail Store</i>                          |
- 9 or more dwelling units in a Combined Use Building with any of the above uses*
- Multiple Dwelling with 9 or more dwelling units*
- Residential Care Facility*
- Any use accessory to any of the above uses. An *Outdoor Storage Yard* is prohibited.

**16.10.5 PROVISIONS**

- |     |  |                      |
|-----|--|----------------------|
| .1  | Lot Frontage – minimum   | 18.0 m               |
| .2  | Lot Area – minimum   |                      |
|     | For a <i>building</i> containing only non-residential uses   | 400.0 m <sup>2</sup> |
|     | For each <i>dwelling unit</i>  | 85.0 m <sup>2</sup>  |
| .4  | Building Height – maximum  | 20.0 m               |
| .8  | Landscaped Open Space Yard – minimum <i>lot area</i>   | 30.0% of             |
| .15 | For a <i>Combined Use Building</i> , all <i>dwelling units</i> , not including entrances thereto, shall be located above the non-residential uses. |                      |
| .16 | A <i>Multiple Dwelling</i> shall be located above grade, at the rear of non-residential use.   |                      |
| .17 | Exposed flat concrete block walls or exposed flat concrete walls, whether painted or unpainted, are prohibited.                                    |                      |
| .20 | Building Setback – minimum   |                      |



- a) From an *exterior lot line* abutting Tecumseh Road East, for that part of the building having a *building height* of 10.0 m or less 0.0 m
- b) From an *exterior lot line* abutting Tecumseh Road East, for that part of the building having a *building height* of more than 10.0 m: 6.0 m
- c) From an *interior lot line* where a habitable room window faces the *interior lot line* 6.0 m
- d) From an *interior lot line* where a habitable room window does not face the *interior lot line* 3.0 m

.90 *Parking spaces* shall be prohibited in the *front yard* and in any *side yard* within 6m of the *exterior lot line*.

- 2. By-law Number 8600 is further amended by changing the Zoning District Maps or parts thereof referred to in Section 1, of the by-law and made part thereof, so that the zoning district symbol of the lands described in Column 3 shall be changed from that shown in Column 5 to that shown in Column 6:

1. Item Number	2. Zoning District Map Part	3. Lands Affected	4. Official Plan Amendment Number	5. Zoning Symbol	6. New Zoning Symbol
1	15	Part of Lot 146, Concession 1, (PIN 010540374), located on the north side of Tecumseh Road E., between Banwell Road and the City's east limit.	143	MD1.2	CD3.10

- 3. That subsection 1 of Section 20, of said by-law, is amended by adding the following paragraph:

**“438 NORTH SIDE OF TECUMSEH ROAD E., BETWEEN BANWELL ROAD AND THE CITY LIMIT TO THE EAST**

For the land comprising Part of Lot 146, Concession 1, (PIN 010540374), a 6-storey *Combined Use Building* containing a maximum of 71 *dwelling units* plus one or more non-residential uses listed in section 16.10.1 of by-law 8600 having 190m<sup>2</sup> minimum gross floor area with minimum parking requirement of 8 spaces as in by-law 8600, shall be permitted subject to the following additional regulations:

- a) Section 16.10.5.15 of by-law 8600 shall not apply;
- b) Non-residential use shall be located at street level along the south wall of the building, fronting Tecumseh Road East;
- c) *Dwelling units*, indoor amenity areas and other indoor accessory uses to dwelling units, within the ground floor area of a Combined Use Building, shall be located above grade and be placed on the north of the non-residential units;
- d) The floor areas occupied by indoor amenity areas and other indoor accessory uses to the dwelling units shall be excluded from the permitted 190 m<sup>2</sup> minimum gross floor area of non-

- residential use;
  - e) A minimum separation of 30.0 m shall be maintained between the railway right-of-way and a residential, commercial, institutional or recreational use;
  - f) An earth berm having a minimum height of 2.50 m and slopes of 2.5 to 1 or greater, shall be constructed continuously adjacent to the common boundary line between the lot and the railway right of way and maintained in good practice; and
  - g) A chainlink fence having a minimum height of 1.830 m shall be erected continuously along the common boundary line between the lot and the railway right-of-way.
- [ZDM 15; ZNG/6323]

4. The said by-law is further amended by changing the Zoning District Maps or parts thereof referred to in Section 1, of said by-law and made part thereof, so that the lands described in Column 3 are delineated by a broken line and further identified by the zoning symbol shown in Column 5:

1. Item Number	2. Zoning District Map Part	3. Lands Affected	4. Official Plan Amendment Number	5. Zoning Symbol
1	15	Part of Lot 146, Concession 1, (PIN 010540374), located on the north side of Tecumseh Road E., between Banwell Road and the City's east limit.	143	S.20(1)438

DREW DILKENS, MAYOR

CITY CLERK

First Reading - June 13, 2022  
 Second Reading - June 13, 2022  
 Third Reading - June 13, 2022

## BY-LAW NUMBER 94-2022

A BY-LAW TO FURTHER AMEND BY-LAW NUMBER 8600 CITED AS THE  
"CITY OF WINDSOR ZONING BY-LAW"

Passed the 13<sup>th</sup> day of June, 2022.

**WHEREAS** it is deemed expedient to further amend By-law Number 8600 of the Council of The Corporation of the City of Windsor, cited as the "City of Windsor Zoning By-law" passed the 31st day of March, 1986, as heretofore amended:

**THEREFORE** the Council of the Corporation of the City of Windsor enacts as follows:

1. That subsection 1 of Section 20, of said by-law, is amended by adding the following clause:

**447. EAST SIDE OF WINDERMERE ROAD, SOUTH OF WYANDOTTE STREET EAST**

For the lands comprising of Lot 3, Registered Plan 502 (Roll No. 020-070-06600; PIN 01136-0246), a permanent patio (deck) exclusive to the *Restaurant* located on the property to the north, known municipally as 1731-1737 Wyandotte Street East and described as Lot 1, Registered Plan 502 (Roll No. 020-070-06900; PIN 01136-0386), shall be an additional permitted use and the following additional provisions shall apply:

- a) Fence with a height of 1.0 m shall be installed along the east *lot line* and the segment of the north *lot line* which bounds the *rear yard*, save and except a 1.5 m wide opening to provide pedestrian access to the permanent patio.
- b) *Landscaped open space yard* with a minimum depth of 1.2 m shall be installed along the east *lot line* and the segment of the north *lot line* which bounds the *rear yard*, save and except a 1.5 m wide opening to provide pedestrian access to the permanent patio.
- c) Screening fence with a minimum height of 1.8 m shall be maintained along the segment of the south *lot line* which bounds the *rear yard*.
- d) Notwithstanding Table 24.20.5.1 and the registered Site Plan Control Agreement, dated May 16, 1996, for file number SPC-015/96, no *parking spaces* shall be required for the existing legal non-conforming *Business Office* use.

[ZDM 6; ZNG/6670]

2        The said by-law is further amended by changing the Zoning District Maps or parts thereof referred to in Section 1, of said by-law and made part thereof, so that the lands described in Column 3 are delineated by a broken line and further identified by the zoning symbol shown in Column 5:

1. Item Number	2. Zoning District Map Part	3. Lands Affected	4. Official Plan Amendment Number	5. Zoning Symbol
1	6	Lot 3, Registered Plan 502  (known municipally as 642 Windermere Road; Roll No. 020- 070-06600; PIN 01136-0246; situated on the east side of Windermere, south of Wyandotte Street East)		S.20(1)447

DREW DILKENS, MAYOR

CITY CLERK

First Reading     - June 13, 2022  
Second Reading - June 13, 2022  
Third Reading    - June 13, 2022

## BY-LAW NUMBER 95-2022

A BY-LAW TO PROVIDE THAT PART-LOT CONTROL SHALL NOT APPLY TO CERTAIN LAND THAT IS WITHIN REGISTERED PLAN 1196 IN THE CITY OF WINDSOR

Passed the 13<sup>th</sup> day of June, 2022.

**WHEREAS** the *Planning Act, R.S.O. 1990, Chapter P.13*, as amended, provides that part-lot control shall apply where land is within a plan of subdivision registered before or after the coming into force of the Act.

**AND WHEREAS** subsection 7 of Section 50 of the said *Planning Act* provides that the council of a municipality may by by-law provide that part-lot control does not apply to land that is within such registered plan or plans of subdivision or parts thereof as is or are designated in the by-law and where the by-law is passed part-lot control as described in subsection 5 of Section 50, ceases to apply to such land.

**AND WHEREAS** it is deemed desirable that the provisions of the said subsection 5 of Section 50 of the *Planning Act* shall not apply to certain land that is within **REGISTERED PLAN 1196** in the City of Windsor.

**THEREFORE** the Council of the Corporation of the City of Windsor enacts as follows:

1. That the provisions of subsection 5 of Section 50 of the *Planning Act, R.S.O. 1990, Chapter P.13*, do not apply to that part of the land that is within **REGISTERED PLAN 1196**, as described in Schedule "A" attached hereto and forming part of this by-law.
2. This by-law is repealed on June 13, 2024.
3. This by-law shall come into force and take effect after the final passing thereof.

DREW DILKENS, MAYOR

CITY CLERK

First Reading - June 13, 2022  
Second Reading - June 13, 2022  
Third Reading - June 13, 2022



## SCHEDULE "A"

LT 61 PL 1196 SANDWICH WEST; CITY OF WINDSOR  
**PIN 01582-0124 (LT)**

LT 62 PL 1196 SANDWICH WEST; LT 63 PL 1196 SANDWICH WEST; LT  
64 PL 1196 SANDWICH WEST; CITY OF WINDSOR  
**PIN 01582-0117 (LT)**

LT 49 PL 1196 SANDWICH WEST; LT 50 PL 1196 SANDWICH WEST; LT  
51 PL 1196 SANDWICH WEST; CITY OF WINDSOR  
**PIN 01582-0123 (LT)**

LT 52 PL 1196 SANDWICH WEST; LT 53 PL 1196 SANDWICH WEST; LT  
54 PL 1196 SANDWICH WEST; CITY OF WINDSOR  
**PIN 01582-0122 (LT)**

LT 55 PL 1196 SANDWICH WEST; CITY OF WINDSOR  
**PIN 01582-0121 (LT)**

LT 112 PL 1196 SANDWICH WEST; LT 113 PL 1196 SANDWICH WEST; LT  
114 PL 1196 SANDWICH WEST; CITY OF WINDSOR  
**PIN 01582-0111 (LT)**

LT 119 PL 1196 SANDWICH WEST; LT 120 PL 1196 SANDWICH WEST;  
CITY OF WINDSOR  
**PIN 01582-0114 (LT)**

LT 121 PL 1196 SANDWICH WEST; LT 122 PL 1196 SANDWICH WEST; LT  
123 PL 1196 SANDWICH WEST; CITY OF WINDSOR  
**PIN 01582-0115 (LT)**

PART OF ALLEY PLAN 1196; DESIGNATED AS PARTS 5, 6 AND 10 ON  
PLAN 12R28690; CITY OF WINDSOR  
**PART OF PIN 01582-0662 (LT)**

PART OF ALLEY PLAN 1196 CLOSED BY BY-LAW CE1037843;  
DESIGNATED AS PARTS 4, 5 AND 9, ON PLAN 12R28689; CITY OF  
WINDSOR  
**PART OF PIN 01582-0234 (LT)**

ALLEY PL 1196 SANDWICH WEST ABUTTING LTS 124 TO 128 PL 1196;  
DESIGNATED AS PART 10, 12R749; CITY OF WINDSOR  
**PIN 01582-0235 (LT)**

BY-LAW NUMBER 96-2022

A BY-LAW TO FURTHER AMEND BY-LAW NUMBER 8600 CITED AS THE "CITY OF WINDSOR ZONING BY-LAW"

Passed the 13<sup>th</sup> day of June, 2022.

**WHEREAS** it is deemed expedient to further amend By-law Number 8600 of the Council of The Corporation of the City of Windsor, cited as the "City of Windsor Zoning By-law" passed the 31st day of March, 1986, as heretofore amended:

**THEREFORE** the Council of the Corporation of the City of Windsor enacts as follows:

1. By-law Number 8600 is further amended by changing the District Maps or parts thereof of the said by-law and made part thereof, so that the zoning district symbol of the lands therein and hereinafter described shall be changed from that shown in Column 5 hereof to that shown in Column 6 hereof:

1. Item Number	2. Zoning District Map Part	3. Lands Affected	4. Official Plan Amendment Number	5. Zoning Symbol	6. New Zoning Symbol
1	4,5, & 8	Lots 49 to 55 (incl.), Lots 61 to 64 (incl.), Lots 108 to 123 (incl.), Part of Alleys, Plan 1196.	N/A	HRD1.2	RD1.2

2. That Schedule 'A', attached hereto, is hereby declared to form part of this amending by-law.

DREW DILKENS, MAYOR

CITY CLERK

First Reading - June 13, 2022  
Second Reading - June 13, 2022  
Third Reading - June 13, 2022

## SCHEDULE "A"



THIS IS SCHEDULE "A" TO BY-LAW 96-2022

MAYOR \_\_\_\_\_ CLERK \_\_\_\_\_

BY-LAW NUMBER 97-2022

A BY-LAW TO CONFIRM PROCEEDINGS OF THE COUNCIL OF THE CORPORATION OF THE CITY OF WINDSOR AT ITS MEETING HELD ON THE 13<sup>th</sup> DAY OF JUNE, 2022

Passed the 13<sup>th</sup> day of June, 2022.

**WHEREAS** it is deemed expedient that the proceedings of the Council of The Corporation of the City of Windsor at this meeting be confirmed and adopted by by-law;

**THEREFORE** the Council of the Corporation of the City of Windsor enacts as follows:

1. The action of the Council of The Corporation of the City of Windsor in respect to each recommendation contained in the Report/Reports of the Committees and the local Boards and Commissions and each motion and resolution passed and other action taken by the Council of The Corporation of The City of Windsor at this special meeting is hereby adopted and confirmed as if all such proceedings were expressly in this by-law.
2. The Mayor and the proper officials of The Corporation of the City of Windsor are hereby authorized and directed to do all things necessary to give effect to the action of the Council of The Corporation of the City of Windsor referred to in the preceding section hereof.
3. The Mayor and the City Clerk are authorized and directed to execute all documents necessary in that behalf and to affix thereto the seal of The Corporation of the City of Windsor.

DREW DILKENS, MAYOR

CITY CLERK

First Reading - June 13, 2022  
Second Reading - June 13, 2022  
Third Reading - June 13, 2022



**Council Questions: SCM 162/2022**

**Subject: Summary of Outstanding Council Questions as at June 9, 2022**



## OUTSTANDING COUNCIL QUESTIONS

Just a reminder that this is quoted from the 2004 Council report:

**“overdue Council Questions (i.e., outstanding for 30 days or more) be responded to immediately.”**

### Outstanding:

2016 – 1  
2017 – 1  
2019 – 4  
2020 – 8  
2021 – 12  
2022 – 5

### 2016

### Total Outstanding: 1

COUNCIL MEMBER		QUESTION – ISSUES RAISED	
Kusmierczyk	Comm. Economic Development & Innovation	<b>CQ10-2016</b> Asks that administration report back on best practices from other cities regarding metered on-street accessible parking AND to provide feasibility and cost of implementing free metered parking for residents with Accessible Parking Permits.  ST2016 (February 22, 2016)	<b>Type of Response Required</b> -Written Report  <b>CR414/2019</b> <b>ETPS691</b> Referred back to Accessibility Committee and New City Hall Project Steering Committee for consideration. (Aug 26, 2019)

**Total Outstanding: 1**

<b>COUNCIL MEMBER</b>		<b>QUESTION – ISSUES RAISED</b>	
Kusmierczyk	Chief Administrative Officer	<b>CQ36-2017</b> Asks that Administration report back on the 100 resilient cities program which is funding the position of a Chief Resiliency Officer in 4 Canadian cities – and report back on cost and benefits of establishing the position of a Chief Resiliency Officer both in Windsor and regionally.  AS/8286 (September 5, 2017)	<b>Type of Response Required</b> -Written Report

**Total Outstanding: 4**

<b>COUNCIL MEMBER</b>		<b>QUESTION – ISSUES RAISED</b>	
McKenzie	Chief Administrative Officer	<p><b>CQ 7-2019</b></p> <p>Asks that in light of the recent announcement from FCA to eliminate the third shift at the Windsor Assembly Plant and understanding the gravity of the economic impact to our community where as many as 10,000 jobs may be lost or affected, that Administration develop a proposal for Council's review that could incent FCA to consider the possibility of introducing a new product into the Windsor Assembly Plant Facility. In doing so Administration should consider how existing City of Windsor economic development programs could be applied or amended to create a proposal that can help to protect the jobs now at risk both at the Windsor Assembly Plant and across the community generally.</p> <p>SPL/10759 (April 15, 2019)</p>	<p><b>Type of Response Required</b></p> <p>-Written Report</p>
Holt	Comm. Infrastructure Services	<p><b>CQ 12-2019</b></p> <p>Asks that Administration update Council on the Residential Parking Permit Policy, outlining how it can be improved, whether it is accomplishing its stated goals, and recommending changes in the policy to better serve residents in areas with high demand for on-street parking.</p> <p>ST2019 (May 6, 2019)</p>	<p><b>Type of Response Required</b></p> <p>-Written Report</p> <p>Referred back to Admin CR445/2021</p>
Gignac	Comm. Community Services	<p><b>CQ 16-2019</b></p> <p>Asks that Administration prepare a maintenance plan for East Bank of Little River where resident delegations identified a noxious, invasive plant (weed) issue.</p> <p>SR2019 (July 8, 2019)</p>	<p><b>Type of Response Required</b></p> <p>-Written Report</p> <p>CR655/2020 Additional info requested.</p>
Holt	Comm. Infrastructure Services	<p><b>CQ 24-2019</b></p> <p>Asks that Administration report back to council at the September 23, 2019 meeting with addendums to the By-law that identifies 2 of the 9 BIA's as "Tourist Destinations" that extend this benefit to all BIA's and take advantage of the pending wayfinding signage program equally.</p> <p>SW2019 (September 9, 2019)</p>	<p><b>Type of Response Required</b></p> <p>-Written Report</p> <p>CR565/2019 Referred to WIBIAC for comment</p>

## 2020

**Total Outstanding: 8**

COUNCIL MEMBER		QUESTION – ISSUES RAISED	
McKenzie	Comm. Human Health & Services and Comm. Economic Development & Innovation	<p><b>CQ 4-2020</b></p> <p>That Administration prepare a comparative analysis of the Affordable Housing frameworks and incentives that are in place in comparable municipalities. To the extent that the data is available the analysis should consider all forms of affordable housing and the composition of the affordable housing marketplace in the communities analyzed.</p> <p>SS2020 (February 3, 2020)</p>	<p><b>Type of Response Required</b></p> <p>-Written Report</p>
Kaschak	Comm. Economic Development & Innovation	<p><b>CQ 7-2020</b></p> <p>Asks that if Council decides to move forward with reducing the speed limit to 40 km/h on all city residential streets, that administration advise of the timelines and cost to implement this across the city.</p> <p>ST2020 (March 2, 2020)</p>	<p><b>Type of Response Required</b></p> <p>-Written Report</p> <p>CR169/2021 Referred to Admin.</p>
Francis	Chief Administrative Officer	<p><b>CQ8-2020</b></p> <p>Asks Administration to prepare a report for Council's consideration regarding new initiatives, put in place to battle Covid19, that can remain in place to increase safety measures, efficiencies, environmental measures and cost savings moving forward. These measures might include paperless agendas and digital participation in meetings of council, among others.</p> <p>MH/13786 (April 27, 2020)</p>	<p><b>Type of Response Required</b></p> <p>-Written Report</p>
Bortolin	Executive Director Human Resources	<p><b>CQ15-2020</b></p> <p>Ask that administration prepare a report outlining costs and procedures to begin collecting and reporting on data across the corporation tied to visible minority population. This data will be used to help guide future policy decisions and be shared for public consumption. Using other municipalities as comparators and also include any agencies in which the data is tied to. Also, look to connect with the Community Safety &amp; Wellbeing initiative to see where overlapping efforts can be made to share data across the corporation.</p> <p>GM2020 (July 13, 2020)</p>	<p><b>Type of Response Required</b></p> <p>-Written Report</p>

Costante	Executive Director Human Resources	<p><b>CQ17-2020</b></p> <p>It is important that we recognize and acknowledge the historic and systemic nature of racism and discrimination in our country and our City. We understand that to move forward and promote equity and eliminate anti-racism requires reaching out to and hearing from the voices of those in our community and Corporation most impacted by discrimination and racism.</p> <p>In this pursuit, it is also essential that we work towards having a Corporation that is representative of the people it serves and that everyone is treated with respect. As such, I am seeking the input and recommendations of Administration and our Diversity Advisory Committee on the viability of:</p> <ol style="list-style-type: none"> <li>1.Including community-led consultations on systemic racism, under Phase 2 of the City of Windsor Diversity and Inclusion Initiative.</li> <li>2.Seeking the input of those in our Corporation and related entities and our community most affected by racism and discrimination, regarding barriers to hiring and advancement in our Corporation and related entities as part of the Diversity and Inclusion Initiative.</li> <li>3.Including recommendations and input regarding providing historical information and educational materials for City owned statues, buildings and streets named with racist histories as part of the Diversity and Inclusion Initiative, and further developing a plan for inclusive street and property naming practices in the future.</li> </ol> <p>APM2020 (July 13, 2020)</p>	<p><b>Type of Response Required</b></p> <p>-Written Report</p>
McKenzie	Comm. Economic Development & Innovation	<p><b>CQ21-2020</b></p> <p>That given the significant cluster of developments through the Howard Avenue corridor between Cabana and South Cameron and increasing concerns among current residents around the capability of the municipal infrastructure directly impacted to support these developments, that Administration prepare a report evaluating those capacities and what if any appropriate investments should proceed in order to accommodate the new developments. The analysis should include a consolidated traffic impact study, an analysis of the condition of the</p>	<p><b>Type of Response Required</b></p> <p>-Written Report</p>



		roadway, the need for traffic management infrastructure and/or traffic calming as well as active transportation capacities or deficiencies.  ST2020 (July 27, 2020)	
McKenzie	Comm. Economic Development & Innovation	<b>CQ32-2020</b> That Administration review and report back to Council on tree protection and replacement policies as it relates to the City of Windsor's land development bylaws. The review should include information pertaining to replacement ratios and the mechanisms by which trees are protected and required to be protected through the development process as well as the extent to which development is impacting the total tree count under our current framework along with options for Council to consider in terms of protecting trees and increasing tree cover through land development policy.  SRT2020 (December 7, 2020)	<b>Type of Response Required</b> -Written Report
Sleiman	Comm. Economic Development & Innovation	<b>CQ33-2020</b> I ask the City Administration to report back with a written report outlining the number of days it takes to get a building permit and compare it to other municipalities of similar size. I also ask Administration to devise methods of improving the speed of the permitting process.  SB2020 (December 7, 2020)	<b>Type of Response Required</b> -Written Report

**Total Outstanding: 12**

<b>COUNCIL MEMBER</b>		<b>QUESTION – ISSUES RAISED</b>	
Holt	Comm. Economic Development & Innovation	<p><b>CQ 3-2021</b></p> <p>Ask that Administration conduct a parking assessment of the 9 BIAs with the goals of providing enough on/off-street parking to satisfy the business needs without patrons spilling into abutting residential neighbourhoods. If land acquisitions are deemed necessary to fulfil these needs with the construction of additional off-street municipal lots, plan and prioritise individual needs.</p> <p>ST2021 18.4 (March 8, 2021)</p>	<p><b>Type of Response Required</b> -Written Report</p> <p>Referred to 2023 Budget Deliberations</p>
Gignac	Executive Director of Human Resources	<p><b>CQ 7-2021</b></p> <p>Asking Administration to provide a report to Council outlining the policy that regulates procedures after an accident involving City vehicles and any amendments they might propose to update it.</p> <p>ACD2021 &amp; AL2021 18.2 (March 29, 2021)</p>	<p><b>Type of Response Required</b> -Written Report</p>
Holt	Comm. Economic Development & Innovation	<p><b>CQ 14-2021</b></p> <p>Asks that our Active Transportation Coordinator REPORT BACK to the Environment Transportation and Public Safety standing committee on the opportunities to convert strategic remaining alleys in Windsor to active transportation routes and linear parks connecting neighbourhoods before they are sold to abutting land owners. These should augment our current Active Transportation Plan as well as connecting key pedestrian generating land uses which may not be identified and accommodated in that master plan.</p> <p>SAA2021 ST2021 (July 19, 2021)</p>	<p><b>Type of Response Required</b> -Written Report</p>
Costante	Commissioner, Infrastructure Services	<p><b>CQ 15-2021</b></p> <p>Asks that Administration report back on the possible causes of flooding on July 16th based on data collected, and that administration further provide any potential solutions moving forward.</p> <p>SW2021 (July 19, 2021)</p> <p>.</p>	<p><b>Type of Response Required</b> -Written Report</p>

McKenzie	Comm. Legal & Legislative Services	<p><b>CQ 17-2021</b></p> <p>Asks that, to promote greater public safety for all people, that Administration work to develop a by-law for Council consideration to provide the City of Windsor with additional tools within the licensing framework for enforcement agencies to address unsafe and illicit activity in hotels and motels across the community that create dangerous and undesirable situations for motel guests, neighbours, and community members alike. The bylaw development process should include consultation process with industry stakeholders and social service providers, social agencies and health providers from both within and external to the City of Windsor as well as any other stakeholder group deemed appropriate by Administration.</p> <p>AB2021 &amp; MH2021 18.1 (July 26, 2021)</p>	<p><b>Type of Response Required</b></p> <p>-Written Report</p>
Gignac	Comm. Infrastructure Services	<p><b>CQ 18-2021</b></p> <p>Asks that Administration review the current truck routes in Windsor and bring a report with recommendations to Council on updating it as soon as possible.</p> <p>ST2021 18.2 (July 26, 2021)</p>	<p><b>Type of Response Required</b></p> <p>-Written Report</p>
Gill	Windsor Police Services	<p><b>CQ 21-2021</b></p> <p>Asks that Administration and Windsor Police bring a citywide report back to council, there has been an increase in crime in Windsor and especially at the Forest Glade Park. I would like to ask what measures are in place to help combat these issues and what steps will be taken going forward to prevent these problems from reoccurring. Residents are concerned for their safety and I would like to ask what actions can be taken in the future to help protect the community from such crime and violence.</p> <p>SP2021 18.3 (September 27, 2021)</p>	<p><b>Type of Response Required</b></p> <p>-Written Report</p>
Gignac	Licence Commissioner	<p><b>CQ 22-2021</b></p> <p>Asks what vehicles fall under the new city Noisy Vehicle bylaw and how By-Law enforcement and Police are co-ordinating to ensure compliance</p> <p>AB2021 18.4 (September 27, 2021)</p>	<p><b>Type of Response Required</b></p> <p>-Written Report</p>

Kaschak	Licence Commissioner	<b>CQ 23-2021</b> Asks that Administration provide an update for the October 18 <sup>th</sup> meeting of Council regarding the Council approved 2 year pilot project to increase our Bylaw Enforcement Officers hours of work to 37.50 and the preliminary results achieved to date.  AS2021 18.5 (September 27, 2021)	<b>Type of Response Required</b> -Written Report
Gill	Commissioner, Community Services	<b>CQ 25-2021</b> Asks that Administration report back to the council with an estimated cost from enacting The Windsor Police Service's recommendations on the short term solutions to improve the skate parks in Windsor, in terms of lighting and installation of cameras, specifically for the Forest Glade Park  SP2021 (October 25, 2021)	<b>Type of Response Required</b> -Written Report
Gignac	Comm. Economic Development & Innovation	<b>CQ 26-2021</b> Asks that Administration research what municipal zoning bylaws may be in place in other municipalities in Ontario or across Canada that regulate Cannabis retail outlets/consumption areas.  GP/13047 18.3 (November 1, 2021)	<b>Type of Response Required</b> -Written Report
Costante	Comm. Infrastructure Services and Comm. Economic Development & Innovation	<b>CQ 27-2021</b> Asks that Administration report back on opportunities to amend the warrant matrix and incorporate additional factors when determining the installation of 4-way stops in our residential neighbourhoods. This may include certain factors in the warrant threshold be lowered or amended, and may also include other factors such as petitions and school zones to be incorporated in the overall matrix.  ST2021 (November 15, 2021)	<b>Type of Response Required</b> -Written Report

**Total Outstanding: 5**

<b>COUNCIL MEMBER</b>		<b>QUESTION – ISSUES RAISED</b>	
Gignac	Comm. Infrastructure Services	<p><b>CQ 1-2022</b></p> <p>Asks Public Works if adjustment to snow incident response would be more effective if salting of residential streets at least once to prevent ice forming. Intention is not to increase budget to include additional salting but perhaps redirecting multiple salting of arterials to direct 1 to residential streets.</p> <p>SW2022 18.3 (January 17, 2022)</p>	<p><b>Type of Response Required</b></p> <p>-Written Report</p>
Gignac	Comm. Infrastructure Services	<p><b>CQ3-2022</b></p> <p>Asks Administration for a report outlining how deficient residential roads not in the 10 year Capital Budget will be addressed.</p> <p>ACOQ2022 18.3 (February 14, 2022)</p>	<p><b>Type of Response Required</b></p> <p>-Written Report</p>
McKenzie	Comm. Economic Development & Innovation	<p><b>CQ5-2022</b></p> <p>Asks Administration to report back to Council on the appropriateness of adding non-BIA commercial districts as Community Safety Zones in commercial shopping districts that generate similar levels of pedestrian and active transportation activity.</p> <p>ACOQ2022/MI2022 (April 11, 2022)</p>	<p><b>Type of Response Required</b></p> <p>-Written Report</p>
Bortolin	Comm. Legal & Legislative Services	<p><b>CQ6-2022</b></p> <p>Asks that Administration provide Council with an update on the Bylaw enforcement “Dirty Yard” repeat offender fee process outlining the effectiveness of administrating the penalty.</p> <p>ACOQ2022/AB2022 (April 25, 2022)</p>	<p><b>Type of Response Required</b></p> <p>-Written Report</p>
Gignac	Comm. Infrastructure Services	<p><b>CQ8-2022</b></p> <p>Asks Administration to review and report to Council if there is a current policy to prioritize "half width streets" to bring them up to current standard width. Also if there are Provincial regulations as to required standard widths for residential roads.</p> <p>ACOQ2022 &amp; ST2022 18.3 (May 9, 2022)</p>	<p><b>Type of Response Required</b></p> <p>-Written Report</p>

/sg  
as of June 9, 2022





**Council Directives: SCM 161/2022**

**Subject: Outstanding Council Directives as of June 13, 2022**

## Outstanding Council Directives Tracking Log

Updated:2022-06-09

Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
December 17, 2012	CR293/2012	16287	City Planner	That the report of the City Planner dated November 21, 2012 entitled "Exemption from Sandwich Demolition Control By-law 20-2007 — 508, 520, 540, 556, 570, 590, 604, 612, 615, 622, 623, 631, 639, 646, 663, 670, 673, 686, 704, 710, 718, 724, 730, 738, 744, 750, 753, 758-760, 759, 765, 764, 769, 772, 777, 778, 781, 784, and 790 Indian Road, 812 and 862 Mill Street, and 764, 770, 780 and 788 Rosedale Avenue" <b>BE DEFERRED</b> as requested by the Canadian Transit Company, to allow for further discussions with administration on this matter.	Report remains deferred by Council, as per the Commissioner, Legal & Legislative Services.
August 24, 2015	CR159/2015 Clause XI	17893		<b>"Corporate Payroll Business Process Review UPDATE":</b> That the final FTE staffing changes reductions and resultant project savings and completions, <b>BE REPORTED</b> to City Council as part of or prior to the 2018 budget process	Q1 2021
May 16, 2016	CR334/2016	S 76/2016	Commissioner, Corporate Services Chief Financial Officer & City Treasurer	THAT City Council AUTHORIZE the CFO/Commissioner, Corporate Services Chief Financial Officer & City Treasurer(or delegate) to sign Minutes of Settlement as it relates to the Centralized Property Appeals. THAT the CFO/Commissioner, Corporate Services Chief Financial Officer & City Treasurer(or delegate) BE REQUIRED to report the results of the Minutes of Settlement to City Council once all appeals have been finalized.	Ongoing as required

Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
September 19, 2016	CR594/2016	C 176/2016	Chief Administrative Officer	<p>THAT City Council APPROVE the allocation of \$400,000 for the design, creation, installation and unveiling of a commemorative statue to honour the life and work of Hiram Walker; and,</p> <p>THAT City Council APPROVE that \$390,000 BE FUNDED from the 2014 Enhanced Capital Budget Contingency Placeholder for this project with the remaining \$10,000 to BE FUNDED from Councillor Holt's 2016 ward funds; and,</p> <p>THAT City Council APPROVE the sole source retention of artist Mark Williams for the creation, fabrication and installation of a statue/sculpture depicting Hiram Walker and DIRECT administration to prepare an agreement to retain the services of Mr. Williams accordingly; and,</p> <p>THAT the CAO and Commissioner, Legal &amp; Legislative Services BE AUTHORIZED to take any other steps as may be required to bring effect to these resolutions, satisfactory in form to the Commissioner, Legal &amp; Legislative Services, in financial content to the Commissioner, Corporate Services Chief Financial Officer &amp; City Treasurer, and in technical content to the Commissioner, Infrastructure Services; and,</p> <p>THAT the CAO and Commissioner, Legal &amp; Legislative Services BE AUTHORIZED to sign any required documentation as it relates to this project, satisfactory in legal form to the Commissioner, Legal &amp; Legislative Services, in technical content to the Commissioner, Infrastructure Services, and in financial content to the Commissioner, Corporate Services Chief Financial Officer &amp; City Treasurer; and further,</p> <p>THAT administration REPORT BACK on fundraising efforts towards this project within six months.</p>	CAO 4032 - To be completed 2020

## Outstanding Council Directives Tracking Log

Updated:2022-06-09

Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
February 6, 2017	CR59/2017	CM 59/2016	City Planner	<p>THAT the report of the City Planner dated October 26, 2016 entitled “Response to CQ34-2016: Design Guidelines for fencing along Riverside Drive” <b>BE RECEIVED</b> for information; and further,</p> <p>THAT Administration <b>BE DIRECTED</b> to prepare a report for Council’s consideration that would:</p> <ul style="list-style-type: none"> <li>- Institute a by-law standard for decorative fencing and parking areas along Riverside Drive (both the north and south sides of Riverside Drive); and</li> <li>- Include options for incentivizing existing commercial and industrial property owners to be able to upgrade their existing fencing along the waterfront side of Riverside Drive; and</li> </ul> <p>Include costing for decorative fencing from just east of Hiram Walker’s all the way to Strabane.</p>	
August 8, 2017	CR472/2017	C 123/2017	Manager Real Estate Services	That Administration <b>REPORT BACK</b> with respect to conducting an EOI for the rental of the commercial space in the Goyeau Street Parking garage.	

Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
October 16, 2017	CR648/2017	C 180/2017	Chief Administrative Officer	<p>... That pending the restoration of the Street Car #351, City Council APPROVE IN PRINCIPLE the installation of Street Car #351 into a Riverfront Park location with a future report to City Council for site location approval and funding request to install at said location and to develop a maintenance fund for future requirements; and further,</p> <p>That administration BE DIRECTED to undertake a public consultation process on this project, and that following this process, that an administrative report BE PREPARED for Council's consideration no later than 6 months after the Trolley is accepted from the current owner, to provide options as to usage if it is for some kind of vending (e.g., food and/or drink) or anything of that nature, including costs for transportation, placement, and potential sites; and further,</p> <p>That the CAO and Commissioner, Legal &amp; Legislative Services BE AUTHORIZED to sign any other documents required to bring effect to these resolutions, in form satisfactory to the Commissioner, Legal &amp; Legislative Services, in financial content satisfactory to the Commissioner, Corporate Services Chief Financial Officer &amp; City Treasurer/Commissioner, Corporate Services Chief Financial Officer &amp; City Treasurer, and in technical content to the Corporate Leader of Parks, Recreation, Culture and Facilities and Executive Director of Recreation and Culture; and further,</p>	CR297/2018 Fundraising Ongoing
January 15, 2018	B32/2018	S 184/2017	City Forester	<p>THAT the report from the City Forester regarding an update on the progress of a City-wide Tree Inventory Project, a Preventative Tree Maintenance Program and a Urban Forest Management Plan <b>BE RECEIVED</b>; and further,</p> <p>THAT funding for the Preventative Tree Maintenance Program in the estimated annual amount of \$2,080,000 beginning in 2019 <b>BE REFERRED</b> to future Capital budget deliberations; and,</p> <p>That Administration <b>PROVIDE</b> information on any available subsidized programs which may exist by investigating best practices used in other municipalities; and that this information <b>BE PROVIDED</b> during the 2019 Budget deliberation process.</p>	Will be included in Capital Variance Report 2021



## Outstanding Council Directives Tracking Log

Updated:2022-06-09

Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
March 26, 2018	CR155/2018	C 52/2018	Commissioner, Infrastructure Services	<p>...That the Purchasing Department BE AUTHORIZED to issue a Purchase Order to Haddad, Morgan and Associates Ltd. to provide engineering services related to the redevelopment of 6700 Raymond Ave (former Concord School site) for an upset limit of \$75,500 plus taxes; and further,</p> <p>That the Commissioner, Infrastructure Services or designate BE AUTHORIZED to issue the requisite tender for the construction works required for the redevelopment and FURTHER that once the tender results are known that a report be submitted to City Council relative to the award of the contract and identifying a funding source for any projected funding shortfalls that may arise.</p>	On hold pending Planning's review of tiny houses as asked by Councillor Gignac Now that the SMP is complete we need to review the impact of this development however the concepts are still being developed by Planning - update towards end of Dec. 2020
May 7, 2018	CR245/2018	CMC 9/2018	Chief Administrative Officer	That the correspondence from the Ministry of Education dated April 27, 2018 regarding an update to Ontario's commitment to revise the Pupil Accommodation Review Guideline (PARG) <b>BE RECEIVED</b> , and further, that Administration <b>BE DIRECTED</b> to prepare a report for Council's consideration in terms of the possibility for applying on the City of Windsor's behalf for the Call of Proposals being issued this summer to participate in the Voluntary Integrated Planning and Partnerships Initiative (VIPPi), to provide flexible support to local partners that wish to enhance their collective capacity for integrated capital and community planning.	
May 7, 2018	CR253/2018	C 76/2018	Commissioner, Corporate Services Chief Financial Officer & City Treasurer	...That all expenditures made under the exemption <b>BE REPORTED</b> to Council by the 2022 CAN-AM Police-Fire Games General Manager within a reasonable time following the conclusion of the Project.	
May 7, 2018	CR265/2018 ETPS 597	SCM 178/2018 & S 59/2018	Commissioner, Community Services	<p>That the report of the Manager of Parks Development dated March 28, 2018 entitled "Response to CQ4-2016 Lighting at Riverfront Walkway Near Askin Boulevard" <b>BE RECEIVED</b> for information; and,</p> <p>That a report be <b>BROUGHT BACK</b> as part of the 2021 budget deliberations relative to the matter so that council can debate the allocation of the Central Riverfront Park Improvement placeholder funds, including consideration of lighting the riverfront walkway.</p>	Stand alone report or as part of Capital Budget submissions under CRIP or lighting projects for 2021

Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
May 7, 2018	CR275/2018	C 77/2018	Commissioner, Corporate Services Chief Financial Officer & City Treasurer	That Council <b>PROVIDE</b> Riverwest with the City's copyright permission to use banners with the likeness of public artwork owned by the City on condition that Riverwest agrees that the banners contain an acknowledgement of the artists who produced the original artwork; and further,  That Council <b>APPROVE</b> the requested \$5000 indemnity and that this <b>BE CHARGED</b> to the Budget Stabilization Reserve Fund (BSR) and that Administration <b>BE DIRECTED</b> to prepare a draft policy for Council's consideration regarding banners (how they can be requested, all costs associated, etc.) outlining what would be expected.	
June 4, 2018	CR333/2018 Clause VII	S 235/2017 & SCM 75/2018	Commissioner, Corporate Services Chief Financial Officer & City Treasurer	VII. That Administration <b>REPORT BACK</b> to City Council regarding the catalyst project designation under the Building/Property Tax Increment Grant Program.	
August 27, 2018	CR472/2018	C 148/2018	City Planner	That the Walkerville area <b>BE SELECTED</b> as a pilot project area for the Districting Initiative and generally bounded on the north by the Detroit River, on the south by Niagara Street, on the east by Walker Road, and to the west by Gladstone Avenue.  That a design and costing consultant at a cost not to exceed \$100,000.00 (plus HST) <b>BE RETAINED</b> for purposes of performing a higher level design analysis/value engineering and market costing so that the most accurate budget estimates can be obtained and further approved by Council prior to project selection/execution; and further,  That the cost of the design and costing consultant <b>BE FUNDED</b> from the 2018 Enhanced Budget for District Theming previously approved for spending by Council via CR123/2018; and,  That a future Council Report <b>BE PREPARED</b> by the City Planner recommending and describing the specific Walkerville Districting projects to be completed with estimated budgets and timelines for completion.	

## Outstanding Council Directives Tracking Log

Updated:2022-06-09

Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
September 17, 2018	CR503/2018 ETPS 630	SCM 332/2018 & C 122/2018	Commissioner, Community Services	<p>That the report of the Manager Parks Development, dated July 20, 2018, responding to CQ27-2017 regarding first responders signage for parks, <b>BE RECEIVED</b> for information; and,</p> <p>That City Council <b>BE ADVISED</b> that Administration will return to Council in January 2019 to request the approval to proceed with this project and release the 2023 funding allocated as part of the 2018 Enhanced Budget, and further,</p> <p>That Administration <b>BE DIRECTED</b> to develop a wayfinding standards policy based on the results of the Little River Corridor wayfinding signage and markers, as a pilot project, to be brought to City Council for approval.</p>	Wayfinding signage at Little River Corridor install Spring 2020 as Pilot. Report to follow on year after review of pilot (summer/fall 2021)
September 17, 2018	CR512/2018 PHED 589	SCM 257/2018 & SCM 207/2018	City Planner	<p>That Report No. 7 of the Windsor Housing Advisory Committee indicating: That the "Draft" Official Plan policies relating to second unit policies BE ACCEPTED and further, that the recommendation of the Housing Advisory Committee proceed to the Planning, Heritage and Economic Standing Committee and to City Council as expeditiously as possible, <b>BE APPROVED</b>; and,</p> <p>That Administration <b>REPORT BACK</b> to Council on best practices from surrounding Municipalities regarding heights and set back requirements for ancillary structures.</p>	

Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
October 1, 2018	CR550/2018	S 165/2018	Commissioner, Community Services	<p>That the report from Glos Associates Inc. titled “Proposed Relocation/Construction of Lanspeary Park Greenhouse Feasibility Study” and dated September 10, 2018 <b>BE RECEIVED</b>; and further,</p> <p>That Council <b>APPROVE</b> OPTION #2 – Construct a new greenhouse complex at Jackson Park- as the preferred solution; and further,</p> <p>That Council <b>REFER</b> consideration of the associated funding for the chosen option to the 2019 budget deliberations; and further,</p> <p>That subject to funding being approved in the Capital Budget, Administration <b>BE DIRECTED</b> to construct a new greenhouse complex at Jackson Park inclusive of the expansion space for in-house plant production and thereafter proceed to demolish the existing Lanspeary Park Greenhouse Complex, taking into consideration any heritage features or buildings contained thereon, and restore the subject area to parkland; and further,</p> <p>That Administration <b>BE DIRECTED</b> to offer options for the expansion of the demonstration house on the site within the re-development plans for Lanspeary Park (options showcasing the low impact re-development); and further,</p> <p>That Administration <b>BE DIRECTED</b> to provide information on production numbers for having this in-house versus externally for this service (how much does the taxpayer benefit from having this in-house).</p>	<p>Info on production numbers will be included in the report for approving the precommitment of remaining funds required anticipated in early January 2021. Lanspeary redevelopment plans/report will follow public open house, late 2021 early 2022.</p>

## Outstanding Council Directives Tracking Log

Updated:2022-06-09

Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
January 7, 2019	CR4/2019	C 217/2018	Chief Administrative Officer	<p>1. That City Council <b>AUTHORIZE</b> Administration to proceed with, and <b>PRE-COMMIT</b> funding for, the additional 2018 Enhanced Capital Projects identified in Appendix A for the 2019 calendar year; and,</p> <p>2. That the award of tenders or RFP's for the identified works <b>BE PRE-APPROVED</b>, subject to being within the allocated budget and in accordance with Purchasing by-law 93-2012, and that the Chief Administrative Officer and Commissioner, Legal &amp; Legislative Services <b>BE AUTHORIZED</b> to sign all relevant agreements, in form satisfactory to the Commissioner, Legal &amp; Legislative Services, in financial content satisfactory to the Commissioner, Corporate Services Chief Financial Officer &amp; City Treasurer and in technical content satisfactory to the Commissioner, Infrastructure Services, City Planner or Corporate Leader of Parks, Recreation, Culture and Facilities; and,</p> <p>3. That Administration <b>REPORT BACK</b> to City Council through a Communication Report(s) the results of all tenders that were awarded and approved, with any that require additional funding to be reported to Council separately.</p>	
February 4, 2019	CR35/2019	C 11/2019	Commissioner, Corporate Services Chief Financial Officer & City Treasurer	<p>That City Council <b>APPROVE</b>, as per the requirements of the Leadership Asset Management Program (LAMP), the use of the tools and guidelines for Triple bottom line plus (TBL+), Whole life-cycle (WLC) and Business Case Evaluation (BCE) as developed through the LAMP grant and approved by the Asset Planning Steering Committee; and,</p> <p>That City Council <b>DELEGATE</b> authority to the Asset Planning Steering Committee to provide oversight to the implementation of this framework and to amend the guidelines and tools as may be deemed necessary as such guidelines and tools are integrated within the Corporation; and further,</p> <p>That Administration <b>BE DIRECTED</b> to prepare a report for Council's consideration on methods that could be used to accelerate the process for implementation</p>	



Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
March 25, 2019	CR120/2019	C 43/2019	Commissioner, Legal & Legislative Services	That the report of the Commissioner, Corporate Services Chief Financial Officer & City Treasurer regarding the Mayor, Councillors and Appointees 2018 Statement of Remuneration and Expenses BE RECEIVED for information; and further, That in a municipal election year, NO COUNCILLOR SHALL BE PERMITTED to commit to any conferences after the date of an election in a municipal election year; and further, That Administration BE DIRECTED to prepare a report for Council's consideration on a process that would allow all members of Council to access conference materials and summary notes for information purposes, from those Councillors that attend conferences.	
April 1, 2019	B8/2019	C 226/2018	Commissioner, Corporate Services Chief Financial Officer & City Treasurer	That City Council <b>RECEIVE</b> the 2019 Capital Budget 7-Year Plan documents reflective of approx. \$845.104 M in total funding; and...  That Administration <b>BE DIRECTED to REPORT BACK</b> to Council regarding the infrastructure deficit and a high-level plan to address it; and,  As amended a total of \$240,000 from the 2019 Grant Matching and Inflationary Pressures project (FIN-001-19) be reallocated to: Capitol Theatre Capital Improvements (ENG-010-17) in the amount of \$40,000 for the new Marquee sign and Pedestrian Crossing (OPS-001-19) in the amount of \$200,000 for priority pedestrian crossings as identified in agenda Item 11.14.	
April 1, 2019	B14/2019	C 188/2018	Commissioner, Infrastructure Services	That Council <b>FORMALLY REQUEST</b> the Lieutenant Governor of Ontario for permission to divest of the City-owned shore wall along Lake St. Clair; and,  That Administration <b>FURTHER REPORT BACK</b> with respect to the question regarding possible transfer ownership of shore walls and associated land; and...	Letter sent to ERCA Richard Wyma on November 2019. Letter sent by CAO to MNRF January 16, 2020 c/o Mitch Wilson. Cannot be responded to until a response is received. It will likely be a year or more. September 9, 2020 - Lt Gov. has not yet responded

## Outstanding Council Directives Tracking Log

Updated:2022-06-09

Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
April 1, 2019	B61/2019	Reference #C118	Chief Executive Officer Windsor Library	That the recommended “Addition of Friday Branch Hours” (Budget Issue #2019-0358) in the amount of \$98,412 <b>BE NOT APPROVED</b> , and that it <b>BE FUNDED</b> through one-time dollars and that Administration <b>REPORT BACK</b> in 2020.	
May 6, 2019	CR210/2019	C 78/2019	Community Development and Health Services Commissioner – Corporate Leader Social Development and Health	<p>That the report from the Commissioner of Community Development and Health Services and the Administrator/Executive Director of Huron Lodge Long-Term Care Home <b>BE RECEIVED</b> for information; and further...</p> <p>That the Chief Administrative Officer and Commissioner, Legal &amp; Legislative Services <b>BE AUTHORIZED</b> to execute any documents required to establish and participate in the Ontario Health Team, such that any funding does not create an additional annualized cost without the prior approval of City Council, and subject to legal approval by the Commissioner, Legal &amp; Legislative Services, financial approval by the Commissioner, Corporate Services Chief Financial Officer &amp; City Treasurer, and technical approval by the Community Development and Health Commissioner and Executive Director of Huron Lodge; and further,</p> <p>That Administration <b>BE DIRECTED</b> to provide appropriate reports to City Council as the Ontario Health Team is established in Windsor and Essex County.</p>	

Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
July 8, 2019	CR322/2019	C 68/2019	Chief Building Official	<p>That a vacant building registry <b>NOT BE IMPLEMENTED</b> at this time and the vacant building initiative (VBD) <b>BE EXTENDED</b> to July 2020; and,</p> <p>That Building Administration <b>CONTINUE TO REVIEW</b> the effectiveness of the VBI program and <b>PROVIDE</b> City Council costing options to maintain permanent pro-active vacant building enforcement as part of the 2020 Building Inspections budget submission; and,</p> <p>That a sixth goal <b>BE ADDED</b> to the 5 program goals listed in the report, specifically “To mitigate visible blight for the affected neighbourhood”; and,</p> <p>That administration <b>BE DIRECTED</b> to report back in 2020 for a more fulsome breakdown of statistics including types of orders issued, which were successful, which were complied with, and that the report <b>ALSO INCLUDE</b> options for a vacant building registry that expressly includes the topic of access, cost recovery, identification, highest fees possible under the law and the shortest timelines.</p>	
July 8, 2019	CR334/2019	SCM 205/2019 & S 102/2019	City Planner	<p>That this Council Report responding to CQ 1-2019 on the benefits and process to designating Walkerville a Heritage Conservation District under the Ontario Heritage Act <b>BE RECEIVED FOR INFORMATION</b>; and,</p> <p>That Administration <b>BE DIRECTED</b> to proceed with the implementation of the Walkerville Heritage Conservation District Study; and,</p> <p>That administration <b>PROVIDE</b> a fulsome report to the Development and Heritage Standing Committee outlining the process, timelines and next steps.</p>	

## Outstanding Council Directives Tracking Log

Updated:2022-06-09

Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
July 8, 2019	CR340/2019	SCM 183/2019 & S 97/2019	Commissioner, Human & Health Services	<p>...That Administration <b>BE DIRECTED</b> to bring the Community Safety and Well-being Plan to City Council and the Windsor Police Services Board in sufficient time to be adopted prior to the Provincial government's deadline of January 1, 2021; and further,</p> <p>That the Commissioner of Community Development and Health Services or her designate <b>BE REQUESTED</b> to provide regular updates to the Community Services and Parks Standing Committee over the process of development and implementation of the Community Safety and Well-being Plan.</p>	CR337/2020 Extended deadline
July 8, 2019	CR347/2019	SCM 190/2019 & S 79/2019	Commissioner, Human & Health Services	<p>...That the Chief Administrative Officer and Commissioner, Legal &amp; Legislative Services <b>BE AUTHORIZED</b> to execute any agreements, documents and forms required to establish and participate as a Prototype, Service System Manager and/or Direct Delivery Agent, such that any funding does not create an additional annualized City cost without the prior approval of City Council. Authorization would be subject to approval as to legal content by the Commissioner, Legal &amp; Legislative Services, as to financial content by the Commissioner, Corporate Services Chief Financial Officer &amp; Commissioner, Corporate Services Chief Financial Officer &amp; City Treasurer and Commissioner, Corporate Services Chief Financial Officer &amp; City Treasurer or designate, and as to technical content by the Community Development and Health Services Commissioner and the Commissioner, Human &amp; Health Services; and further,</p> <p>That Administration <b>BE DIRECTED</b> to provide appropriate reports and updates to City Council regarding the Ontario Works – Employment Ontario Transformation as information becomes available.</p>	
August 26, 2019	CR424/2019	S 129/2019	Senior Manager Asset Planning	<p>That City Council <b>RECEIVE</b> and <b>APPROVE</b> the attached Asset Management Plan for the City of Windsor in compliance with Ontario Regulation 588/17 – Asset Management Planning for Municipal ; and further,</p> <p>That the report of the Manager of Asset Planning dated July 8, 2019 entitled “Corporate Asset Management Plan” <b>BE DEFERRED</b> to the 2020 Budget Deliberations to allow Council to consider the additional recommendations in the report.</p>	

Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
October 7, 2019	CR495/2019 Clause 6	C 162/2019	Sokol- Asset Planning	<p>1 – That Council <b>AUTHORIZE</b> administration to negotiate a Municipal Sewer Access Agreement between the City of Windsor and Noventa Energy Partners Ltd. for the purpose of connecting to the City's sanitary sewer trunk line at a designated location along Riverside Drive, satisfactory in form to the Commissioner, Legal &amp; Legislative Services, in technical content to the Commissioner, Infrastructure Services, and in financial content to the Commissioner, Corporate Services Chief Financial Officer &amp; Commissioner, Corporate Services Chief Financial Officer &amp; City Treasurer and Commissioner, Corporate Services Chief Financial Officer &amp; City Treasurer; and...</p> <p>6 – That Council <b>DIRECT</b> administration to report back detailing the outcome of the negotiated agreements and other related matters contained in this report.</p>	
October 7, 2019	CR518/2019	SCM 358/2019 & S 146/2019	Executive Director of Operations	<p>That this report in response to CQ 12-2019 – Residential Parking Permit Policy <b>BE RECIEVED</b> by Council for information; and,</p> <p>That Council <b>RESCIND</b> the Onstreet Parking Permits for Agencies Policy as adopted in CR418/2004; and,</p> <p>That Council <b>APPROVE</b> the Agency Parking Permit Policy as proposed in Appendix "A"; and,</p> <p>That Administration <b>BE REQUESTED</b> to report back on options to curb the issues with permit parking including but not limited to no parking zones/limited parking/commuter lots and other solutions that administration deems may be good solutions; and,</p> <p>That this information <b>BE FORWARDED</b> to a future meeting of the Environment, transportation and Public Safety Standing Committee.</p>	Waiting for information related to the lot near Brock Street which we are trying to make arrangements for some alternative parking
November 18, 2019	CR543/2019	S 198/2019	Commissioner, Human & Health Services	...That Administration <b>BE DIRECTED</b> to bring the regional Community Safety and Well-Being Plan to City Council and Essex County Council in sufficient time to be considered prior to the Provincial government's deadline of January 1, 2021.	CR337/2020 Extended deadline



## Outstanding Council Directives Tracking Log

Updated:2022-06-09

Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
November 18, 2019	CR564/2019	SCM 388/2019 & SCM 329/2019	Windsor Police Services	That Report No. 21 of the Windsor BIA Advisory Committee – Increasing enforcement on Wyandotte St. East from Devonshire to Lauzon indicating: That Administration from Windsor Police Services <b>BE REQUESTED</b> to report back on the feasibility of increasing enforcement on Wyandotte Street East from Devonshire to Lauzon Road due to excessive speeding along this corridor.	
December 2, 2019	CR601/2019	SCM 417/2019 & SCM 365/2019		That Report No. 2 of the Committee of Management for Huron Lodge indicating: That Administration <b>BE REQUESTED</b> to report back on the history of per diem funding in long term care homes and the comparators relating to the allocation of food per diems in other institutions	

Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
December 2, 2019	CR608/2019	SCM 412/2019 & S 200/2019	Commissioner, Human & Health Services	<p>That this report from the Executive Director of Housing and Children's Services regarding the Windsor Essex Child Care and Early Years Service System Plan 2020-2025 <b>BE ACCEPTED</b>; and,</p> <p>That the Executive Director of Housing and Children's Services <b>BE AUTHORIZED</b> to submit The Windsor Essex Child Care and Early Years Service System Plan 2020-2025 to the Province of Ontario's Ministry of Education on or before December 31, 2019 as required under the Child Care and Early Years Act; 2014 (CCEYA); and,</p> <p>That the Executive Director of Housing and Children's Services <b>REPORT BACK</b> to Council should the Ministry of Education have substantial changes and/or recommendations that are directed by the Minister to be incorporated into The Windsor Essex Child Care and Early Years Service System Plan 2020-2025; and,</p> <p>That the Executive Director of Housing and Children's Services <b>BE AUTHORIZED</b> to submit subsequent reports/updates on The Windsor Essex Child Care and Early Years Service System Plan 2020-2025 to the Ministry of Education if required; and further,</p> <p>That Administration <b>REPORT BACK</b> to the Community Services and Parks Standing Committee once further analysis is completed regarding the causal data related increase in vulnerability as reported in the Early Development Instrument (EDI) and further analysis in terms of breakdowns of EarlyON programs and usage.</p>	
December 16, 2019	CR621/2019	C 203/2019	Commissioner, Corporate Services Chief Financial Officer & City Treasurer	<p>That City Council <b>APPROVE</b> the issuing of a Request for Proposal (RFP), in accordance with Purchasing Bylaw 93-2012 and amendments hereto, for the potential development of a photovoltaic generation Net Metering project on suitable City of Windsor properties; and,</p> <p>That the Council <b>DIRECT</b> Administration to report back on the outcome of the RFP identifying the successful proponent along with a detailed business case analysis.</p>	

## Outstanding Council Directives Tracking Log

Updated:2022-06-09

Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
January 20, 2020	CR40/2020	SCM 485/2019 & S 230/2019	Commissioner, Corporate Services Chief Financial Officer & City Treasurer	That the Environment, Transportation & Public Safety Standing Committee sitting as the Transit Windsor Board of Directors <b>APPROVE</b> the Transit Master Plan – More Than Transit (the Plan) as follows: 1. That the Plan <b>BE</b> the roadmap for Transit Windsor from the years 2020 to 2028 to follow with annual reviews and updates; and,  2. That any 2019 operating revenue surplus to a maximum of \$250,000 <b>BE TRANSFERRED</b> to Capital to fund a Garage Feasibility Study in order to implement the plan; and,  3. That the recommendations as set out by Administration for capital and operating needs for 2020 through to 2028 <b>BE DEFERRED</b> to the City of Windsor Annual Operating and Capital Budget with regards to implementation of the plan.	
January 27, 2020	B9/2020	SCM 299/2019 & S 167/2019	Commissioner, Infrastructure Services	That Administration <b>BE DIRECTED</b> to prepare a report for Council's consideration related to options for curbside garbage collection instead of alley collection citywide wherever possible.	Report written as C 151/2020. Will be updated with new financial information and is expected to go to December 16 ETPS
January 27, 2020	B58/2020	AFB/13467	Chief Administrative Officer	That Administration <b>BE DIRECTED</b> to implement a Zero Based Participatory Full Council Budget model for the 2021 Budget deliberations and further that a report <b>BE PROVIDED</b> in the Spring of 2020 outlining the parameters and options for a Zero Based Budget model for Council's consideration and decision.	

Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
February 24, 2020	CR99/2020 CR605/2019 CSPS79	SCM 58/2020 & C 160/2019	Commissioner, Community Services	<p>...That the International Relations Committee <b>BE REQUESTED</b> to review the \$25,000 commitment for the You + Me sculpture project due to the updated information regarding the \$50,000 financial commitment to this project by the Rotary Club of Windsor-Roseland;</p> <p>That the International Relations Committee <b>RECONSIDER</b> committing \$25,000 in the You + Me sculpture project; and further,</p> <p>That, in the absence of the International Relations Committee reconsidering a \$25,000 financial commitment, the International Relations Committee <b>REPORT BACK</b> to Council with their plan for the \$25,000 that was previously committed to the You + Me sculpture project.</p>	Completed - a small portion (\$5,000) was allocated to the You +Me sculpture project and of the remaining \$20,000 the IRC has approved \$14,000 for the Sister Cities Pole
April 27, 2020	CR149/2020	C 76/2020	Commissioner, Corporate Services Chief Financial Officer & City Treasurer	<p>That the results of the email poll conducted by the Deputy Commissioner, Legal &amp; Legislative Services on April 8, 2020 approving the following recommendation BE CONFIRMED AND RATIFIED:</p> <p>That, consistent with CR 127/2020 which provided all taxpayers property tax relief in the form of an extension for payment of the third interim tax instalment, City Council EXTEND the due date for the 2020 1st quarter remittance of the Municipal Accommodation Tax (MAT) from April 30, 2020 to June 30, 2020; and further,</p> <p>That there WILL BE NO late payment charges in either form, penalty and/or interest through the period June 30, 2020; and further,</p> <p>That the Commissioner, Legal &amp; Legislative Services AMEND By-law 133-2018;</p> <p>and further,</p> <p>That administration BE DIRECTED to prepare a report for Council's consideration, as soon as possible, on options available for the City of Windsor to use the Municipal Accommodation Tax (MAT) to help the local hospitality industry as a result of the ongoing COVID-19 pandemic.</p>	

## Outstanding Council Directives Tracking Log

Updated:2022-06-09

Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
June 15, 2020	CR315/2020	C 68/2020	Commissioner, Community Services	That the report of the Manager – Parks Development, dated April 1, 2020 entitled “E-Scooters in Parks” <b>BE DEFERRED</b> until the spring of 2021 as a pilot project for 2020 would be limited due to the Covid-19 pandemic.	Report will be updated and brought forward to Council in Spring 2021.
July 27, 2020	CR380/2020	C 145/2020	Community Development and Health Commissioner & Corporate Leader – Social Development and Health	<p>That the Review of Emergency Shelter Services in Windsor Essex Report by Vink Consulting Inc. BE RECEIVED for information; and,</p> <p>That the Executive Director of Housing and Children’s Services BE DIRECTED to bring forward reports to City Council addressing the recommendations for the future state of the emergency shelter system, including implementation plans with associated costs; and,</p> <p>That the Executive Director of Housing and Children’s Services BE AUTHORIZED to pursue funding options to support any required operational and capital investments and further, should such funding options be in the forms of grants and/or loans; and,</p> <p>That the Chief Administrative Officer and Commissioner, Legal &amp; Legislative Services, or their designates, BE AUTHORIZED to sign any applications and take any such actions as required to secure funding provided such documents are in a form satisfactory to the Commissioner, Legal &amp; Legislative Services, satisfactory in financial content to the Commissioner, Corporate Services Chief Financial Officer &amp; City Treasurer, and technical content to the Executive Director of Housing and Children’s Services; and further,</p> <p>That Administration BE DIRECTED to report back to the Community Services and Parks Standing Committee on a quarterly basis on timelines and progress of the report.</p>	



Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
November 7, 2020	CR553/2020 DHSC 207	SCM 311/2020 & S 53/2020	City Planner	<p>I. That Administration <b>BE REQUESTED</b> to give notice of intention to designate the property located at 436 Askin Ave in accordance with Part IV of the Ontario Heritage Act; within 60 days from September 25, 2020; and,</p> <p>II. That the request to demolish the enclosed porch and balcony at the rear of 436 Askin Ave to facilitate a rear addition and conversion to a semi-detached dwelling as currently proposed, <b>BE REFUSED</b>, in accordance with the Ontario Heritage Act, Section 30 Effect of the Notice of Designation; and,</p> <p>III. That Administration <b>REPORT BACK</b> to Council regarding initiation of a Heritage Conservation District Area Study for this area; and, that the report include suggestions related to potential boundaries, optional designation of a Heritage Conservation District Study Area Bylaw, timing of the study and funding considerations.</p>	
November 7, 2020	CR564/2020	C 211/2020	Commissioner, Infrastructure Services	<p>That the report of Public Works – Operations, dated October 23, 2020 entitled “Purchase of Six, 2021 Chevrolet Bolt, Fully Electric Vehicles for Building Department” <b>BE RECEIVED</b>; and further,</p> <p>That the existing RFP <b>BE RESCINDED</b> and that a new RFP <b>BE ISSUED</b> for hybrid mini-vans that would replace these vehicles and that Administration <b>BE DIRECTED</b> to come back with a draft policy for Council’s consideration on how to replace vehicles while at the same time supporting the Climate Change Plan.</p>	
November 23, 2020	CR585/2020	C 220/2020	Commissioner, Community Services	That the report of the Senior Manager – Facilities dated November 6, 2020 entitled “Corporate Security Plan and Risk Assessment” <b>BE DEFERRED</b> to allow for additional information from administration.	
December 7, 2020	CR616/2020 Clause 6	C 54/2020	Commissioner, Corporate Services Chief Financial Officer & City Treasurer	6. That administration <b>BE DIRECTED</b> to report back at a high level on the economic impact and any perceived impacts on development if Council were to revisit eliminating the industrial exemption.	
December 21, 2020	CR655/2020	SCM 374/2020 & S 164/2020	Commissioner of Parks, Recreation, Culture and Corporate Facilities	That Administration <b>BE REQUESTED</b> to report back to Council outlining the costs associated with undertaking an invasive species management strategy citywide.	

## Outstanding Council Directives Tracking Log

Updated:2022-06-09

Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
January 18, 2021	CR40/2021 ETPS 802	SCM 16/2021 & S 155/2020	Commissioner, Economic Development & Innovation	That the report of the Environment, Transportation and Public Safety Standing Committee of its meeting held December 16, 2020 regarding “Wyandotte Street East Corridor Review” <b>BE REFERRED</b> back to Administration to narrow the focus as soon as possible, and to satisfy the Active Transportation Master Plan by providing cycling infrastructure along Wyandotte Street East and further, that in-person public meetings <b>BE HELD</b> once permitted, as part of a consultation process that would include residents and businesses in the subject area.	
February 1, 2021	CR64/2021 CR26/2021 DHSC 226 DHSC 227 DHSC 228 Clause 6	SCM 2/2021 & S 170/2020	City Planner	...6. That the City Planner <b>BE DIRECTED</b> to report back to Council on a range of options available to address the concerns arising from the 2650 Metcalf property operating as a Transport Terminal.	
February 8, 2021	CR69/2021	SCM 44/2021	Commissioner, Corporate Services Chief Financial Officer & City Treasurer	That Council <b>RECEIVE</b> the external advisory report and <b>ENDORSE IN PRINCIPLE</b> the recommendations enclosed in Windsor Works: An Economic Development Strategy for the City's Future Growth; and further,  That Council <b>DIRECT</b> Administration to undertake the work required in order to prepare a detailed implementation plan as soon as practical and provide quarterly updates to Council on progress.	

Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
February 22, 2021	B10/2021 ETPS 808	SCM 41/2021 & S 190/2020	Commissioner, Infrastructure Services	<p>That a pilot project for 2021 <b>BE ESTABLISHED</b> from September to December of 2021 related to Route 18 that provides service from the East End, from Tecumseh Mall to St. Clair College, to <b>BE COST-SHARED</b> between the City of Windsor and St. Clair College at a cost of \$90,000 each; and,</p> <p>That the amount of \$90,000 for the City of Windsor's portion <b>BE FUNDED</b> from the Budget Stabilization Reserve (BSR) Fund for 2021; and,</p> <p>That administration <b>BE DIRECTED</b> to enter into discussions with St. Clair College and the Student Council for the potential of a "St. Clair College Pass Program"; and further,</p> <p>That the pilot project <b>BE RE-VISITED</b> by Council in early December, 2021.</p>	
February 22, 2021	B26/2021		Commissioner, Infrastructure Services	That the Street Extension and Trunk Sewer Project on Jarvis (ECP-010-07) <b>BE DEFERRED</b> for a period of 1 year to allow for the completion of the Environmental Assessment and public consultation to be conducted.	
March 29, 2021	CR110/2021	Clerk's File: SP2021		That the report of the Windsor Police Service entitled "Strategic Plan: 2020-2022" <b>BE REFERRED</b> to a future meeting of Council to allow for representatives from the Windsor Police Service to be in attendance and available for questions	
March 29, 2021	CR111/2021	Clerk's File: MU2021		That the "Windsor Utilities Commission 2020 Summary Report: Water + Imagination = Quality of Life" <b>BE REFERRED</b> to a future meeting of Council to allow for representatives from Windsor Utilities Commission to be in attendance and available for questions	

## Outstanding Council Directives Tracking Log

Updated:2022-06-09

Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
April 19, 2021	CR161/2021	SCM 108/2021 & S 9/2021	Commissioner, Economic Development & Innovation	<p>1. That the Community Safety Zone Policy, attached as Appendix 1 of this report, <b>BE ADOPTED</b>.</p> <p>2. That Traffic By-law 9148 <b>BE AMENDED</b> as listed and attached in Appendix 2 of this report.</p> <p>3. That the Commissioner, Legal &amp; Legislative Services BE DIRECTED to prepare the necessary documents to amend the by-law.</p> <p>4. That Administration <b>BE REQUESTED</b> to provide a report back on the cost of adding the nine Business Improvement Areas as Community Safety Zones.</p>	
April 19, 2021	CR165/2021	C 41/2021	Commissioner of Parks, Recreation, Culture and Corporate Facilities	<p>...That Council <b>APPROVE</b> the ongoing operational costs, including an increased Windsor Auxiliary Police presence within the Riverfront Trail with all associated costs related to the pilot project of e-scooters in Parks to be charged to the Parks Operations operating budget in 2021 and funded by the Budget Stabilization Reserve (BSR) during the pilot project period in 2021 as outlined in the financial matters section of this report, and that Administration include a submission on the funds required in the following year as part of the 2022 Operating Budget deliberations and further, That Administration <b>REPORT BACK</b> to Council on the results of the pilot project including ongoing costs after its completion.</p>	
April 19, 2021	CR168/2021	SCM 110/2021 & S 24/2021	Commissioner, Economic Development & Innovation	<p>That \$1,000,000 <b>BE TAKEN</b> from the 2020 yearend operating budget surplus and <b>APPLY THIS AMOUNT</b> to establish a traffic calming capital project budget for traffic calming initiatives; and further,</p> <p>That administration <b>BE DIRECTED</b> to report back to Council with a policy, including a rollout plan, that would allow residents to request speed bumps if the majority in the subject block wish to see that happen.</p>	
May 3, 2021	CR179/2021	C 51/2021	Commissioner, Legal & Legislative Services	<p>That the report of the Senior Legal Counsel and Student-at-Law dated April 9, 2021 entitled "Response to CR591/2020 – Cannabis Odour" <b>BE DEFERRED</b> to allow for a further report once the enforceability of the Town of Leamington's Cannabis Regulation By-law is considered by the Superior Court of Justice and the Normal Farm Practices Protection Board, and that the report also include possible enforcement options that would be available for Council's consideration.</p>	

Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
May 3, 2021	CR182/2021 Clause 4	C 53/2021	Commissioner, Corporate Services Chief Financial Officer & City Treasurer	4. That Administration <b>BE DIRECTED</b> to review the application of discounts applied to the first and second sub-classes of farmland awaiting development and provide recommendations to City Council with regards to the elimination of the tax rate discounts as part of the 2022 Tax Policy report.	
May 17, 2021	CR213/2021	MBA/3183 MBA/2988 &	City Planner	That the correspondence from Paul Mullins on behalf of Assumption Parish dated April 23, 2021 requesting support from the City of Windsor to be recognized by Parks Canada as a Nationally Significant Historic Site, <b>BE REFERRED</b> to administration for review and a report back to Council for consideration, specifically as it would pertain to the ramifications designation would have on city property.	
June 21, 2021	CR285/2021	C 87/2021	Chief Administrative Officer	That Council <b>RECEIVES</b> the report from the Chief Administrative Officer on June 7, 2021, which provides a preliminary organizational review of The Corporation of the City of Windsor; and, That Council <b>DIRECTS</b> Administration to report back to Council with a Corporate Strategic Plan that will provide strategic and tactical objectives that will continue to create an organization that is innovative, agile, collaborative, solution-oriented, efficient, and effective; and, That Council <b>DIRECTS</b> Administration to create an Implementation Playbook for the 20-Year Strategic Vision to ensure the continued rapid realization of its goals; and, That Council <b>DIRECTS</b> Administration to report back to Council with amendments to the Delegated Authority By-law No. 208-2008 to remove “red tape” and enable rapid execution of Council’s vision and direction; and, That Council <b>AMENDS</b> CAO By-law No. 218-2002 to reflect the current organizational structure, i.e. clarifying that the CAO shall recommend to Council the appointment and dismissal of Corporate Leadership Team members and statutory officers but not Department Heads; and, That Council <b>DIRECTS</b> Administration to retain an expert in effective board governance and facilitation, and schedule a closed Council Workshop for education purposes.	



Outstanding Council Directives Tracking Log

Updated:2022-06-09

Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
July 19, 2021	CR321/2021	GCE2021 & ACOQ2021	Commissioner, Legal & Legislative Services	<p>That the correspondence from the Commissioner, Legal &amp; Legislative Services dated July 13, 2021 regarding “Response to CQ11-2021: Ontario Conservation Authority Working Group consultations” <b>BE RECEIVED</b>; and further,</p> <p>That administration <b>BE DIRECTED</b> to schedule an education/information session for Council and administration regarding core services and optional services, in order to allow Council to then issue a submission to the Ontario Conservation Authority Working Group as part of their consultations that are currently underway for both Phase 1 and 2.</p>	

Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
July 19, 2021	CR331/2021 CSPS 155	SCM 223/2021 & S 80/2021	Commissioner, Community Services	<p>That the report from the Cultural Development Coordinator regarding the expansion of the City of Windsor's Poet Laureate program <b>BE RECEIVED</b>; and further,</p> <p>That the Poet Laureate program, established as an ongoing program, <b>BE REBRANDED</b> as the 'Poet Laureate and Storytellers' program; and further,</p> <p>That the program <b>BE UPDATED</b> to include two new positions – Indigenous Storyteller and Multicultural Community Storyteller; and further,</p> <p>That the additional funding required for the expanded program honorariums and programming initiatives in 2021 in the amount of \$9,500 <b>BE CHARGED</b> to the Culture and Events operating budget; and further,</p> <p>That Council <b>PRE-COMMIT</b> a \$9,500 increase to the Culture and Events 2022 operating budget; and further,</p> <p>That Administration <b>BE DIRECTED</b> to seek nominations for recognized Windsor literary and spoken word artists for the position of Indigenous Storyteller and Multicultural Community Storyteller, to be appointed for two-year terms; and further,</p> <p>That the administrative report <b>BE REFERRED</b> to the Diversity Committee for review and comment; and,</p> <p>That additional consultations <b>BE CONDUCTED</b> as appropriate.</p>	
July 26, 2021	CR360/2021 DHSC 310	SCM 243/2021 & S 87/2021	Commissioner, Economic Development & Innovation	<p>1. That Report No. S 87/2021 entitled "Updates to the 2021 Ontario Heritage Act Amendments" <b>BE RECEIVED</b> for information; and,</p> <p>2. That the City Planner <b>BE DIRECTED</b> to prepare amendments to City of Windsor Official Plan, Delegation By-law, and create new Heritage Procedures/By-law, or utilize other policies or tools as required, to address the legislative changes and for Council's consideration.</p>	

## Outstanding Council Directives Tracking Log

Updated:2022-06-09

Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
July 26, 2021	CR363/2021 DHSC 313	SCM 246/2021 & S 71/2021	Commissioner, Economic Development & Innovation	<p>1. That Report No. S 71/2021 updating City Council on the use and implementation of the Brownfield Redevelopment Community Improvement Plan (CIP) and tabling issues to be addressed as part of the CIP update <b>BE RECEIVED</b> for information; and,</p> <p>2. That the City Planner <b>BE DIRECTED</b> to consult with stakeholders regarding potential changes to the Brownfield Redevelopment CIP outlined in Report No. S 71/2021 and prepare any necessary CIP amendments for Council's consideration.</p>	
July 26, 2021	CR365/2021 CR333/2021	C 98/2021	Commissioner, Human & Health Services	<p>...That the Executive Director of Housing and Children's Services <b>BE DIRECTED</b> to develop a proposed model for the Housing Hub project and bring back recommendations to City Council for consideration; and further,</p> <p>That Administration <b>PREPARE</b> an official letter on behalf of City Council to help advocate other levels of government to address funding gaps with respect to mental health and addictions to ensure the new housing hub is a success.</p>	
September 27, 2021	CR387/2021	C 116/2021	Commissioner, Corporate Services Chief Financial Officer & City Treasurer	<p>That the 2020 Annual Investment Compliance Report for the year ending December 31, 2020 <b>BE RECEIVED</b> for information; and further,</p> <p>That City Council <b>SUPPORT</b> the following actions with regards to the development and implementation of an enhanced investment strategy:</p> <p>That Administration <b>BE AUTHORIZED</b> to prepare a Request for Proposal (RFP) for Investment Advisory services; and further,</p> <p>That Administration <b>BE DIRECTED</b> to explore alternative options to traditional financial instruments to maximize overall investment returns for the City; and further,</p> <p>That Administration <b>BE AUTHORIZED</b> to prepare an Expression of Interest (EOI) to seek interest in the development of an in-house solution for managing current and projected cash flows more efficiently through the use of technology; and further,</p> <p>That Administration <b>BE DIRECTED</b> to report back to City Council the results of the above noted actions.</p>	

Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
September 27, 2021	CR402/2021 ETPS 841	SCM 263/2021 & S 69/2021	Commissioner, Infrastructure Services	<p>That the report of the Engineer I dated June 4, 2021 entitled “Alley Standards Development – City Wide” BE RECEIVED for information; and,</p> <p>That Administration BE DIRECTED to develop an Alley Standards Development Committee as outlined in the report; and,</p> <p>That the Alley Standards Development Committee <b>REPORT BACK</b> to Council with a fulsome report regarding alleys across the City, including but not limited to the following:</p> <ul style="list-style-type: none"> <li>• Capital and Annual Costs associated with developing, maintaining and enforcing a set of standards for all of the City’s alleys</li> <li>• Clear Alley Standards</li> <li>• Different Kinds of Alleys</li> <li>• Alley Closure Process</li> <li>• Additional Dwelling Units accessing alleys and required maintenance</li> <li>• Collection of Garbage and Refuse at Curbside in a consistent manner</li> <li>• Data and Statistics related to resources spent on 311 calls in alleyways regarding clean up/rodent or other issues</li> </ul>	
October 4, 2021	CR429/2021 DHSC 326	SCM 296/2021, S 41/2020, AI 7/2021, & AI 10/2021	Commissioner, Economic Development & Innovation	<p>That the report of the Senior Planner – Policy and Special Studies dated February 27, 2021 entitled “Closure of Part of Dodsworth Street, Between Kay Street and Malden Road, Between 5168 and 5180 Malden Road - Applicant: T. Fasan - SAS/5917 - Ward 1” <b>BE REFERRED</b> back to administration to allow administration the opportunity to work with the Applicant to come to an amenable resolution for everyone involved</p>	

## Outstanding Council Directives Tracking Log

Updated:2022-06-09

Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
October 4, 2021	CR432/2021 DHSC 321	SCM 289/2021 & S 75/2021	Commissioner, Economic Development & Innovation	<p>That the City Planner <b>BE DIRECTED</b> to report back to the Development &amp; Heritage Standing Committee before the end of 2021 with the following:</p> <p>a. Official Plan policy options to further enhance and strengthen densification and intensification as part of the City's overarching growth strategy, including timelines, funding and resource options to prepare that background work for an OPA or OP update; and,</p> <p>b. In order to better inform that policy work around, the Chief Planner is requested to report back with options to host one (or more) design charette workshops to co-create with community leaders a vision for a complete community city block that would capture the very best of global placemaking practices when certain density thresholds are achieved.</p>	
October 4, 2021	CR448/2021 ETPS 858	SCM 313/2021 & S 110/2021	Commissioner, Infrastructure Services	<p>That the report of the Manager of Contracts, Maintenance &amp; Field Services dated August 19, 2021 entitled "Response to CQ 5-2021 Pedestrian Walkways – City Wide" <b>BE RECEIVED</b> for information; and,</p> <p>That the addition of signage at pedestrian inter-block walkways in the wintertime <b>BE APPROVED</b>; and,</p> <p>That administration <b>BE DIRECTED</b> to monitor the effectiveness for a period of 2 years; and,</p> <p>That funding in the amount of \$18,600 <b>BE APPROVED</b>; and,</p> <p>That the amount <b>BE FUNDED</b> from the Budget Stabilization Reserve Fund (BSR)</p>	
October 4, 2021	CR452/2021	C 123/2021 & C 129/2021	Commissioner, Infrastructure Services	<p>That the report of the Senior Manager of Engineering dated September 17, 2021 entitled "Festival Plaza Improvement – Final Design – Ward 3" and the report of the Manager of Parks Development dated August 23, 2021 entitled "Waterfront Beacon – Street Car # 351" <b>BE REFERRED</b> back to Administration to allow for a comprehensive report related to how the Festival Plaza and Waterfront Beacon can be linked to the new City Hall Esplanade moving forward; including but not limited to information related to bathroom facilities, food services, and other options, for Council's consideration.</p>	



Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
October 25, 2021	CR461/2021	C 124/2021	Commissioner, Corporate Services / Chief Financial Officer / City Treasurer	<p>That City Council <b>APPROVE</b> the development of a pilot Hybrid Work Program as proposed in Appendix A (attached) for a period of one year starting in October 2021; and,</p> <p>That Administration <b>PROVIDE</b> City Council with a progress report after one year for further recommendations; and,</p> <p>That the draft of the new Hybrid Work Procedure (Appendix B) <b>BE RECEIVED FOR INFORMATION;</b> and,</p> <p>That the White Paper on Future of Working Remotely in Ontario's Single Tier Municipalities dated June 2021 (Appendix C) prepared on behalf of the Regional Single Tier Human Resources Group <b>BE RECEIVED FOR INFORMATION.</b></p>	

## Outstanding Council Directives Tracking Log

Updated:2022-06-09

Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
October 25, 2021	CR466/2021 ETPS 845	SCM 300/2021, SCM 224/2021, C 152/2021 & SCM 350/2021	Commissioner, Economic Development & Innovation	<p>That Report No. 110 of the Windsor Essex County Environment Committee (WECEC) of its meeting held June 24, 2021 indicating: That the proposal from the Windsor-Essex Youth Climate Council for a pop-up temporary separated bike lane on University Avenue BE ENDORSED. <b>BE APPROVED</b>; and,</p> <p>That City Council <b>ENDORSE</b> the creation of a pilot project for temporary separated bike lanes along University Ave. in 2022; and,</p> <p>That Administration <b>CONSULT</b> with external stakeholders including the University of Windsor, the DWBIA, Bike Windsor-Essex and other organizations or groups as appropriate to develop a pilot project for separated bike lanes along University Ave.; and,</p> <p>That Administration <b>BE REQUESTED</b> to report back to Council with a more detailed outline of the pilot project related to consistency including but not limited to financial implications and the following:</p> <ul style="list-style-type: none"> <li>• Administration would engage with the University of Windsor to identify any potential funding or cost-sharing;</li> <li>• Administration, working collaboratively across departments, would develop design alternatives for the University Avenue corridor;</li> <li>• These alternatives would be presented to stakeholders in a public consultation program; and,</li> <li>• Administration would report back to Council with a recommended alternative, results of public consultation, and cost estimates to seek Council's approval for construction as well as for funding approval of associated capital and operating costs.</li> </ul>	

Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
October 25, 2021	CR475/2021 CR452/2021	C 123/2021	Commissioner, Community Services	<p>I. That City Council <b>DIRECT</b> Administration to work with the consultant to develop different costing options for the final detailed design of the Festival Plaza that vary in price, and once completed, undertake public and stakeholder consultations on those designs. This will give Council the opportunity to determine the level of investment that should be made at Festival Plaza; and,</p> <p>II. That City Council <b>DIRECT</b> the Manager of Parks Development to apply for a site-specific amendment to the Official Plan and Zoning By-law 8600 to allow the necessary buildings and structures within Festival Plaza to extend above the crown of the pavement of Riverside Drive as all potential options will require an amendment; and,</p> <p>III. That City Council <b>DIRECT</b> Administration to create a detailed connectivity plan, including public consultation, between the riverfront and the adjacent areas along the CRIP footprint with special consideration for downtown areas. The plan should not include tunnels underneath Riverside Drive but rather should have multiple access points to link the northside of Riverside Drive to the southside of Riverside Drive focusing on pedestrian safety and include options for physical design changes to Riverside Drive; and further,</p> <p>IV. That City Council <b>DIRECT</b> Administration to prepare a comprehensive Council report that incorporates all of the above recommendations.</p>	

## Outstanding Council Directives Tracking Log

Updated:2022-06-09

Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
October 25, 2021	CR476/2021 CR452/2021	C 129/2021	Commissioner, Community Services	<p>That City Council <b>APPROVE</b> the design of the building and the terraces for the Legacy Beacon as the new home for Streetcar No. 351, located on the waterfront North of Riverside Drive at the foot of Caron Avenue in Legacy Park (Appendix A); and,</p> <p>That the City Planner <b>BE DELEGATED</b> the authority to approve the Site Plan Control Application and <b>BE AUTHORIZED</b> to approve minor changes to the design to allow for the construction for a permanent building (Legacy Beacon) to house Streetcar No. 351; and,</p> <p>That City Council <b>APPROVE</b> and pre-commit \$1,000,000 in 2023 from the Central Riverfront Festival Plaza (PFO-003-15) for the incremental construction costs needed at the Legacy Beacon; and further,</p> <p>That Administration <b>REPORT BACK</b> to Council with a business case for the concession/terrace area prior to the 2022 budget deliberation meetings or as soon as possible thereafter.</p>	

Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
November 1, 2021	CR496/2021 CSPS 163	SCM 329/2021 & S 51/2021	Commissioner, Community Services	<p>That the report of the Senior Manager of Facilities dated May 13, 2021 entitled “CQ 4-2021 - Proposal for Council consideration to provide menstrual hygiene products in select municipal buildings free of charge” <b>BE RECEIVED</b> for information; and,</p> <p>That Council <b>APPROVE</b> a 1-year pilot program-Option C as outlined in the administrative report, to provide free menstrual products in public women's, men's, universal and family washrooms at the locations listed below:</p> <ul style="list-style-type: none"> <li>• WFCU Centre</li> <li>• Windsor Water World</li> <li>• Windsor International Aquatic and Training Centre</li> <li>• Capri Pizzeria Recreation Complex</li> <li>• 350 City Hall Square W.</li> <li>• 400 City Hall Square E; and,</li> </ul> <p>That the estimated cost of \$19,000 plus HST <b>BE FUNDED</b> from the City's Budget Stabilization Reserve (BSR) Fund; and further,</p> <p>That the City's Advisory Committees, Boards and Commissions (ABC's) <b>BE MADE AWARE</b> of the pilot program and <b>BE REQUESTED</b> to adopt a similar program at their facilities where applicable; and,</p> <p>That Administration <b>REPORT BACK</b> to Council with a six month interim report to provide a status update, and after one year with the results of the pilot program to request annual operating funds through the 2023 budget process.</p>	



## Outstanding Council Directives Tracking Log

Updated:2022-06-09

Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
November 1, 2021	CR497/2021 CSPS 164	SCM 333/2021 & S 132/2021	Commissioner, Community Services	<p>That the report of the Cultural Development Coordinator and Manager of Culture &amp; Events dated September 16, 2021 entitled "Update of Round 2 of the Arts, Culture and Heritage Fund 2021 – City Wide" <b>BE RECEIVED</b> for information; and,</p> <p>That Administration <b>REPORT BACK</b> during the 2022 budget deliberations with potential increases to the Arts, Culture and Heritage Fund Grant Program funding envelope that matches and keeps pace with growth in the community; and,</p> <p>That Administration <b>BE DIRECTED</b> to investigate the possibility of private sector partnerships to augment the funding envelope that this program delivers to the community.</p>	
November 15, 2021	CR520/2021 ETPS 867	SCM 363/2021 & S 136/2021	Commissioner, Economic Development & Innovation	<p>That the report of Transportation Planning Senior Engineer dated September 27, 2021 entitled "2019 Road Safety Report – City-Wide" <b>BE RECEIVED</b> for information; and further,</p> <p>That the data in this report <b>BE REVIEWED</b> and <b>BROUGHT FORWARD</b> when decisions related to development patterns and other investment in infrastructure are being considered including but not limited to opportunities for development or planning.</p>	
November 15, 2021	CR526/2021	C 132/2021	Commissioner, Corporate Services	<p>That Council <b>APPROVE</b> the award of RFP 41-21, Municipal Property Tax Software Solution, to CentralSquare Canada Software Inc. as the successful and sole proponent, for an initial period of three (3) years, with potential renewals for successive one (1) year periods, provided all terms and conditions remain the same; and...</p> <p>That a funding plan to address annual operating budget impacts of the new tax software <b>BE REFERRED</b> to the 2023 Operating Budget deliberation process if the Pay-As-You-Go funding plan is not approved as part of the 2022 operating budget deliberation process.</p>	

Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
December 13, 2021	B28/2021	C 176/2021	Commissioner, Corporate Services / Chief Financial Officer / City Treasurer	<p>That City Council <b>APPROVE</b> the following recommendations upon the City receiving written confirmation from the Green and Inclusive Community Building grant provider that the City's grant application for the Re-imagined Adie Knox Herman project has been awarded the requested funding:...</p> <p>That City Council <b>DIRECT</b> Administration to report to City Council on options for Adie Knox Herman project funding, should the grant not be awarded in whole or in part.</p>	
December 13, 2021	B34/2021 CR419/2021	C 113/2021	Commissioner, Community Services	That the report of the Project Administrator dated August 27, 2021 entitled "Proposed Artificial Turf Sports Field - Ward 6 & 7" <b>BE REFERRED</b> back to Administration to continue to explore artificial turf options with stakeholders for Council's consideration.	
December 13, 2021	B38/2021	C 171/2021	Commissioner, Corporate Services / Chief Financial Officer / City Treasurer	<p>That this report with regards to the Power to Impose a Tax on Vacant Residential Units <b>BE RECEIVED</b> for information; and further,</p> <p>That City Council <b>APPROVE</b> the development of a Vacant Residential Unit Tax program for the City of Windsor; and further,</p> <p>That Administration <b>BE DIRECTED</b> to conduct further research in terms of designing and implementing a Vacant Residential Unit Tax program inclusive of community consultation and that a fully developed program be brought back to City Council for approval prior to implementation; and further,</p> <p>That City Council <b>APPROVE</b> the use of funding from the Budget Stabilization Reserve to offset any shortfall in the projected revenue up to the \$100,000 that has been included in the 2022 Operating Budget in the event that the implementation of a Vacant Residential Unit Tax program be delayed.</p>	
December 13, 2021	B47/2021	Clerk's File: GH/14271	Commissioner, Human & Health Services	That the Executive Director Housing and Children's Services <b>BE REQUESTED</b> to report back to the Community Services and Parks Standing Committee related to pressures specific to the Rent Assistance Program in 2022	

## Outstanding Council Directives Tracking Log

Updated:2022-06-09

Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
December 20, 2021	CR553/2021	C 190/2021	Commissioner, Corporate Services / Chief Financial Officer / City Treasurer	<p>That the December 7, 2021 report from PSD Citywide Inc. entitled “Improving the Identification, Monitoring, and Tracking of Development Charges Related Projects at the City of Windsor”, which is financed by the Province of Ontario’s Audit and Accountability Fund, <b>BE RECEIVED</b> for information; and,</p> <p>That the City Treasurer <b>BE AUTHORIZED</b> to post the PSD Citywide Inc. reports to the corporate website by no later than January 31, 2022 in accordance with the grant’s requirements; and,</p> <p>That Administration <b>BE DIRECTED</b> to further assess the reports developed by PSD Citywide Inc. AND the recommendations presented in the report to better understand their value and any cost implications, AND where deemed appropriate bring those items forward for consideration in future budgets.</p>	
December 20, 2021	CR555/2021 ETPS 864	SCM 361/2021, C 154/2021 & AI 19/2021	Commissioner, Corporate Services Chief Financial Officer & City Treasurer	<p>1) That City Council <b>RECEIVE</b> the report of the Supervisor of Environmental Sustainability &amp; Climate Change dated October 7, 2021, entitled "Response to Council directive regarding applying a climate lens to the City's purchasing practices – City Wide"; and,</p> <p>2) That City Council <b>APPROVE</b> Administration proceeding with identified strategies for 2021/2022 including:</p> <ul style="list-style-type: none"> <li>• Form an internal Sustainable Purchasing Team;</li> <li>• Update current purchasing procedures/documents to ensure environmental sustainability and climate change is being considered;</li> <li>• Update the City's Sustainable Purchasing Guide (2015) to reflect updates for climate change considerations; and</li> <li>• Join the Canadian Collaboration for Sustainable Procurement for 2021/2022; and,</li> </ul> <p>3) That Administration <b>REPORT BACK</b> to City Council by the third quarter (Q3) 2022 with a report on achievements and possible next steps.</p>	

Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
January 17, 2022	CR19/2022 CR543/2021 Clause 7, 8 & 9	C 141/2021 & AI 1/2022	Commissioner, Economic Development & Innovation	<p>...7. That the annual operating cost requirements <b>BE REFFERED</b> to the 2023 budget.</p> <p>8. That Administration <b>BE DIRECTED</b> to report back to Council with a by-law and policy amendment for Council consideration that provides for options to add signalized traffic management tools at school crossing checkpoints on arterial roadways where they do not currently exist and are not likely to meet the full scope of warrant criterion as is currently applied and that this information <b>BE BROUGHT FORWARD</b> to Council by the 2nd quarter of 2022.</p> <p>9. That Administration <b>BE DIRECTED</b> to report back specifically related to the school crossing at the Cabana and Caron/Clara intersection with potential funding options to install a pedestrian acitivated traffic light at that location.</p>	
January 17, 2022	CR20/2022	C 6/2022	Chief Administrative Officer	<p>That City Council <b>APPROVE</b> waiving the fees associated with the BIA Parklet, Curbside and Sidewalk Cafes for 2022; and,</p> <p>That City Council <b>APPROVE</b> increasing the current 15 minutes free parking via the City of Windsor Parking App to 60 minutes, and also include surface parking lots; and...</p> <p>That the financial impacts from these recommendations <b>BE REPORTED</b> to City Council as part of the 2022 Quarterly Variance reports as COVID 19 pandemic costs; and,</p> <p>That the following recommendation <b>BE DEFERRED</b> to the January 31, 2022 Council Meeting to allow for administration to provide additional information related to the financial impacts of waiving licence fees, and deferring the business licence deadline beyond June 30, 2022, to the next calendar year, including waiving penalties/interest charges:</p>	

## Outstanding Council Directives Tracking Log

Updated:2022-06-09

Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
January 31, 2022	CR33/2022 CSPS 172	SCM 6/2022 & SCM 403/2021	Commissioner, Community Services	<p>That the minutes of the Community Public Art Advisory Committee of its meeting held October 12, 2021 <b>BE RECEIVED</b> for information; and further,</p> <p>That the portion in the subject minutes pertaining to the sculptural tribute honouring music industry pioneer Rosalie Trombley <b>BE REFERRED</b> back to Administration to determine whether a public display can be considered, either at the Chimczuk Museum or the Art Gallery of Windsor, and that this <b>BE REPORTED BACK</b> to Council.</p>	
January 31, 2022	CR36/2022 CSPS 175	SCM 9/2022 & S 144/2021	Commissioner, Human & Health Services	<p>That the report of the Coordinator of Housing Administration &amp; Policy dated October 28, 2021 entitled "Rent Supplement Program Expiries and Mitigation Strategy – City Wide" <b>BE RECEIVED</b> for information; and,</p> <p>That the Corporation of the City of Windsor <b>REQUEST</b> the Province of Ontario and Government of Canada to provide further financial support to extend funding for rent supplement/housing allowance programs and include reference to impacts of the sunseting of rent supplement and housing allowance programs including specific data points; and,</p> <p>That the Association of Municipalities of Ontario (AMO) <b>BE REQUESTED</b> to quantify these impacts province-wide; and,</p> <p>That the resolution <b>BE FORWARDED</b> to Windsor and Essex County Members of Parliament (MPs), Windsor and Essex County Members of Provincial Parliament (MPPs), the Federation of Canadian Municipalities (FCM), the Association of Municipalities of Ontario (AMO), the Ontario Municipal Social Services Association (OMSSA), the Ministry of Municipal Affairs and Housing, and the Premier of Ontario; and further,</p> <p>That, should the sunseting of rent supplement and housing allowance programs proceed, the impacts <b>BE REPORTED</b> to Council prior to the March 31, 2022 deadline along with options to mitigate those impacts.</p>	



Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
February 14, 2022	CR57/2022	C 18/2022	Commissioner, Legal & Legislative Services	<p>That the report of the Commissioner of Legal &amp; Legislative Services dated January 31, 2022 entitled "Response to CQ 24-2021 - By-law to Require All Property Owners to Maintain Their Properties Free from Rodents and Further Researching Best Practices from Other Municipalities - City Wide" <b>BE RECEIVED</b> for information; and further,</p> <p>That administration <b>BE DIRECTED</b> to report back at a future date on the data collected and potential strategies to target the issue of rodents in our community; and further,</p> <p>That Administration <b>BE DIRECTED</b> to initiate an education and awareness campaign so residents are more aware of the existing Rodent Extermination Program.</p>	
February 14, 2022	CR70/2022 CR47/2022 CR542/2021	C 169/2021 & AI 2/2022	Chief Administrative Officer	That the report of the (Acting) Executive Initiatives Coordinator dated October 29, 2021 entitled "Streamlining Approvals to Enable Rapid Execution of Council's Vision and Direction" <b>BE REFERRED</b> to a Strategic Planning Session of Council.	
February 28, 2022	CR82/2022	C 28/2022	Commissioner, Infrastructure Services	<p>That the Street Lighting Policy, attached as Appendix B of this report, <b>BE ADOPTED</b>;</p> <p>That the Local Improvement Policy, attached as Appendix A of this report, <b>BE ADOPTED</b>; and,</p> <p>That the annual operating cost requirements <b>BE REFERRED</b> to the 2023 budget process.</p>	
February 28, 2022	CR83/2022	C 26/2022	Commissioner, Infrastructure Services	<p>That the report of the Engineer III dated February 10, 2022 entitled "A Provisional By-Law for the Repair and Improvement to the McKee Drain – Wards 1 and 2" <b>BE DEFERRED</b> to a future City Council meeting to allow time for Administration to meet with the affected property owner for further discussion; and further,</p> <p>That Administration <b>BE DIRECTED</b> to request funding from the Ministry of Transportation (MTO) for all expenses associated with any repair and improvements (construction works) to the McKee Drain as it provides a drainage outlet to the Rt. Honourable Herb Gray Parkway and was altered due to the development of the Parkway project.</p>	

## Outstanding Council Directives Tracking Log

Updated:2022-06-09

Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
March 21, 2022	CR98/2022	Clerk's File: SW2022	Commissioner, Infrastructure Services	<p>That the correspondence from the Friends of the Court dated February 14, 2022 regarding the condition of Brock Street between Sandwich Street and Peter Street <b>BE RECEIVED</b> for information; and further,</p> <p>That Administration <b>REPORT BACK</b> outlining the cost differential between repaving Brock Street between Sandwich Street and Peter Street during the reconstruction phase of Sandwich Street instead of a stand alone project after the reconstruction, when a response to CQ 3-2022 asked on February 14, 2022 requesting a report outlining how deficient residential roads not in the 10 year capital budget will be addressed is brought forward to Council.</p>	
March 21, 2022	CR102/2022 CR538/2020	C 187/2020 & AI 21/2021	Commissioner, Legal & Legislative Services	<p>That the report of the Senior Legal Counsel dated September 18, 2020 entitled "Council Question CQ23-2019 - Payday Loan Establishments - City Wide" <b>BE RECEIVED</b> for information; and further,</p> <p>That Administration <b>PROCEED</b> with establishing a cross-sectoral committee with the appropriate partners and representatives to acquire local information and develop a strategy to distribute education materials regarding alternative financial options and supports; and,</p> <p>That Administration <b>REPORT BACK</b> to the Community Services Standing Committee after one year with a summary of work completed to date.</p>	
April 11, 2022	CR139/2022	C 20/2022	Commissioner, Legal & Legislative Services	<p>1. That the report of the Policy Analyst dated February 1, 2022 entitled "Cost of Adding the BIAs as Community Safety Zones" <b>BE RECEIVED</b> for information; and,</p> <p>2. That Traffic By-law 9148 <b>BE AMENDED</b> to implement Community Safety Zones in all of the Business Improvement Areas (BIAs); and, that the City Solicitor <b>BE DIRECTED</b> to prepare the necessary documents to amend the by-law; and further,</p> <p>3. That the required ongoing annual maintenance costs associated with the signs installed for Community Safety Zones in the BIAs <b>BE REFERRED</b> to the 2023 Operating Budget, and that the \$48,000 in capital costs <b>BE FUNDED</b> in the manner detailed in the Financial Matters section of the report.</p>	

Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
April 11, 2022	CR150/2022 ETPS 887	SCM 85/2022 & S 29/2022	Commissioner, Infrastructure Services	That Administration <b>BE AUTHORIZED</b> to initiate a Home Flood Protection Program on a pilot basis; and further,  That Administration <b>REPORT BACK</b> to City Council on completion of the pilot program.	
April 11, 2022	CR154/2022	C 61/2022	Commissioner, Legal & Legislative Services	That the results of the email poll authorized by Mayor Drew Dilkins on Wednesday April 6, 2022 approving the following <b>BE CONFIRMED AND RATIFIED:</b>  That Council <b>DIRECTS</b> Administration to <b>SUSPEND</b> the Hybrid Work Policy effective immediately, and to ensure all staff are working in-office no later than May 1, 2022 and for Administration to <b>NOTIFY</b> City Council once a structured and effective implementation plan has been developed such that the Policy is able to be rolled-out in a manner that benefits the Corporation.	
April 25, 2022	CR171/2022	C 54/2022	Commissioner, Legal & Legislative Services	That the report of the (Acting) Licence Commissioner and the Executive Initiatives Coordinator dated March 25, 2022 entitled “Residential Rental Licensing By-law—Wards 1 & 2” and draft by-law, “A By-law Respecting the Licensing of Residential Rental Housing Units,” attached as Appendix A, <b>BE RECEIVED</b> for information; and,  That Council <b>APPROVE</b> the proposed residential rental licensing framework described in this report; and,  That Council <b>APPROVE</b> the attached draft by-law, to be brought into effect upon the final implementation of the approved framework; and further,  That Administration <b>REPORT BACK TO COUNCIL</b> on the results of the two-year pilot study within Wards 1 and 2.	

## Outstanding Council Directives Tracking Log

Updated:2022-06-09

Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
April 25, 2022	CR184/2022 CSPS 181	SCM 112/2022 & S 39/2022	Commissioner, Community Services	<p>That the report of the Manager of Culture &amp; Events dated March 15, 2022 entitled “City of Windsor Lancaster Bomber FM 212 Progress Report 2019-2021 – Ward 3” <b>BE RECEIVED</b> for information; and further,</p> <p>That Administration <b>BE DIRECTED</b> to report to City Council regarding a fundraising strategy, a plan for assembly of the aircraft, and options to display the aircraft to the public once the assembly portion has been completed.</p>	
May 9, 2022	CR195/2022	C 1/2022	Commissioner, Infrastructure Services	<p>That the report of the Engineer II dated January 5, 2022 entitled “Response to CQ13-2021 – Basement Flood Risk Reduction Update – Ward 7” <b>BE RECEIVED</b> for information; and further,</p> <p>That administration <b>BE DIRECTED</b> to report back to Council on what effective monitoring program can be put in place to give early warning in order to mitigate future flooding events and make proper adjustments to the system as needed.</p>	

Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
May 9, 2022	CR202/2022	C 69/2022	Commissioner, Corporate Services	<p>That City Council <b>RECEIVE</b> the information contained in the 2022 Tax Policy Report; and,</p> <p>That City Council <b>APPROVE</b> the following Tax Policy Principles which will be used to calculate the 2022 Final Property Tax Rates:</p> <p>1. That the Optional Tax Classes of office building, shopping center, parking lot/vacant commercial land, and large industrial CONTINUE to be used in the establishment of annual property tax rates.</p> <p>2. That Administration <b>CONTINUE TO MONITOR</b> the application of a Small Business Class and <b>REPORT BACK</b> to City Council for further direction as part of the 2023 Tax Policy report.</p> <p>3. That tax reductions for the first sub-classes of farmland awaiting development (FAD 1) <b>BE REDUCED ANNUALLY</b> by the allowed 10% starting in taxation year 2022 through to taxation year 2025.</p> <p>4. That tax reductions for the second sub-classes of farmland awaiting development (FAD 2) <b>BE ELIMINATED</b>.</p> <p>5. That the Municipality <b>CONFIRM</b> the threshold on the tax level for eligible new construction at 100%...</p>	
May 9, 2022	CR203/2022	C 72/2022	Commissioner, Corporate Services	<p>...That Council <b>APPROVE</b> the Operating &amp; Capital Budget timelines for the development of the 2023 Operating &amp; Capital budgets as outlined in Table A (Operating &amp; Capital Budget Timeline) of this report; and,</p> <p>That Administration <b>BE DIRECTED</b> to bring forward a 2023 Operating Budget that provides options to meet Council's fiscal targets status quo from the 2022 Budget Deliberation process with a 0% tax increase option along with 5% departmental reduction targets being brought forward for each department and that this also apply to all City Agencies, Boards, and Commissions (ABCs); and,...</p>	
May 9, 2022	CR204/2022	C 72/2022	Commissioner, Corporate Services	That Administration <b>BE REQUESTED</b> to bring back recommendations for budget engagement tools for the 2023 Budget Deliberation process for Council's consideration.	



## Outstanding Council Directives Tracking Log

Updated:2022-06-09

Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
May 9, 2022	CR208/2022	SCM 120/2022 & S 14/2022	Commissioner, Infrastructure Services	That the report of the Environment, Transportation & Public Safety Standing Committee of its meeting held April 27, 2022 entitled “Ward 9 Ditch Survey and Inspection of Private Culverts and Private Catch Basins -- Ward 9” <b>BE DEFERRED</b> to a future meeting of Council to allow for affected residents to meet with Administration regarding their specific issues.	
May 9, 2022	CR209/2022	SCM 121/2022 & S 42/2022	Commissioner, Corporate Services	<p>That the report of the Community Energy Plan Administrator dated April 8, 2022 entitled “Science Based Targets for GHG Reduction – City Wide” <b>BE RECEIVED</b> for information; and,</p> <p>That City Council <b>APPROVE IN PRINCIPLE</b> Windsor’s Science Based Targets of a 68% reduction in city-wide emissions (scope 1 and 2) and a 55% reduction in corporate-wide emissions (scope 1 and 2) below 2005 baseline by 2030; and,</p> <p>That City Council <b>APPROVE IN PRINCIPLE</b> a NET ZERO Target for 2050; and.</p> <p>That Administration <b>BE DIRECTED</b> to report back with an updated strategy to reach these targets by November 2023 that considers implementation timelines, resourcing and financial impacts of meeting science-based targets; and further,</p> <p>That Administration <b>BE DIRECTED</b> to send a letter to the County of Essex and City of Detroit requesting their support of Windsor’s Science Based Targets for GHG Reduction.</p>	
May 9, 2022	CR 222/2022	SW2022 15	Commissioner, Corporate Services	That Administration <b>BE DIRECTED</b> to bring back a report before the next winter season, on possible ways that we can address the large gap between the demand for Snow Angels and the number of residents that are assisted. Options should include, but not be limited to improving our recruiting efforts of volunteers and/or providing the service with a set fee or no fee.	

Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
May 30, 2022	CR228/2022	C 82/2022	Commissioner, Community Services	<p>That the report of the Executive Initiatives Coordinator dated May 4, 2022 entitled “CQ 7-2022 - Response to CQ 7-2022 - Expansion of Dog Parks within the City - City Wide” <b>BE RECEIVED</b> for information; and,</p> <p>That Administration <b>BE DIRECTED</b> to begin the process of adding two additional dog parks at Oakwood Park and Elizabeth Kishkon Park after the community has <b>BEEN CONSULTED</b> as outlined in the Dog Park Policy; and,</p> <p>That these additional dog parks <b>BE FUNDED</b> as follows: reallocation of \$750,000 in 2022 PAYG funds and the reallocation and pre-commitment of \$130,000 in 2023 PAYG funds from capital project PFO-009-12 – Parks Bridges/Shelters/Buildings/Capital Rehabilitation Program to capital project PFO-010-17 – Dog Park Development, to be replaced with the transfer of \$880,000 from Fund 151 – Parkland Acquisition Reserve back to capital project PFO-009-12; and further,</p> <p>That Administration <b>REPORT BACK</b> to Council with potential edits to the Dog Park Policy to allow smaller, urban parks to be used as dog parks as well.</p>	

## Outstanding Council Directives Tracking Log

Updated:2022-06-09

Meeting Date	Motion/ Resolution	Report Number	CLT Member/ Executive Director	Action	Status
May 30, 2022	CR246/2022	SCM 145/2022	Commissioner, Corporate Services	<p>That the attached Internal Audit Summary report provided by PricewaterhouseCoopers LLP (PwC) for the period December 1, 2021 to April 30, 2022 comprised of the following:</p> <p>a) Summary of use of unallocated effort  b) Complaints and Investigations  c) Road Infrastructure Maintenance Processes VFM Internal Audit Report  d) Smart City Cyber Risk Mitigation Internal Audit Report  e) Management Action Plan Validation Report  f) Annual Performance Report</p> <p><b>BE RECEIVED</b> for information; and,</p> <p>That City Council <b>AUTHORIZE</b> administration to proceed to implement the Management Action Plans as prescribed in the internal audit reports; and further,</p> <p>That City Council <b>DIRECT</b> administration to report on the progress of the implementation of the Management Action Plans and that such reports should coincide with the Auditor General's internal audit updates to City Council.</p>	
May 30, 2022	CR250/2022 Clause II	C 95/2022	Commissioner, Infrastructure Services	<p>I. That the report of the Project Administrator, Corporate Projects, dated May 18, 2022 entitled "Peace Fountain Replacement – Ward 6" and its appendices (attached) <b>BE RECEIVED</b> for information; and,</p> <p>II. That Council <b>DIRECT</b> Administration to <b>MOVE FORWARD</b> with Option 1: Floating Fountain design concept, and that Administration <b>PERFORM</b> public consultation and <b>BRING FORWARD</b> final design, costing and funding options for presentation to Council for the 2023 Capital Budget; and...</p>	

**Clerk's Note:** The listing of items prior to January 1, 2011 should not be considered complete at this point in time.

**Clerk's Note:** This summary chart is not intended to replace the actual minutes of all proceedings.



planning@erca.org

P.519.776.5209

F.519.776.8688

360 Fairview Avenue West  
Suite 311, Essex, ON N8M 1Y6

June 09, 2022

Council Services Department  
City of Windsor  
350 City Hall Square West, Room 530  
Windsor, ON N9A 6S1

June 13, 2022 Council Agenda  
Item 8.1 - Written Submission

Attention: City of Windsor (Clerk's Office)

RE: Zoning By-Law Amendment: (Z-144-09)  
0 E C ROW AVE E & 2087 BANWELL RD  
ARN 373907089000600, 373909004003403, 373909004003503, 373909004003600,  
373909004003700, 373909004003800, 373909004003900, 373909004004000,  
373909004004100, 373909004004200, 373909004003450, 373909004003465,  
373909004004300, 373909004003402, 373909004003502; PIN: 014080951, 014082109,  
014082056, 014082033, 014081265, 014081259, 014081258, 014081333, 014081332,  
014081256, 014082050, 014082051, 014081255, 014082053, 014082055  
Applicant: City of Windsor

The following is provided, as a result of our review of Zoning By-Law Amendment Z-144-09, to permit a proposed EV battery manufacturing facility to be located at Banwell Road and E.C. Row Avenue East.

While the site is partially zoned to permit manufacturing uses, an amendment to Zoning By-law 8600 is necessary to permit the proposed manufacturing facility across the entire site. It is also desirable to establish flexible land use permissions that minimizes the need for future permissions, as construction and operations begins.

## **DELEGATED RESPONSIBILITY TO REPRESENT THE PROVINCIAL INTEREST IN NATURAL HAZARDS (PPS) AND REGULATORY RESPONSIBILITIES OF THE CONSERVATION AUTHORITIES ACT**

The following comments reflect our role as representing the provincial interest in natural hazards as outlined by Section 3.1 of the Provincial Policy Statement of the *Planning Act*, as well as our regulatory role as defined by Section 28 of the *Conservation Authorities Act*.

The above noted lands are subject to our Development, Interference with Wetlands and Alteration to Shorelines and Watercourses Regulation under the *Conservation Authorities Act* (Ontario Regulation No. 158/06). The parcel falls within the regulated area of the Lachance Drain, Gouin Drain and the Banwell Drain. The property owner will be required to obtain a Permit and/or Clearance from the Essex Region Conservation Authority, prior to any construction or site alteration or other activities affected by Section 28 of the *Conservation Authorities Act*. For Minister's Zoning Order (MZO), an Authority must grant permission to the applicant to carry out the activity, provided an MZO has been made. A permission which is granted, may be subject to conditions, as prescribed by the Authority.

Please be advised that the municipal drains in the area, typically, have an unregistered working space, where the municipality has the right to use to maintain or repair the drain. In addition, specific building

setbacks for the proposed development, from a municipal drain, are applicable. Any proposed modifications / improvements to the municipal drains in this area, may require certain approvals to be completed under the *Drainage Act*.

## **RISK MANAGEMENT AND SOURCE PROTECTION PLAN**

The subject property may lie wholly or partially within the Event Based Area (EBA) of the Essex Region Source Protection Plan, which came into effect October 1, 2015.

The Source Protection Plan was developed to provide measures to protect Essex Region's municipal drinking water sources. As a result of these policies, new projects in these areas may require approval by the Essex Region Risk Management Official (RMO) to ensure that appropriate actions are taken to mitigate any potential drinking water threats.

Should your proposal require the installation of fuel storage on the site, please contact the RMO to ensure the handling and storage of fuel will not pose a significant risk to local sources of municipal drinking water.

The Essex Region's Risk Management Official can be reached by email at [riskmanagement@erca.org](mailto:riskmanagement@erca.org) or 519-776-5209 ext. 214. If a Risk Management Plan has previously been negotiated on this property, it will be the responsibility of the new owner to contact the Essex Region Risk Management Official to establish an updated Risk Management Plan. For any questions regarding Source Water Protection and the applicable source protection plan policies that may apply to the site, please contact the Essex Region Risk Management Official.

## **WATERSHED BASED RESOURCE MANAGEMENT AGENCY**

The following comments are provided in an advisory capacity as a public commenting body on matters related to watershed management.

### **SECTION 1.6.6.7 PPS, 2020 - Stormwater Management**

We are concerned with the potential impact of the quality and quantity of runoff in the downstream watercourse due to future development of this site. We recommend that the municipality ensure through the Site Plan Control process that the release rate for any future development is controlled to the capacity available in the existing storm sewers/drains. In addition, that stormwater quality and stormwater quantity are addressed up to and including the 1:100 year storm event and be in accordance with the guidance provided by the Stormwater Management Planning and Guidance Manual, prepared by the Ministry of the Environment (MOE, March 2003) and the Windsor-Essex Region Stormwater Management Standards Manual.

The Council Report notes that the proposed redevelopment is subject to Site Plan Control (SPC). Therefore, we request to be included in the circulation of the Site Plan Control application. We reserve to comment further on stormwater management concerns, until we have had an opportunity to review the specific details of the proposal through the site plan approval stage.





## **PLANNING ADVISORY SERVICE TO PLANNING AUTHORITIES - NATURAL HERITAGE POLICIES OF THE PPS, 2020**

The following comments are provided from our perspective as an advisory service provider to the Planning Authority on matters related to natural heritage and natural heritage systems, as outlined in Section 2.1 of the Provincial Policy Statement of the *Planning Act*. The comments in this section do not necessarily represent the provincial position and are advisory in nature for the consideration of the Planning Authority.

Our information indicates that the subject property may support habitat of endangered species and threatened species. As per Section 2.1.7 of the PPS, 2020 – “*Development and site alteration shall not be permitted in habitat of endangered species and threatened species, except in accordance with provincial and federal requirements*”. All species listed as endangered or threatened (aquatic species, plants, mammals, birds, reptiles, amphibians, etc.) as well as their related habitats, are protected under the Ontario *Endangered Species Act*. Prior to initiating any proposed works on this property, it is the proponent’s responsibility to contact the Species at Risk Branch of the Ontario Ministry of Environment, Conservation & Parks (MECP) to ensure all issues related to the *Endangered Species Act* are addressed. All inquiries regarding the *Endangered Species Act* should be made with Permissions and Compliance Section of the MECP (e-mail address: [SAROntario@ontario.ca](mailto:SAROntario@ontario.ca)).

### **FINAL RECOMMENDATION**

The ERCA has no further concerns with the proposed zoning by-law amendment application at this time, for the proposed EV battery manufacturing facility. Please advise the ERCA of the outcome of Council's request for an Minister's Zoning Order (MZO) for this proposed development and location, in the City of Windsor.

If you have any questions or require any additional information, please contact the undersigned.

Sincerely,



Kim Darroch, B.A.(Hons.), M.PL., RPP, MCIP  
Team Lead, Planning Services

**From:** CP Proximity-Ontario <[CP\\_Proximity-Ontario@cpr.ca](mailto:CP_Proximity-Ontario@cpr.ca)>

**Sent:** June 6, 2022 3:15 PM

**To:** clerks <[clerks@citywindsor.ca](mailto:clerks@citywindsor.ca)>

**Subject:** RE: Windsor City Council Meeting - Monday June 13, 2022 at 4:00 p.m.

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Good Afternoon,

Canadian Pacific generally does not oppose to commercial and/or non-residential type developments adjacent to our right of way.

Notwithstanding that stated above, any development should still meet certain recommendations based upon site specific conditions and intended use. CP continues to recommend that all proposed developments follow the 2013 Proximity Guidelines (Prepared for The Federation of Canadian Municipalities and The Railway Association of Canada). It is incumbent upon the developer to incorporate these safety measures to a degree that satisfies the requirements of the relevant agencies issuing building and development permits. The 2013 Proximity Guidelines can be found here: <http://www.proximityissues.ca/>

We would appreciate being circulated with all future correspondence related to Residential or Commercial/Industrial developments.

Thank you,

CP Proximity Ontario



**CP Proximity Ontario**

**[CP\\_Proximity-Ontario@cpr.ca](mailto:CP_Proximity-Ontario@cpr.ca)**

7550 Ogden Dale Road SE, Building 1  
Calgary AB T2C 4X9

**From:** Janet Macbeth

**Sent:** June 9, 2022 11:20 AM

**To:** clerks <[clerks@citywindsor.ca](mailto:clerks@citywindsor.ca)>

**Cc:** Dean Jacobs

**Subject:** Re: Request for Provincial Assistance with Zoning for Automotive Battery Manufacturing Facility located at Banwell Road and EC ROW Avenue East (Ward 9) WIFN comments

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Boozhoo Windsor Council and Planning Team,

At the beginning of each of council's meetings there is an acknowledgement about the land and territory. Acknowledgment is needed, but action is needed more. Walpole Island First Nation (WIFN) knows that the timelines are very sensitive to fully securing this project. WIFN is prepared to engage completely with the project proponent, the City of Windsor, and Ontario; but we do have concern that a MZO will shortchange the consultation process with WIFN. We ask for commitments from the City of Windsor that WIFN is fully consulted, accommodated, and provided with meaningful benefits. This project will have a huge impact on the region, and we need to work together so that that impact is a positive one for WIFN. We ask that you make clear to the ministry when requesting the MZO that consultation, accommodation, and the securement of benefits to WIFN is a priority for the City of Windsor if this project is to go forward.

Miigwech,

Janet Macbeth (she/her)  
Consultation Manager  
Walpole Island First Nation

**June 13, 2022 Council Agenda**  
**Item 11.2 - Written Submission Jayme Lauzon**

	2019	House 1	House 2		2020	House 1		House 2		House 3			2021	House 1		House 2		House 3			2022	House 1		House 2		House 3	
Date	Precip (mm)	#cycles	#cycles		Precip (mm)	# cycles	# gallons	# cycles	# gallons	# cycles	# gallons		Precip (mm)	# cycles	# gallons	# cycles	# gallons	# cycles	# gallons		Precip (mm)	# cycles	# gallons	# cycles	# gallons	# cycles	# gallons
JANUARY	27.9				89.2	2513	17591	880	6160				19.9	783	5481	481	3367	1295	9065		6.0	271	1897	254	1778	347	3470
FEBRUARY					14.0	657	4599	444	3108				42.0	315	2205	298	2086	651	6510		49.4	699	4893	506	3542	1442	14420
MARCH	62.3				87.6	1738	12166	777	5439				59.3	1249	8743	609	4263	1209	12090		46.9	1254	8778	612	4284	1407	14070
APRIL	152.1				45.8	976	6832	535	3745				34.8	603	4221	392	2744	500	5000		61.1	1028	7196	493	3451	943	9430
MAY	100				84.9	1253	8771	549	3843				49.9	529	3703	294	2058	466	4660		80.5	982	6874	488	3416	905	9050
JUNE	96.5				70.5	537	3759	333	2331				116.1	785	5495	575	4025	1073	10730		39.7	175	1225	111	777		
JULY	42.4				123.9	710	4970	603	4221				168.9	1261	8827	1217	9364	1851	18510								
AUGUST	16.9				213.3	1727	12089	1540	10780	1207	8449		77.9	360	2520	494	3458	688	6880								
SEPTEMBER	94.3				78.1	697	4879	642	4494	1031	7217		128.7	723	5061	638	4466	1160	11600								
OCTOBER	116.8	607	236		68.8	459	3213	379	2653	801	5607		136.0	1368	9576	914	6398	1751	17510								
NOVEMBER	29.7	943	573		63.5	701	4907	447	3129	1673	11711		45.6	862	6034	533	3731	756	7560								
DECEMBER	48.9	859	470		25.5	930	6510	654	4578	1918	13426		78.0	1598	11186	946	6622	1333	13330								
TOTALS	814.9				965.1	12,898	90,286	7,783	54,481	6,630	46,410		957.1	10,436	73,052	7,391	51,737	12,733	89,131		283.6	4,409	30,863	2,464	17,248	5,044	50,440

**NOTES:**

Pump Spy Device (Sump Pump Smart Outlet) - <https://products.pumpspsy.com/>

Pumpspsy - The volume estimate is based on the average 1/2 HP sump pump with a 10 foot lift. If that pump moves 3,600 gallons per hour (60 gallons a minute).

Data shown is pulled from 3 different residents on Woodward who have Pump Spy Smart Outlet

June 10, 2022

**BY EMAIL**

c.o.: [clerks@citywindsor.ca](mailto:clerks@citywindsor.ca)

To: The Members of Windsor City Council

Honourable Mayor Drew Dilkens

Councillor Fred Francis, Councillor Fabio Costante, Councillor Rino Bortolin, Councillor Chris Holt, Councillor Ed Sleiman, Councillor Jo-Anne Gignac, Councillor Jeewen Gill, Councillor Gary Kaschak, Councillor Kieran McKenzie and Councillor Jim Morrison

**RE: Submissions of the Chippewas of Kettle and Stony Point First Nation and Caldwell First Nation for June 13<sup>th</sup> Windsor (the “City”) City Council Meeting - Request for Provincial Assistance with Zoning for Automotive Battery Manufacturing Facility located at Banwell Road and EC ROW Avenue East**

We write to you together today as the Chiefs of Chippewas of Kettle and Stony Point First Nation and Caldwell First Nation (together, the “**First Nations**”).

On June 8<sup>th</sup> we were made aware that a resolution in respect of the zoning order for the LGES-Stellantis electric vehicle battery plant project (the “**Project**”) was coming before Council today. We write to you today to express our deep concern in respect of the consultation process which has been undertaken to date for this Project. **We ask you to delay voting on the motion proposed today to ensure that there is sufficient time for appropriate consultation and accommodation of our First Nations prior to any decision being made.**

As you know, each of our First Nations are holders of constitutionally protected Aboriginal and Treaty rights. To ensure that these rights are respected our communities require, and Canadian law demands, detailed information and meaningful consultation on projects contemplated in our traditional territories. It is only through this process of consultation that we are able to ascertain the possible impacts which projects may have on our rights. Only when we know what these impacts may be can we suggest practical mitigation measures or negotiate appropriate benefits in relation to these works.

In the case of the Project, we have had no opportunity to consider any of this. We cannot come to you today with specific impact concerns or with proposals for mitigation measures, as we simply do not know what they may be. We have not had opportunity to set foot on the lands proposed for the Project to undertake any analysis whatsoever.

On June 8<sup>th</sup> we received a letter from Mr. Tom Bedford, the Executive Director of the Automotive Battery Office of the Ontario Ministry of Economic Development, Job Creation and Trade (the “**Letter**”) in respect of the status of the Project (*attached as Schedule “A” hereto*).

The Letter sets out the possibility of real impacts caused by the Project and raises some key concerns for our First Nations.



First, Mr. Bedford confirms that archeological field work has been undertaken by a City hired archeologist on the Project site and that a report on the related findings is being prepared for submission to the Province.

We did not have opportunity to engage with the archeologist hired by the City, nor to have our own archeologists undertake independent review. That a report would be finalized and submitted for a project of this nature without first engaging in Indigenous consultation processes is both surprising and unacceptable. Caldwell First Nation has two consulting expert archeologists which are used for works of this nature who are ready and available for the Project. Neither has been engaged.

Second, Mr. Bedford sets out preliminary conclusions which the Ministry has made in respect of species at risk and environmental impacts of the Project. Earlier this year, Chiefs of Chippewas of Kettle and Stony Point First Nation and Caldwell First Nation made requests for funding to facilitate our own site assessment for the Project so that our field technicians could walk the land and undertake preliminary archaeological and environmental scans. The request was denied by the Ministry and to date we have been unable to undertake this work. As a result, we have no preliminary first-hand site assessment information to begin the process of determining what the effects of the Project might be on our communities.

Note that in the Letter Mr. Brown indicates that the Ministry *will* be sharing previously completed studies and surveys. Not only have our communities not had opportunity to review these materials, we have not even *received* them yet. In light of this fact alone, to make any decisions in respect of the Project today would be entirely premature.

As signatories to treaties with Canada, we expect that benefits derived from works undertaken in our treaty lands will be shared with us and harms to our rights will be compensated for. We need to have a full understanding of what impacts the Project will have on our rights, in order determine what shape such measures should take in this case.

We hope we can rely on your support in ensuring that adequate consultation and meaningful accommodation of our First Nations is undertaken in respect of the Project.

Regards,

Chief Mary Duckworth  
Caldwell First Nation

Chief Jason Henry  
Chippewas of Kettle and Stony Point First  
Nation

**Cc:**

Tom Bedford, Executive Director, Automotive Battery Office, Ontario Ministry of Economic Development, Job Creation and Trade

Susan Ecclestone, Director, Species at Risk Branch, Ministry of the Environment, Conservation and Parks

Heather Malcolmson, Director, Client Services and Permissions Branch, Ministry of the Environment, Conservation and Parks

Kathleen O'Neill, Director, Environmental Assessment Branch, Ministry of the Environment, Conservation and Parks

Ian Kerr, Regional Director, Ministry of Municipal Affairs and Housing

Lise Chabot, Director, Indigenous Relations and Ministry Partnerships Branch, Ministry of Indigenous Affairs

Derek Burgess, Project Director, Automotive Battery Office, Ministry of Economic Development, Job Creation and Trade

Patrick Somers, Team Lead, Automotive Battery Office, Ministry of Economic Development, Job Creation and Trade

Greg Atkinson, Senior Planner, City of Windsor

**Schedule "A"**

**CORRESPONDENCE OF MR. TOM BEDFORD, THE EXECUTIVE DIRECTOR OF THE  
AUTOMOTIVE BATTERY OFFICE OF THE ONTARIO MINISTRY OF ECONOMIC  
DEVELOPMENT, JOB CREATION AND TRADE DATE JUNE 8, 2022**

**Ministry of Economic  
Development,  
Job Creation and Trade**

Trade Policy, Industry and  
Investment Division  
21st Floor  
777 Bay Street  
Toronto, ON M5G 2N4

**Ministère du Développement  
économique,  
de la Création d'emplois et du  
Commerce**

Division des politiques  
commerciales, de l'industrie et  
des investissements  
21e étage  
777, rue Bay  
Toronto, ON M5G 2N4



June 8, 2022

To: Philip Lee ([philip.lee@southwindcorp.ca](mailto:philip.lee@southwindcorp.ca));

Attention: Don Richardson ([don@ibabraiding.com](mailto:don@ibabraiding.com)); Waverley Birch ([wbirch@ibabraiding.com](mailto:wbirch@ibabraiding.com)), Emily Ferguson ([emily@ibabraiding.com](mailto:emily@ibabraiding.com)); Samantha Shrubsole ([sam@ibabraiding.com](mailto:sam@ibabraiding.com), Claire Sault ([Claire.Sault@kettlepoint.org](mailto:Claire.Sault@kettlepoint.org)) and ([consultation@kettlepoint.org](mailto:consultation@kettlepoint.org))

Good morning Philip,

I am writing today to provide updates and additional information related to the proposed LGES-Stellantis electric vehicle battery plant project (the "Project") in Windsor, Ontario and to outline the ongoing and future Crown decisions that may impact your community's Aboriginal or treaty rights.

The Ministry of Economic Development, Job Creation and Trade (the "Ministry") will continue to coordinate the communications regarding the Crown's duty to consult Indigenous communities about the Project. However, to be clear, the ministries that have responsibility for issuing permits and approvals with regards to the Project will ensure the duty to consult Indigenous communities associated with their proposed decisions, if triggered, is fulfilled.

As mentioned in my previous correspondence of March 25<sup>th</sup>, and my update of April 16<sup>th</sup>, the proposed Project timelines and key decisions for the Project are becoming clearer, as outlined below.

**Proponents' timelines**

On June 2<sup>nd</sup>, Stellantis and LG Energy Solution announced NextStar Energy Inc. as the official name of its battery joint venture company. The proponents have advised us that their goal is to begin site preparation such as grading and foundation pouring in early August, pending receipt of the necessary permits and approvals. Further, we

understand that their goal is to begin major structural construction in December of this year.

### **Archaeological field work**

As you know from our earlier correspondence, the City of Windsor retained an archaeologist, Dillon Consulting. Dillon Consulting completed the archaeological field work on the unassessed portions of the site on May 10<sup>th</sup>. The archaeologist, is in the process of preparing a report for submission to the Ministry of Heritage, Sport, Tourism, and Cultural Industries. Further information will be provided to your community by Dillon upon the completion and submission of the report.

### **Previous clearances and technical documentation**

To support your consideration of the Project's potential impacts on your community's Aboriginal and treaty rights, we are also sharing several reports and studies that were conducted previously on a large portion of the site. Prior Stage 1-4 archaeological studies on that portion of the site were completed between 2014 and 2021 and species at risk surveys were completed in 2020 and 2021. These archaeological and species survey documents provide background information and indicate that large portions of the site were previously cleared from an archaeological and species at risk perspective.

As the files are large in size, we have created a sharepoint repository for the information, and can work to provide access to the appropriate members of your community and consultation team. Patrick Somers ([Patrick.Somers@Ontario.ca](mailto:Patrick.Somers@Ontario.ca)) from our Automotive Battery Office team, copied on this email, will follow up with your team to provide access to these documents. If needed, he can also work with your team to provide alternative methods to access the documents. Please feel free to have your team contact him directly with any issues or questions about accessing these materials.

### **Site planning and zoning**

I would also like to provide an update on the site planning process and the relevant timelines. On Friday, June 3<sup>rd</sup>, you should have received a notification from the City of Windsor regarding their upcoming Council meeting of Monday, June 13<sup>th</sup>. A copy of that notification is included with this letter. At that meeting, the City of Windsor will be considering a resolution to the Province requesting assistance with zoning to support the Project. If you are interested in providing a written submission for consideration at this meeting or register to provide a delegation at this meeting, please follow the process outlined in the City's notice and contact the City at [clerks@citywindsor.ca](mailto:clerks@citywindsor.ca) . As indicated in the City's notice, written submissions or registrations to participate electronically must be received by noon on Friday, June 10<sup>th</sup>.

If Windsor Council approves the resolution on the 13<sup>th</sup>, I will provide your community with a copy of materials sent to the Province by the City to accompany their resolution as soon as they are received. I anticipate that this will include a planning justification



report, which will include site details and the rationale behind the request. If assistance is sought by the City, Ministry staff would make appropriate recommendations to the Minister regarding a request to the Minister of Municipal Affairs and Housing for a Minister's Zoning Order.

The Ministry will continue consulting with your community in June and July to identify any potential impacts to your community's Aboriginal and treaty rights or other concerns related to the proposed site usage and potential Crown decisions to support the zoning relief. Information your community provides in the City's Council meeting process, if any, would be considered as part of the Crown's duty to consult assessment associated with potential Crown decisions relating to the zoning.

### **Species at Risk**

The Province is committed to protecting species that are at risk and their habitats by providing strong environmental oversight while working to support development opportunities in Ontario. This includes prohibitions in the *Endangered Species Act, 2007* (ESA) against activities that impact species at risk and their habitats as well as provisions that allow activities to proceed with restrictions, mitigations, and other conditions.

Preliminary assessment work of the site is being undertaken by the City of Windsor. Survey work in 2021 pertaining to the eastern sections of the project site concluded that it is unlikely that there are species at risk and therefore unlikely that any development work in that section of the Project site will impact species at risk or their habitat. Those portions of the Project site are primarily used for agriculture or industrial uses. Work carried out this spring and which is ongoing has identified endangered Butler's Gartersnake and its habitat within the southwest corner of the project site. These areas are identified on the map in Appendix A. In addition, information indicates a potential for the presence of the following species and their habitat in the area of the project location:

- Eastern Foxsnake – Carolinian population (endangered)
- Purple Twayblade (threatened)
- Bank Swallow (threatened)
- Barn Swallow (threatened)
- Chimney Swift (threatened)
- Colicroot (endangered)
- Dense Blazing Star (threatened)
- Eastern Meadowlark (threatened)
- Kentucky Coffee-tree (threatened)
- Willowleaf Aster (threatened)

Further survey work to be completed by the end of June 2022 by the City, who will confirm the species at risk or their habitat that are present in the southwest corner of the project site, and if so, the potential impacts of the Project. If it is determined that the Project is likely to affect Butler's Gartersnake or any other species at risk or their

habitat, a permit or some other form of authorization to engage in the Project under the ESA will be required in order for the Project to proceed.

While the Ministry is not aware of any, the Ministry is seeking your community's input on the Project's potential impacts on species at risk in relation to Aboriginal and treaty rights. Should you or your community have any input, questions or concerns about this matter as it relates to the potential impacts to these species at risk, please let us know as soon as possible.

I look forward to continuing to consult with your community about this project, and exploring opportunities for your community's economic participation in the project.

Specifically, I wish to respond to your questions about how the project site was selected, the process for site selection, and how to have the site you have identified be considered for future projects. As a follow-up to that earlier discussion around the site your community is interested in developing, I would propose scheduling a meeting with your community at the earliest opportunity to present an overview of our site selection programs and discuss potential steps. To that end, I will send a separate email with a presentation deck that I would propose to use to guide the potential discussion.

Given the anticipated upcoming Crown decisions, if your community is interested, I would also like to revisit our previous offer to provide capacity funding to your community to support consultation. While we were unable to finalize an agreement prior to the election period, we are able to re-visit that now.

Thank you for your consideration of this letter. The Province will continue to consult with your community as the Project continues to evolve. If you have any concerns or questions at this stage, please contact me at [Tom.Bedford@ontario.ca](mailto:Tom.Bedford@ontario.ca) or my colleague Derek Burgess, Project Director at [Derek.Burgess@ontario.ca](mailto:Derek.Burgess@ontario.ca).

Regards,

Tom Bedford  
Executive Director, Automotive Battery Office  
Ontario Ministry of Economic Development, Job Creation and Trade

**Cc:**

Susan Ecclestone, Director, Species at Risk Branch, Ministry of the Environment, Conservation and Parks

Heather Malcolmson, Director, Client Services and Permissions Branch, Ministry of the Environment, Conservation and Parks

Kathleen O'Neill, Director, Environmental Assessment Branch, Ministry of the Environment, Conservation and Parks

Ian Kerr, Regional Director, Ministry of Municipal Affairs and Housing

Lise Chabot, Director, Indigenous Relations and Ministry Partnerships Branch, Ministry of Indigenous Affairs

Derek Burgess, Project Director, Automotive Battery Office, Ministry of Economic Development, Job Creation and Trade

Patrick Somers, Team Lead, Automotive Battery Office, Ministry of Economic Development, Job Creation and Trade

Greg Atkinson, Senior Planner, City of Windsor

## Appendix A: Project Location and Species at Risk



**By-law 71-2022 LaChance Drain  
Third Reading Only**

**BY-LAW NUMBER 71-2022**

**A PROVISIONAL BY-LAW TO PROVIDE FOR  
THE REPAIR AND IMPROVEMENT OF THE  
LACHANCE DRAIN**

Passed the 13<sup>th</sup> day of June, 2022.

**WHEREAS** the Council of The Corporation of the City of Windsor has procured a Drainage Report for the Lachance Drain as prepared by Dillon Consulting Limited, dated April 12, 2022 ("Drainage Report") which report is attached hereto and forms part of this provisional by-law;

**THEREFORE** the Council of the Corporation of the City of Windsor enacts as follows:

**WHEREAS** the Council of The Corporation of the City of Windsor has procured a Drainage Report for the Lachance Drain as prepared by Dillon Consulting Limited, dated April 12, 2022 ("Drainage Report") which report is attached hereto and forms part of this provisional by-law;

**AND WHEREAS** the affected property owners have been given notice of and a public meeting was held on March 14, 2022 to provide the affected property owners an opportunity to comment;

**AND WHEREAS** the estimated total cost of the drainage works is \$614,000.00;

**AND WHEREAS** \$581,500.00 is the estimated amount to be contributed by the City of Windsor for the construction of the drainage works in accordance with the Special Benefit in the Schedule of Assessment in the Drainage Report;

**AND WHEREAS** the Council of The Corporation of the City of Windsor is of the opinion that the repair and improvement of the Lachance Drain is desirable;

**THEREFORE** the Council of the Corporation of the City of Windsor, pursuant to the provisions of s.45 of the Drainage Act, R.S.O. 1990, hereby enacts as follows:

1. **THAT** the Drainage Report for the New Drain Alignment of a Portion of the Lachance Drain dated April 12, 2022, as prepared by Dillon Consulting Limited, attached hereto as Schedule "A", is hereby adopted and the drainage works as therein indicated and set forth are hereby approved and shall be completed in accordance therewith.
2. **THAT** The Corporation of the City of Windsor's share of the cost of the said drainage works in the amount of \$581,500.00 shall be charged against all of the lands in the City of Windsor.



3. **THAT** this By-law shall come into force upon and take effect on the day of the final passing thereof.

DREW DILKENS, MAYOR

CITY CLERK

First Reading - April 25, 2022  
Second Reading - April 25, 2022  
Third Reading - June 13, 2022

DRAINAGE REPORT  
FOR THE

NEW DRAIN ALIGNMENT OF A PORTION  
OF THE  
LACHANCE DRAIN

IN THE  
CITY OF WINDSOR &  
TOWN OF TECUMSEH



(FINAL)  
12 APRIL 2022  
MARK D. HERNANDEZ, P.ENG.  
DILLON FILE NO. 22-3612

Corporation of the City of Windsor  
Engineering – Design and Development  
350 City Hall Square, Suite 310  
Windsor, Ontario  
N9A 6S1



Drainage Report for the  
**NEW DRAIN ALIGNMENT OF A PORTION OF THE LACHANCE DRAIN**  
In the City of Windsor & Town of Tecumseh

Mayor and Council:

Instructions

The Municipality received a request from Roll No. 090-040-04300 (Pointe East Windsor Ltd.) to repair and improve the Lachance Drain. Council accepted the request under Section 78 of the Drainage Act and on 20 December 2021 appointed Dillon Consulting Limited to prepare a report. The proposed works involves realigning a portion of the existing drain to accommodate a proposed development.

Watershed Description

The Lachance Drain commences along the south side of Intersection Road at Shawnee Road where it flows westerly along a closed channel to approximately the western boundary of Lot 148 where it begins to flow in an open channel until the centre of Lot 140. It then flows southerly for 243 metres. At this point the open channel flows westerly for 797 metres. The open channel drain outlets in to the Little River Drain.

The total length of the drain is approximately 2,442 metres. The watershed area is approximately 82.2 ha (203.1 acres) which consists of approximately 50.9 ha (125.7 acres) within the Town of Tecumseh and 31.3 ha (77.4 acres) within the City of Windsor.

The lands comprising the watershed are under mixed agricultural and residential use. There is little topographic relief. From the Ontario Soil Survey (provided by the Ontario Ministry of Agriculture, Food and Rural Affairs), the principle surficial soil in the study area is described as Brookston Clay. Brookston Clay is characterized as a very slow draining soil type. Most of the agricultural land parcels are systematically tiled.

3200 Deziel Drive  
Suite 608  
Windsor, Ontario  
Canada  
N8W 5K8  
Telephone  
519.948.5000  
Fax  
519.948.5054

## Drain History

The recent history of Engineers' reports for the Lachance Drain follows:

- 3 May 2019 by Mark D. Hernandez, P.Eng.: The report recommended the repair and improvement of the entire drain including brushing, cleaning of existing culverts and the establishment of grass buffer strips.
- 7 September 1988 by Lou Zarlenga, P.Eng.: This report recommends the improvement of the entire drain including the replacement of all culverts. The existing drain including culverts was found to be in disrepair. Further, a proposed residential development at the upstream end of the Lachance Drain required the improvements which were in addition to the maintenance activities. The report included a recommendation that all excess excavated material be trucked away.
- 3 August 1968 by C.G.R. Armstrong, P.Eng.: This report recommended cleaning of the drain to address accumulated sediment.

## On-Site Meeting

An on-site meeting was held on 14 March 2022. A record of this meeting is provided in Schedule 'A-1', which is appended hereto.

## Survey

Our survey was carried out on 3 March 2022. The survey is comprised of the recording of topographic data in the location of proposed new drain.

## Design Considerations

The Design and Construction Guidelines published by the Ontario Ministry of Agriculture, Food and Rural Affairs (OMAFRA) recommends that open drainage systems and farm crossings serving farmlands be designed to effectively contain and convey the peak runoff generated from a storm event having a frequency of occurrence of 1 in 2 years. The new open drain has been designed to have the same conveyance capacity as the existing drain.

In addition, we have reviewed the performance of the realigned drain during a 1:100 year storm event and have found that the hydraulic grade lines are reduced by approximately 0.03m. We have also reviewed the Little River Floodplain Mapping Study which is currently being completed and have found that the revised drain alignment does not fall within an existing floodplain.

We believe that these design standards should provide a reasonable level of service, but it should be clearly understood that runoff generated from large storms or fast snow melts may sometimes exceed the capacity of the proposed systems and result in surface ponding for short periods of time.



## Allowances

In accordance with Sections 29 and 30 of the Drainage Act, we have made a determination of the amount to be paid for damages to the lands and crops (if any) occasioned by the operation of equipment and the disposal of material excavated from the drain. We have not provided a land allowance since the lands occupied by the abandoned drain when filled will become re-usable and no longer encumbered. Where the existing drain is infilled, the regained land will offset the land used for the new alignment and the grass buffer strips. Therefore, no Section 29 allowances have been provided in this report.

For affected lands which are presently occupied by the abandoned Lachance Drain, we have not provided a damage allowance since the said lands occupied by the abandoned drain when filled in become re-usable and are no longer encumbered. For the lands which may be disturbed during the drain excavation works, we have recommended the restoration to original or better than original conditions in lieu of providing a damage allowance.

## Recommendations and Cost Estimate

We recommend the existing drain segment between Station 0+797 and Station 1+706 be replaced with a new drain alignment to accommodate a new land development. The overall length of the realignment is approximately 1,030 metres comprised of an open channel. The proposed open drain shall have a 1.5 metre bottom width with 1.5:1 side slopes so as not to decrease the habitat footprint that the existing drain currently provides and to promote bank stability.

The drain realignment is to be constructed off-line of the existing drain. The banks of the new open channel shall be fully vegetated and stabilized with stone erosion protection prior to connecting the new alignment and infilling the existing drain.

In regards to existing farm tile drainage, it shall be the responsibility of the landowner to provide for the relocation and extension thereof which presently outlet into the drain section to be abandoned. The new outlets into the realigned drain are entirely at the landowner's expense. Where existing farm tiles are directly crossed by the new drain alignment, the Contractor shall be required to provide a new outlet into the realigned drain.


Based on our review of the history, the information obtained during the site meeting and our examination and analysis of the survey data, we recommend that the New Drain Alignment of a Portion of the Lachance Drain be repaired and improved as described below:







Item	Description	Amount
	<u>OPEN DRAIN WORK</u>	
1.	Brushing within existing drain channel from Station 0+797 to Station 1+706 including disposal by stockpiling and burning on adjacent agricultural lands or by trucking off-site.	\$2,500.00
2.	Strip topsoil full depth (minimum 300 mm thickness) over the entire drain realignment cross section width prior to drain excavation (approximately 11,500 m <sup>2</sup> ). All topsoil materials shall be deposited on the lands adjacent to the working corridor and kept separate from drain excavation materials.	\$23,000.00
3.	Excavate new open drain along proposed alignment, approximately 1,030 lineal metres, Station 0+788A to Station 1+818A (approximately 17,720 m <sup>3</sup> ) including trucking to temporary stockpile area on south side of abandoned Lachance Drain.	\$240,000.00
4.	Removal and restoration of existing chainlink fence at Station 0+788A to accommodate drain construction.	\$1,000.00
5.	Remove all vegetation and organic materials from existing drain channel cross section prior to infilling drain. Fill existing drain channel, Station 0+797 to Station 1+706, including compaction (approximately 8,730 m <sup>3</sup> ).	\$70,000.00
6.	Placement of existing topsoil including fine grading over the following:	
	a) On top of the old drain course at 300 mm depth (approximately 7,800 m <sup>2</sup> ).	\$15,600.00
	b) On new drain banks at 50 mm depth (approximately 13,600 m <sup>2</sup> ).	\$27,200.00
7.	Removal and disposal of existing culverts, as follows:	
8.	a) Bridge No. 3-1350 mm diameter, 9.2 m long	\$1,500.00



Item	Description	Amount
	b) Bridge No. 4-1300 mm diameter, 13.9 m long	\$1,500.00
9.	Hydraulic seeding (bonded fibre matrix) of new re-aligned drain channel including 1 metre grass buffer on both sides and full bank slopes from Station 0+788A to 1+818A (approximately 14,630 m <sup>2</sup> ).	\$102,000.00
10.	Supply and installation of stone erosion protection (minimum 300 mm thickness), as follows:	
	a) Drain bank erosion protection on both banks at Station 0+788A (approximately 135 m <sup>2</sup> ).	\$10,800.00
	b) Drain bank erosion protection on both banks at bend at Station 1+414A to Station 1+433A (approximately 240 m <sup>2</sup> ).	\$19,200.00
	c) Drain bank erosion protection on both banks at bend at Station 1+775A to Station 1+818A (approximately 490 m <sup>2</sup> ).	\$39,000.00
11.	Excavation of a 300 mm deep and 2.0 m wide bottom, refuge stilling pool in the channel below the design gradeline (6 m long) immediately downstream of new drain alignment. Also included is a 300 mm thick stone rip rap lining complete with filter fabric underlay.	\$1,600.00
12.	Temporary silt control measures during construction	\$800.00
	SUB-TOTAL – EXCLUDING SECTION 26 COSTS	\$555,700.00
13.	Survey, report, assessment and final inspection (cost portion)	\$23,500.00
14.	Expenses and incidentals (cost portion)	\$1,500.00
15.	ERCA application, review and permit fee	<u>\$800.00</u>
	TOTAL ESTIMATE – LACHANCE DRAIN	\$581,500.00

The estimate provided in this report excludes applicable taxes and was prepared according to current materials and installation prices as of the date of this report. In the event of delays from the time of filing of the report by the Engineer to the time of tendering the work, it is understood that the estimate of

cost is subject to inflation. The rate of inflation shall be calculated using the Consumer Price Index applied to the cost of construction from the date of the report to the date of tendering.

### Assessment of Costs

The individual assessments are comprised of three (3) assessment components:

- i. Benefit (*advantages relating to the betterment of lands, roads, buildings, or other structures resulting from the improvement to the drain*).
- ii. Outlet Liability (*part of cost required to provide outlet for lands and roads*).
- iii. Special Benefit (*additional work or feature that may not affect function of the drain*).

We have assessed the estimated costs against the affected lands and roads as listed in Schedule 'C' under "Value of Special Benefit", "Value of Benefit" and "Value of Outlet." Details of the Value of Special Benefit listed in Schedule 'C' are provided in Schedule 'D'.

### Assessment Rationale

Special Benefit assessments shown in Schedule 'C' were derived as follows:

1. As the proposed works are directly a result of the proposed development and the entire drain was repaired and improved in 2019, the realignment costs and all associated construction and engineering costs for preparation and consideration of this report shall be assessed 100% against the landowner (Pointe East Windsor Ltd.) of Roll No. 090-040-04300.

### Utilities

It may become necessary to temporarily or permanently relocate utilities that may conflict with the construction recommended under this report. In accordance with Section 26 of the Drainage Act, we assess any relocation cost against the public utility having jurisdiction. Under Section 69 of the Drainage Act, the public utility is at liberty to do the work with its own forces, but if it should not exercise this option within a reasonable time, the Municipality will arrange to have this work completed and the costs will be charged to the appropriate public utility.

### Future Maintenance

After completion, the new drain alignment shall be maintained by the City of Windsor for the respective portion of drain located within the municipality at the expense of the lands and road herein assessed in Schedule E,” and in the same relative proportions subject, of course, to any variations that may be made under the authority of the Drainage Act. The assessments are based on an arbitrary amount of \$10,000.00.

We recommend that the costs of future works of repair and maintenance of the New Alignment of a Portion of the Lachance Drain be carried out as described below:

1. For tile main outlet repairs including stone erosion protection as required, at the location of the said main tile outlets, the Drainage Superintendent and/or Engineer may direct the contractor to make these repairs at the expense of the landowner. Private tile repairs shall be assessed 100% against the property on which the said tile exists.
2. Bank failure repairs caused by surface water inlets on abutting lands along the drain shall be assessed 100% to the abutting landowner.

### Drawings and Specifications

Attached to this report is Schedule ‘F’, which are specifications setting out the details of the recommended works and Schedule ‘G’ which represent the drawings that are attached to this report.

Page 1 of 5: Overall Watershed Plan

Page 2 of 5: Detail Plan

Page 3 of 5: Profile

Page 4 of 5: Cross Sections

Page 5 of 5: Miscellaneous Details

### Approvals

The construction and/or improvement to drainage works, including repair and maintenance activities, and all operations connected there are subject to the approval, inspection, by-laws and regulations of all Municipal, Provincial, Federal and other authorities having jurisdiction in respect to any matters embraced by the proposed works. Prior to any construction or maintenance works, the Municipality or proponent designated on the Municipality’s behalf shall obtain

all required approvals/permits and confirm any construction limitations including timing windows, mitigation/off-setting measures, standard practices or any other limitations related to in-stream works.



### Grants

In accordance with the provisions of Sections 85, 86 and 87 of the Drainage Act, a grant in the amount of 33-1/3 percent of the assessment eligible for a grant may be made in respect to the assessment made under this report upon privately owned lands used for agricultural purposes (eligible for farm tax credit). The assessments levied against privately owned agricultural land must also satisfy all other eligibility criteria set out in the Agricultural Drainage Infrastructure Program policies. In this particular circumstance, the entire cost of the work will be levied against Pointe East Windsor Ltd. and therefore, none of the assessed cost is eligible for a grant from the Ministry of Agriculture, Food and Rural Affairs. We are not aware of any lateral drains involved in this work that would not be eligible for a grant. We recommend that application be made to the Ontario Ministry of Agriculture and Food in accordance with Section 88 of the Drainage Act, for this grant, as well as for all other grants for which this work may be eligible.

Respectfully submitted,

DILLON CONSULTING LIMITED

Mark D. Hernandez, P.Eng.

MDH: wlb

Our File: 22-3612





# Meeting Minutes

Subject: Realignment of the Lachance Drain  
 Date: March 14, 2022  
 Location: Virtual Conference Call  
 Our File: 22-3612  
 Distribution: Distribution

## Attendees

Sam Paglia	Drainage Superintendent, Town of Tecumseh
Andrew Dowie	Drainage Superintendent, City of Windsor
Mark Fishleigh	County of Essex
Boro Samcevic	Landowner
Anna Franck	Landowner
Frank Gresch	Landowner
Mark Hernandez	Dillon Consulting Ltd.
Oliver Moir	Dillon Consulting Ltd.

## Notes

Item	Discussion	Action by
1.	Andrew Dowie provided a brief introduction to the project.	
2.	Mark Hernandez provided the following information on the project:	
2.1.	The proposed realignment of the drain is to be along Banwell Road and the railway with the purpose of opening the Pointe East lands for development.	
2.2.	The realigned drain will have the same or better performance than the existing drain.	
2.3.	The report will have an expedited timeline. It is anticipated the report will be submitted this month for consideration where notices for the Meeting to Consider along with the report will be sent to landowners.	
2.4.	The survey has been completed.	
2.5.	The report will include drain history, design considerations, a cost estimate of the proposed work, drawings, specifications and details of how the cost of the work and future work will be assessed.	
2.6.	Since the proposed work is for the benefit of Pointe East Windsor Ltd. only, and the drain was just recently maintained and paid for by the watershed, no cost will be assessed to upstream lands.	
2.7.	There will be two (2) assessment schedules, one for capital costs and one for future maintenance.	
2.8.	The future maintenance schedule for the drain will show the proportions of the cost that upstream landowners will be required to pay.	
3.	Mark Hernandez acknowledged there have been many drainage works on the	

Item	Discussion	Action by
4.	Lachance Drain and that it is solely due to timing of landowners requests. Anna Franck commented on lots of flooding on both sides of Banwell Road. What are the plans for stormwater management for future development?	
4.1.	Mark Hernandez responded that additional flows require a stormwater management plan that has to be submitted to the City for review and approval.	
5.	Boro Samcevic asked if this report was for a development?	
5.1.	Mark Hernandez responded 'yes'.	
6.	Boro Samcevic asked if this report was upgrading the drain for the developer's benefit?	
6.1.	Mark Hernandez responded that the relocation of the drain is for the developer's benefit.	
7.	Boro Samcevic asked if we will be receiving more documents and notices of meetings?	
7.1.	Andrew Dowie indicated that the notice for the Meeting to Consider and the final report will be mailed out to landowners prior to the target date of April 26.	
8.	Boro Samcevic asked who pays the future maintenance assessments should the landowner change?	
8.1.	Mark Hernandez identified that all assessments are tied to the property, not the property owner.	

### Errors and/or Omissions

These minutes were prepared by Oliver Moir who should be notified of any errors and/or omissions.

**"SCHEDULE C"**  
**SCHEDULE OF ASSESSMENT**  
**REALIGNMENT OF A PORTION OF THE LACHANCE DRAIN**  
**CITY OF WINDSOR & TOWN OF TECUMSEH**

**CITY OF WINDSOR**

**PRIVATELY-OWNED - AGRICULTURAL LANDS**

Roll No.	Con.	Description	Area Affected (Acres) (Ha.)		Owner	Special Benefit	Benefit	Outlet	Total Assessment
090-040-04300	3	Plan 65 Lot 13-18 Pt. Lot 12	44.43	17.98	Pointe East Windsor Limited	\$581,500.00	\$0.00	\$0.00	\$581,500.00
Total on Privately-Owned - Agricultural Lands.....						\$581,500.00	\$0.00	\$0.00	\$581,500.00
<b>TOTAL ASSESSMENT (City of Windsor) .....</b>						<b>\$581,500.00</b>	<b>\$0.00</b>	<b>\$0.00</b>	<b>\$581,500.00</b>

	(Acres)	(Ha.)
<b>Total Area:</b>	<b>44.43</b>	<b>17.98</b>

**"SCHEDULE D"**  
**DETAILS OF SPECIAL BENEFIT**  
**REALIGNMENT OF A PORTION OF THE LACHANCE DRAIN**  
**CITY OF WINDSOR**

**SPECIAL BENEFIT ASSESSMENT**  
**(AGRICULTURAL LANDS NON - GRANTABLE)**

Roll No.	Owner	Item Description	Estimated Cost	Cost of Report	Special Benefit
090-040-04300	Pointe East Windsor Ltd.	Costs associated with realignment of the drain	\$555,700.00	\$25,800.00	\$581,500.00
<b>Total Special Benefit Assessment (Agricultural Lands Non-Grantable).....</b>			<b>\$555,700.00</b>	<b>\$25,800.00</b>	<b>\$581,500.00</b>
<b>OVERALL TOTAL SPECIAL BENEFIT ASSESSMENT .....</b>					<b>\$581,500.00</b>

**"SCHEDULE E"**  
**SCHEDULE OF ASSESSMENT FOR FUTURE MAINTENANCE**  
**REALIGNMENT OF A PORTION OF THE LACHANCE DRAIN**  
**CITY OF WINDSOR & TOWN OF TECUMSEH**

**CITY OF WINDSOR**

**MUNICIPAL LANDS:**

Description	Area Affected (Acres) (Ha.)		Owner	Special Benefit	Benefit	Outlet	Total Assessment
Banwell Road	2.45	0.99	City of Windsor	\$0.00	\$468.00	\$306.00	\$774.00
Total on Municipal Lands.....				\$0.00	\$468.00	\$306.00	\$774.00

**PRIVATELY-OWNED - AGRICULTURAL LANDS (GRANTABLE)**

Roll No.	Con.	Description	Area Affected (Acres) (Ha.)		Owner	Special Benefit	Benefit	Outlet	Total Assessment
090-040-04300	3	Plan 65 Lot 13-18 Pt. Lot 12	44.43	17.98	Pointe East Windsor Limited	\$0.00	\$1,018.00	\$1,104.00	\$2,122.00
090-040-03700	3	N. Pt. Lot 141	21.35	8.64	Pointe East Windsor Limited	\$0.00	\$322.00	\$345.00	\$667.00
090-040-03600	3	N. Pt. Lot 140	9.19	3.72	Pointe East Windsor Limited	\$0.00	\$137.00	\$126.00	\$263.00
Total on Privately-Owned - Agricultural Lands (Grantable).....						\$0.00	\$1,477.00	\$1,575.00	\$3,052.00

<b>TOTAL ASSESSMENT (City of Windsor) .....</b>						<b>\$0.00</b>	<b>\$1,945.00</b>	<b>\$1,881.00</b>	<b>\$3,826.00</b>
			(Acres)	(Ha.)					
Total Area:			77.42	31.33					

**TOWN OF TECUMSEH**

**MUNICIPAL LANDS:**

Description	Area Affected (Acres) (Ha.)		Owner	Special Benefit	Benefit	Outlet	Total Assessment
Block 'A'							
Roads	6.08	2.46	Town of Tecumseh	\$0.00	\$156.00	\$753.00	\$909.00
Lands	18.66	7.55	Town of Tecumseh	\$0.00	\$286.00	\$1,390.00	\$1,676.00
Intersection Road	2.84	1.15	Town of Tecumseh	\$0.00	\$73.00	\$353.00	\$426.00
Total on Municipal Lands.....				\$0.00	\$515.00	\$2,496.00	\$3,011.00



**PRIVATELY-OWNED - NON-AGRICULTURAL LANDS:**

Roll No.	Con.	Description	Area Affected (Acres) (Ha.)		Owner	Special Benefit	Benefit	Outlet	Total Assessment
570-45990	2	Pt. Lot 146 RP12A4263 Pt. 1	1.68	0.68	Heather & Daniel Smith	\$0.00	\$12.00	\$59.00	\$71.00
570-46100	3	Pt. Lot 145 RP12R10430 Pt. 2	2.37	0.96	Jean & Alec Fauteux	\$0.00	\$14.00	\$68.00	\$82.00
570-46150	2	Pt. Lot 145 RP12R10430 Pt. 1	0.99	0.40	Emelie & David Pedro	\$0.00	\$10.00	\$49.00	\$59.00
570-46200	3	N. Pt. Lot 144 RP12R11521 Pt. 3	0.82	0.33	Georgeo Ahad	\$0.00	\$10.00	\$48.00	\$58.00
570-46202	3	Pt. Lot 144 RP12R21404 Pts. 1&2	0.59	0.24	Veerpal & Tejpal Sanghera	\$0.00	\$8.00	\$41.00	\$49.00
570-46203	3	Pt. Lot 144 RP12R11521 Pts. 4&5	0.79	0.32	Lisa & John Sisti	\$0.00	\$10.00	\$47.00	\$57.00
570-46205	3	Pt. Lot 144 RP12R11521 Pts. 6&7	0.79	0.32	Dobrovoje Vukovic	\$0.00	\$10.00	\$47.00	\$57.00
570-34700 (Severed Parcel)	3	Pt. Lot 148	0.55	0.22	Clement & Jeannette Lachance	\$0.00	\$8.00	\$39.00	\$47.00

Total on Privately-Owned - Non-Agricultural Lands.....						\$0.00	\$82.00	\$398.00	\$480.00
--	--	--	--	--	--	--------	---------	----------	----------

**PRIVATELY-OWNED - AGRICULTURAL LANDS (GRANTABLE)**

Roll No.	Con.	Description	Area Affected (Acres) (Ha.)		Owner	Special Benefit	Benefit	Outlet	Total Assessment
570-45902	2	Lot 147&148 RP12R1064 Pt. 2	6.47	2.62	2034053 Ontario Limited	\$0.00	\$33.00	\$161.00	\$194.00
570-45950	2	Pt. Lot 146 RP12R4263 Pt. 2 RP12R5826 Pts. 1&2	6.00	2.43	1486044 Ontario Limited	\$0.00	\$31.00	\$149.00	\$180.00
570-45930	2	W. Pt. Lot 147 RP12R6571 Pt. 1	6.18	2.50	1486044 Ontario Limited	\$0.00	\$32.00	\$153.00	\$185.00
570-34700	3	Pt. Lot 148	19.22	7.78	Clement & Jeannette Lachance	\$0.00	\$98.00	\$478.00	\$576.00
570-34550	3	Pt. Lot 147 RP12R13756 Pt. 2	16.31	6.60	Clement Lachance	\$0.00	\$83.00	\$405.00	\$488.00
570-34500	3	N. Lot 144 to N. Pt. Lot 147 RP12R13756 Pt. 1	35.38	14.32	Eugene Lachance	\$0.00	\$181.00	\$879.00	\$1,060.00

Roll No.	Con.	Description	Area Affected (Acres) (Ha.)		Owner	Special Benefit	Benefit	Outlet	Total Assessment
Total on Privately-Owned - Agricultural Lands (Grantable).....						\$0.00	\$458.00	\$2,225.00	\$2,683.00
TOTAL ASSESSMENT (Town of Tecumseh) .....						\$0.00	\$1,055.00	\$5,119.00	\$6,174.00
			(Acres)	(Ha.)					
Total Area:			125.72	50.88					
OVERALL TOTAL ASSESSMENT (City of Windsor & Town of Tecumseh).....						\$0.00	\$3,000.00	\$7,000.00	\$10,000.00
			(Acres)	(Ha.)					
Total Area:			203.14	82.21					

“SCHEDULE F”  
DRAINAGE REPORT FOR THE  
NEW DRAIN ALIGNMENT OF A PORTION OF THE LACHANCE DRAIN  
IN THE CITY OF WINDSOR & TOWN OF TECUMSEH

SPECIAL PROVISIONS - GENERAL

1.0 GENERAL SPECIFICATIONS

The General Specifications attached hereto is part of “Schedule F.” It also forms part of this specification and is to be read with it, but where there is a difference between the requirements of the General Specifications and those of the Special Provisions which follow, the Special Provisions will take precedence.

2.0 DESCRIPTION OF WORK

The work to be carried out under this Contract includes, but is not limited to, the supply of all labour, equipment and materials to complete the following items:

OPEN DRAIN WORK

- Brushing within existing drain channel from Station 0+797 to Station 1+706 including disposal by stockpiling and burning on adjacent agricultural lands or by trucking off-site.
- Strip topsoil full depth (minimum 300 mm thickness) over the entire drain realignment cross section width prior to drain excavation (approximately 11,500 m<sup>2</sup>). All topsoil materials shall be deposited on the lands adjacent to the working corridor and kept separate from drain excavation materials.
- Excavate new open drain along proposed alignment, approximately 1,030 lineal metres, Station 0+788A to Station 1+818A (approximately 17,720 m<sup>3</sup>).
- Removal and restoration of existing chainlink fence at Station 0+788A to accommodate drain construction.
- Remove all vegetation and organic materials from existing drain channel cross section prior to infilling drain. Fill existing drain channel, Station 0+797 to Station 1+706, including compaction (approximately 8,730 m<sup>3</sup>).
- Placement of existing topsoil including fine grading over the following:
  - On top of the old drain course at 300 mm depth (approximately 7,800 m<sup>2</sup>).
  - On new drain banks at 50 mm depth (approximately 12,600 m<sup>2</sup>).

- Removal and disposal of existing culverts, as follows:
  - Bridge No. 3-1350 mm diameter, 9.2 m long
  - Bridge No. 4-1300 mm diameter, 13.9 m long
- Hydraulic seeding of new re-aligned drain channel including 1 metre grass buffer on both sides and full bank slopes from Station 0+788A to 1+833A (approximately 14,630 m<sup>2</sup>).
- Supply and installation of stone erosion protection (minimum 300 mm thickness), as follows:
  - Drain bank erosion protection on both banks at Station 0+788A (approximately 135 m<sup>2</sup>).
  - Drain bank erosion protection on both banks at bend at Station 1+414A to Station 1+433A (approximately 240 m<sup>2</sup>).
  - Drain bank erosion protection on both banks at bend at Station 1+775A to Station 1+818A (approximately 490 m<sup>2</sup>).
- Excavation of a 300 mm deep and 2.0 m wide bottom, refuge stilling pool in the channel below the design gradeline (6 m long) immediately downstream of new drain alignment. Also included is a 300 mm thick stone rip rap lining complete with filter fabric underlay.
- Temporary silt control measures during construction

### 3.0 ACCESS TO THE WORK

Access to the existing drain from Station 1+040 to Station 1+706 and the new alignment from Station 0+800A to 1+818A shall be from the west side of Banwell Road (just south of Bridge No. 5). The Contractor shall make his/her own arrangements for any additional access for his/her convenience. All road areas and grass lawn areas disturbed shall be restored to original conditions at the Contractor's expense. From Station 0+720 to Station 0+800 access to the drain shall be through property Roll No. 090-040-03502 (City of Windsor). The Contractor shall limit activity to be within the working corridors. The existing fence is to be temporarily removed and reinstated following the work. Any damage to the fence is to be at Contractor's expense.

### 4.0 WORKING AREA

For the repair and improvement of the Lachance Drain, the working corridor shall be 12 metres north of the north top of bank from Station 0+788A to Station 1+423A which includes the 1.0 metre grass buffer strip. From Station 1+423A to Station 1+818A the

working corridor shall be 12 metres west of the west top of bank which includes the 1.0 metre grass buffer strip. From Station 1+040 to Station 1+706 on the existing drain the existing 9 metre working corridor on the south side of the drain shall be widened to 25 metres to accommodate the stockpiled material. From Station 0+788 to Station 1+040 on the existing drain the working corridor shall be 9 metres on the east side of the drain on Roll No. 090-040-03600 (Pointe East Windsor Ltd.) to accommodate the filling in of the existing drain. The excess material shall be temporarily stockpiled along the 25 m wide corridor or in an alternative location as agreed to by the Drainage Superintendent and landowner. Banwell Road shall remain open during the construction period and traffic control (found in General Specifications) maintained at all times. Temporary lane closures will only be considered with the approval of the Road Authority.

FROM STA.	TO STA.	PRIMARY (See Note 1)	SECONDARY (See Note 2)
0+788A	1+423A	12 m wide on north side of drain	Sanitary sewer easement on the south side of drain
1+423A	1+818A	12 m wide on west side of drain	Road Conveyance Corridor on east side of drain

Note 1: *Primary working corridor* indicates the access corridor along the side of the drain where excavation and levelling is recommended (unless noted otherwise below and/or in the specifications, as well as all purposes listed for Secondary Working Corridors).

Note 2: *Secondary working corridor* indicates the access corridor alongside the drain where construction equipment may travel for the purpose of trucking, drain bank repairs, tile inlet repairs, surface water inlet repairs, grass buffer strips and other miscellaneous works.

No disposal of fill or levelling of materials shall be permitted within a secondary working corridor. As further specified, use of this secondary working corridor may be further restricted due to site condition. Read all specifications, drawings and/or notes before completing works.

*\*Note: In the event that a landowner owns the property on both sides of the drain, the landowner can choose which side of the drain to place the spoil. The landowner should advise the Drainage Superintendent of their preference of spoil placement before improvements to the drain are made so that the Drainage Superintendent can notify the Contractor in advance. If the landowner selects the opposite side from the identified working corridor, the contractor may temporarily use the selected side of the drain. The permanent working corridor will remain as identified in this report until revised through a future report under the Act.*



## SPECIAL PROVISIONS – OPEN DRAIN

### 5.0 BRUSHING

Brushing shall be carried out on the entire drain within the above identified sections of the drain where required and as specified herein. All brush and trees located within the drain side slopes shall be cut parallel to the side slopes, as close to the ground as practicable. Tree branches that overhang the drain shall be trimmed. Small branches and limbs are to be disposed of by the Contractor along with the other brush. Tree stumps, where removed to facilitate the drain excavation and reshaping of the drain banks, may be burned by the Contractor where permitted; otherwise, they shall be disposed of, off the site. The Contractor shall make every effort to preserve mature trees which are beyond the drain side slopes, and the working corridors. If requested to do so by the Drainage Superintendent, the Contractor shall preserve certain mature trees within the designated working corridors (see Section 4.0).

Except as specified herein, all brush and trees shall be stockpiled adjacent to the drain within the working corridors. Stockpiles shall not be less than 100 m apart and shall be a minimum of 2.0 m from the edge of the drain bank. All brush, timber, logs, stumps, large stones or other obstructions and deleterious materials that interfere with the construction of the drain, as encountered along the course of the drain are to be removed from the drain by the Contractor. Large stones and other similar material shall be disposed of by the Contractor off the site.

Following completion of the work, the Contractor is to trim up any broken or damaged limbs on trees which remain standing, disposing of the branches cut off along with other brush and leaving the trees in a neat and tidy condition. Brush and trees removed from the working area are to be put into piles by the Contractor, in locations where they can be safely burned, and to be burned by the Contractor after obtaining the necessary permits, as required. If, in the opinion of the Drainage Superintendent, any of the piles are too wet or green to be burned, he shall so advise the Contractor to haul away the unburned materials to an approved dump site. Prior to, and during the course of burning operations, the Contractor shall comply with the current guidelines prepared by the Air Quality Branch of the Ontario Ministry of Environment and shall ensure that the Environmental Protection Act is not violated. Since the trees and brush that are cut off flush with the earth surface may sprout new growth later, it is strongly recommended that the Municipality make arrangements for spraying this new growth at the appropriate time so as to kill the trees and brush.

As part of this work, the Contractor shall remove any loose timber, logs, stumps, large stones or other debris from the drain bottom and from the side slopes. Timber, logs, stumps, large stones or other debris shall be disposed of off-site.

## 6.0 NEW OPEN DRAIN CONSTRUCTION

### 6.1 Setting Out

Benchmarks are provided on the attached drawings. From these benchmarks, the Contractor will do his own setting out. The setting out by the Contractor shall include but shall not be limited to the preparation of grade sheets, the installation of centreline stakes, grade stakes, offsets, and sight rails.

If, during the setting out, the Contractor finds a discrepancy in the benchmarks provided by the Engineer in the attached drawings, or is uncertain as to the interpretation of the information provided or the work intended, he shall notify the Engineer immediately for additional verification or clarification before proceeding with construction.

The Contractor shall be responsible for the true and proper setting out of the works and for the correctness of the position, levels, dimensions and alignment of all parts of the work. The Contractor shall take every precaution and to ensure that the property limit is clearly and regularly marked and to have its accuracy confirmed by a professional land surveyor prior to constructing any part of the new drain.

If, at any time during the progress of the works, an error shall appear or arise in the position, levels, dimensions or alignment of any part of the works, the Contractor shall, at his own expense, rectify such error to the satisfaction of the Engineer, unless such error is based on incorrect data supplied in writing by the Engineer.

### 6.2 Profile and Excavation of New Drain Construction

Excavation shall be carried out in accordance with the profile shown on the drawings for the drain relocation. In all cases, the Contractor shall use the benchmarks to establish the proposed grade. However, for convenience, the drawings provide the approximate depth from the surface of the ground and from the existing drain bottom to the proposed grades. The Contractor shall not excavate deeper than the gradelines shown on the drawings.

Should over excavation of the drain bank occur, the Contractor will not be permitted to repair with native material packed into place by the excavator and re-shaped. Should over excavation occur, the Contractor will be required to have a bank repair detail engineered by a Professional Engineer (hired by the Contractor), to ensure long term stability of the bank is maintained. Such repairs shall be subject to approval by the Engineer and will be at no extra cost to the item.

All excavation work shall be done in such a manner as to not harm any vegetation or trees, not identified in this report or by the Drainage Superintendent for clearing. Any damages to trees or vegetation caused by the Contractors work shall be rectified to the satisfaction of the Drainage Superintendent. The Contractor shall exercise caution around existing tile outlets and shall confirm with the property owners that all tiles have been located and tile ends repaired as specified.

#### 6.3 Topsoil Stripping, Salvaging and Re-use on new drain banks

Prior to any drain excavation from Station 0+788A to Station 1+818A, the topsoil shall be stripped across a minimum 12 m width over the proposed new drain location (1,030 m length) and temporarily stockpiled within the designated working corridors. Drain excavation materials shall be stockpiled separately. The salvaged topsoil shall be trucked within the working corridor where it can be later spread and levelled to a minimum 50 mm thickness on the banks of the newly constructed drain alignment and on the existing filled in drain to a minimum 300 mm thickness. It is anticipated that the amount of topsoil stripped will be greater than the amount required to fully dress the banks of the new drain alignment, however if needed, the Contractor may elect to import screened topsoil to complete the work at their expense. Excess topsoil shall not be removed from the site.

#### 6.4 Construction of relocated drain portion offline

The Contractor shall construct the realigned Lachance Drain offline from Station 0+788A continuing upstream to Station 1+818A and stopping short of and without connecting into the abandoned Lachance Drain at Station 0+788. The purpose of the off line drain construction is to fully establish a grass lined channel and stabilize the banks to minimize erosion and sediment transport once the off-line drain is subsequently connected. Over this time period, the abandoned Lachance Drain shall remain open and drainage maintained through the original alignment.

During the construction of the off-line drain, the temporary stockpiled materials along the east and south sides of the abandoned Lachance Drain shall be placed no closer than 2 m from the edge of the drain. Openings to be provided within the stockpiled windrow where necessary to ensure surface drainage is maintained. Prior to constructing the off-line drain, the rock flow check dam shall be installed across the bottom of the drain in accordance with specification OPSD 219.211.

#### 6.5 Stone erosion protection on new drain banks

Stone erosion protection at the drain bend locations, where specified, shall be constructed at same time as the new channel excavation between Station 0+788A and Station 1+818A.

## 6.6 Filling and Levelling of Abandoned Lachance Drain

Native soil materials excavated from the new Lachance Drain alignment shall be used to fill the portion of the abandoned Lachance Drain original alignment. The work may proceed once approval has been given to connect the offline relocated Lachance Drain at Station 1+818A. Prior to the infilling of the open drain, the Contractor shall remove all vegetation, organic debris and topsoil from the existing drain. The native materials used to fill the drain shall be placed in maximum 250 mm loose lifts and compacted with a sheepsfoot type compaction equipment capable of achieving 95% of the maximum standard proctor density or better. For any existing lateral and main tile outlets that may exist within abandoned Lachance Drain, the Contractor shall mark them for future relocation. The relocation of lateral drain tiles is the responsibility of the landowner.

## 7.0 STONE EROSION PROTECTION (SEP)

The Contractor shall supply and install the required quantities of graded stone rip-rap erosion protection materials where specified. All stone to be used for erosion protection shall be 125 - 250 mm clear quarried rock or OPSS 1001 placed over a non-woven filter fabric Terrafix 270R or approved equivalent. Concrete rip-rap will not be permitted.

The minimum thickness requirement of the erosion stone layer is 300 mm with no portion of the filter fabric to be exposed.

## 8.0 ROCK CHECK DAM

Rock check dam shall be installed at the downstream end of the proposed works prior to commencing construction. The location and exact dimensions of the rock check dam will be confirmed with the Drainage Superintendent prior to installation. Installation shall be in accordance with OPSD 219.211 with the modifications to size as discussed with the Drainage Superintendent.

The rock check dam will not be removed until vegetation is established in the new channel or as directed by the Drainage Superintendent.

## 9.0 HYDRAULIC SEEDING OF DRAIN BANKS ON NEW DRAIN CHANNEL

The newly established drain banks and all existing grassed areas disturbed by construction shall be hydraulic mulch seeded as specified herein. The surface shall be predominantly fine and free from weeds and other unwanted vegetation. All other loose surface litter shall be removed and disposed of.

Bonded Fibre Matrix shall consist of thermally refined wood fibers and 10% cross-linked hydro-colloidal tackifiers. It should be 100% biodegradable. The curing period shall be not more than 48 hours. Bonded Fibre Matrix shall be hydraulically applied and after application be capable of adhering to the soil. In a dry state, shall be comprised of not

less than 70% by weight of long, stranded wood fibres held together by organic or mineral bonding agents or both.

Bonded Fibre Matrix shall be applied at a minimum rate of 3,700 kg of dry product per 10,000 m<sup>2</sup>. It shall be thoroughly mixed with water in a hydraulic seeder and mulcher at a rate of 20-30 kg of dry product to 500-600 litres of water to form a homogeneous slurry. Refer to OPSS.PROV 804 for specifications.

Seeding and mulching shall be a one step process in which the seed, fertilizer and hydraulic mulch are applied simultaneously in a water slurry via the hydraulic seeder/mulcher. The materials shall be added to the supply tank while it is being loaded with water. The materials shall be thoroughly mixed into a homogeneous water slurry and shall be distributed uniform, cohesive mat over the prepared surface. The materials shall be measured by mass or by a mass-calibrated volume measurement, acceptable to the Drainage Superintendent.

The hydraulic seeder/mulcher shall be equipped with mechanical agitation equipment capable of mixing the materials into a homogenous state until applied. The discharge pumps and gun nozzles shall be capable of applying the material uniformly. Grass seed shall be Canada No. 1 grass seed mixture meeting the requirements of a Waterway Slough Mixture as supplied by Growmark or approved equal, as follows:

Creeping Red Fescue	20%
Meadow Fescue	30%
Tall Fescue	30%
Timothy	10%
White Clover	10%

Bags shall bear the label of the supplier indicating the content by species, grade and mass. Seed shall be applied at a rate of 200 kg per 10,000 m<sup>2</sup>. Fertilizer shall be 8-32-16 applied at 350 kg per 10,000 m<sup>2</sup>. It shall be in granular form, dry, free from lumps and in bags bearing the label of the manufacturer, indicating mass and analysis. The hydraulic seeding shall be deemed "Completed by the Contractor" when the seed has established in all areas to the satisfaction of the Engineer. Re-seeding and/or other methods required to establish the grass will be given consideration to achieve the end result and the costs shall be incidental to the works.



## GENERAL SPECIFICATIONS

### 1.0 AGREEMENT AND GENERAL CONDITIONS

The part of the Specifications headed "Special Provisions" which is attached hereto forms part of this Specification and is to be read with it. Where there is any difference between the requirements of this General Specification and those of the Special Provisions, the Special Provisions shall govern.

Where the word "Drainage Superintendent" is used in this specification, it shall mean the person or persons appointed by the Council of the Municipality having jurisdiction to superintend the work.

Tenders will be received and contracts awarded only in the form of a lump sum contract for the completion of the whole work or of specified sections thereof. The Tenderer agrees to enter into a formal contract with the Municipality upon acceptance of the tender. The General Conditions of the contract and Form of Agreement shall be those of the Stipulated Price Contract CCDC2-Engineers, 1994 or the most recent revision of this document.

### 2.0 EXAMINATION OF SITE, PLANS AND SPECIFICATIONS

Each tenderer must visit the site and review the plans and specifications before submitting his/her tender and must satisfy himself/herself as to the extent of the work and local conditions to be met during the construction. Claims made at any time after submission of his/her tender that there was any misunderstanding of the terms and conditions of the contract relating to site conditions, will not be allowed. The Contractor will be at liberty, before bidding to examine any data in the possession of the Municipality or of the Engineer.

The quantities shown or indicated on the drawings or in the report are estimates only and are for the sole purpose of indicating to the tenderers the general magnitude of the work. The tenderer is responsible for checking the quantities for accuracy prior to submitting his/her tender.

### 3.0 MAINTENANCE PERIOD

The successful Tenderer shall guarantee the work for a period of one (1) year from the date of acceptance thereof from deficiencies that, in the opinion of the Engineer, were caused by faulty workmanship or materials. The successful Tenderer shall, at his/her own expense, make good and repair deficiencies and every part thereof, all to the satisfaction of the Engineer. Should the successful Tenderer for any cause, fail to do so, then the Municipality may do so and employ such other person or persons as the Engineer may deem proper to make such repairs or do such work, and the whole costs,

charges and expense so incurred may be deducted from any amount due to the Tenderer or may be collected otherwise by the Municipality from the Tenderer.

#### 4.0 GENERAL CO-ORDINATION

The Contractor shall be responsible for the coordination between the working forces of other organizations and utility companies in connection with this work. The Contractor shall have no cause of action against the Municipality or the Engineer for delays based on the allegation that the site of the work was not made available to him by the Municipality or the Engineer by reason of the acts, omissions, misfeasance or non-feasance of other organizations or utility companies engaged in other work.

#### 5.0 RESPONSIBILITY FOR DAMAGES TO UTILITIES

The Contractor shall note that overhead and underground utilities such as hydro, gas, telephone and water are not necessarily shown on the drawings. It is the Contractor's responsibility to contact utility companies for information regarding utilities, to exercise the necessary care in construction operations and to take other precautions to safeguard the utilities from damage. All work on or adjacent to any utility, pipeline, railway, etc., is to be carried out in accordance with the requirements of the utility, pipeline, railway, or other, as the case may be, and its specifications for such work are to be followed as if they were part of this specification. The Contractor will be liable for any damage to utilities.

#### 6.0 CONTRACTOR'S LIABILITY

The Contractor, his/her agents and all workmen or persons under his/her control including sub-contractors, shall use due care that no person or property is injured and that no rights are infringed in the prosecution of the work. The Contractor shall be solely responsible for all damages, by whomsoever claimable, in respect to any injury to persons or property of whatever description and in respect of any infringement of any right, privilege or easement whatever, occasioned in the carrying on of the work, or by any neglect on the Contractor's part.

The Contractor, shall indemnify and hold harmless the Municipality and the Engineer, their agents and employees from and against claims, demands, losses, costs, damages, actions, suits, or proceedings arising out of or attributable to the Contractor's performance of the contract.

#### 7.0 PROPERTY BARS AND SURVEY MONUMENTS

The Contractor shall be responsible for marking and protecting all property bars and survey monuments during construction. All missing, disturbed or damaged property bars and survey monuments shall be replaced at the Contractor's expense, by an Ontario Land Surveyor.

## 8.0 MAINTENANCE OF FLOW

The Contractor shall, at his/her own cost and expense, permanently provide for and maintain the flow of all drains, ditches and water courses that may be encountered during the progress of the work.

## 9.0 ONTARIO PROVINCIAL STANDARDS

Ontario Provincial Standard Specifications (OPSS) and Ontario Provincial Standard Drawings (OPSD) shall apply and govern at all times unless otherwise amended or extended in these Specifications or on the Drawing. Access to the electronic version of the Ontario Provincial Standards is available online through the MTO website, free of charge to all users. To access the electronic standards on the Web, go to <http://www.mto.gov.on.ca/english/transrd/>. Under the title Technical Manuals is a link to the Ontario Provincial Standards. Users require Adobe Acrobat to view all pdf files.

## 10.0 APPROVALS, PERMITS AND NOTICES

The construction of the works and all operations connected therewith are subject to the approval, inspection, by-laws and regulations of all Municipal, Provincial, Federal and other authorities having jurisdiction in respect to any matters embraced in this Contract. The Contractor shall obtain all approvals and permits and notify the affected authorities when carrying out work in the vicinity of any public utility, power, underground cables, railways, etc.

## 11.0 SUBLETTING

The Contractor shall keep the work under his/her personal control, and shall not assign, transfer, or sublet any portion without first obtaining the written consent of the Municipality.

## 12.0 TIME OF COMPLETION

The Contractor shall complete all work on or before the date fixed at the time of tendering. The Contractor will be held liable for any damages or expenses occasioned by his/her failure to complete the work on time and for any expenses of inspection, superintending, re-tendering or re-surveying, due to their neglect or failure to carry out the work in a timely manner.

## 13.0 TRAFFIC CONTROL

The Contractor will be required to control vehicular and pedestrian traffic along roads at all times and shall, at his/her own expense, provide for placing and maintaining such barricades, signs, flags, lights and flag persons as may be required to ensure public safety. The Contractor will be solely responsible for controlling traffic and shall appoint a representative to maintain the signs and warning lights at night, on weekends and

holidays and at all other times that work is not in progress. All traffic control during construction shall be strictly in accordance with the Occupational Health and Safety Act and the current version of the Ontario Traffic Manuals. Access to the electronic version of the Ontario Traffic Manual is available online through the MTO website, free of charge to all users. To access the electronic standards on the Web, go to <http://www.mto.gov.on.ca/english/transrd/>, click on "Library Catalogue," under the "Title," enter "Ontario Traffic Manual" as the search. Open the applicable "Manual(s)" by choosing the "Access Key," once open look for the "Attachment," click the pdf file. Users require Adobe Acrobat to view all pdf files.

Contractors are reminded of the requirements of the Occupational Health and Safety Act pertaining to Traffic Protection Plans for workers and Traffic Control Plan for Public Safety.

#### 14.0 SITE CLEANUP AND RESTORATION

As part of the work and upon completion, the Contractor shall remove and dispose of, off-site any loose timber, logs, stumps, large stones, rubber tires, cinder blocks or other debris from the drain bottom and from the side slopes. Where the construction works cross a lawn, the Contractor shall take extreme care to avoid damaging the lawn, shrubs and trees encountered. Upon completion of the work, the Contractor shall completely restore the area by the placement and fine grading of topsoil and seeding or sodding the area as specified by the Engineer or Drainage Superintendent.

#### 15.0 UTILITY RELOCATION WORKS

In accordance with Section 26 of the Drainage Act, if utilities are encountered during the installation of the drainage works that conflict with the placement of the new culvert, the operating utility company shall relocate the utility at their own costs. The Contractor however will be responsible to co-ordinate these required relocations (if any) and their co-ordination work shall be considered incidental to the drainage works.

#### 16.0 FINAL INSPECTION

All work shall be carried out to the satisfaction of the Drainage Superintendent for the Municipality, in compliance with the specifications, drawings and the Drainage Act. Upon completion of the project, the work will be inspected by the Engineer and the Drainage Superintendent.

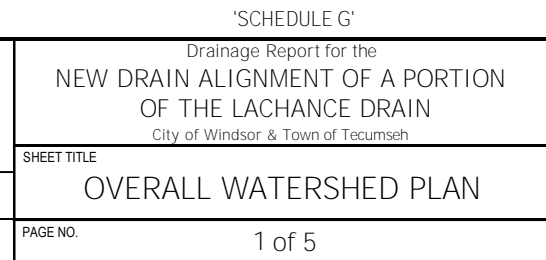
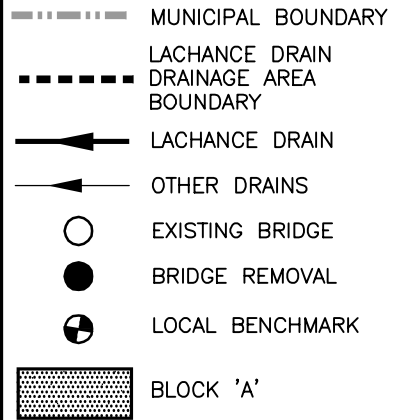
Any deficiencies noted during the final inspection shall be immediately rectified by the Contractor.

Final inspection will be made by the Engineer within 20 days after the Drainage Superintendent has received notice in writing from the Contractor that the work is completed, or as soon thereafter as weather conditions permit.

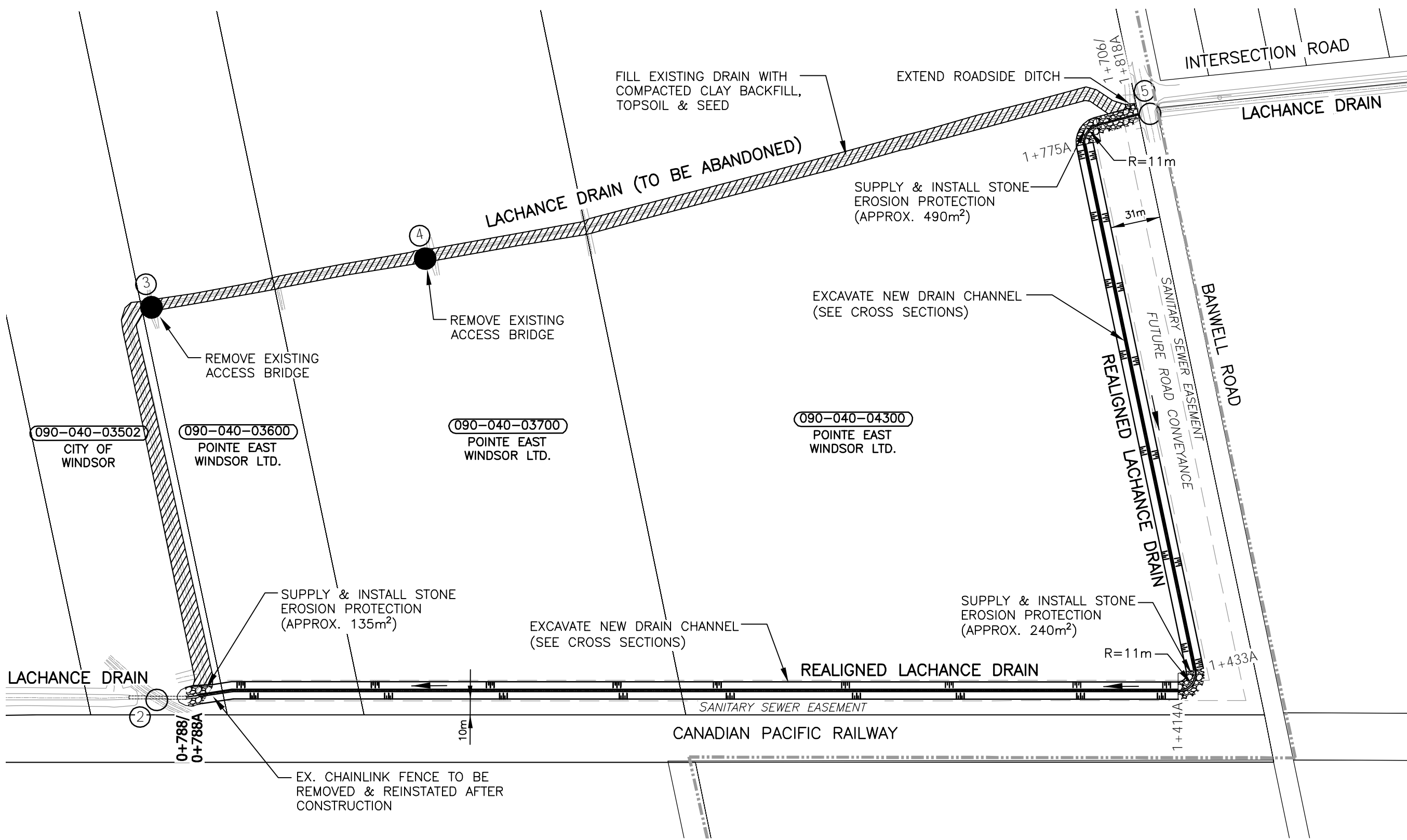
## 17.0 FISHERIES CONCERNS

Standard practices to be followed to minimize disruption to fish habitat include embedment of the culvert a minimum 10% below grade, constructing the work 'in the dry' and cutting only trees necessary to do the work (no clear-cutting). No in-water work is to occur during the timing window unless otherwise approved by the appropriate authorities.





Apr 12, 2022 - 1:29pm C:\pw working directory\projects 2022\dillon\_33\wb\dms08904\223612-03-DRN-CON.dwg



DETAIL  
SCALE=1:2,500



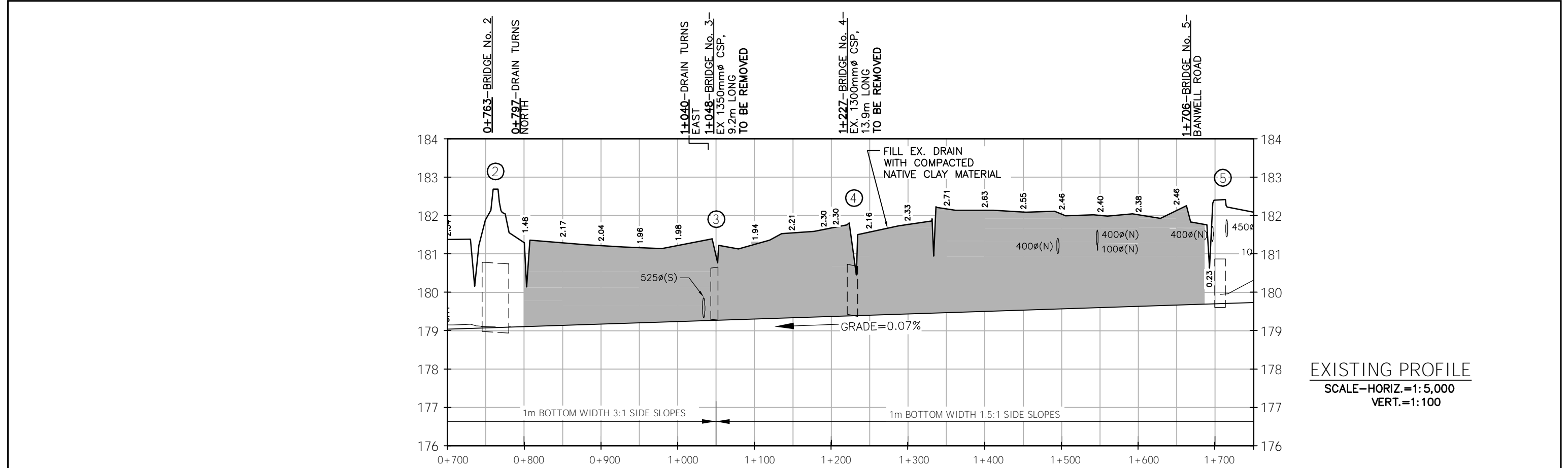
Conditions of Use  
Verify elevations and/or dimensions on drawing prior to use. Report any discrepancies to Dillon Consulting Limited.  
Do not scale dimensions from drawing.  
Do not modify drawing, re-use it, or use it for purposes other than those intended at the time of its preparation without prior written permission from Dillon Consulting Limited.

1	FINAL REPORT SUBMISSION	APR. 12/22	MDH		
No.	Consolidated Council Agenda - June 13, 2022	DATE	BY		

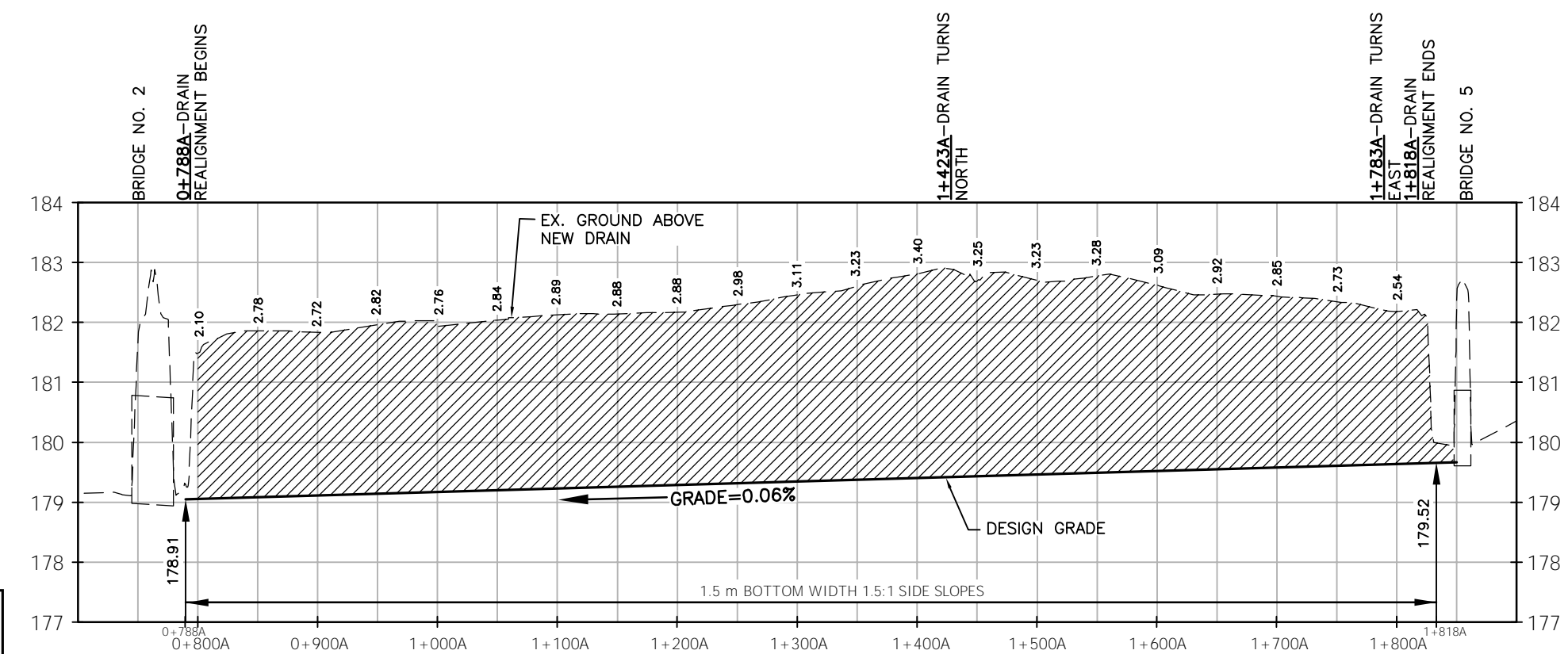
DESIGN	MDH	REVIEWED BY	CDP
DRAWN	WLB	CHECKED BY	TRO
DATE	April 12, 2022		
SCALE	AS SHOWN		

  
**DILLON CONSULTING**  
PROJECT NO. 22-3612  
DRAWING SCALES BASED ON A 11" X 17" SHEET

'SCHEDULE G'  
Drainage Report for the  
**NEW DRAIN ALIGNMENT OF A PORTION OF THE LACHANCE DRAIN**  
City of Windsor & Town of Tecumseh  
SHEET TITLE **DETAIL PLAN**  
PAGE NO. **2 of 5**




EXISTING PROFILE  
SCALE-HORIZ.=1: 5,000  
VERT.=1:100

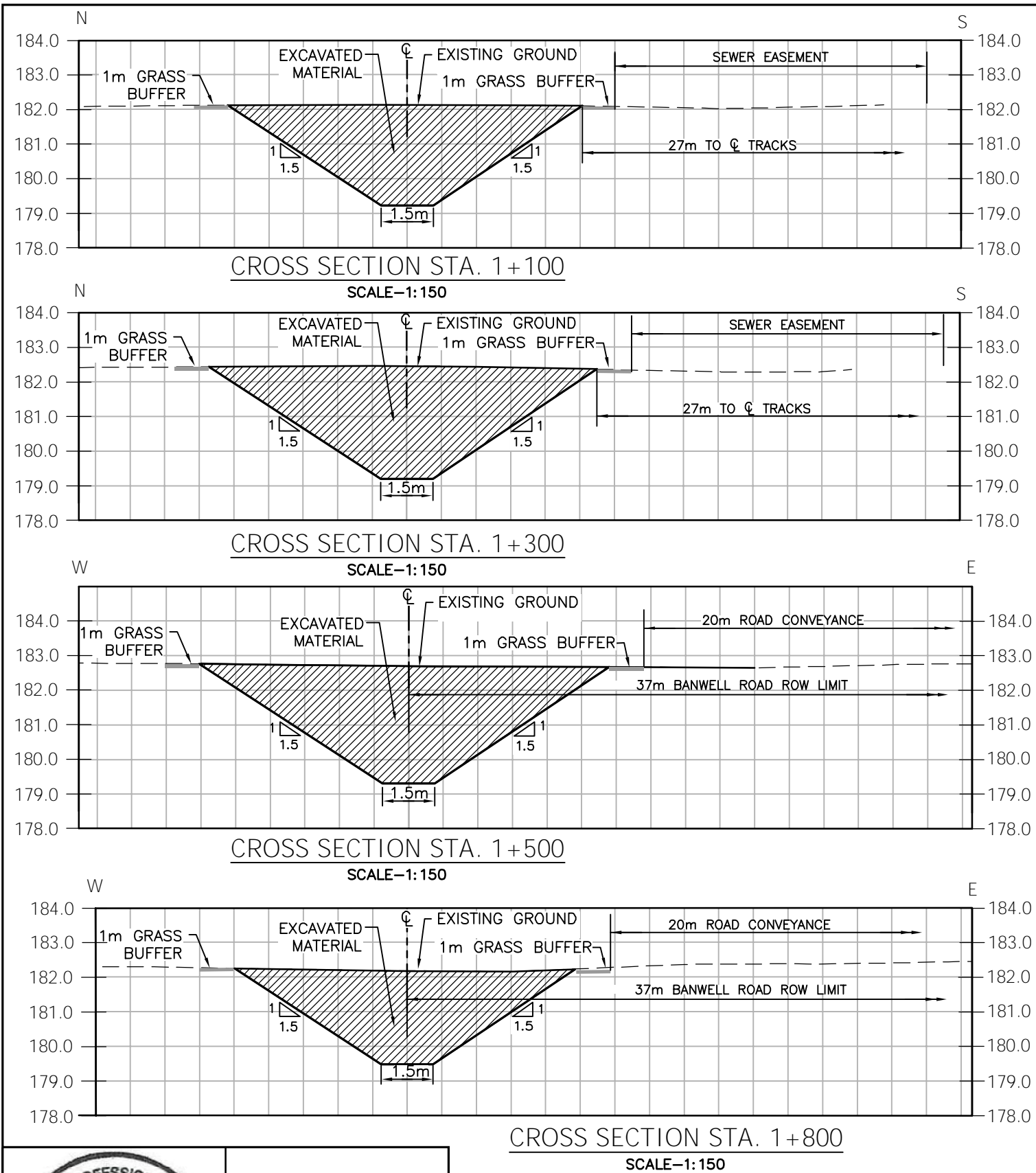


PROPOSED REALIGNED  
PROFILE  
SCALE-HORIZ.=1: 5,000  
VERT.=1:100



<div>Conditions of Use</div> <div>Verify elevations and/or dimensions on drawing prior to use. Report any discrepancies to Dillon Consulting Limited.</div> <div>Do not scale dimensions from drawing.</div> <div>Do not modify drawing, re-use it, or use it for purposes other than those intended at the time of its preparation without prior written permission from Dillon Consulting Limited.</div>					DESIGN	REVIEWED BY		Drainage Report for the	
					MDH	CDP		NEW DRAIN ALIGNMENT OF A PORTION	
					DRAWN	CHECKED BY		OF THE LACHANCE DRAIN	
					WLB	TRO		City of Windsor & Town of Tecumseh	
					DATE		PROJECT NO.	SHEET TITLE	
					April 12, 2022				
	1	FINAL REPORT SUBMISSION		APR. 12/22	MDH	SCALE		DRAWING SCALES BASED ON A 11" X 17" SHEET	PAGE NO.
No.	Consolidated Council Agenda - June 13, 2022			DATE	BY	AS SHOWN			





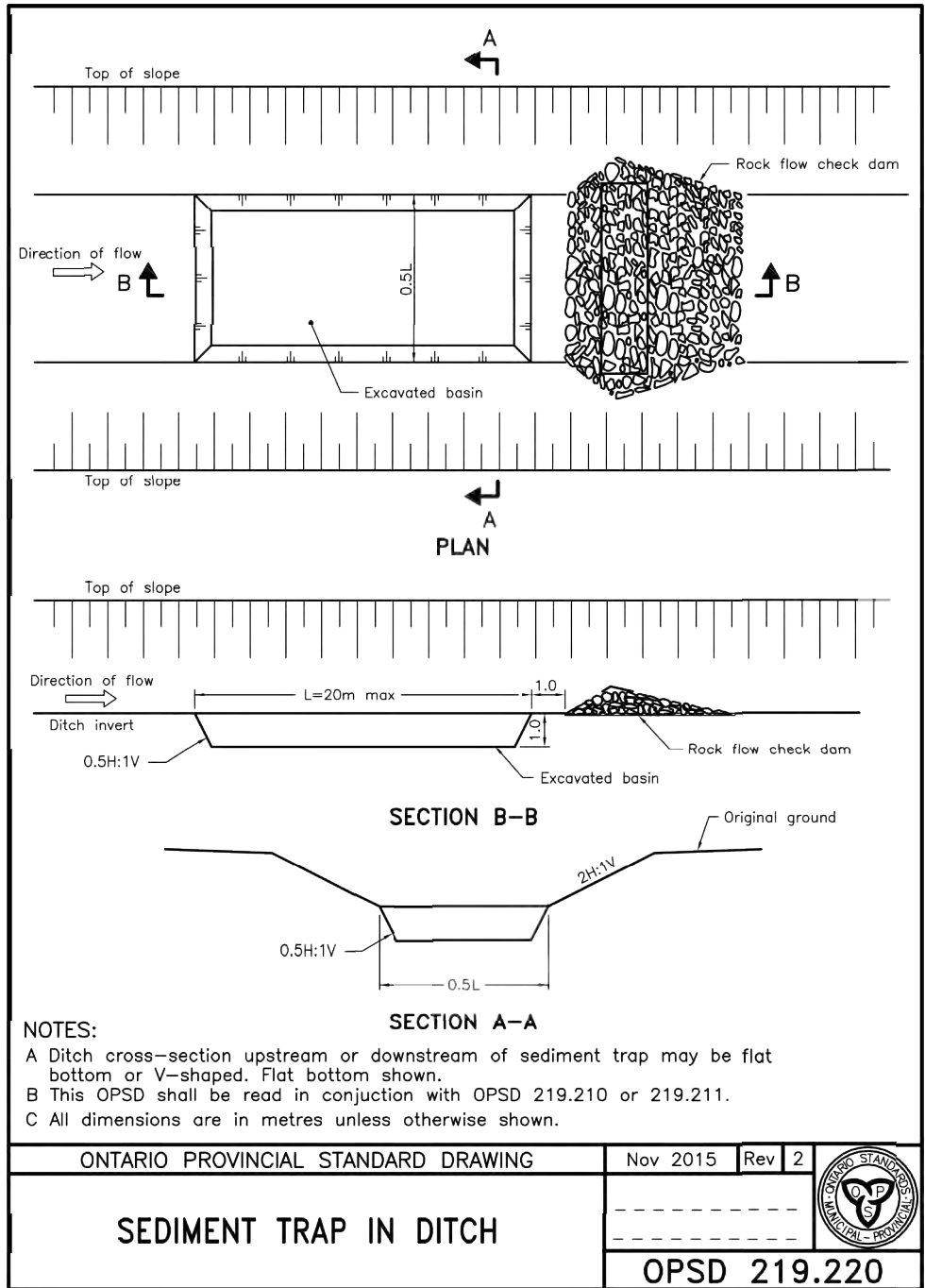
**SITE BENCHMARKS**

BM1—PKNAIL IN TOP OF CONCRETE MANHOLE ON WEST SIDE OF BANWELL ROAD AT INTERSECTION ROAD. PKNAIL IS 0.4M WEST OF THE EDGE AND 0.7M SOUTH OF THE NORTH EDGE AT APPROXIMATELY STA. 1+690  
ELEVATION=182.774m

BM2— TOP OF SANITARY MANHOLE RIM AT APPROX. STA. 1+000A IMMEDIATELY SOUTH OF NEW DRAIN ALIGNMENT. ELEVATION=182.54m

BM3— TOP OF SANITARY MANHOLE RIM AT APPROX. STA. 1+150A IMMEDIATELY SOUTH OF NEW DRAIN ALIGNMENT. ELEVATION=182.62m

**NOTE: CONTRACTOR TO VERIFY BENCHMARKS PRIOR TO CONSTRUCTION.**



**NOTES:**

A Ditch cross-section upstream or downstream of sediment trap may be flat bottom or V-shaped. Flat bottom shown.

B This OPSD shall be read in conjunction with OPSD 219.210 or 219.211.

C All dimensions are in metres unless otherwise shown.

ONTARIO PROVINCIAL STANDARD DRAWING	Nov 2015	Rev 2	
SEDIMENT TRAP IN DITCH			
OPSD 219.220			

Conditions of Use

Verify elevations and/or dimensions on drawing prior to use. Report any discrepancies to Dillon Consulting Limited.

Do not scale dimensions from drawing.

Do not modify drawing, re-use it, or use it for purposes other than those intended at the time of its preparation without prior written permission from Dillon Consulting Limited.

1	FINAL REPORT SUBMISSION	APR. 12/22	MDH	DESIGN	MDH	REVIEWED BY	CDP		PROJECT NO. 22-3612	DRAWING SCALES BASED ON A 11" X 17" SHEET	SHEET TITLE NEW DRAIN ALIGNMENT OF A PORTION OF THE LACHANCE DRAIN City of Windsor & Town of Tecumseh
No.	Consolidated Council Agenda - June 13, 2022	DATE	BY	DRAWN	WLB	CHECKED BY	TRO				
				DATE	April 12, 2022						
				SCALE	AS SHOWN			PAGE NO. 4 of 5			

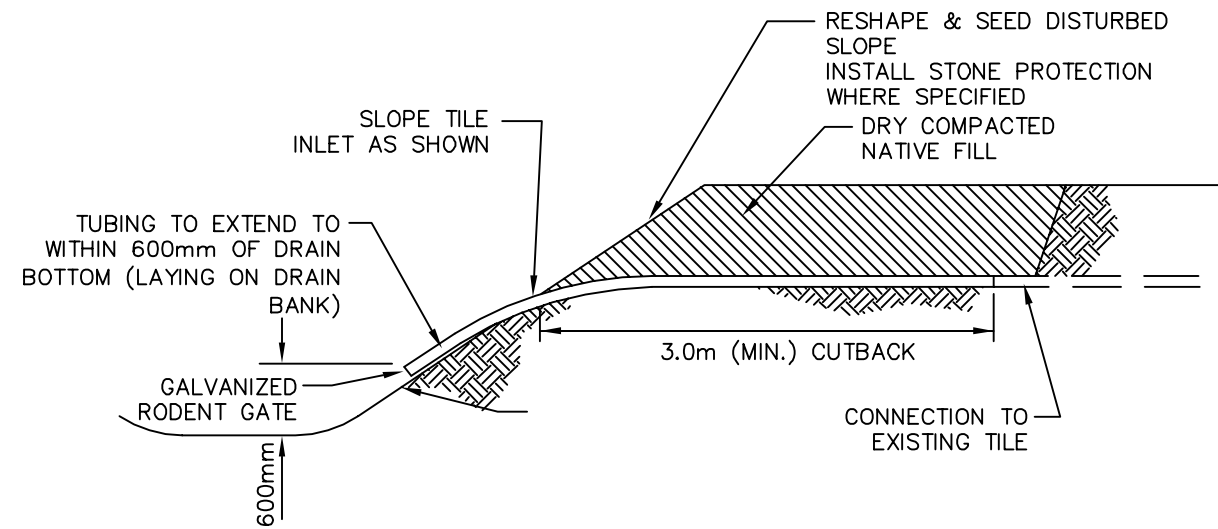
'SCHEDULE G'

Drainage Report for the

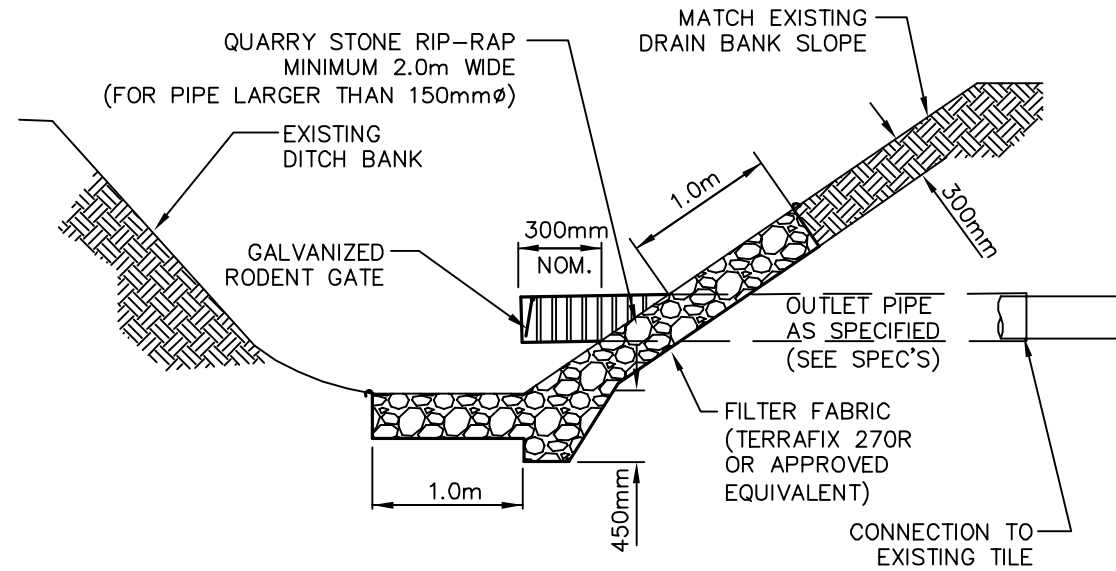
NEW DRAIN ALIGNMENT OF A PORTION  
OF THE LACHANCE DRAIN

CITY OF WINDSOR & TOWN OF TECUMSEH

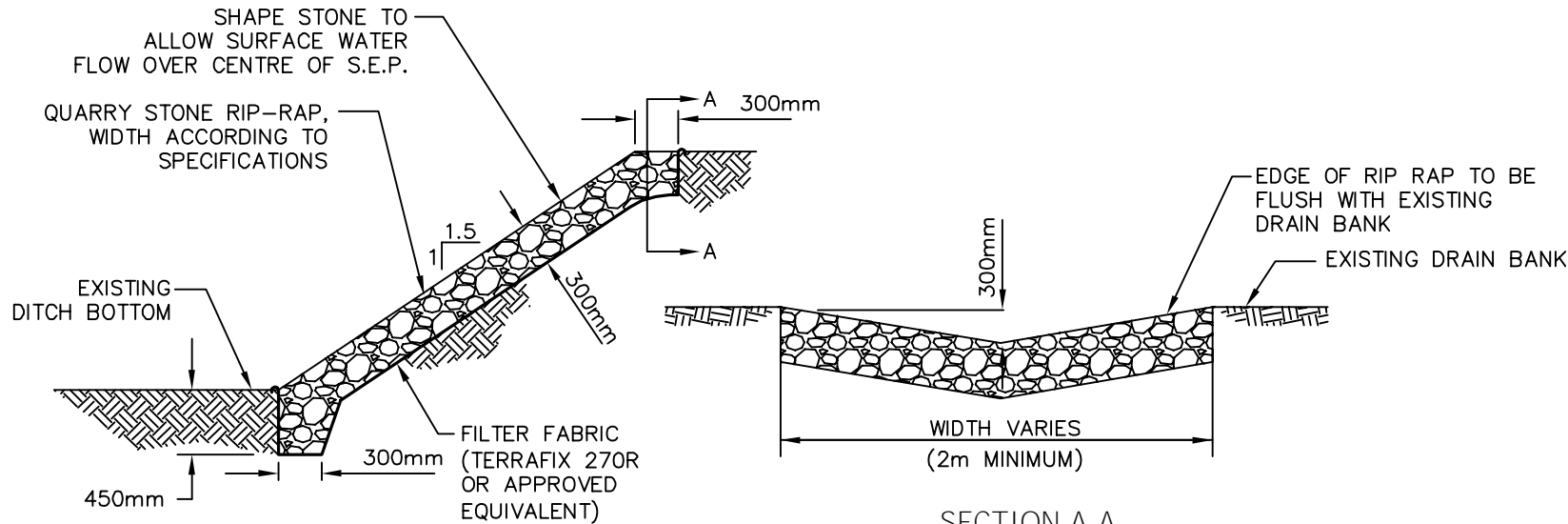
CROSS SECTIONS



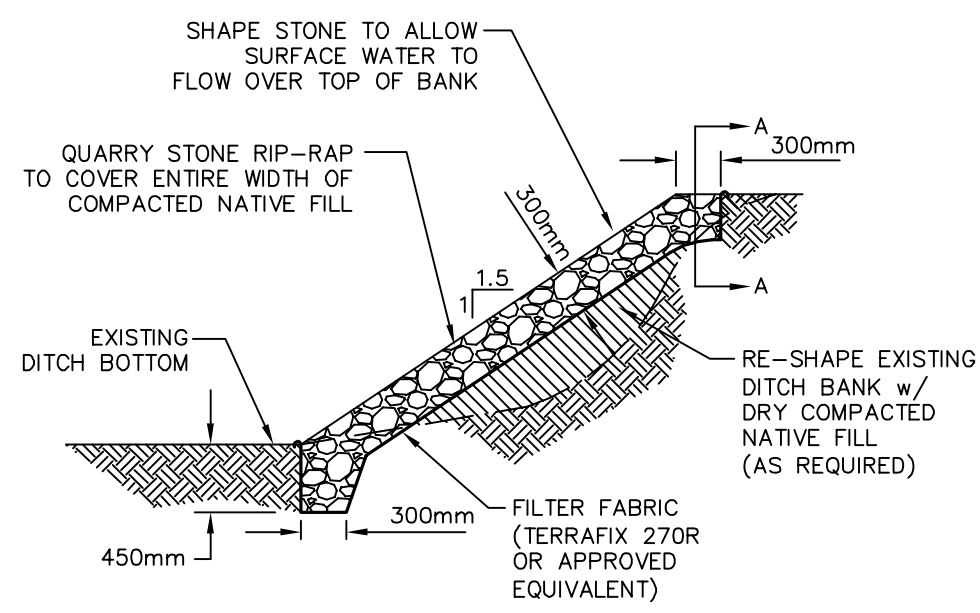
TYPICAL DRAINAGE TUBING TILE INLET  
REPLACEMENT DETAIL  
(FOR TILES 150mmØ OR SMALLER)  
NOT TO SCALE



TYPICAL CSP TILE INLET  
REPLACEMENT DETAIL  
NOT TO SCALE



TYPICAL DITCH BANK WASHOUT  
DETAIL w/ RIP-RAP  
NOT TO SCALE



TYPICAL DITCH BANK WASHOUT  
DETAIL w/ BACKFILLING & RIP-RAP  
NOT TO SCALE



Conditions of Use  
Verify elevations and/or dimensions on drawing prior to use. Report any discrepancies to Dillon Consulting Limited.  
Do not scale dimensions from drawing.  
Do not modify drawing, re-use it, or use it for purposes other than those intended at the time of its preparation without prior written permission from Dillon Consulting Limited.

1	FINAL REPORT SUBMISSION	APR. 12/22	MDH	
No.	Consolidated Council Agenda - June 13, 2022	DATE	BY	

DESIGN	MDH	REVIEWED BY	CDP
DRAWN	WLB	CHECKED BY	TRO
DATE	April 12, 2022		
SCALE	AS SHOWN		

**DILLON CONSULTING**

PROJECT NO. 22-3612

DRAWING SCALES BASED ON A 11" X 17" SHEET

'SCHEDULE G'  
Drainage Report for the  
**NEW DRAIN ALIGNMENT OF A PORTION OF THE LACHANCE DRAIN**  
City of Windsor & Town of Tecumseh

SHEET TITLE  
**MISCELLANEOUS DETAILS**

PAGE NO.  
5 of 5